

A G E N D A

REGULAR MEETING OF THE PLAN COMMISSION OF THE VILLAGE OF WILLOWBROOK TO BE HELD ON WEDNESDAY, February 3, 2021 AT 7:00 P.M. AT THE WILLOWBROOK POLICE DEPARTMENT, TRAINING ROOM, 7760 QUINCY STREET, WILLOWBROOK, ILLINOIS.

DUE TO THE COVID 19 PANDEMIC, THE VILLAGE WILL BE UTILIZING A CONFERENCE CALL FOR THIS MEETING.

THE PUBLIC CAN UTILIZE THE FOLLOWING CALL IN NUMBER:

Dial in Phone Number: 312-626-6799

Meeting ID: 870 3339 8604

Password: 604471

Written public comments can be submitted by no later than 6:00pm on February 3, 2021 to planner@willowbrook.il.us.

1. CALL TO ORDER
2. ROLL CALL
3. OMNIBUS VOTE AGENDA
 - A. Waive Reading of Minutes (APPROVE)
 - B. Minutes - January 13, 2021
4. PLAN COMMISSION CONSIDERATION: Continuation of Zoning Hearing Case 21-01: Consideration of a petition requesting approval of a special use permit for a fast food establishment and a special use permit for a drive-through in the B-2 Community Shopping District, including certain variations from Title 9 of the Village Code. The applicant seeks to demolish the existing gas/service station and construct a one-story building and drive-through with associated site improvements on the property. The Applicant is Hakim Yala of Panda Express, Inc., 1683 Walnut Grove Avenue, Rosemead CA 91770. The property owner is True North Energy, LLC, 10346 Brecksville Road, Brecksville OH 44141.
 - A. PUBLIC HEARING
 - B. DISCUSSION/RECOMMENDATION

5. PLAN COMMISSION CONSIDERATION: Zoning Hearing Case 21-03:
Consideration of a petition requesting approval of a special use permit for a planned unit development, including a financial institution with drive through, fast-food establishment with drive through, an automobile washing and cleaning facility, including certain relief, exceptions and variations from Title 9 and Title 10 of the Village Code; approval of a Preliminary Plat of Subdivision; and approval of a Preliminary Plat of PUD. The applicant for this petition is Alex Katz of G.W. Property Group LLC, 2211 N. Elston Avenue, Suite 304, Chicago IL 60614. The property owner is Viren-Gill Ltd., LLC, 735 Plainfield Road, Willowbrook IL 60527.
 - A. PUBLIC HEARING
 - B. DISCUSSION/RECOMMENDATION
6. VISITOR'S BUSINESS
7. COMMUNICATIONS
8. ADJOURNMENT

MINUTES OF THE REGULAR MEETING OF THE PLAN COMMISSION HELD ON WEDNESDAY, JANUARY 13, 2021 AT THE WILLOWBROOK POLICE DEPARTMENT, TRAINING ROOM, 7760 QUINCY STREET, WILLOWBROOK, ILLINOIS

DUE TO THE COVID19 PANDEMIC THE VILLAGE WILL BE UTILIZING A ZOOM CONFERENCE CALL FOR THIS MEETING

1. CALL TO ORDER

Chairman Kopp called the meeting to order at the hour of 7:13p.m.

2. ROLL CALL

Those physically present at VOW Police station were Chairman Kopp and Commissioner Soukup Present Via Zoom COVID -19 Pandemic were Commissioners Remkus, Kaczmarek, Kaucky, Walec, Vice Chairman Wagner and Building Official Roy Giuntoli Also, present Via Zoom were Planning Consultant Anne Choi and Lisa Shemroske from the Village Hall

3. OMNIBUS VOTE AGENDA

The items on the Omnibus Vote Agenda were as follows:

- A. Waive Reading of Minutes (APPROVE)
- B. Minutes – Special Meeting, December 16,2020

MOTION: Made by Commissioner Remkus seconded by Commissioner Soukups approve the Omnibus Vote Agenda as presented.

MOTION DECLARED CARRIED

4. PLAN COMMISSION CONSIDERATION: Zoning Hearing Case 21-01; This hearing will be continued due to an incomplete application. (Applicant: Hakim Yala of Panda Express, Inc.,1683 Walnut Grove Avenue, Rosemead CA 91770.The Property owner is True North Energy, LLC, 10346 Brecksville Road Brecksville OH 44141)

- A. PUBLIC HEARING
Opened at 7:15pm
- B. DISCUSSION/RECOMMENDATION

See Court Reporter Minutes for Discussion and Recommendation

MOTION: The following motion was to continue Zoning Hearing Case 21-01 till the February 3, 2021 Regular Plan Committee meeting made by Vice Chairman Wagner and Second by Commissioner Remkus, All in favor.

MOTION DECLARED CARRIED

5. PLAN COMMISSION CONSIDERATION: Zoning Hearing Case 21-02: Consideration of a petition to approve a variation from Section 9-5B-3(D)4 to reduce the rear yard setback from thirty feet (30') to twenty-five feet (25') and approval of a variation from Section 9-5B-3(G) to increase the maximum FAR from 0.03 to 0.39 to allow construction of a 1,081 square foot addition to house an indoor swimming pool, and other such relieve from Title 9 of the Village Code necessary.

A. PUBLIC HEARING

Closed public hearing at 8:03pm

B. DISCUSSION/RECOMMENDATION

See Court Reporter Minutes for Discussion and Recommendation.

MOTION: The Plan Commission does not recommend the petition presented for Zoning Hearing Case 21-02 was made by Commissioner Remkus and second my Vice Chairman Wagner.

Roll Call Vote: AYES: Commissioner Remkus, Soukup, Kaczmarek, Kaucky, Walec, Vice Chairman Wagner, and Chairman Kopp NAYS: None

6. VISTOR'S BUSINESS.

None

7. COMMUNICATIONS

Planner Choi informed Commissioners the two items that will be discussed at the February 3, 2021 Plan Commission meeting. One being for the Continued Zoning case hearing 21-01 second one for Willowbrook Bowl now known at 735 Plainfield road PUD. Hinsdale Commons drive thru will be dropping off an application and hopefully will be on Agenda for March 2021 meeting. Building Official Giuntoli permanent signal at Kingery and Plainfield should be on by the end of the month. Side entrance on Plainfield making progress.

8. ADJOURNMENT

MOTION: Made by Commissioner Remkus seconded by Vice Chairman Wagner to adjourn the meeting of the Plan Commission at the hour of 8:13 p.m. all in favor

UNANIMOUS VOICE VOTE
PRESENTED, READ, AND APPROVED,

MOTION DECLARED CARRIED

February 3, 2021

Chairman

1/13/2021

**VILLAGE OF WILLOWBROOK PROCEEDINGS BEFORE THE
PLANNING & ZONING COMMISSION OF THE VILLAGE OF
WILLOWBROOK**

ROBIN HEJNAR

VILLAGE OF WILLOWBROOK
PROCEEDINGS BEFORE THE
PLANNING & ZONING COMMISSION
OF THE VILLAGE OF WILLOWBROOK

WEDNESDAY, JANUARY 13, 2021

7:00 p.m.

RECORD OF THE PROCEEDINGS VIA ZOOM had at the meeting held before the Planning & Zoning Commission of Willowbrook, on Wednesday, the 13th day of January 2021, commencing at 7:00 p.m., as reported by Robin Hejnar, a Certified Shorthand Reporter and Registered Professional Reporter and Notary Public in and for the County of DuPage and State of Illinois.

1 APPEARANCES:

2

3 Daniel Kopp - Chairman

4 John Wagner - Vice-Chairman

5 Lisa Shemroske - Secretary

6 Commissioner Soukup - Member

7 Maciej Walec - Member

8 Catherine Kaczmarek - Member

9 Leonard Kaucky - Member

10 William Remkus - Member

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17 Staff Also Present:

18 Ann Choi - Planning Consultant

19 Roy Giuntoli - Building Official

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1 (Wherein, begins the Zoom-recorded audio
2 transcribed by Court Reporter.)

3 CHAIRMAN KOPP: Let's get started then.
4 This is the regular meeting of the Plan Commission of
5 the Village of Willowbrook. I ask -- I call this
6 meeting to order and ask the Plan Commission secretary
7 to call the role.

8 MS. SHEMROSKE: Commissioner Remkus?

9 MR. REMKUS: Here.

10 MS. SHEMROSKE: Commissioner Soukup?

11 MR. SOUKUP: Here.

12 MS. SHEMROSKE: Commissioner Kaczmarek?

13 MS. KACZMAREK: Here.

14 MS. SHEMROSKE: Commissioner Kaucky?

15 MS. KAUCKY: Here.

16 MS. SHEMROSKE: Commissioner Walec?

17 MR. WALEC: Here.

18 MS. SHEMROSKE: Vice Chairman Wagner?

19 MR. WAGNER: Here.

20 MS. SHEMROSKE: Chairman Kopp?

21 CHAIRMAN KOPP: Here.

22 MS. SHEMROSKE: Planner Ann Choi?

23 MS. CHOI: Here.

24 MS. SHEMROSKE: Building Official Roy

1 Giuntoli?

2 MR. GIUNTOLI: Present via Zoom.

3 MS. SHEMROSKE: And for the record, I'm Lisa
4 Shemroske.

5 CHAIRMAN KOPP: Next item on the agenda is
6 the omnibus vote agenda. Would any of the commissioners
7 like an item removed from the omnibus vote agenda; if
8 not, would someone make a motion to approve the omnibus
9 vote agenda?

10 MR. REMKUS: Remkus so moved.

11 CHAIRMAN KOPP: Soukup seconded here.

12 MS. SHEMROSKE: Thank you.

13 CHAIRMAN KOPP: So all in favor of the
14 omnibus vote agenda, please say, Aye.

15 ALL MEMBERS: Aye.

16 CHAIRMAN KOPP: Opposed, say Nay. So that
17 motion carries.

18 Next item, item four on the agenda, is
19 Zoning Hearing Case 21-01. The applicant for that is
20 Hakim Yala of Panda Express of Rosemead, California.
21 The property is -- the property owner is True North
22 Energy of Brecksville, Ohio.

23 Because of an incomplete application,
24 [reporter joins Zoom meeting live] we are going to

1 continue this hearing until the next regular hearing
2 on -- what date, Ann?

3 MS. CHOI: February 3rd, 2021. So I believe
4 we have to open up the hearing, continue it, and then
5 not close the hearing.

6 CHAIRMAN KOPP: Okay. So I am opening
7 Zoning Hearing Case 21-01. The applicant, as I said, is
8 Hakim Yala of Panda Express, of Rosemead, California,
9 and the property owner is True North Energy, LLC, of
10 Brecksville, Ohio.

11 What's the address of the property, Ann?

12 MS. CHOI: I'm sorry, it's 7505 Kingery. It
13 is right across the street from the Potbelly. So it's
14 that Shell Gas Station.

15 CHAIRMAN KOPP: Do you think we need to do
16 it by motion, or can I just continue it?

17 MS. CHOI: I think we do everything by
18 motion, so I would say --

19 CHAIRMAN KOPP: Okay. Will someone make a
20 motion to continue Zoning Hearing Case 21-01 to the next
21 regular meeting of the Plan Commission on February 3rd?

22 MR. WAGNER: Wagner, so moved.

23 MR. REMKUS: Remkus second.

24 CHAIRMAN KOPP: All in favor say, Aye.

1 ALL MEMBERS: Aye.

2 CHAIRMAN KOPP: Opposed, say Nay.

3 Okay. So that hearing is continued.

4 Next item on the agenda, No. 5, the purpose
5 is public hearing for Zoning Hearing Case No. 21-02.

6 The purpose of this public hearing shall be to consider
7 a petition for approval of a variation from Section
8 9-5B-3(D)4 to reduce the rear yard setback from 30 feet
9 to 25 feet, and approval of a variation from Section
10 9-5B-3(G) to increase the maximum floor area ratio from
11 0.30 to 0.39 to allow construction of a 1,081 square
12 foot addition to house an indoor pool, and such other
13 relief from Title 9 of the Village Code as necessary,
14 and the property with and address of 6401 Meadow Lane,
15 Willowbrook, Illinois.

16 The applicant for this petition is Pete
17 Baftiri, and the property owner is Jain Bhagwan of 5S541
18 Radcliffe Road, Naperville, Illinois 60563. A copy of
19 this notice was published in the December 24, 2020,
20 Edition of the Doings Newspaper.

21 Ann, would you like to make your report?

22 MS. CHOI: Yes. Thank you, Chairman; and
23 good evening, Commissioners.

24 The petitioner, Pete Baftiri, of A&E Luxury

1 Homes, on behalf of the property owners at 6401 Meadow
2 Lane, the subject property of this petition are
3 requesting the following:

4 For the construction of a 26-and-a-half foot
5 by 42-foot, 10-inch addition off the rear exterior wall
6 of the existing residence. The addition is proposed to
7 a maximum height of 21 feet, and would house an indoor
8 swimming pool for therapeutic purposes to serve its
9 elderly residents.

10 The construction of the addition of the size
11 desired would encroach, approximately, 5.54 feet into
12 the required rear yard setback of the property, and
13 increase the allowable floor area. As such, the
14 applicant has applied for the following two variations.

15 Again, as you stated, approval of a
16 variation from Section 9-5B-3(D)4 to reduce the rear
17 yard setback from 30 feet to 24.46 feet. The public
18 hearing notice actually stated 25 feet, but it is
19 actually 24.46 feet. I just wanted to highlight that.

20 And the second variation is approval of a
21 variation from Section 9-5B-3(G) to increase the maximum
22 FAR from 0.30 to 0.39 to allow for the construction of a
23 1,081 square foot addition.

24 The subject property is zoned R2

1 single-family residence, and are adjacent to properties
2 also in the R2 single-family residence to the north,
3 south, east and west.

4 This application is subject to review by the
5 Plan Commission regarding the requested zoning relief,
6 and some background, this parcel was platted back in
7 1978 as part of the Gallagher and Henry Waterford Unit
8 No. 2 Subdivision.

9 The existing single-family resident
10 currently -- residents currently conform to the minimum
11 requirements of the zoning ordinance. However, the
12 proposed addition would add just under 1,100 square feet
13 of floor area, thereby increasing that FAR by
14 130 percent. The required rear yard setback is
15 currently 30 feet, and the proposed addition would
16 encroach into this area by 5-and-a-half feet.

17 It should also be noted that the current lot
18 depth does not conform to the current zoning regulations
19 since this subdivision was platted in 1978. If the
20 subject property was platted to today's standards, the
21 variation request for reduced rear yard setback would
22 likely be unnecessary. However, the variation request
23 for increased FAR would still be necessary.

24 Staff included the following recommended

1 conditions for this petition:

2 The applicant shall revise the site plan
3 included as attachment five of the staff report to
4 reflect the correct variation to reduce the rear yard
5 from 25 feet to 24.46 feet. I believe this has already
6 been revised.

7 The variations granted shall only apply to
8 the proposed addition to accommodate an indoor swimming
9 pool. The proposed addition shall not be used as
10 habitable space or for non-recreational purposes,
11 including, but not limited to bedrooms, living rooms,
12 kitchens, et cetera.

13 The variations shall be null and void of
14 construction if the proposed use is not commenced, and a
15 certificate of completion is not granted within
16 12 months of the date of any approval of the variation
17 by the Village Board.

18 I did want to note that I received three
19 letters -- actually, two letters and an email from
20 neighbors, which the Plan Commission have now received
21 by email, at least two of them. These letters include
22 16 signatures in opposition to the petition; and I can
23 read these letters now, to include as part of the
24 transcript or during public comment, whichever you

1 prefer, Chairman Kopp; and I just want to say that these
2 letters highlight the concerns over existing water
3 drainage problems in the area that might be exacerbated
4 by increased impervious area.

5 CHAIRMAN KOPP: Why don't we -- why don't
6 you go ahead and read the letters now.

7 MS. CHOI: Okay. They're quite lengthy, so
8 bear with me.

9 "To the Plan Commission of the Village of
10 Willowbrook:

11 We, the adjacent neighboring property owners
12 of the subject property offer the following concerns for
13 consideration in review of proposal of the following
14 Case No. PC21-02.

15 A petition for approval of a variation from
16 Section 9-5B-3(D)4 to reduce the rear yard setback from
17 30 feet to 25 feet," which is now 24.46 feet, "and
18 approval of a variation from Section 9-5B-3(G) to
19 increase the maximum FAR from 0.30 to 0.39 to allow
20 construction of a 1,081 square foot addition to house an
21 indoor swimming pool at the property located at 6401
22 Meadow Lane.

23 The design plans prepared by Agama Design
24 Architecture do not adequately depict or address how

1 storm water will be controlled, or the potential impact
2 to neighboring properties. The impervious area of the
3 rear yard will be greatly reduced, impacting the natural
4 drainage flow. Drainage flow is a very real concern of
5 the adjacent property owners.

6 The plans depict graphically what seems to
7 be a gutter with no vertical leaders indicated on the
8 pitched gable sides of the elevations of the proposed
9 addition. Without indication of where the storm water
10 is running, we assume the storm water will sheet flow on
11 the existing grading, thereby increasing the flow rate
12 of the storm water on the existing grading.

13 In review of the proposed engineering plans,
14 the notation that all existing grading is to remain can
15 not be accurate. The proposed addition crosses the site
16 contour line of spot elevation 719.54 feet to
17 716.51 feet in the northeastern corner of the site.
18 Northeastern corner of the site is the lowest point of
19 the site, and holds the most storm water runoff
20 currently.

21 As indicated on the engineering plans, a
22 storm water manhole is located in the southeast corner
23 of the site. However, all storm water runoff is being
24 directed to the northeast corner. Although the proposed

1 addition shows the grade to be flat, this is not
2 accurate, nor are the engineering plans and design plans
3 coordinated to provide same information.

4 We request that consideration of this
5 petition be continued to allow adequate time for the
6 applicant/property owner to properly study and then
7 present to the Plan Commission the impact of the
8 proposed variation upon storm water drainage and what
9 types of remedial measures, if any, might adequately
10 prevent or mitigate the issue.

11 In terms of whether the proposed variation
12 will alleviate some demonstratable and unusual hardship
13 posed by the COVID-19 outbreak, we suggest that the Plan
14 Commission take a fresh look at this factor after a
15 study of the storm water drainage issue. The hardships
16 and restrictions associated by the outbreak may be
17 alleviated over the first half of 2021 as a vaccination
18 program continues to ramp. Health clubs may reopen
19 prior to completion of construction, if not prior to
20 commencement.

21 Finally, as part of the proposal, we would
22 like to see a site logistics plan to ensure the limits
23 of construction are held off the adjacent properties.
24 The existing vegetation along the property lines is

1 requested to be protected from any and all site
2 disturbances.

3 We also object to the variation to increase
4 the maximum FAR to 0.30 to 0.39. We believe the Village
5 of Willowbrook intended for the maximum FAR to align
6 with open space and green space ideals of suburban
7 community desires.

8 We hope you will take our concerns into
9 consideration, and make the decision to disapprove for
10 any request for variance or building that may cause harm
11 to our properties. Thank you."

12 I believe this was from Kevin Webb, who's a
13 resident to the rear.

14 MR. WEBB: That is correct.

15 MS. CHOI: I have a second letter from Jan
16 and Carol Miecznikowski. I'm sorry if I'm butchering
17 the last name. They live at 6420 Tremont Street.

18 And this letter reads:

19 "We are a neighboring property owner of the
20 subject property offering the following two concerns for
21 consideration in review of the Case No. PC21-02.

22 Our first concern relates to the drainage of
23 water in the area. Having recently made an investment
24 to finally solve a persistent drainage problem on our

1 property, we are concerned that the proposed
2 construction would cause the problems to return.
3 Therefore, we are not in favor of granting any
4 exceptions to the ordinances unless assurances can be
5 given that the planned construction will not cause any
6 drainage issues on neighboring properties.

7 Our second concern is with the precedence
8 that would be set by the proposed changes to the setback
9 in FAR. Although, seemingly a single home on a single
10 street, such an accommodation could become precedent,
11 and, therefore, a slippery slope, which would transform
12 the look and feel of the neighborhood incrementally
13 without a broader hearing of the matter, and without an
14 exclusive decision on the part of the Village.

15 Thank you in advance for considering our
16 inputs. Best regards, Jan and Carol Miecznikowski."

17 And then the third I just received via email
18 at 6:00 p.m., and this resident is Rob Goodridge at 6354
19 Meadow Lane; and he writes:

20 "Dear Mr. Pabst, we would prefer to maintain
21 the rear yard setback to remain at 30 feet and the
22 maximum FAR of 0.30. Thank you."

23 And that was it.

24 CHAIRMAN KOPP: If you're done, Ann, we'll

1 allow the applicant to make his presentation.

2 MR. BAFTIRI: Good evening, everyone. My
3 name is Ardi Baftiri. I am here with Pete Baftiri, A&E
4 Luxury Homes. Thank you, everyone, for hearing our
5 petition.

6 CHAIRMAN KOPP: I'm sorry to interrupt you.
7 You will need to be sworn in.

8 (Whereupon, Ardi Baftiri is duly
9 sworn.)

10 MR. BAFTIRI: Again, thanks, everyone; and
11 thank you, Ann, for your help and guidance to this
12 point.

13 So thank you for also reading those concerns
14 from the neighbors, understandable, and we want to point
15 out some of the key points from our proposal.

16 We believe that this is a neighborhood in
17 transition. There's a lot of new construction homes,
18 some remodeling. We think that this remodel to the
19 house will add value to the neighborhood, and as it's
20 revitalizing one of the older houses in that
21 neighborhood, I just want to point out there's also new
22 windows --

23 (Phone rings.)

24 MR. BAFTIRI: So the remodel has --

1 THE REPORTER: Anil, if you can mute
2 yourself. Continue.

3 MR. BAFTIRI: So the remodel also consists
4 of new windows, new tile roof, new stucco, and just
5 high-quality construction generally.

6 Overall, we think it's going to increase the
7 value of the surroundings properties and for the
8 neighborhood. The purpose of the addition is because
9 it's for therapeutic purposes. The residents are
10 elderly, and it is for therapy, because of the COVID
11 pandemic, they are not able to go to other public
12 facilities, and, so, this is kind of the only reasonable
13 alternative given the risks to their health.

14 And regarding the points that were noted
15 from the neighbors, the rear yard setback would, I
16 believe, to my understanding, would not be required if
17 the house was built to date, but regardless still, it's
18 a very minor five-foot setback. The vegetation would be
19 preserved and protected to the best of our abilities,
20 and most likely we'll be adding vegetation there,
21 screening and privacy for all the neighbors, including
22 the residents.

23 As for the drainage issues, our engineers
24 are very experienced and they will comply with any

1 drainage requirements that the Village requests, if
2 approved here, during the review process with Village
3 staff afterwards, and, so, we don't believe that
4 drainage will be an issue given the design proposed.

5 And I'll pass it here to Pete Baftiri for
6 any additional comments.

7 MR. P. BAFTIRI: I just want --

8 CHAIRMAN KOPP: Mr. Baftiri will need to be
9 sworn in.

10 (Whereupon, Pete Baftiri is duly
11 sworn.)

12 MR. P. BAFTIRI: Thank you, everyone, again.

13 So I just wanted to add an additional
14 statement for the project, that the remodel that's
15 already done, it's very high-end remodeling. The owners
16 spent a lot of money, and the house was completely brand
17 new; and by adding pool to this property, we think that
18 this will add value to the neighborhood. It's not going
19 to hurt the neighborhood. Actually, the neighborhood
20 will benefit a lot by having a brand new house remodeled
21 with the new standards, with today's standards, and
22 having a pool on this neighborhood.

23 This neighborhood is changing. I've been a
24 builder for 23 years, and I've seen changes in different

1 neighborhoods, Willowbrook -- I lived in Willowbrook and
2 all western suburbs.

3 So this neighborhood kind of is in
4 transition with some old homes. I have clients who want
5 to tear it down, build new ones. It's very good
6 location. Some of them are between doing the remodel.
7 So in the future, I think this subdivision will change;
8 and by initiating this change, in FAR, by asking for
9 this variation by adding a pool to this property, I
10 think it's going to add a lot of value to the
11 neighborhood.

12 For the drainage, neighbors are right, they
13 should be concerned, but one thing, I can promise that
14 our engineers, civil engineers will address every issue,
15 and we'll comply with all the requirements Village
16 requires, without affecting the neighborhood.

17 And the other thing is the way how it's
18 designed, and I know that the neighbor next door on the
19 north side -- it's going to be next to the property on
20 the north side, actually, that neighbor will get more
21 privacy from these neighbors. By adding the pool on the
22 back, they'll have really good privacy; and the pool
23 house, it won't be that tall, if you see on the
24 blueprint.

1 On the rear backyard, only 5.4 -- like 5.6
2 feet, I think we ask for variance; and that one --
3 again, there is a lot of vegetation, existing one; and I
4 did talk to the owner, they're going to do, also,
5 landscaping changes. They're going to add a lot of
6 vegetation to beautify this lot.

7 If you have any additional questions, let me
8 know.

9 CHAIRMAN KOPP: Do any of the commissioners
10 have any questions for either Ann or for the applicant?
11 I'll get started.

12 So, Ann, the letter from the Village's
13 engineer about drainage, he just said that it's not of
14 a -- if I'm reading this correctly, that it's not of the
15 size that -- he doesn't make an opinion about whether
16 this is going to affect the drainage.

17 MS. CHOI: Well -- so I followed up with him
18 after I received this email, and he wanted to clarify a
19 few things.

20 So he said that the grading plan that was
21 referenced in the first letter is not a plan for the
22 currently proposed project. I believe that maybe Agama
23 Design Architecture, they presumably took a plan from a
24 2016 submittal. I think back in 2016 there was an

1 application with the Building Department for an addition
2 to the home, which was never built. So they likely took
3 that 2016 submittal, and superimposed a proposed
4 addition on that plan.

5 In our engineer's letter, he stated that, at
6 the time of building permit, they will need a grading
7 plan, if the variations are approved; and based on what
8 was submitted, it was difficult for him to make more
9 than general comments on what the code requires.

10 However, he will definitely take into
11 consideration that there are existing drainage problems
12 at the northeast corner of the site because he wasn't
13 aware of that prior.

14 CHAIRMAN KOPP: And my other question to you
15 has to go with FAR; and I don't know that you can answer
16 this, because we have this aerial, your Exhibit 2, and
17 some of these houses, at least, based on lot coverage,
18 looked like they're probably much larger houses than
19 this house, even this house with the addition.

20 MS. CHOI: Yes, you are correct.

21 So I actually went on Google Street View to
22 look at some of those homes. I believe the ones that
23 look like they -- that are more L-shaped, and they have
24 an extension towards the back, they're actually

1 one-story, if I'm not mistaken.

2 I was looking for older, perhaps, zoning
3 cases that might have approved variations for FAR. I
4 couldn't find any in this neighborhood, so I believe
5 they're probably complying with current FAR, because
6 they're 1-to-1-and-a-half stories.

7 CHAIRMAN KOPP: Okay. That's it for my
8 questions.

9 Any other commissioners have questions of
10 Ann or the applicant?

11 MR. REMKUS: One question I had was that,
12 one of the things -- that we're doing this, wanting a
13 variation because of the COVID. I think -- myself, I
14 don't want to get into granting variations because of
15 COVID. I think that's a really slippery slope for us to
16 go down.

17 This is something -- it's a pandemic, it's
18 worldwide, but it's not going to be here forever. When
19 we grant a variation to do something, the variation we
20 grant is going to be there forever, so I'm a little
21 worried about that.

22 MR. WAGNER: Commissioner Wagner. I guess I
23 would point out a couple things, or ask a question.

24 This subdivision, when it was developed, was

1 developed within the guidelines of the Village of
2 Willowbrook, I believe, and it may not meet the current
3 zoning standard, which typically changes over time. I
4 don't think it was developed in the county. I think
5 because that and Rogers Farm was also developed at a
6 time when it was a subdivision to Willowbrook, which
7 this backs up to. That's No. 1.

8 No. 2, I think, to some degree, the hearing
9 may be moot because the notice is incorrect, and I
10 believe -- the request is for greater than what was
11 published, even though it's minor, I would question it.

12 The third thing is, these standards for
13 variation -- I'd have a hard time with the -- many of
14 the things that have been brought up within that; and,
15 No. 1, that the property in question can't have a
16 reasonable return without this variation being granted.
17 I question that.

18 And I would agree that the reasoning for the
19 COVID is not something that should be recognized,
20 because it's something that we're dealing with, but
21 isn't a long-term situation that looks like it would be
22 running with the property as the variation does. So I
23 would raise those questions. Thank you.

24 CHAIRMAN KOPP: Anything else from -- any

1 questions from any other commissioners?

2 MS. KACZMAREK: I have a question for the
3 applicant.

4 You mentioned that this is going to add
5 value to the property. I'm just curious, do you have a
6 Realtor, or anyone that you reached out to just to see
7 if at all this would provide value to the property in
8 the long run?

9 MR. P. BAFTIRI: So when I said it's going
10 to add value to the property -- the existing property is
11 proposing -- this is old house, but, also, I said it's
12 going to add value to the neighborhood by having the
13 house with a pool, high-end -- high finishes, it's going
14 to be a positive impact to the neighborhood and to
15 property itself.

16 So based on my experience -- and, also, I'm
17 real estate agent -- yeah, definitely when we get these
18 kind of homes, and nice neighborhood, this location is
19 very preferred location in Willowbrook, very good
20 schools.

21 When they see people in these kind of
22 homes -- because that's true, right now we have COVID,
23 but in the future it will be gone, with the vaccines and
24 all that, but, still, adding this pool to this house and

1 to this neighborhood is going to help and increase the
2 value of old properties, not just for this house.

3 MS. KACZMAREK: Ann, in the neighborhood, do
4 you know offhand if we have other properties that have
5 indoor pools?

6 MS. CHOI: I'm not that familiar with the
7 Willowbrook neighborhood and property. I don't know if
8 Roy Giuntoli knows more.

9 MR. GIUNTOLI: This is Roy Giuntoli,
10 building official.

11 None that come right off the top of my head
12 on the inside in this neighborhood. I know of one in a
13 different neighborhood, but not within Waterford. It's
14 nearby, it's on the other side of Madison Street, but
15 it's not in Waterford.

16 And then I know another one in town that
17 really isn't a swimming pool, it's more of a -- one of
18 those type of pools that you swim in place in. There's
19 a name for it, and I'm sorry I'm drawing a blank for it.

20 MS. KACZMAREK: I think I know what you're
21 saying, yeah.

22 MR. GIUNTOLI: And that's in another
23 neighborhood, a little bit further to the east, but none
24 come to mind -- over the last 15 years -- of being

1 permitted in this subdivision. Of course, the map shows
2 some outdoor pools, and there's been a couple here and
3 there, new ones over the years in Waterford, but as far
4 as inside, I can't recall anything off the top of my
5 head.

6 MS. KACZMAREK: Okay. Thank you.

7 MR. GIUNTOLI: You're welcome. Thank you.

8 CHAIRMAN KOPP: If the commissioners are
9 done, do any members of the public have any questions or
10 comments?

11 All right, go ahead, Mr. Webb.

12 (Whereupon, Kevin Webb is duly sworn.)

13 MR. WEBB: So I'm one of the authors of the
14 one letter that you received an objection, and that is
15 the rather -- kind of longer one there, that is dated
16 January 12th.

17 If you can see attached to it is a signature
18 page. I collected all those signatures yesterday
19 afternoon. So within an approximate three-hour period,
20 I was able to get -- what do we got there? I think it
21 was 15 different households that all surround this
22 particular property that are objecting.

23 So in regards to some of the things that the
24 applicant is saying, that this will build value in the

1 neighborhood, well, you've got, just from yesterday,
2 16 -- well, 16 property owners, the other one that Ann
3 pointed out -- that would object to that. I don't see
4 where this would be adding value to my property by
5 having a house that's got an indoor pool, which that is
6 a very extremely unique type of thing, as far as I would
7 believe, that people would want when buying a house.
8 Because if you've ever walked into an indoor pool, and
9 you walk in that front door of that natatorium, what's
10 it smell like? Chlorine. So unless they're okay with
11 having their entire house smell like chlorine, an indoor
12 pool is something that I would never ever desire to
13 have.

14 As far as all the work that they're doing to
15 the exterior, that, again, is just of their opinion,
16 that it's adding value to the neighborhood. If you,
17 Commissioners, ever have a chance, please drive by and
18 look at what they're doing to the house. Again, that
19 is -- as far as I believe, and so do all the other ones
20 that I've spoke with, this is a personal preference of
21 the design to the exterior -- to the house.

22 Now, we can't object to how they want to do
23 the exterior of their house; that's their choice, but
24 from my own personal standpoint, I don't think that that

1 exterior of the house is adding any value between the
2 roof and the pink walls.

3 As far as our objection to the FAR -- so
4 when that was put in place, that was done for a reason,
5 and that was to leave open space within this community.

6 They're talking -- the applicant's talking
7 about a transition of our community. I'm not too
8 sure -- I don't know of any transition. People can put
9 on new sidings on their home, and that's not a
10 transition. New roofs need to be put on if these houses
11 were built in the 1970's. They've reached their maximum
12 life, and then some, and need to be replaced, but as far
13 as the community transitioning, again, that is a
14 personal opinion that is just exclusive to them, and not
15 any of the 15 individuals that I spoke with yesterday;
16 and if I needed to, I could probably, within a week, if
17 not quadruple the number of people that I have on this
18 list, get many, many more than that by just going around
19 the neighborhood and finding out people's opinion on
20 that.

21 So I feel that, when we start letting just
22 one individual build more on their land in this aspect,
23 then what's to keep -- how can you deny the next one and
24 then the next one? And before we know it, we look like

1 Clarendon Hills, or we look like Hinsdale, that are on
2 top of each other.

3 Me, personally, I like the open space. I
4 want that extra 30 feet -- even though it's the
5 neighbor's yard, I want to have a nice distance between
6 us just for that open feeling, okay?

7 I don't have any questions for them.

8 One other concern would just be as far as
9 the drainage. I want to say it was three years ago that
10 there was major work done on a storm drainage pipe in
11 the -- along the property line there. That did a little
12 bit of mediation to the flooding problem that all of us
13 are experiencing, but as stated by the Miecznikowskis,
14 they had to do additional work.

15 I know Fred Stein, who's on the call, he did
16 major work in his backyard to put in tiles, I believe it
17 was, but he can attest to that. My neighbor, who's
18 directly behind them has done a lot of work to help with
19 the flooding, but it still floods; and the corner, my
20 northwest corner, which is adjacent to this property, it
21 floods, and there's a storm drain right there.

22 So as far as mitigation of that, we have a
23 lot of concern when you're going to put up a nearly
24 1,100 square foot structure of concrete, then you're

1 going to pour an in-ground pool -- I don't know how many
2 yards of concrete that's going to be or how far down
3 it's gonna, but another thing that people need to
4 realize is, we're high on the water table here.

5 My sump pump, even when it's not raining, it
6 runs. When it runs, it runs constantly. Why? I have
7 two. I got two sump pumps to suck that water out of
8 here; and if you're going to displace a permeable area
9 and drop all that concrete, then where's that water
10 going to go? Into our yards and the potential to our
11 basements. So that's an objection I have in regards to
12 this type of work.

13 I hope you all take this into consideration,
14 and thank you for your time.

15 CHAIRMAN KOPP: Mr. Webb, is your address
16 6410 Tremont?

17 MR. WEBB: That's correct, sir.

18 CHAIRMAN KOPP: Okay. Anyone else? So we
19 have three callers. Since I can't see your hands --
20 Mr. Stein?

21 THE REPORTER: I'm sorry, I didn't
22 understand that.

23 CHAIRMAN KOPP: Mr. Stein's going to speak;
24 and, Robin, you can swear him in.

1 (Whereupon, Fred Stein is duly sworn.)

2 MR. STEIN: Thanks for the opportunity. As
3 mentioned, I am one of the neighboring homeowners. I'm
4 at 6350 Tremont Street. I've been here for 16 years. I
5 just want to give a little bit of color to some of the
6 water issues.

7 So my property touches that corner that's
8 been noted -- that northeast corner of the applicant's
9 property, the low point. I mean, I'll tell you that
10 during heavy rain storms; and sometimes not so heavy,
11 you can actually see the water flow coming from the
12 yards to mine, and I've seen it over the 16 years. It
13 got to be such a problem, frankly, that it almost looked
14 like an irrigation ditch running through the middle of
15 my yard, towards the well at the far corner from the
16 applicant's property.

17 Grass wouldn't grow, and water would stand
18 there; and I mean, for a persistently long time, more
19 than a week, more than ten days sometimes. Ducks would
20 land in it. Even before I fixed it a year ago, ducks
21 would land in there, and squawk around. That's how bad
22 it was.

23 And this last year I finally resolved to do
24 something about it. I got a quote. They wanted to

1 charge me \$910,000 to put down a hundred feet of drain
2 tile, to do some other things. I personally didn't want
3 to spend that money; and seeing that I was home every
4 day, working there with COVID, and wanted to get some
5 exercise, I resolved to do it myself.

6 Neighbors being good neighbors around me --
7 frankly, everyone on this call that signed that letter
8 stopped by to see what I was doing, and neighbors on
9 each side helped me dig the trench for the hundred feet.
10 It was a true community effort. Matter of fact, all the
11 neighbors from the adjacent lots came over and spent
12 time as I was digging; I'll say everyone except for the
13 neighbor at 6401, the applicant's property.

14 I never met them. I'm not sure they're
15 truly aware of the issue being caused by the runoff, but
16 there's been substantial time and effort put in to
17 resolving the problem. I believe it is resolved. I've
18 had no further problems after that. We did a lot of
19 work. I put a rain guard in as well, doing everything I
20 could to help soak up that water. I'd hate to see all
21 that hard effort by all the neighbors go to waste; and,
22 so, I really do think it's incumbent upon the applicant,
23 prior to any building permit, or anything else moving
24 forward, to take a really hard look at this.

1 This has been a long time issue here; and
2 what they're asking for, indoor pool -- again, I've been
3 in this neighborhood 16 years, and been in these close
4 western suburbs for 55 of my 56 years. There isn't any
5 indoor pools around here, and it is somewhat of an
6 extraordinary request. I was very surprised when I saw
7 that someone was willing to sink that money -- and I
8 mean sink, sink that money into that kind of a
9 structure.

10 Thanks for your time and consideration.

11 CHAIRMAN KOPP: Either Mr. Pennino or any of
12 the Zemans -- I can't tell if you wanted to speak to
13 this matter. You don't have to, but if you want to.

14 MR. ZEMAN: This is Jerry Zeman.

15 (Whereupon, Jerry Zeman is duly
16 sworn.)

17 MR. ZEMAN: First off, I would just like to
18 state that I did sign Mr. Webb's petition, and I concur
19 with both -- my wife and I both concur with both
20 Mr. Webb -- I'm sorry, I didn't get the last name,
21 Mr. Stein's notation of the problems that they're
22 having, and I would like to comment relative to the
23 water.

24 I live immediately across the street from

1 the property at question, at 6404 Meadow Lane, and at my
2 property, my sump pump, in times of heavy rain, also
3 runs almost constantly, to the point where, when I did
4 have a sump pump failure, I did get water in my
5 basement's addition, and we've had to put in a
6 water-energized sump pump to deal with that issue should
7 our sump pump fail. So we have an extremely high water
8 table.

9 In addition, we do get some seepage during
10 heavy rains already, which is why we had -- when we put
11 our addition on, and increased the size of our basement
12 at the same time, we had put in additional weeping tile
13 per code to deal with all of this additional water; and
14 I actually have to have a piece of null board at the
15 bottom of my basement going down to the weeping tile, so
16 if I do get any seepage coming down the basement walls,
17 it goes right into the weeping tile and goes into the
18 sump.

19 We have been a resident of this neighborhood
20 now for 32 years. We did an addition on our house in
21 2009, which was a code-compliant addition, to the point
22 where we went so far as to ensure that we matched the
23 brick on the side of our house, on the 300-square foot
24 addition that we did. We used the same color of roofing

1 tile so that the property looked like it was the
2 original build of the structure. So it's already been
3 expressed on this property, that people around here do
4 not recognize the improvements that have been done to
5 the property as being, let's say, consistent with the
6 look of the surrounding homes, and we don't understand
7 the assertion that this is going to improve the value of
8 our properties.

9 Thank you very much for the opportunity to
10 speak, and look forward to your favorable consideration
11 from the petitions that you've received.

12 CHAIRMAN KOPP: All right. Anyone else?

13 Okay. Do the commissioners have any final
14 questions of Ann or the applicant?

15 MR. PENNINO: Hello?

16 THE REPORTER: Mr. Martin?

17 CHAIRMAN KOPP: I'm sorry, did you want to
18 speak Mr. Pennino?

19 MR. PENNINO: Yes. Just to confirm --

20 THE REPORTER: Before you continue, I'll
21 swear you in. Can you raise your right hand?

22 (Whereupon, Martin Pennino is duly
23 sworn.)

24 MR. PENNINO: I live a little bit further

1 away from the house -- not too far, on Hiddenbrook, that
2 backs up to Creekside Park, and I just want to confer,
3 that, yes, whenever it rains heavily, ours -- we have
4 two sump pumps and they're constantly going on and off
5 all night long, all day long. There definitely is water
6 issues in this area. Even though we're high above the
7 Creekside Park, there's still a lot of water issues.
8 I've been a resident for 18 years, and pretty much -- we
9 had to get two new sump pumps recently because they just
10 ran out from being used so much.

11 One other question I had is, the size of the
12 pool sounds pretty large. If this is really a
13 therapeutic pool, why wouldn't a smaller therapeutic
14 pool be used, one that would just be a little bit bigger
15 than a human body or something like that? I don't know
16 why they need so much space for a pool. It sounds more
17 like a multiple use than just therapy.

18 I think that's all I have right now. That's
19 it. Thank you.

20 CHAIRMAN KOPP: Okay. Was one of the
21 commissioners starting to ask a question or say
22 something?

23 All right. If not, the applicant, you have
24 the opportunity to have the last word before we close

1 this public hearing.

2 MR. P. BAFTIRI: Thank you again.

3 So all these comments -- the neighbors --
4 they make, they're very valid, especially for this
5 drainage; and I'm doing something every day, in running
6 into these issues, but I want, again, to confirm that,
7 whatever we do is Village requirement. Design -- we
8 meet all the requirements, we can comply with all
9 requirements, because we know the water, it's a problem.

10 There are multiple options now to address
11 that. We can do dry well, or we could do other testing
12 to see what's the absorption in the area. So I know
13 we're going to meet this requirement, and not affect the
14 neighbors.

15 And with regard to the FAR -- I'm looking at
16 the aerial pictures that is in Exhibit 2 -- all three
17 neighbors, north, south and west, they have bigger homes
18 than this neighbor, higher FAR. So what they're asking
19 to meet comes to the same size house. All they have a
20 protrusion, except like my client is asking. If you
21 look at Exhibit 2, I think it's going to be -- after
22 this pool house, it's going to look almost similar to
23 the existing homes and all three neighbors next to this
24 property.

1 That's all I have to add. Thank you.

2 CHAIRMAN KOPP: All right. If that's it, we
3 will close the public hearing for Zoning Hearing Case
4 21-01 [sic], and then the commissioners will have their
5 discussion, and then we will vote whether to recommend
6 this matter to the Village trustees.

7 For myself, when I first read this
8 application and looked at the materials, I assumed that
9 this was -- that I was going to approve this just based
10 on what was presented. However, I look at a zoning
11 ordinance as if it's almost a contract between people,
12 so that when people buy a property, they can rely on the
13 fact that the zoning ordinance is going to somewhat
14 protect their neighborhood. As Mr. Webb said, you can't
15 stop someone from painting their house purple or doing
16 something like that, but you have some expectations
17 based on the zoning ordinance; and the fact that the
18 neighbors -- all of the immediate neighbors are opposed
19 to granting a variance here means that, for me, I am not
20 going to vote in favor of this; and for me, it's purely
21 because of the zoning ordinance. It's not the
22 drainage -- not that the drainage is not an important
23 issue. I deal with it. On the Plan Commission here, we
24 constantly hear about drainage, because that's just

1 endemic to Willowbrook, but we have to trust that
2 engineers can solve that issue, but I am not in favor of
3 the increased FAR or the rear yard reduction variation
4 just because the neighbors are not.

5 Now, the neighbors have to understand, these
6 folks could scale down their pool, and perhaps redesign
7 it so it fits within -- if this isn't approved by the
8 way, we're going to have a vote, and then it's going to
9 go to the trustees, but the neighbors obviously have to
10 understand, that they could scale this down and fit
11 within the zoning ordinance, and then there would be
12 nothing to say, other than building code issues, which
13 the drainage is.

14 MS. CHOI: Actually, can I interject?

15 So if they were to even revise and put in a
16 smaller footprint -- their current house is already
17 maxed out at FAR, so they would still have to probably
18 come back and request a variation to FAR.

19 CHAIRMAN KOPP: Okay. Then, in that case --
20 so I strike that comment, that -- again, I'm not in
21 favor then of the increased FAR or the zoning variance
22 because the neighbors aren't, and just then -- in the
23 same case, if one of the neighbors shows up, we're
24 not -- I'm not going to be in favor of that one.

1 I have no idea how the other commissioners
2 feel. Would any commissioners like to state their
3 opinions before we take the vote?

4 MR. REMKUS: I'd like to say one thing, that
5 I agree with you, that it's an awfully large addition;
6 and the drainage is a concern to me, because being --
7 serving on this Plan Commission for so long, it's an
8 issue that constantly and constantly comes up, and it's
9 getting worse and worse, and we have to be a lot more
10 conscious about things like that; and going through all
11 the papers and everything, I didn't see anything -- one
12 of the neighbors mentioned that they built a rain guard.
13 Well, that's one of the approved things that has come
14 about the last few years for -- to slow down the
15 transition of water so it's retained on property, and
16 even something -- a small addition like this, I would
17 look on it more favorable if they were doing something
18 with the water they're displacing, but there's no --
19 nothing that forces them to do that, but it's something
20 that you look on more favorably when you're showing that
21 you're going to do something with the access water that
22 you're going to displace; and the FAR, that's what
23 they're doing, they're displacing more water. So I'm
24 just not in favor of it.

1 CHAIRMAN KOPP: All right. Our sample
2 motion assumes that we are going to approve this
3 application, or recommend this application, and I
4 suspect we are not.

5 So will someone make a motion that the Plan
6 Commission not recommend the petition for -- presented
7 at Zoning Hearing Case PC21 -- oh, I identified it as
8 21-01 before, but this one was 21-02.

9 Would someone make the motion that we not
10 recommend this?

11 MR. REMKUS: So moved.

12 THE REPORTER: Wagner, I didn't hear you.

13 MR. WAGNER: Wagner so moved, or second.

14 MR. WALEC: I second.

15 CHAIRMAN KOPP: I ask the Plan Commission
16 Secretary to call the vote.

17 MS. SHEMROSKE: Commissioner Remkus?

18 MR. REMKUS: Are we asking to disapprove,
19 right? So I would say "yes" to disapprove?

20 CHAIRMAN KOPP: Yeah, sorry. "Yes" to
21 disapprove.

22 MS. SHEMROSKE: Commissioner Soukup?

23 MR. SOUKUP: Not approve.

24 MS. SHEMROSKE: Commissioner Kaczmarek?

1 MS. KACZMAREK: Yes.

2 MS. SHEMROSKE: Commissioner Kaucky?

3 MS. KAUCKY: Yes.

4 MS. SHEMROSKE: Commissioner Walec?

5 MR. WALEC: Disapprove.

6 MS. SHEMROSKE: Vice Chairman Wagner?

7 MR. WAGNER: Yes.

8 MS. SHEMROSKE: And Chairman Kopp?

9 CHAIRMAN KOPP: Yes.

10 All right. For the petitioners, again, this
11 is a recommendation, and it's not binding. Only the
12 trustees can make the binding determination. Although,
13 I will warn you, they usually follow our
14 recommendations.

15 MR. BAFTIRI: Thank you very much.

16 CHAIRMAN KOPP: So that's the end of that
17 matter.

18 (WHICH WERE ALL THE PROCEEDINGS HAD.)

19

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24

1 STATE OF ILLINOIS)
) SS:
 2 COUNTY OF DUPAGE)

3

4 I, ROBIN HEJNAR, a certified shorthand reporter
 5 and registered professional reporter do hereby certify:

6 That prior to being examined, the witness in
 7 the foregoing proceeding was by me duly sworn to testify
 8 to the truth, the whole truth, and nothing but the
 9 truth;

10 That said proceedings were taken remotely
 11 before me at the time and places therein set forth and
 12 were taken down by me in shorthand and thereafter
 13 transcribed into typewriting under my direction and
 14 supervision;

15 I further certify that I am neither counsel
 16 for, nor related to, any party to said proceedings, not
 17 in anywise interested in the outcome thereof.

18 In witness whereof, I have hereunto subscribed
 19 my name.

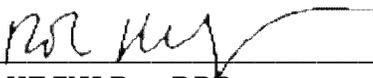
20 Dated: January 25, 2021

21

22

23

24



 ROBIN HEJNAR, RPR
 CSR No. 084-004689





Village of Willowbrook
 Staff Report to the Plan Commission

Plan Commission Date:	February 3, 2021		
Prepared By:	Ann Choi, Village Planning Consultant		
Case Title:	Zoning Hearing Case No. 21-01: 7505 Kingery Highway Panda Express SUP		
Applicant:	Hakim Yala Panda Express, Inc. 1683 Walnut Grove Avenue Rosemead, CA 91770		
Property Owner:	True North Energy, LLC 10346 Brecksville Road Brecksville, OH 44141		
Action Requested:	Consideration of a petition requesting approval of a special use permit for a fast food establishment and a special use permit for a drive-through in the B-2 Community Shopping District, including certain variations from Title 9 of the Village Code. The Applicant seeks to demolish the existing gas/service station and construct a one-story building and drive-through with associated site improvements.		
Applicable Regulations:	Zoning Ordinance		
Location:	7505 Kingery Highway, Willowbrook IL 60527		
PINs:	09-26-400-013		
Existing Zoning:	B-2 Community Shopping with a Special Use for a Gasoline Service Station		
Proposed Zoning:	B-2 Community Shopping with a Special Use for a Fast Food Establishment and Drive-through		
Existing Land Use:	Highway Commercial		
Property Size:	0.28 acres		
Surrounding Land Use:	Use	Zoning	
	North	Willowbrook/Wingren Plaza	B-2/Special Use
	South	Red Roof PLUS	B-4
	East	Light Manufacturing	M-1
	West	Potbelly's Restaurant	B-2/Special Use

Necessary Action by Plan Commission Open Public Hearing, accept testimony, and approve a recommendation to the Village Board. A sample motion can be found on pages 13 and 14.



Documents Attached:

Attachment 1: Legal Description

Attachment 2: Submitted Plans

- a) **"ALTA-NSPS Land Title Survey"** – 2 sheets (Job. No. 18003769), dated 8/10/2020, bearing the latest revision date of 9/23/2020, resubmitted on January 19, 2021 and prepared by NORR LLC.
- b) **"Site Layout Plan"** – 1 sheet, Sheet C04.0, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, bearing the latest revision date of 01/19/2021 and prepared by NORR Architects Engineers Planners.
- c) **"Architectural Drawings"** – 5 sheets, Sheets A-101, A-200, A-201, A-202, A-300, Panda Project #: D7058, Arch. Project #: JCDT180453, bearing an issue date of 11-04-2020, and prepared by NORR Architects Engineers Planners.
- d) **"Proposed Sign Package"** – 17 pages, Project #: C59897, dated 12.19.2018, bearing the latest revision date of 01.19.2020, and prepared by Priority Sign.
- e) **"Final Engineering Plans"** – 18 sheets, Panda Project #: D7058, Atwell Project #: 18003769, bearing an issue date of 11/20/2020, bearing the latest revision date of 01/19/2021 and prepared by NORR Architects Engineers Planners.
(Includes Landscape Plan)
- f) **"Engineer's Opinion of Probable Construction Cost"** – 3 pages, dated 11/20/2020, and prepared by Atwell Group.
- g) **"Site Traffic Study"** – 11 pages, dated January 5, 2020 and prepared by Atwell Group.
- h) **"Traffic Turn Analysis"** – 1 sheet, Sheet EX-01, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, and prepared by NORR Architects Engineers Planners.
- i) **"Traffic Plan"** – 1 sheet, Sheet EX-02, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, and prepared by NORR Architects Engineers Planners.
- j) **"Photometric Plan"** – 1 sheet, Sheet L01.0, dated 10/29/2020, and prepared by Villa Lighting.
- k) **"Construction Schedule"** – 1 page, dated 11/17/2020, and prepared by Atwell Group.

Attachment 3: Standards for Special Use Permits

Attachment 4: Standards for Variations

Attachment 5: Traffic Regulation Agreement – 11 pages, resubmitted by Atwell Group on 01/19/2021.

Attachment 6: Public Hearing Notice

Attachment 7: Village of Willowbrook Review Letter(s)



Background

Description of Site

The subject property is located at the southeast corner of Illinois (IL) Route 83/Kingery Highway and 75th Street, just south of Willowbrook/Wingren Plaza shopping center and east of the Potbelly's restaurant. The subject property is currently located in the B-2 Community Shopping Zoning District, is adjacent to the north, northwest and west by properties also located in the B-2 Community Shopping Zoning District and is immediately adjacent to property located in the B-4 Highway and Service Business District to the south and the M-1 Light Manufacturing District to the east. The one-acre property is regular in shape and is currently served by three existing driveways.

Exhibit 1: Map View of the Subject Property



Surrounding Uses

Surrounding zoning and uses include a shopping center (Willowbrook Plaza Shopping Center) including a Club Champion, Orange Theory Fitness, Fedex Print and Ship Center, Kabob-Q restaurant, a couple of nail salons, and associated parking lots in the B-2 zoning district to the north, a hotel (Red Roof Inn) and associated parking lot in the B-4 zoning district to the south, a fast-casual restaurant (Potbelly's) in the B-2 zoning district to the west, and manufacturing/sales uses in the M-1 zoning district to the east. The Village of Willowbrook's Comprehensive Plan designates the site for "Highway Commercial" uses, which corresponds to the B-2 Community Shopping Zoning District.



Exhibit 2: Zoning Map (B-2 Community Shopping)



Existing Streets and Circulation

Illinois Route 83/Kingery Highway is designated as a Regional Arterial and forms the western boundary of the site with a required right-of-way width of 200 feet. A right-in/right-out access drive on IL Route 83 is located approximately 100 feet south of 75th Street. The access drive provides one inbound lane and one outbound lane restricted to right-turn movements by the median on IL Route 83.

75th Street is designated as a Major Arterial west of IL Route 83 and is designated as an Industrial Collector east of IL Route 83. 75th Street forms the northern boundary of the subject property with a required right-of-way width of 60 feet. There are two access drives on 75th Street, with the westernmost access drive allowing right-in only into the subject property. The current allows full movement access on 75th Street.

History

Prior Zoning Request

The previous ordinances regulating this facility are as follows:

- Ordinance No. 77-O-14 – Original special use ordinance establishing the use, but without specific bulk standards.
- Ordinance No. 94-O-16 – Amended original special use to permit the construction of a second driveway on 75th Street and various site and landscaping improvements and granted the following variations:
 1. That Section 9-3-7(A)1 of the Village Code of the Village of Willowbrook establishing a minimum required setback on Illinois Route 83 be varied by reducing same from one hundred feet (100') to five feet, four inches (5'4") for the existing overhead canopy, to fifteen feet (15') for the existing



- gasoline pump dispenser islands, and to fifty-eight feet (58') for the existing service station building.
2. That Section 9-6-1(A) of the Village Code of the Village of Willowbrook establishing the minimum lot area for two or more uses on a lot be varied by reducing same from two (2) acres to one (1) acre.
 3. That Section 9-6B-3(A) of the Village Code of the Village of Willowbrook establishing minimum lot area be varied by reducing same from two (2) acres to one (1) acre.
 4. That Section 9-6B-3(E)1 of the Village Code of the Village of Willowbrook establishing front yard setbacks be varied by reducing same for the existing canopy from sixty feet (60') to five feet, four inches (5'4"), for the existing gasoline pump dispenser islands from sixty feet (60') to fifteen feet (15'), and for the existing service station building from sixty feet (60') to fifty-eight feet (58').
 5. That Section 9-6B-3(E)3 of the Village Code of the Village of Willowbrook establishing exterior side yard setbacks be varied by reducing same from sixty feet (60') to forty-three feet (43').
 6. That Section 9-10-5(G) of the Village Code of the Village of Willowbrook establishing parking area setbacks within required yards, be varied by reducing same from fifteen feet (15') to zero (0) setback for the front yard impervious surface setback, and from fifteen feet (15') to ten feet (10') for the exterior side yard setback.
 7. That Section 9-10-5(I)2(b) of the Village Code of the Village of Willowbrook establishing parking area pavement improvements be varied to permit a substitution from the required all-weather hard surface pavement improvement for all parking areas within a non-residential district to a gravel surface for the screened vehicle storage area.
 8. That Section 9-10-5(I)2(c) of the Village Code of the Village of Willowbrook establishing required parking area improvements be varied to permit a substitution for the required concrete barrier curb and gutter for all parking areas within a non-residential district to permit individual wheel stops for the southern parking area.
 9. That Section 9-10-5(L)2(b)2(a) of the Village Code of the Village of Willowbrook establishing required minimum access driveway curb radius be varied by reducing same from thirty feet (30') to fifteen feet (15').
 10. That Section 9-10-5(L)2(b)2(b) of the Village Code of the Village of Willowbrook establishing required minimum access driveway inside curb radius be varied by reducing same from twenty-five feet (25') to ten feet (10').
 11. That Section 9-10-5(I)2(b) of the Village Code of the Village of Willowbrook establishing parking area pavement improvements be varied to permit a substitution from the required all-weather hard surface pavement improvement for all parking areas within a non-residential district to a gravel surface for the screened vehicle storage area.
 12. That Section 9-10-5(I)2(c) of the Village Code of the Village of Willowbrook establishing required parking area improvements be varied to permit a substitution for the required concrete barrier curb and gutter for all parking areas within a non-residential district to permit individual wheel stops for the southern parking area.
 13. That Section 9-10-5(L)2(b)2(a) of the Village Code of the Village of Willowbrook establishing required minimum access driveway curb radius be varied by reducing same from thirty feet (30') to fifteen feet (15').
 14. That Section 9-10-5(L)2(b)2(b) of the Village Code of the Village of Willowbrook establishing required minimum access driveway inside curb radius be varied by reducing same from twenty-five feet (25') to ten feet (10').



Overview

Development Proposal

The subject property is currently occupied by a single building that houses a Shell gasoline and service station and a convenience mart, including a detached canopy covering four existing gasoline pump dispenser islands. The subject property likely contains contaminants due to the existing use. The current property owner, True North Energy, LLC, will be responsible for securing a No Further Remediation (NFR) letter from the Illinois Environmental Protection Agency (IEPA).

Panda Express, Inc. (the "Applicant"), proposes to demolish the existing 2,131 square foot Shell gasoline and service station with convenience mart and is requesting approval of a special use for the construction of a 2,300 square foot Panda Express fast food restaurant with a drive-through and associated site improvements. A special use permit is requested to allow for both the fast food establishment and a proposed drive-through within the Village of Willowbrook's B-2 Community Shopping Zoning District. The fast food establishment will include over fifty (50) interior seats and no permanent outdoor dining is proposed. The proposed single-story building is situated towards the northwest portion of the subject property and will require several variations because the proposed building will not meet required setbacks, minimum required lot area for the proposed uses, and certain landscape requirements.

Requested Action

Approval of a special use permit within the B-2 Community Shopping District zoning classification for a fast food establishment and a special use permit for a drive-through in the B-2 Community Shopping District, including the following variations from Title 9 of the Village Code. The Applicant seeks to demolish the existing gas/service station and construct a one-story building and drive-through with associated site improvements.

Requested Variations

The proposed improvements for the use include the variations listed below.

1. A variation from Section 9-3-3(C) to decrease the minimum lot area for two (2) or more special uses from two acres (2 AC) to one acre (1 AC).
2. A variation from Section 9-3-7(A)1 to decrease the minimum special setback on Illinois Route 83 from one hundred feet (100') to fifty feet (50') for the proposed building, to fifty-six and six tenths feet (56.6') for the proposed menu board, and to sixty-two and four tenths feet (62.4') to the order canopy.
3. A variation from Section 9-6-1(A) to decrease the minimum lot area for two (2) or more special uses (fast food establishment and drive-through) from two acres (2 AC) to one acre (1 AC).
4. A variation from Section 9-6B-3(A) to decrease the minimum lot area for all uses from two acres (2 AC) to one acre (1 AC).
5. A variation from Section 9-6B-3(C)1 to decrease the minimum lot width of 200 feet for restaurants from two hundred feet (200') to one hundred seventy feet (170').
6. A variation from Section 9-6B-3(E)1 to decrease the minimum front yard setback on Illinois Route 83 from sixty feet (60') to fifty feet (50') for the proposed building and to fifty-six and six tenths feet (56.6') for the proposed menu board.
7. A variation from Section 9-6B-3(E)3 to decrease the minimum exterior side yard setback on 75th Street from sixty feet (60') to forty-three and four tenths feet (43.4') for the proposed building, to thirty-two and seven tenths feet (32.7') for the proposed menu board, and to twenty-five feet (25') to the order canopy.
8. A variation from Section 9-10-5(L)2(b)2(a) to decrease the radius connecting street pavement edge and driveway edge on 75th street from thirty-five feet (35') to twenty-five feet (25').



9. A variation from Section 9-10-5(L)2(d) to decrease the driveway location on Route 83 from seventy feet (70') to twenty-five and seven tenths feet (25.7').
10. A variation from Section 9-10-5(L)2(e) to decrease the minimum spacing between separate driveway entrances from four hundred feet (400') to approximately three hundred thirteen feet (+/- 313') on Illinois Route 83 and to approximately one hundred sixty-nine feet (+/-169') on 75th Street.
11. A variation from Section 9-10-5(L)2(f) to decrease the minimum spacing between driveway entrance and right-of-way of an adjacent intersecting street from five hundred feet (500') to ninety-four and three tenths feet (94.3') on Illinois Route 83 and to one hundred forty-six and two tenths feet (146.2') on 75th Street.
12. A variation from Section 9-10-5(L)2(g) to eliminate the requirement of a concrete median separation and to reduce the driveway storage from one hundred feet (100') to zero feet on Illinois Route 83 and 75th Street.
13. A variation from Section 9-14-2(D)2(c)4 to decrease the minimum foundation landscape area from seven feet (7') to four and five tenths feet (4.5') along the southern façade, to five and two tenths feet (5.2') along the northern portion of the eastern building façade and from seven feet (7') to zero feet along the western building façade and remaining portion of the eastern building façade.

While it appears that there are a number of variations, staff met with the developer early in the process to discuss all of them, and it should be noted that five (5) of the thirteen (13) of the requested variations are due to existing conditions such as lot width and existing driveway locations. A more detailed description of the variations is outlined below.

Staff Analysis

Access, Circulation and Parking/Loading

A Site Traffic Study and a Truck Turn Analysis were submitted, and planning staff supplied a review letter dated January 13, 2021 which includes the Village traffic consultant's comments as part of **Attachment 7** for the Plan Commission's review. The subject property is currently accessed from both IL Route 83/Kingery Highway and 75th Street. The Panda Express site layout proposes constructing new access driveways in the approximate location of the existing access driveways, with the exception of the westernmost right-in only site access drive on 75th Street which will be closed off and curbed. The access on 75th Street will be a full movement for inbound traffic and limited to a right-out only. The access on IL Route 83/Kingery Highway will remain to be a right-in/right-out access.

The single-lane drive-through has a clockwise rotation around the proposed building and offers a second bypass lane after patrons submit their orders at the menu board. The proposed fast food establishment can be accessed via the driveway on IL Route 83/Kingery Highway for northbound travelers. Vehicles will enter from this driveway and proceed along the south edge of the site, past a row of surface parking spaces. Vehicles will make two left turns after driving past the proposed building to enter the drive-through. The approach from the 75th Street driveway also has a clockwise rotation where vehicles will make an immediate right to proceed directly into the drive-through lane.

According to the Traffic Plan, the proposed site layout is designed to provide a total storage length of seven (7) vehicles for the drive-through window queue, with placement of the menu/order board at the fifth car. Furthermore, two (2) drive-through parking pick up spaces are proposed to help reduce drive-through queueing as needed during peak business hours.

Data was collected from three (3) similar nearby Panda Express drive-through restaurants and includes seven (7) consecutive days of drive-through transaction counts, divided into 30-minute time spans. The traffic study



Exhibit 4 Perspective Rendering (East and South Elevations Facing Parking)



Zoning Bulk Requirements

Setbacks: The Applicant is requesting three variations from the setback requirements of the Zoning Ordinance. The property is zoned B-2 (with a special use). A detailed discussion of specific setbacks and important bulk exceptions and variations for the proposed project is provided in **Exhibit 4** and **Exhibit 5** shown below and on the following page. As previously discussed in the History section of this report, the subject property is further governed by Ordinance No. 94-O-16, which provided significant zoning relief from the specific setbacks and B-2 bulk regulations. Staff also considered the reduced setbacks of adjoining properties and the Village’s history of allowing reduced setbacks in similar instances.

Exhibit 4: Specific Setbacks Compliance Table

Specific Setbacks				
Item	Code Section or Ordinance	Code Requirement	Proposed	Departure
Route 83 Setback	9-3-7(A)1 94-O-16 Sect. 3	100'	50' (building) 56.6' (menu board) 62.4' (order canopy) 5'-4" for existing overhead canopy 25' for existing gasoline pump dispenser islands 58' existing service station building	Yes

It should be noted that relief is requested from the one hundred foot (100') specific setback and the sixty foot (60') front yard setback on Illinois Route 83 for the proposed building, menu board, and order canopy which encroaches into the front and exterior side yards. Staff recommends approval of these variations since similar variations were granted through Ordinance No. 94-O-16 for the existing service station building.



Exhibit 5: B-2 Zoning District Compliance Table

B-2 Zoning District Bulk Regulations				
Min. Lot Area	9-6B-3(A) <i>94-0-31 Sect. 3</i>	2 acres <i>1.0 acre</i>	1.0 acre	Yes
Min. District Area	9-6B-3(B)	2 or more acres	N/A	
Min. Lot Width	9-6B-3(C)1 <i>94-0-31 Sect. 6</i>	200' <i>170'</i>	170' <i>See 9-3-12</i>	Yes (Existing Condition)
Min. Lot Depth	9-6B-3(D)	200'	256.24'	None
Min. Front Yard Setback (Kingery Hwy)	9-6B-3(E)1 <i>94-0-31 Sect. 7</i>	60' <i>5'-4" (overhead canopy) 15' (bill board) 55' (service station)</i>	50' (building) 56.6' (menu board)	Yes
Min. Interior Side Yard Setback (south lot line)	9-6B-3(E)2	30'	63.6'	None
Min. Exterior Side Yard Setback (75 th St)	9-6B-3(E)3 <i>94-0-31 Sect. 8</i>	60' <i>43'</i>	43.4' (building) 32.7' (menu board) 25' (order canopy)	Yes
Min. Rear Yard Setback (east lot line)	9-6B-3(E)4	40'	162.8'	None
Min. Transition Yard	9-6B-3(E)5		N/A	
Max. Lot Coverage	9-6B-3(F)	50%	6% (2,607 SF)	None
Max. Height	9-6B-3(G)	30'	23'	None
Max. Floor Area Ratio	9-6B-3(H)	0.30	0.06 (2,607 SF/43,561 SF)	None

Lot Dimensions: The subject property has a minimum lot area of approximately 43,561 square feet, or 1.0 acre. The subject property has a lot width of one hundred seventy feet (170') and a lot depth of two hundred fifty-six feet (256'). A variation for reduced lot width requested and granted under Ordinance No. 94-0-16. However, Staff is including a variation request for reduced lot width as part of this petition.

Building Height: The maximum height of a building in the B-2 Zoning District is forty feet (30'). The proposed building height for the fast food establishment is twenty-three feet (23') and is therefore in compliance with the Zoning Ordinance.

Density and Coverage: The maximum lot coverage (building coverage) in the B-2 Zoning District is 50% per Section 9-6B-3(F). The proposed building has a building footprint of approximately 2,300 square feet. On a one-acre lot, the building coverage is approximately 6% and is below the maximum 50% threshold. As a one-story building, the floor area ratio (FAR) is approximately 0.06, which is below the minimum required 0.30 FAR. Both the requirements for lot coverage and FAR have been satisfied.

Parking & Loading

The proposed site layout will provide a total of forty-six (46) parking spaces, including two (2) accessible spaces, located along the east and south lot lines, east of the proposed building and within the interior of the subject property. There will also be two (2) dedicated drive-through pick-up spaces located along the southwest area of the site to help reduce drive-through queueing as needed during peak business hours. There are no loading berths proposed as the Zoning Ordinance does not require any for structures that have a gross floor area of 5,000 square feet or below.



Parking Area/Pavement Setbacks

Parking area/pavement setbacks are regulated in the Parking Section of the Zoning Ordinance. A twenty-five foot (25') parking area setback is required along the Illinois Route 83 frontage, a fifteen-foot (15') parking area setback is required along the 75th Street frontage, and a 10-foot (10') setback is required along the interior side and rear lot lines. The proposed site plan complies with the pavement setback requirements required by the Village Code.

Contamination Issues

The Applicant has a purchase agreement in place with the current property owner/seller (True North Energy, LLC) for the property that includes an addendum outlining the removal procedure for the contaminants. This agreement states a No Further Remediation (NFR) letter from the Illinois Environmental Protection Agency (IEPA) is the responsibility of the current property owner/seller; therefore, the purchase of the property is contingent on the current property owner/seller to obtain the NFR letter from the IEPA (that is acceptable to the Applicant for its intended use of the property). The Village is agreeable to this and has added a condition of approval that the NFR letter will be required at time of building permit if the seller and buyer have the NFR letter as a condition of the sale closing.

Stormwater Management

An abbreviated Stormwater Submittal was provided to the Village. The Village Engineer confirmed the findings of the study stating that the subject property does not contain floodplain or wetlands, and the development does not reach the net new impervious thresholds to trigger Best Management Practices or Detention. The Village Engineer has reviewed the engineering for stormwater compliance and identified no issues. A copy of his review letter is included as **Attachment 7** for your review.

Trash Enclosure

The Applicant is proposing a six-foot (6') height trash enclosure on the southeast portion of the site lot, to the east side of the building, where the open side of the enclosure is oriented so that, to the greatest extent possible, it does not face towards an abutting property or street. A black and white trash enclosure plan and details were provided by the Applicant. Per Section 9-12-11 of the Village Code, the trash enclosure must be constructed to look like masonry and shall appear to the color and style of the building. Staff recommends that this be included as a condition for the Plan Commission's future recommendation to the Village Board.

Site Lighting

The Village Engineer's December 21, 2020 memo states that site lighting is in general compliance with Village Code and standard engineering methods. Please note that the Village reserves the right to require glare shields to be installed, should it deem appropriate.

Landscaping

The modifications to the site are an improvement over the existing conditions. Additional foundation, interior parking lot, and perimeter landscape areas will improve circulation and safety, and are an engineering asset since it means that no additional stormwater improvements are required. A significant amount of new landscaping is being provided site wide. The proposed Landscape Plan is shown in **Attachment 2**. Staff anticipates that only minor changes will be required as the proposed development appears to provide sufficient landscaping, but a condition has been added to ensure that the landscape plan will address the comments in the planning review letter outlined in **Attachment 7**. There are some minor revisions that need to be included in the current landscape plan; however, staff is confident that appropriate landscaping can be achieved on the site. Therefore, staff is recommending a condition of approval that requires future approval of the landscape plan by staff prior to Village Board approval.



Signage

The new free-standing sign on a brick base is proposed in generally the same location as the existing free-standing sign, at the northeast corner of the subject property. The proposed free-standing sign complies with the current ordinance and current configuration of the property. The sign has dimensions of 12 feet x 6'-5 7/8" feet, or a total of 36 square feet per side (72 square feet total). It will be set back a minimum of five feet (5') from the north and west lot lines, contains brick in keeping with the design of the principal structure, and will be constructed to a maximum height of twelve feet (12'). Because the proposed free-standing sign exceeds a height of eight feet (8') above average surrounding grade, the total sign surface area allowed is reduced by thirty square feet (30 SF).

Exhibit 6: Proposed Free-Standing Sign



Based on the length of the exterior side lot line and the reductions taken because of the height of the free-standing sign, a total of two hundred ninety square feet (290 SF) of signage is allowed, which covers both wall and free-standing signs. The sum of all signs shown on the plans is two hundred thirty-three and nineteen hundredths square feet (233.19 SF). One wall sign is proposed on each of the north, east and west sides of the building as shown in the sign package. The south wall sign has 53.27 square feet of sign surface area, while the north, east and west wall signs have 36 square feet of sign surface area each. The Zoning Ordinance allows only one wall sign for each street frontage or "business site frontage". With its corner location, the tenant is assured a total of four (4) wall signs. Staff is interpreting all four elevations as having "business site frontage", which allows for four wall signs. A condition of approval is added that confirms no additional wall signage will be allowed as part of this approval.

Exhibit 7: Proposed Wall Signs



Findings of Fact



Standards for Special Use

Section 9-14-5(B) of the Willowbrook Zoning Ordinance establishes seven (7) standards for a Special Use Permit that must be evaluated by the Plan Commission and Village Board. Recommendations may include conditions of approval if appropriate to mitigate any negative impacts created by the special use permit. The Applicant's responses are provided in **Attachment 3**.

Standards for Variations

Section 9-14-4(E) of the Willowbrook Zoning Ordinance establishes seven (7) standards that must be evaluated by the Plan Commission and Village Board. Recommendations may include conditions of approval if appropriate to mitigate any negative impacts created by the variations. A list of the variation standards is provided in **Attachment 4**, along with the proposed findings to be incorporated in the Plan Commission's recommendation and the Village Board's ordinance if approved.

Staff Recommendation

Generally, staff has been supportive of the demolition of the existing building on this site for a new fast food establishment with a drive through. The building meets the user's needs, and the site improvements create an improved situation than exists today even though variations are required. The use will provide nearby dining opportunities for students and visitors to Hinsdale South High school. The special use and variations are supported. Planning staff would also recommend acceptance of the submitted written Findings of Fact in response to the Standards for Special Use Permits and Standards for Variations from the Zoning Ordinance sought, which are included as **Attachments 3 and 4** of this report. If the Plan Commission wishes to support the project, staff recommends approval of the following sample motion:

Sample Motion

Based on the submitted petition and testimony presented, I move that the Plan Commission approve and adopt the Findings of Fact submitted in response to the Standards for Special Use Permits and Standards for Variations outlined in **Attachments 3 and 4**, respectively, of the staff report prepared for PC 21-01 for the February 3, 2021 Plan Commission meeting; and that the Plan Commission recommend to the Village Board approval of a special use for a fast food establishment with a drive-through, including the list of variations as outlined in the staff report prepared for PC 21-01 for the February 3, 2021 Plan Commission meeting, and as deliberated by the Plan Commission, according to the plans listed in the Staff Report for PC 21-01 for the February 3, 2021 Plan Commission meeting, and subject to the following conditions:

1. Improvements shall be in substantial compliance with all plans described in **Attachment 2** of the staff report prepared for PC 21-01 except as may be modified in response to compliance with Village Codes or conditions recommended by the Plan Commission and as approved by the Village.
2. Approved signage is limited to two hundred ninety square feet (290 SF) of signage including one monument sign and four wall signs in substantial conformance with the sign package included in **Attachment 2** of the staff report.
3. The Landscape Plan shall be revised, resubmitted, and approved by the Village in compliance with Village Codes prior to final zoning approval from the Village Board.
4. Plans shall be revised prior to being considered by the Village Board in compliance with the planning comments outlined in **Attachment 7** of the staff report prepared for PC 21-01 for the February 3, 2021 Plan Commission meeting.
5. The trash enclosure shall be constructed to look like masonry and shall appear similar to the color and style of the building.



6. All landscaped areas shall be constructed, and landscape material installed prior to the issuance of any permanent occupancy permit for the subject realty, or such earlier time, as is reasonably practical.
7. A separate sign permit shall be obtained for the proposed building signage, pursuant to the Village Code.
8. That as part of the final approval by the Village, the Applicant shall enter into a Traffic Agreement in substantial conformance outlined in **Attachment 6** of the staff report prepared for PC 21-01 for the February 3, 2021 Plan Commission meeting.
9. A permit is required from IDOT prior to any work in the Route 83 right of way.
10. A permit is required from the Du Page County Public Works Department for the sanitary sewer connection.
11. The Applicant must file a Notice of Intent with the IEPA prior to the start of construction.
12. The property owner shall provide the Village with a letter of No Further Remediation (NFR) from the IEPA prior to a building permit being issued.
13. Prior to building permit, the Village will require a Plat of Easement to grant access to the B-Box at the building. This can be a 10-foot-wide non-exclusive easement over the water service from 75th Street to the building.
14. All outstanding comments from the Village must be addressed prior to final zoning approval from the Village Board.

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Attachment 1
Legal Description

THE NORTH 170.0 FEET OF THAT PART OF THE SOUTHEAST 1/4 OF SECTION 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING EAST OF THE EAST RIGHT OF WAY OF STATE ROUTE 83, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT 102.65 FEET EAST OF THE NORTHWEST CORNER OF SAID SOUTHEAST 1/4; THENCE EAST, ALONG THE NORTH LINE OF SAID SOUTHEAST 1/4, A DISTANCE OF 256.24 FEET; THENCE SOUTH, ALONG A LINE PARALLEL WITH THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170 FEET; THENCE WEST, ALONG A LINE PARALLEL WITH THE NORTH LINE OF SAID SOUTHEAST 1/4, A DISTANCE OF 256.24 FEET TO THE INTERSECTION OF SAID PARALLEL LINE WITH THE EAST RIGHT OF WAY OF SAID STATE ROUTE 83, WHICH POINT IS 102.45 FEET EAST OF THE WEST LINE OF SAID SOUTHEAST 1/4; THENCE NORTH, ALONG THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170.0 FEET TO THE POINT OF BEGINNING, IN DUPAGE COUNTY, ILLINOIS.

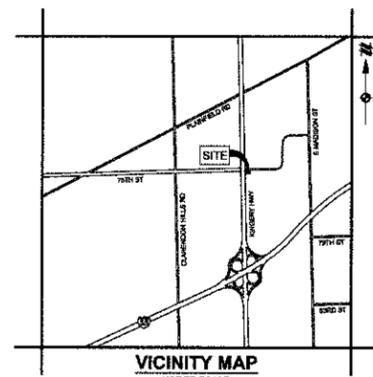
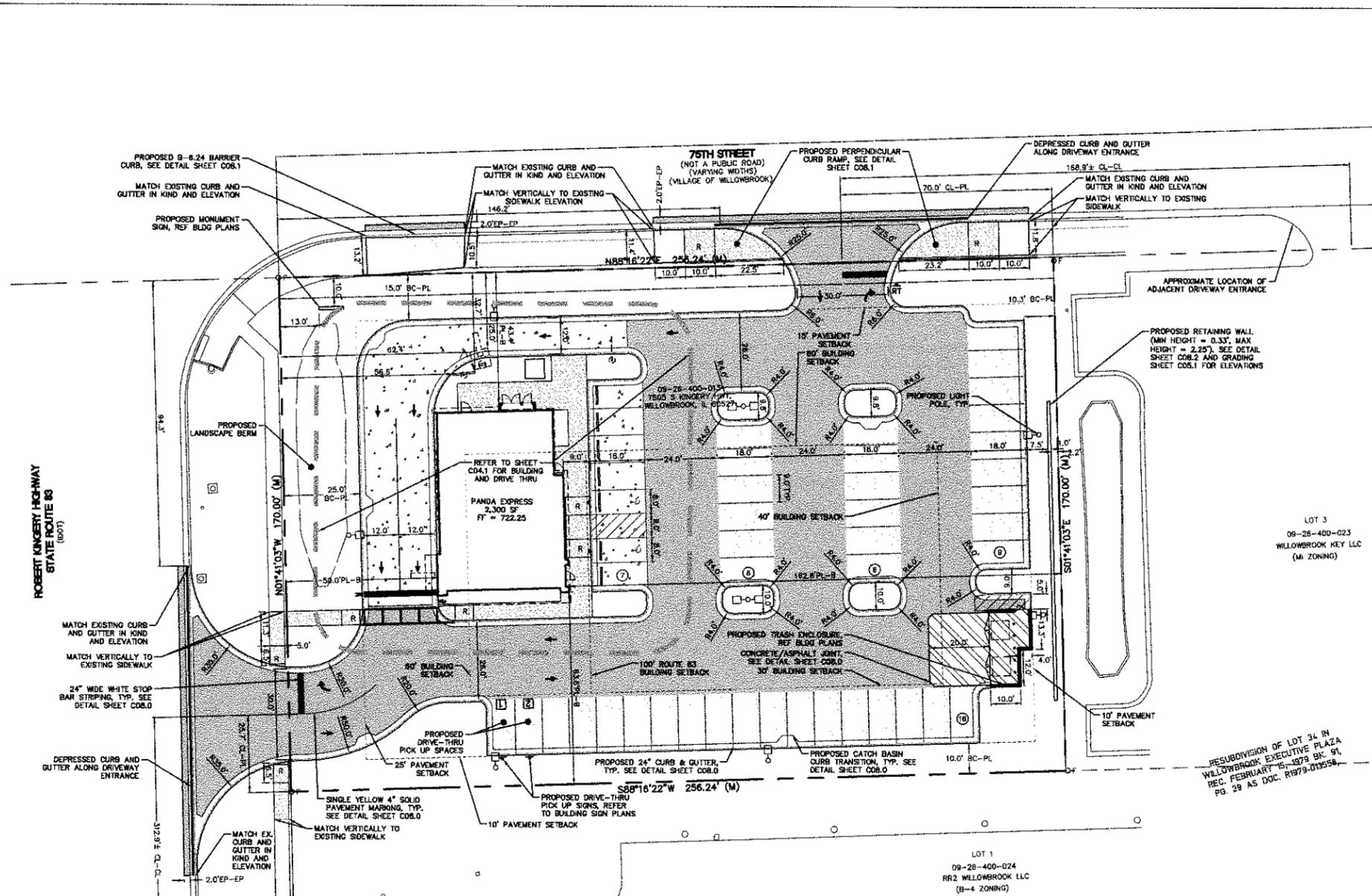
Property Address: 7505 South Kingery Highway, Willowbrook, IL 60527

PIN #: 09-26-400-013



Attachment 2
Submitted Plans

1. **"ALTA-NSPS Land Title Survey"** – 2 sheets (Job. No. 18003769), dated 8/10/2020, bearing the latest revision date of 9/23/2020, resubmitted on January 19, 2021 and prepared by NORR LLC.
2. **"Site Layout Plan"** – 1 sheet, Sheet C04.0, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, bearing the latest revision date of 01/19/2021 and prepared by NORR Architects Engineers Planners.
3. **"Architectural Drawings"** – 5 sheets, Sheets A-101, A-200, A-201, A-202, A-300, Panda Project #: D7058, Arch. Project #: JCDT180453, bearing an issue date of 11-04-2020, and prepared by NORR Architects Engineers Planners.
4. **"Proposed Sign Package"** – 17 pages, Project #: C59897, dated 12.19.2018, bearing the latest revision date of 01.19.2020, and prepared by Priority Sign.
5. **"Final Engineering Plans"** – 18 sheets, Panda Project #: D7058, Atwell Project #: 18003769, bearing an issue date of 11/20/2020, bearing the latest revision date of 01/19/2021 and prepared by NORR Architects Engineers Planners.
6. **"Engineer's Opinion of Probable Construction Cost"** – 3 pages, dated 11/20/2020, and prepared by Atwell Group.
7. **"Site Traffic Study"** – 11 pages, dated January 5, 2020 and prepared by Atwell Group.
8. **"Traffic Turn Analysis"** – 1 sheet, Sheet EX-01, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, and prepared by NORR Architects Engineers Planners.
9. **"Traffic Plan"** – 1 sheet, Sheet EX-02, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, and prepared by NORR Architects Engineers Planners.
10. **"Landscape Plan"** – 2 sheets, Sheets L01.0 and L01.1, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, bearing the latest revision date of 01/19/2021 and prepared by NORR Architects Engineers Planners.
11. **"Photometric Plan"** – 1 sheet, Sheet L01.0, dated 10/29/2020, and prepared by Villa Lighting.
12. **"Construction Schedule"** – 1 page, dated 11/17/2020, and prepared by Atwell Group.



LAYOUT LEGEND

---	BOUNDARY LINE
- - - -	EXISTING ROW
---	EXISTING CURB AND GUTTER
---	PROPOSED CURB AND GUTTER
---	PROPOSED DEPRESSED CURB AND GUTTER
---	EXISTING EASEMENT LINE
---	PROPOSED LANDSCAPE BERM
---	SETBACK LINE
○	PARKING SPACE COUNT
R	ADA RAMP
BC	DIMENSION TO BACK OF CURB
PL	DIMENSION TO PROPERTY LINE
CL	DIMENSION TO CENTER LINE
EP	DIMENSION TO EDGE OF PAVEMENT
EP	DIMENSION TO BUILDING
□	PROPOSED LIGHT POLE/WALL PACK LIGHT
+	PROPOSED SIGN
↓ ADA	PROPOSED ACCESSIBLE PARKING SPACE SIGN
↓ S	PROPOSED STOP SIGN
↓ DNE	PROPOSED DO NOT ENTER SIGN
↓ RT	PROPOSED RIGHT TURN ONLY SIGN
▨	PROPOSED REINFORCED CONCRETE PAVEMENT, SEE DETAIL SHEET C08.2
▨	PROPOSED CONCRETE SIDEWALK, SEE DETAIL SHEET C08.2
▨	PROPOSED STANDARD DUTY ASPHALT PAVEMENT, SEE DETAIL SHEET C08.2
▨	PROPOSED HEAVY DUTY ASPHALT PAVEMENT, SEE DETAIL SHEET C08.2
▨	TRASH ENCLOSURE PAVEMENT, SEE DETAIL SHEET C08.2

- NOTES:**
- SEE SHEET C01.1 FOR CONSTRUCTION NOTES.
 - ALL DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 - ALL RADII ARE 3.0' UNLESS OTHERWISE NOTED.
 - REFER TO ARCHITECTURAL PLANS FOR SITE LIGHTING ELECTRICAL PLANS.
 - ALL STRIPING SHALL BE 4" WHITE PAINT UNLESS OTHERWISE NOTED. ALL STRIPING SHALL BE APPLIED WITH TWO COATS OF PAINT AT MANUFACTURER'S RECOMMENDED RATE. SEE STRIPING DETAIL SHEET C08.0

INTERIOR LANDSCAPING

SITE AREA EXCLUDING FRONT YARD, ROW, BUILDINGS, AND STRUCTURES: 38,578 SF

REQUIRED INTERIOR LANDSCAPING: 3,658 SF (10%)

PROVIDED INTERIOR LANDSCAPING: 8,408 SF (26%)

ADJACENT LAND USE DATA

ZONING DISTRICT	LAND USE	DEVELOPMENT NAME
NORTH: B2 COMMUNITY SHOPPING	SHOPPING CENTER	WILLOWBROOK PLAZA
SOUTH: B4 HWY & SERVICE BUSINESS	HOTEL	RED ROOF INN
EAST: M1 LIGHT MANUFACTURING	MANUFACTURING/SALES	WILLOWBROOK KEY LLC
WEST: B2 COMMUNITY SHOPPING	RESTAURANT	POTBELLY

SITE DATA

*ITEMS MARKED WITH AN ASTERISK VARY FROM THE VILLAGE CODE AND WILL REQUIRE A VARIANCE

JURISDICTION: VILLAGE OF WILLOWBROOK

ZONING DISTRICT: B-2 COMMUNITY SHOPPING

SITE AREA CALCULATIONS

SITE AREA CALCULATIONS:	1.0 ACRES
TOTAL SITE AREA	1.0 ACRES
PERVIOUS AREA	0.29 ACRES
IMPERVIOUS AREA	0.71 ACRES

RESTAURANT BUILDING AREA: 2,300 SF
 TRASH ENCLOSURE AREA: 302 SF
 TOTAL BUILDING AREA: 2,607 SF

MIN. LOT AREA

REQUIRED	PROVIDED
2.0 ACRES	1.00 ACRES (43,561 SF)*
MIN. LOT WIDTH	200'
MIN. LOT DEPTH	200'

MAX. LOT COVERAGE

REQUIRED	PROVIDED
50%	6.0%
MAX. BUILDING HEIGHT	30'
MAX. FLOOR AREA RATIO	0.30

EXISTING BUILDING SETBACK

REQUIRED	PROVIDED
FRONT (WEST)	60'
ROUTE B3 (WEST)	100'
SIDE EXTERIOR (NORTH)	40'
SIDE INTERIOR (SOUTH)	100'
REAR (EAST)	30'

PROPOSED BUILDING SETBACK

REQUIRED	PROVIDED
FRONT (WEST)	60'
ROUTE B3 (WEST)	100'
SIDE EXTERIOR (NORTH)	60'
SIDE INTERIOR (SOUTH)	30'
REAR (EAST)	40'

PARKING SETBACK

REQUIRED	PROVIDED
FRONT (WEST)	25'
SIDE EXTERIOR (NORTH)	15'
SIDE INTERIOR (SOUTH)	10'
REAR (EAST)	10'

DRIVE AISLE WIDTH: 24' UNLESS OTHERWISE NOTED

FLOOD HAZARD ZONE X
 FIRM MAP NO. 17043C0189A, DATED 8/1/2019

EXISTING INFORMATION:
 EXISTING SITE CONDITIONS ARE BASED ON AN ALTA/NPS/LAND TITLE SURVEY BY ATWELL, LLC DATED 8/23/2020

SITE LIGHTING:
 PHOTOMETRICS DESIGNED BY OTHERS. POLE LOCATIONS ARE SHOWN FOR REFERENCE ONLY. CONTRACTOR SHALL VERIFY FINAL LOCATIONS WITH PHOTOMETRICS PLAN AND OWNER PRIOR TO CONSTRUCTION.

PARKING DATA

LOADING BERTHS	0
REQUIRED	0
PROVIDED	0

REQUIRED PARKING CALCULATION:
 1 PARKING SPACE PER 100 SF OF FLOOR AREA
 1 SPACE/100 SF * 2,300 SF = 23 SPACES

TOTAL REQUIRED SPACES: 23 SPACES

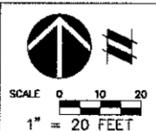
PROVIDED

REGULAR (9' X 18')	42 SPACES
DRIVE THRU PICKUP (9' X 18')	2 SPACES
ADA (9' X 18')	2 SPACES
TOTAL PROVIDED SPACES	46 SPACES



THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN ALL APPROPRIATE VIEWS ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MAY BE OCCURRED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

24-HOUR CONTACT:
 PANDA PM
 HAKIM YALA
 847-477-5125



ROSENE E. HAYDO
 062-081201
 REGISTERED
 PROFESSIONAL
 ENGINEER
 OF
 ILLINOIS
 DATE: 1/19/2021
 EXP: 11/30/2021

CONTRACTOR SHALL PROTECT ALL ITEMS OUTSIDE LIMITS OF CONSTRUCTION UNLESS OTHERWISE NOTED IN THE CONSTRUCTION PLANS OR SPECIFICATIONS.

CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITIES (LOCATIONS AND ELEVATIONS) PRIOR TO STARTING CONSTRUCTION AND ALERT ENGINEER TO ANY DISCREPANCIES IMMEDIATELY.



PANDA EXPRESS, INC.
 1683 Walnut Grove Ave.
 Rosemead, California
 91770
 Telephone: 626.796.9888
 Facsimile: 626.372.8288

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REVISIONS:

VILLAGE PLAN REV. #1	01/19/2021
----------------------	------------

ISSUE DATE:

PC SUBMITTAL	11/20/2020
--------------	------------

DRAWN BY: LEH

PANDA PROJECT #: D7058
 ATWELL PROJECT #: 18003769



PANDA EXPRESS
 WARM & WELCOME 2300
 7505 KINGERY HIGHWAY
 WILLOWBROOK, IL 60527

SITE LAYOUT PLAN

C04.0

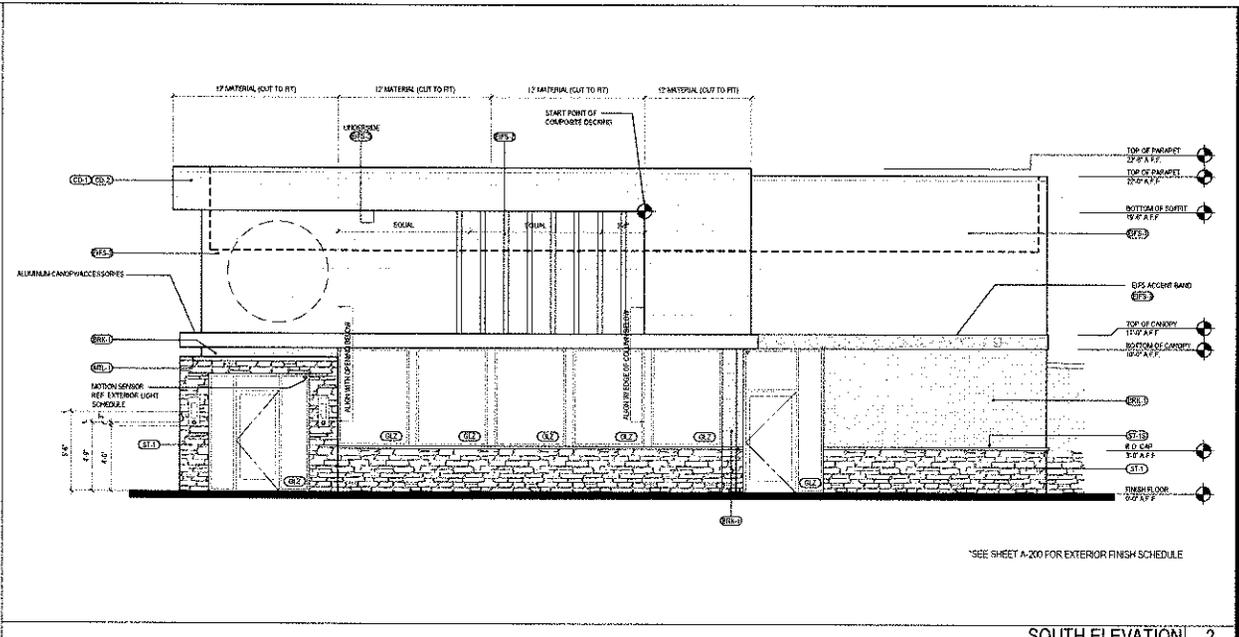
TRUE WARM & WELCOME 2300

EXTERIOR FINISH SCHEDULE

NO.	MANUFACTURER	MPOR	DESCRIPTION	FINISH
010	T&B		SEAL	
011	PTC	STORHENSENSENCE SYSTEM	SW 1600 INDOOR SIDE	PNE
012	STO	STORHENSENSENCE SYSTEM	SW 1600 CITY SCOPE	PNE
013	STO	STORHENSENSENCE SYSTEM	SW 1600 ROOM ONE	PNE
014	CORONADO STONE PRODUCTS	SW SERIES	#1 GREY	
015	CORONADO STONE PRODUCTS	INDUSTRIAL LEDGE	SMALL GREY	
016	HERRON	HORIZON	LINE	POLYMER GEL COVERAGE RANDOM (POLY-GROUDS)
017	HERRON	HORIZON	TUDOR BROWN	POLYMER GEL COVERAGE RANDOM (POLY-GROUDS)
018	EXCEPTIONAL METALS		"PANDA EXPRESS SIGNAGE"	
019			CLEAR GLAZING	

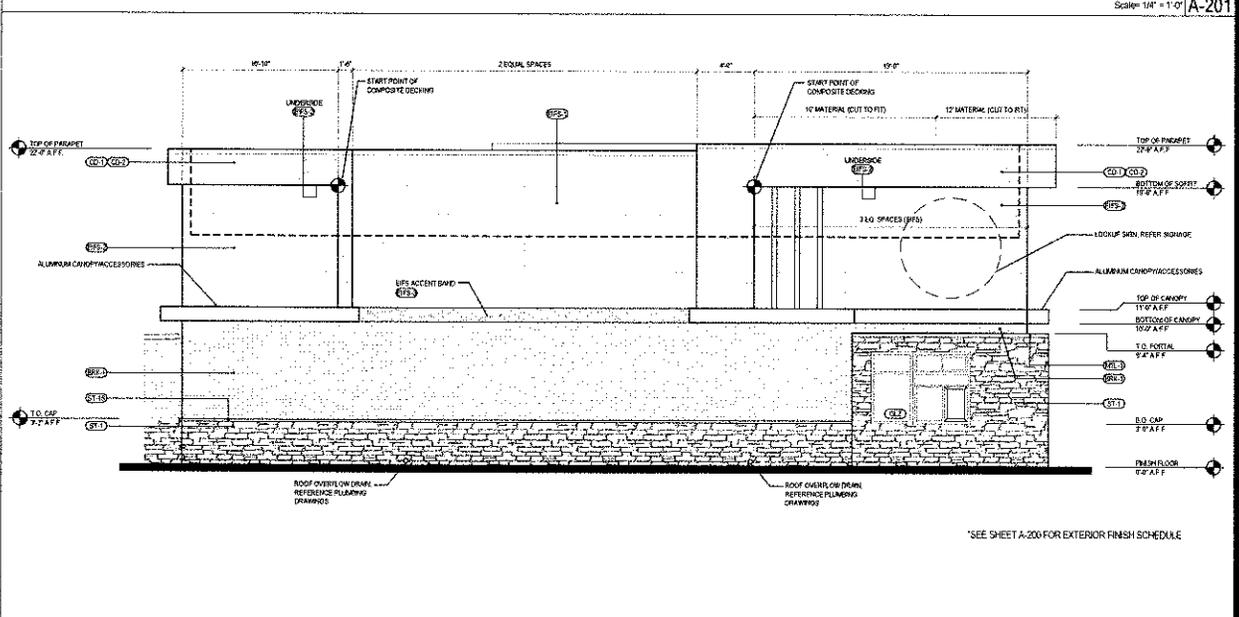
FACADE COVERAGE

	FRONT ELEV. (WEST)	REAR ELEV. (EAST)	ENTRY ELEV. (SOUTH)	SHOPE THRU ELEV. (NORTH)
BRICK	12%	24%	10%	38%
	116 SF	237 SF	137 SF	386 SF
BFS	38%	48%	18%	4%
	352 SF	328 SF	296 SF	541 SF
STONE	18%	9%	14%	18%
	174 SF	79 SF	192 SF	248 SF
HERRON	15%	9%	11%	11%
	147 SF	72 SF	184 SF	150 SF



*SEE SHEET A-200 FOR EXTERIOR FINISH SCHEDULE

SOUTH ELEVATION 2
Scale: 1/4" = 1'-0" A-201



*SEE SHEET A-200 FOR EXTERIOR FINISH SCHEDULE

NORTH ELEVATION 1
Scale: 1/4" = 1'-0" A-201



PANDA EXPRESS, INC.
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Facsimile: 626.372.8258

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REVISIONS:

ISSUE DATE:
PC SUBMITTAL 11-04-2020

DRAWN BY: CP

PANDA PROJECT # D7058
ARCH PROJECT # JCD116X453

NORR

PANDA EXPRESS
TRUE WARM & WELCOME 2300
7555 HICKORY HIGHWAY
WILLOWBROOK, IL 60527

A-201

EXTERIOR ELEVATIONS

TRUE WARM & WELCOME 2300/RS

FACADE COVERAGE

	FRONT ELEV (WEST) 977 SF	REAR ELEV (EAST) 854 SF	ENTRY ELEV (SOUTH) 1,255 SF	DRIVE THRU ELEV (NORTH) 1,365 SF
BRICK	17%	34%	39%	33%
GLASS	11%	10%	13%	14%
STUCCO	30%	44%	18%	49%
CONCRETE	22%	12%	30%	4%
TOTAL	100%	100%	100%	100%



PERSPECTIVE FACING SOUTHWEST | 2

Scale - NTS | A-202



PERSPECTIVE FACING NORTHEAST | 1

Scale - NTS | A-202



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Telephone: 626 706 6800
Facsimile: 626 777 6285

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REVISIONS	DATE	DESCRIPTION

ISSUE DATE

PG SUBMITTAL	11-04-2020
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DRAWN BY CP

PANDA PROJECT # 07058
ARCH PROJECT # JCD1180453



PANDA EXPRESS
TRUE WARM & WELCOME 2300
7505 KINGERY HIGHWAY
WILLOWBROOK, IL 60527

A-202

EXTERIOR
PERSPECTIVES

TRUE WARM & WELCOME 2300 R3



PANDA EXPRESS INC.
1983 Walnut Grove Ave.
Rosemead, California
91770
Telephone: 626.789.9809
Facsimile: 626.772.8298

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REVISIONS

NO.	DESCRIPTION	DATE

ISSUE DATE

PC SUBMITTAL 11-24-2020

DRAWN BY: CP

PANDA PROJECT #: 07058
ARCH PROJECT #: JCD1180451

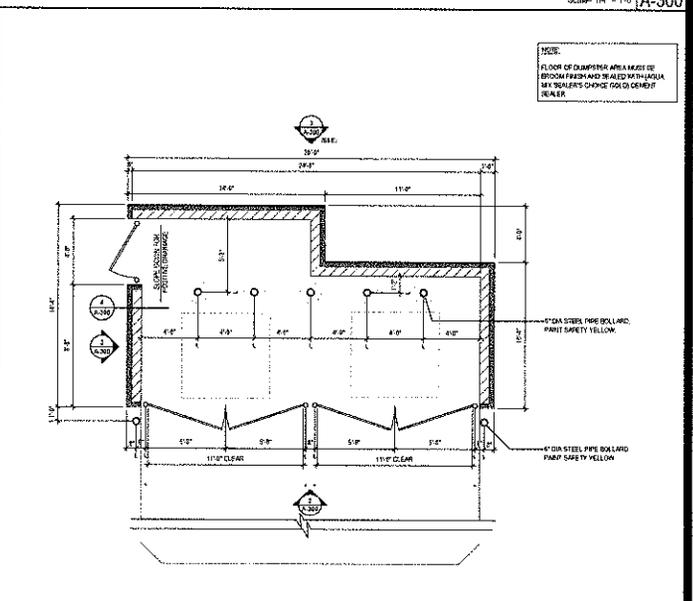
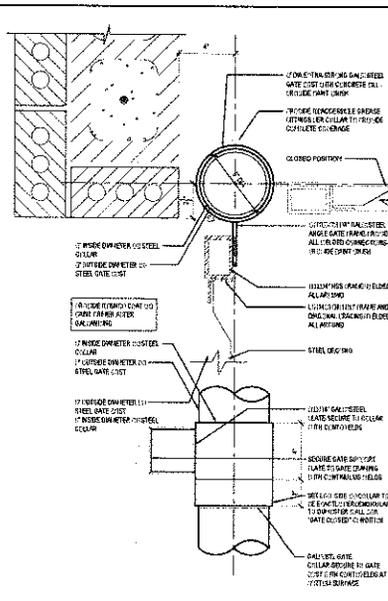
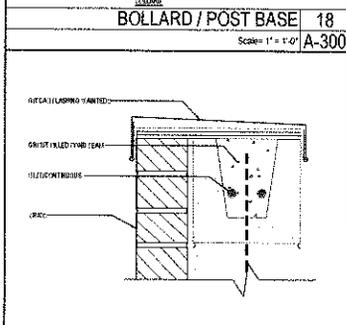
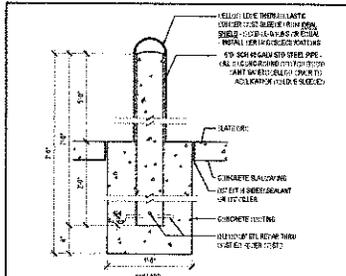
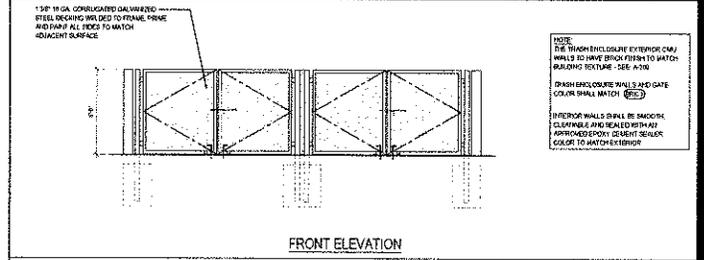
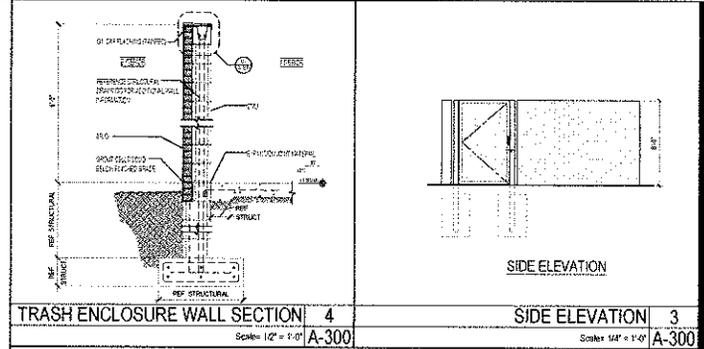
NORR

PANDA EXPRESS
TRUE WARM & WELCOME 2300
7525 KINGSERY HIGHWAY
WILLOWBROOK, IL 60127

A-300

TRASH ENCLOSURE
DETAILS

TRUE WARM & WELCOME 2300 P3





Proposed Sign Package
Willowbrook, IL
7505 S. Kingery
D7058

12.19.2018

Revisions:

- 01.07.2019 | Added new site plan, removed rear and drive thru wall signs
- 01.08.2019 | Added new site plan, added S8, added new elevations
- 03.04.2019 | Added new site plan and changed S2 placement
- 03.12.2019 | Added new site plan and added signs
- 03.27.2019 | Removed short storefront lockup logo and drive thru left side lockup logo
- 03.28.2019 | Added new site plan
- 10.30.2020 | Added new site plan
- 11.02.2020 | Adjusted code balances for S3, S4, S9
- 12.16.2020 | Added S11
- 01.19.2021 | Revised size of D8 faces, added brick base to monument

C59897

Code Summary

SIGN	LOCATION	TYPE	HEIGHT	WIDTH	ALLOWED	PROPOSED	BALANCE
S1	SHORT STOREFRONT	STACKED RED LETTERSET	2'-6"	13'-5 1/2"	320	53.19	266.81
S2	ENTRANCE	LOCKUP LOGO	6'-0"	6'-0"	266.81	36	230.81
S3	BUILDING REAR	LOCKUP LOGO	6'-0"	6'-0"	230.81	36	194.81
S4	DRIVE THRU	LOCKUP LOGO	6'-0"	6'-0"	194.81	36	158.81
S9	STREET	MONUMENT	12'-0"	6'-0"	158.81	72	86.81

NOTE: Exception required for two additional wall signs

Code Allowance

1.25 sq ft X 256 = 320 sq ft allowed for entire lot.

Elevation

Long Storefront	320 aggregate
Short Storefront	320 aggregate

Allowed

Elevation

Drive Thru	320 aggregate
Rear	320 aggregate

Allowed



Revisions:

Removed S3 & S4 / KB / 1.7.19
 Added red note / KB / 3.4.19
 Added new signs / KB / 3.13.19

Removed S2 and S3 / KB / 3.27.19

Added illuminated cabinets (10/30/20) PB
 Adjusted code balances | PV | 11.02.2020
 X

File Location:
 Drive/Clients/

STND
 CSTM



Date: 12.19.2018

Designer: KB PM: MC

City/State: Willowbrook, IL

Address: 7505 S. Kingery

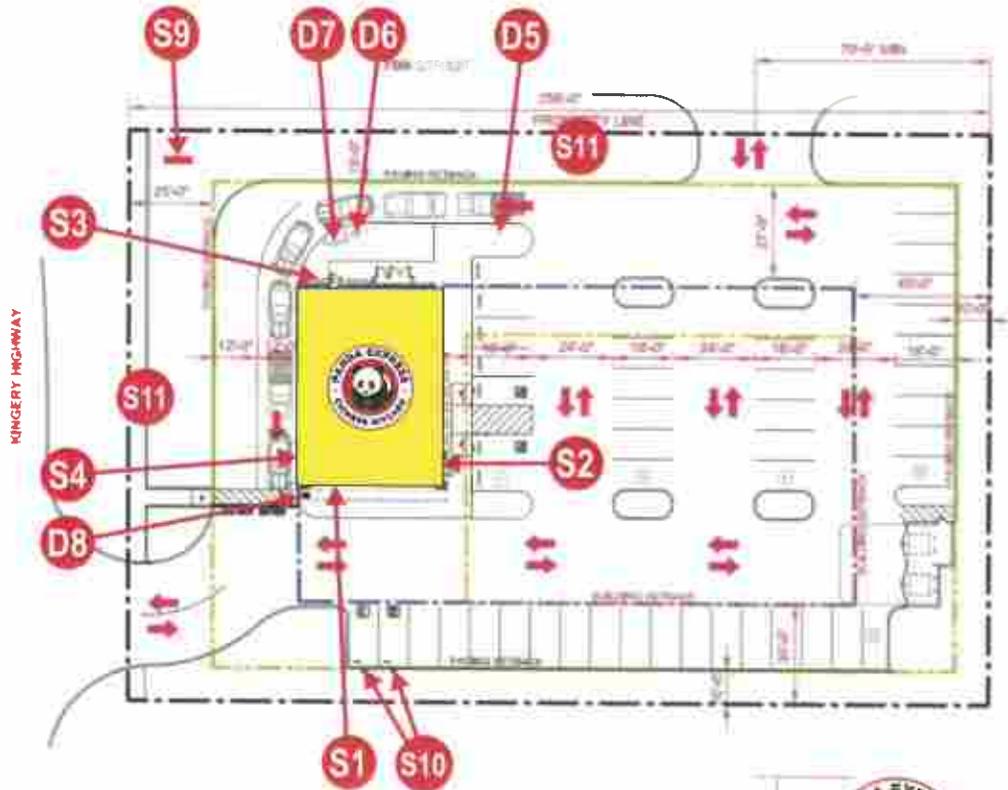
Drawing #

C59897 - 6

OE #

1200111

Site Plan



PANDA EXPRESS



S1 2'-6" stacked red letterset

S2

S3

S4

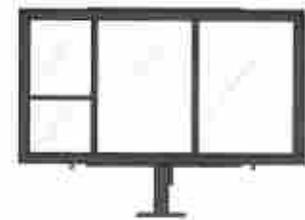
6' Lock Up Logo



D5



D6



D7



D8

3'-6"



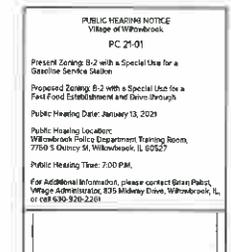
12'-0"

S9

Monument



S10



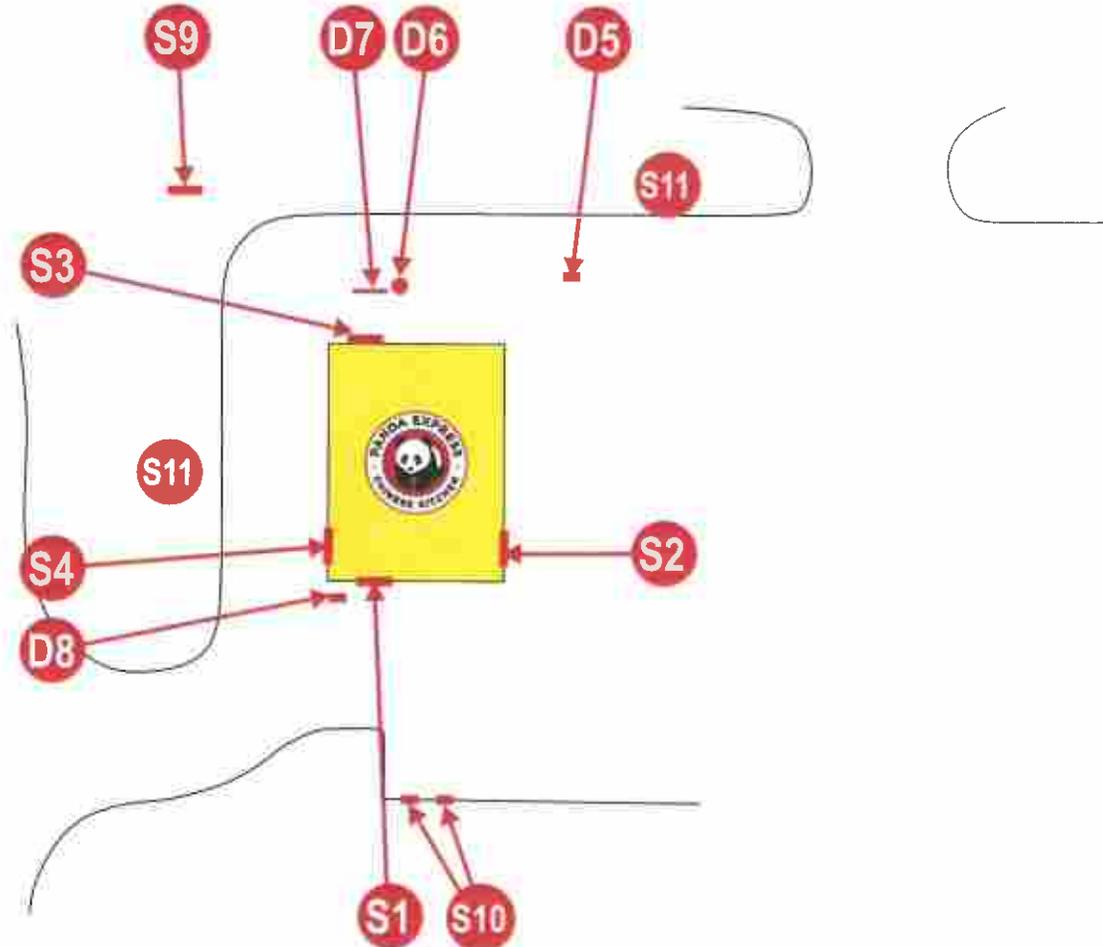
S11



Revisions:	Added new site plan / KB / 3.13.19	File Location:	STND
Added new site plan / KB / 1.7.19	Removed S2 and S3 / KB / 3.27.19	Drive/Clients/	CSTM
Added new site plan / KB / 1.8.19	Added new site plan / KB / 3.28.19	<input type="checkbox"/> AS	<input type="checkbox"/> CR
Added new site plan / KB / 3.4.19	Added new site plan (10/30/20) PB	<input type="checkbox"/> EN	

Date:	12.19.2018	City/State:	Willowbrook, IL	Drawing #	C59897-7
Designer:	KB	PM:	MC	Address:	7505 S. Kingery
				OE #	111708

Site Plan



Revisions:
Added new site plan / KB / 1.7.19
Added new site plan / KB / 1.8.19
Added new site plan / KB / 3.4.19

Removed S2 and S3 / KB / 3.27.19
Added new site plan / KB / 3.28.19
Added new site plan (10/30/20) PB
X

File Location:	STND	___
Drive/Clients/	CSTM	___
<input type="checkbox"/> AS	<input type="checkbox"/> CR	<input type="checkbox"/> EN

Date:	12.19.2018
Designer:	KB
PM:	MC

City/State:	Willowbrook, IL
Address:	7505 S. Kingery

Drawing #	C59897-6
OE #	120191

Elevations

Scale | 3/32" = 1'



Scale | 1/4" = 1'

SIGN	LOCATION	TYPE	HEIGHT	WIDTH	ALLOWED	PROPOSED	BALANCE
S1	SHORT STOREFRONT	STACKED RED LETTERSET	2'-6"	13'-5 1/2"	320	53.19	266.81



Revisions:	
Added new elevation (10/30/20) PB	X
Added new elevation / KB / 1.8.19	X
Added S2 / KB / 3.13.19	X
Removed lockup logo, made letters red / KB / 3.27.19	X

File Location: STND
 Drive/Clients/ CSTM

AS CR EN

Date: 12.19.2018

Designer: KB PM: MC

City/State: Willowbrook, IL

Address: 7505 S. Kingery

Drawing # **CS9897-4**

OE # **1000000**

Elevations

Scale | 3/32" = 1'



Scale | 1/4" = 1'

SIGN	LOCATION	TYPE	HEIGHT	WIDTH	ALLOWED	PROPOSED	BALANCE
S2	DRIVE THRU	LOCKUP LOGO	6'-0"	6'-0"	205.81	36	169.81



Revisions:

Added new elevation / KB / 1.6.19	Removed 5' lockup logo on left / KB / 3.27.19
Changed to drive thru elevation / KB / 3.4.19	Added new elevation (10/30/20) PB
Added S3 & S4 / KB / 3.13.19	X
	X

File Location: STND _____
 Drive/Clients/ CSTM _____

AS CR EN

Date: 12.19.2016

City/State: Willowbrook, IL

Drawing # **C59897-5**

Designer: KB PM: MC

Address: 7505 S. Kingery

OE # **1000111**

Elevations

Scale | 3/32" = 1'



SIGN	LOCATION	TYPE	HEIGHT	WIDTH	ALLOWED	PROPOSED	BALANCE
S3	DRIVE THRU	LOCKUP LOGO	6'-0"	6'-0"	230.81	36	194.81



Revisions:	x
Revised code balance PV 11.02.2020	x
x	x
x	x

File Location:	STND
Drive/Clients/	CSTM
AS	CR
EN	

Date:	10.30.2020
Designer:	PB
PM:	MC

City/State:	Willowbrook, IL
Address:	7505 S. Kingery

Drawing #	C59897-1
OE #	12/01/20

Elevations

Scale | 3/32" = 1'



SIGN	LOCATION	TYPE	HEIGHT	WIDTH	ALLOWED	PROPOSED	BALANCE
S4	DRIVE THRU	LOCKUP LOGO	6'-0"	6'-0"	194.81	36	158.81



Revisions:	
Revised code balance PV 11.02.2020	x
x	x
x	x
x	x

File Location: STND
 Drive/Clients/ CSTM

AS CR EN

Date: 10.30.2020

City/State: Willowbrook, IL

Designer: PB PM: MC

Address: 7505 S. Kingery

Drawing #

C59897 - 1

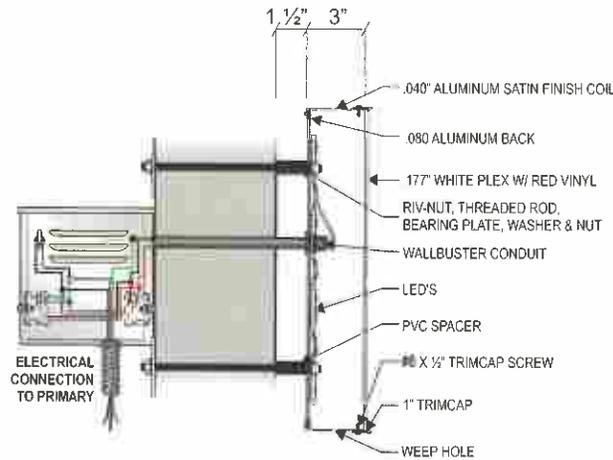
OE #

1703111

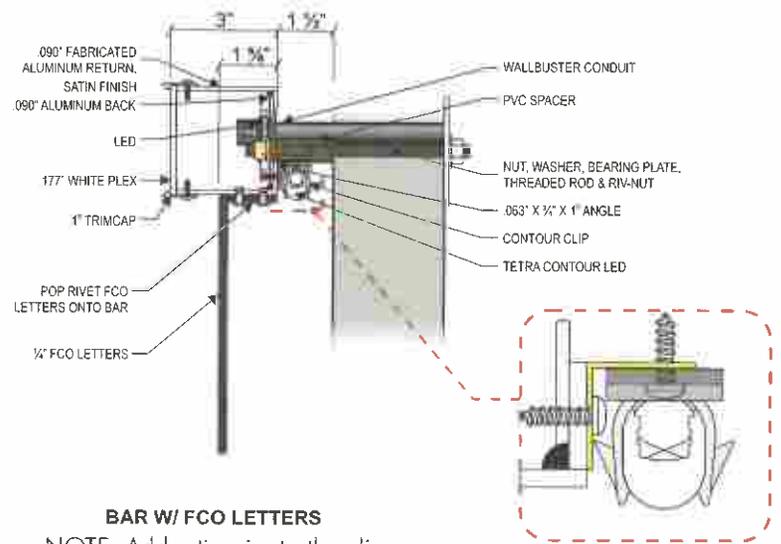
S1

Red Face-Lit Letterset - Qty: 1

Install new face-lit illuminated letterset pin-mounted to wall with aluminum FCO tagline.



DUAL LIT CHANNEL LETTER



BAR W/ FCO LETTERS

NOTE: Add a tie wire to the clip that holds the LED strip in

TETRA CONTOUR LED ATTACHMENT

GENERAL SPECIFICATIONS:

FACE-LIT LETTERS:

-  FACES..... .177" WHITE #7328 PLEX W/ 1ST SURFACE 3M #3630-33 RED
-  TRIMCAP.... 1" BLACK
-  RETURNS.. .040" X 3" BLACK / WHITE COIL PAINTED TO MATCH BLACK 7C, SATIN FINISH
- BACKS..... .080" ALUMINUM

BAR W/ FCO LETTERS:

-  FACES.....177" WHITE #7328 PLEX
-  TRIMCAP.....1" WHITE
-  RETURNS.....125" X 3" FABRICATED WHITE COIL RETURN, SATIN FINISH
- BACKS......090" ALUMINUM, INTERIOR PAINTED WHITE
- DOWNLIGHT...063" BRAKE FORMED ALUMINUM BOX BEHIND THE BAR WITH DOWNLIGHT LENS AND WHITE LED'S
- EXPRESS.....25" FCO ALUMINUM LETTER PAINTED TO MATCH BLACK 7C SATIN FINISH



	A	B	C	D	E	F	SF (F x B)
PE-CL-30R	2'-6"	13'-5 1/2"	9 1/8"	7'-7 11/16"	2 13/16"	3'-11 1/4"	53.19



Revisions:	x	
x	x	
x	x	
x	x	

File Location:	STND	
Drive/Clients/	CSTM	
 AS	 CR	 EN

Date:	10.30.2020
Designer:	PB
PM:	MC

City/State:	Willowbrook, IL
Address:	7505 S. Kingery

Drawing #	C59897
OE #	177000

S2 Face Illuminated Lockup Logo - Qty: 1

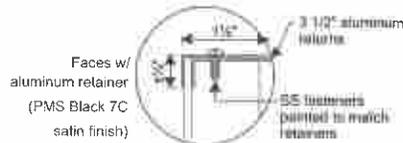
S3 Install new face-lit lockup logo as shown.

S4

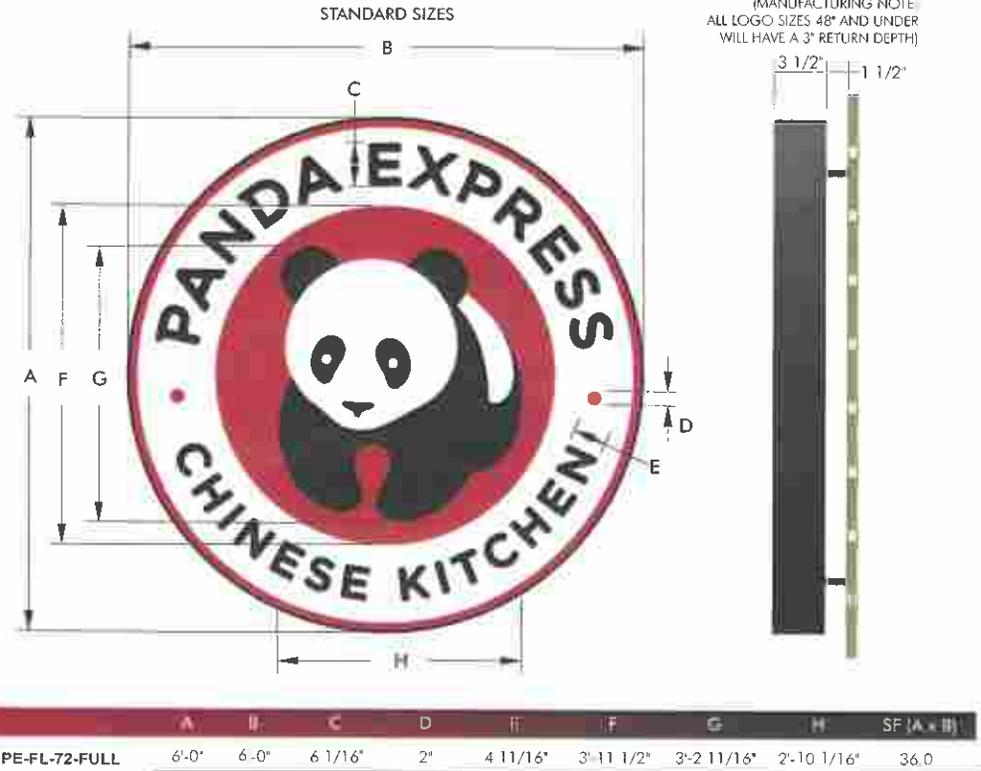
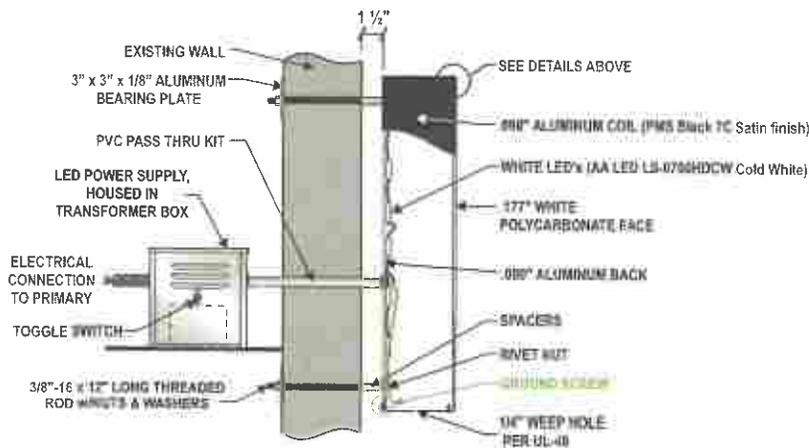
GENERAL SPECIFICATIONS:

FACES:..... 177" WHITE POLYCARBONATE W/APPLIED VINYL GRAPHICS
 RETAINER:.... 1" BLACK TRIMCAP / 1 1/2" ALUMINUM RETAINER (SEE DETAILS BELOW)
 RETURNS:.... .040" X 3" BLACK / WHITE COIL PAINTED TO MATCH BLACK 7C SATIN FINISH

COLOR SPECIFICATIONS FOR FACE



4'-0" AND OVER LOGO



MANUFACTURING NOTE
 ALL LOGO SIZES 48" AND UNDER
 WILL HAVE A 3" RETURN DEPTH



Revisions:	x
x	x
x	x
x	x

File Location:	STND	—
Drive/Clients/	CSTM	—
AS	CR	EN

Date:	10.30.2020
Designer:	PB
PM:	MC

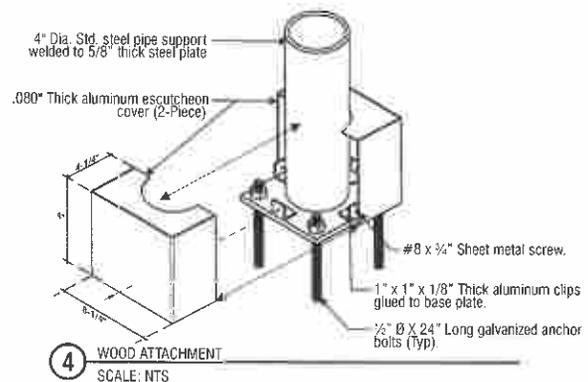
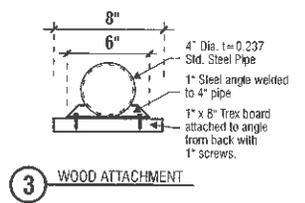
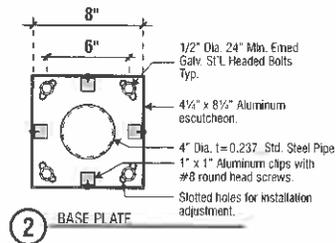
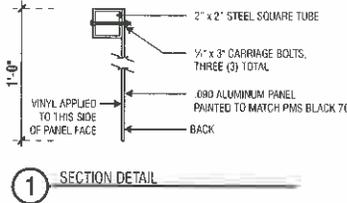
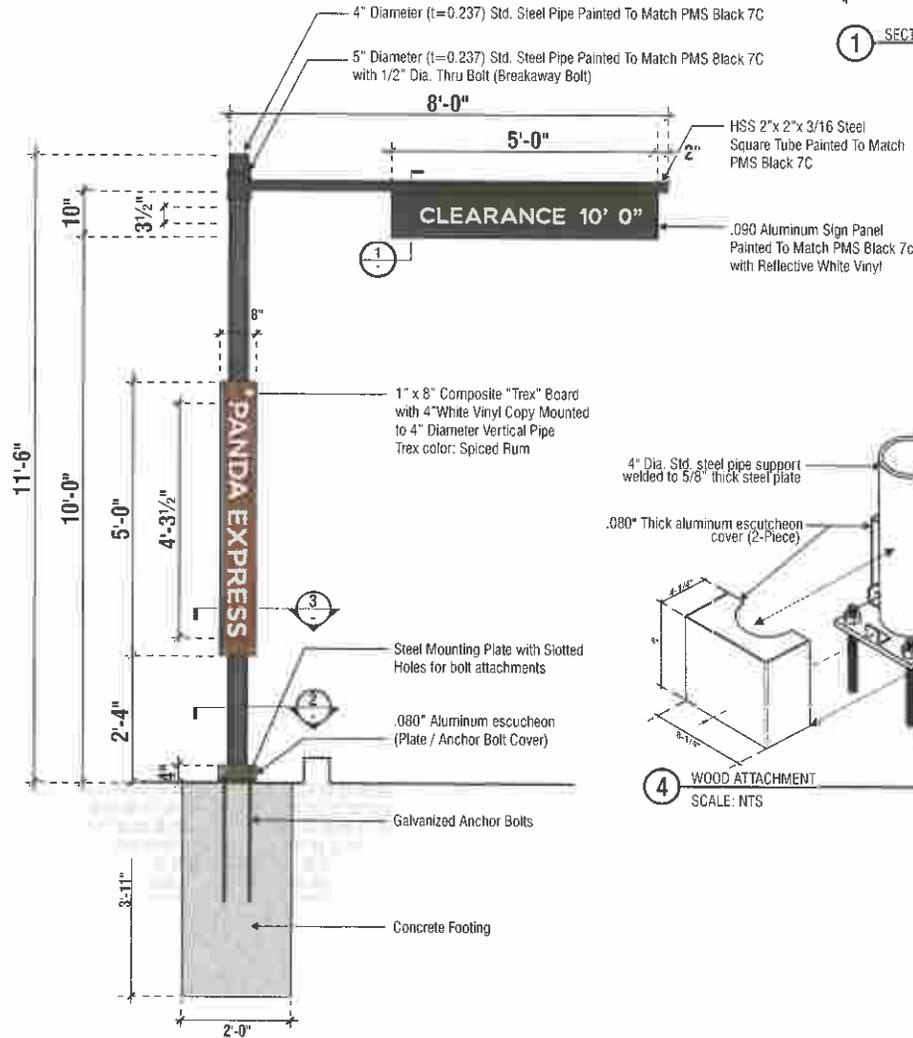
City/State:	Willowbrook, IL
Address:	7505 S. Kingery

Drawing #	C59897
OE #	132000

D5

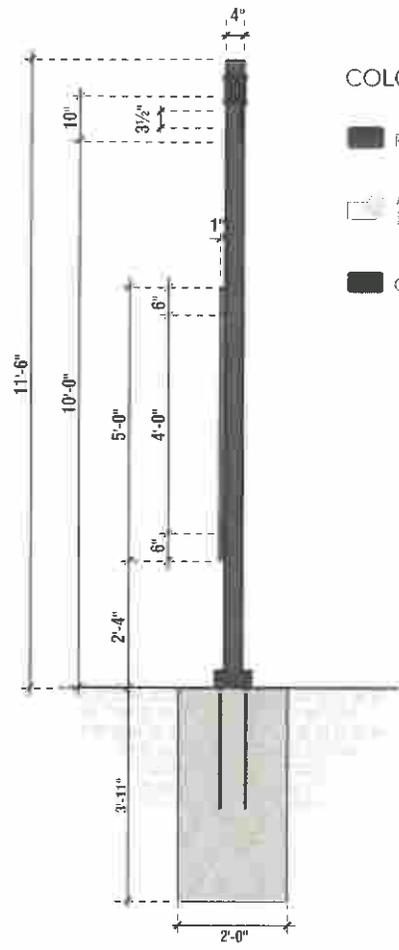
DT-L Clearance Bar - (Qty: 1)

Install new DT-L Clearance Bar sign. Refer to site plan for exact location.



COLOR SPECIFICATIONS

- Painted to match PMS Black 7C
- Arlon #4100-02 Reflective White Film
- Composite "Trex" Board



J NON-ILLUMINATED CLEARANCE BAR
 QUANTITY: ONE (1) SET REQUIRED
 SCALE: 1:40

SIDE VIEW - NON-ILLUMINATED CLEARANCE BAR
 SCALE: 1:40



Revisions:	x
x	x
x	x
x	x

File Location:	STND
Drive/Clients/	CSTM
AS	CR
	EN

Date:	10.30.2020
Designer:	PB
PM:	MC

City/State:	Willowbrook, IL
Address:	7505 S. Kingery

Drawing #	C59897
OE #	120000

D6

DT COD Canopy - (Qty: 1)

Install new DT COD Canopy per site plan for exact location.

REFER TO CONSTRUCTION DRAWINGS FOR EXACT FOUNDATION & MANUFACTURING SPECIFICATIONS

COLOR SPECIFICATIONS

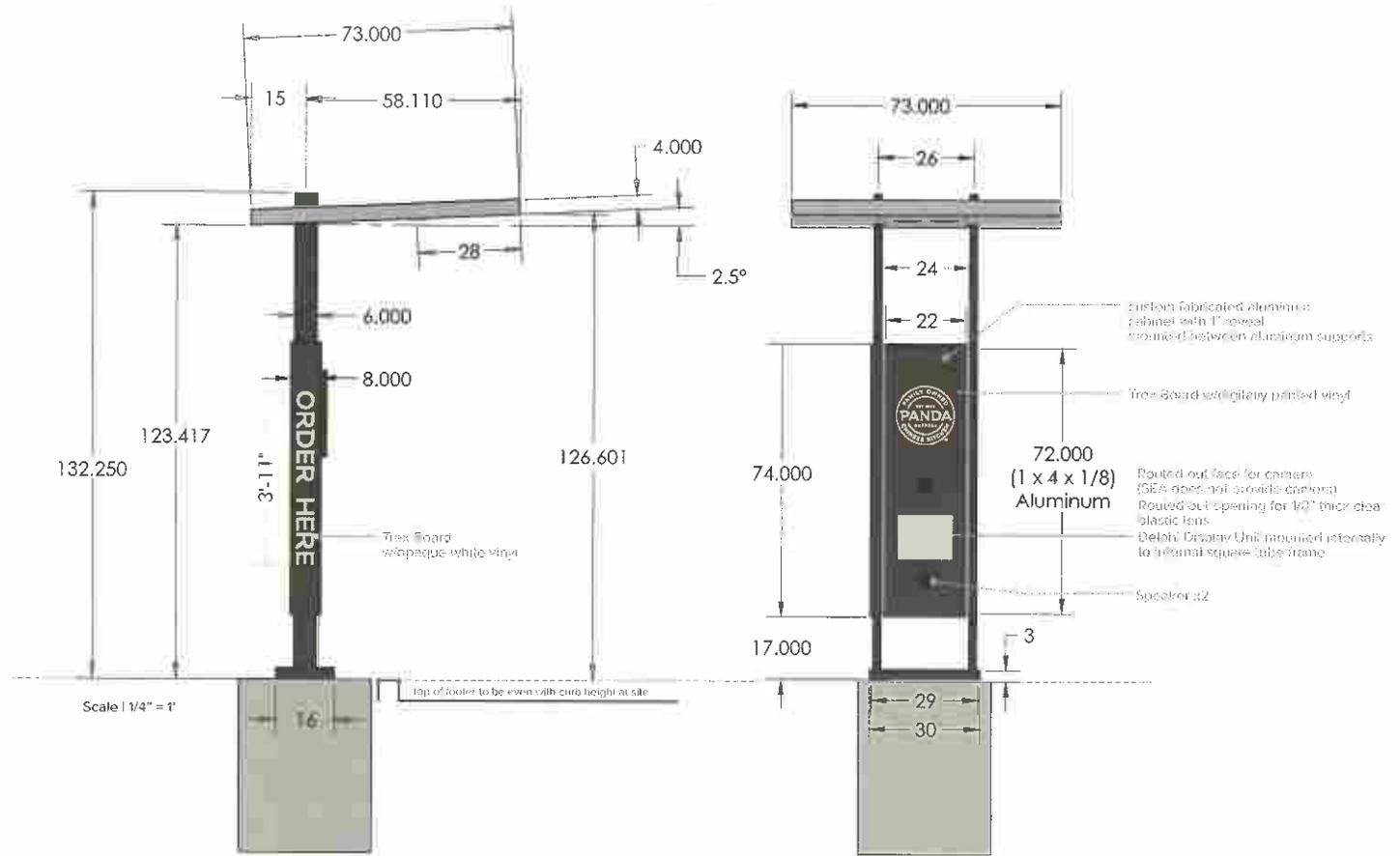
- Trex Board
<https://www.lowes.com/pd/Trex-Transcend-12-ft-Spiced-Rum-Composite-Fascia-Deck-Board/1000712838>
- Clear Vinyl Digitally Printed (FAMILY SEAL)
- Opaque White Vinyl (ORDER HERE TEXT ONLY)

CABINET CANOPY PAINT COLORS

- Aluminum painted to match PMS Black 7C
- Aluminum painted to match Matthews Brushed Silver



Trex Panel Scale | 1 1/2" = 1'



Revisions:		
x		
x		
x		
x		

File Location:	STND	
Drive/Clients/	CSTM	
AS	CR	EN

Date:	10.30.2020
Designer:	PB
PM:	MC

City/State:	Willowbrook, IL
Address:	7505 S. Kingery

Drawing #	C59897
OE #	1103001

D7

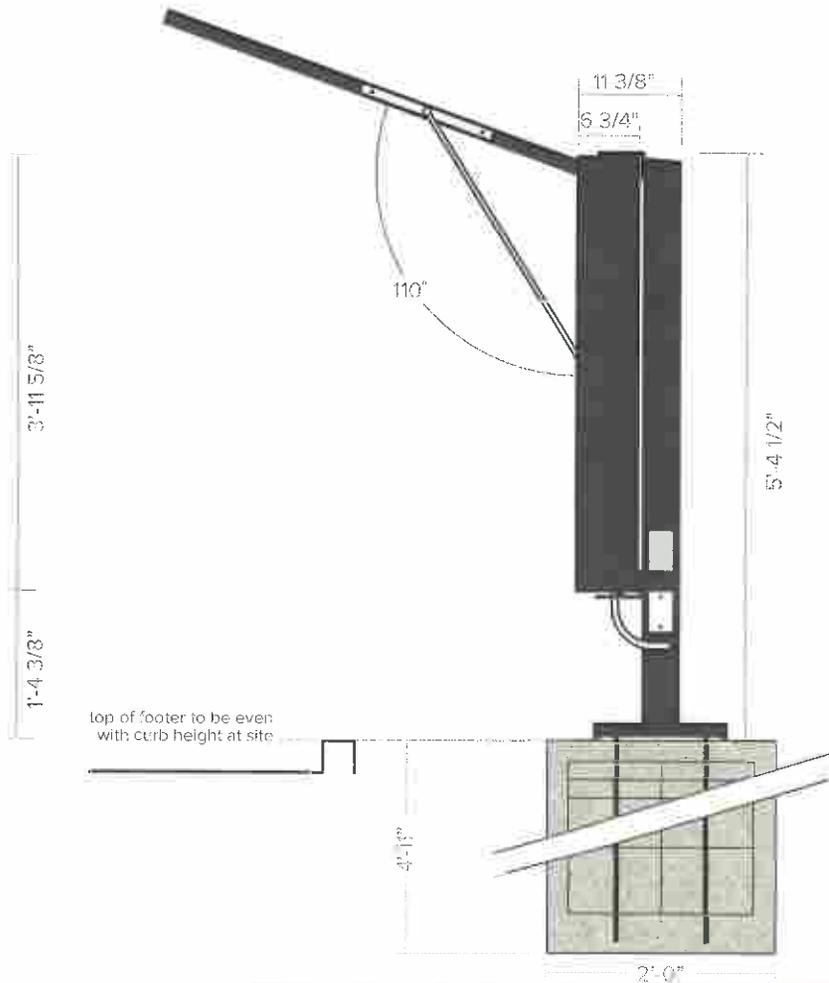
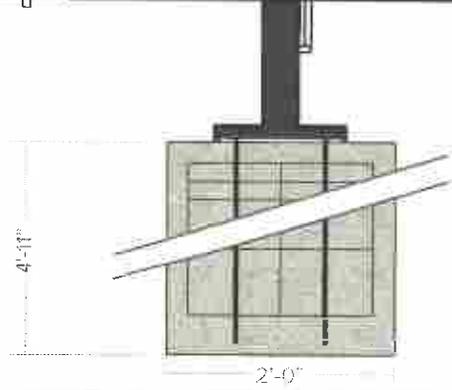
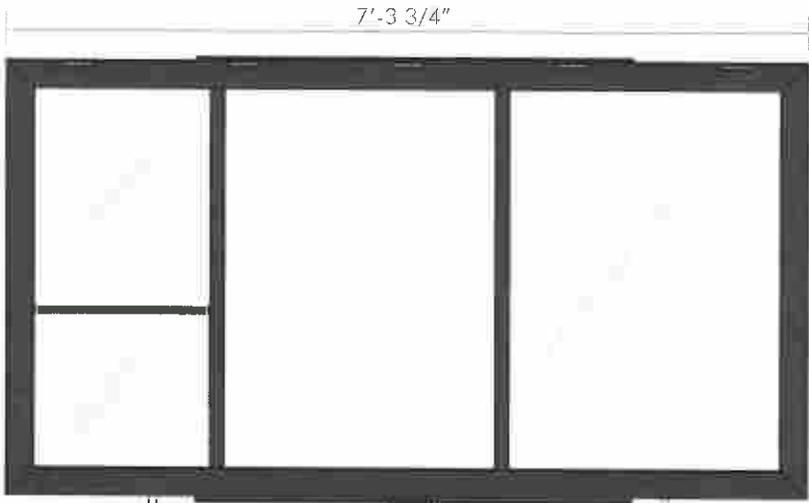
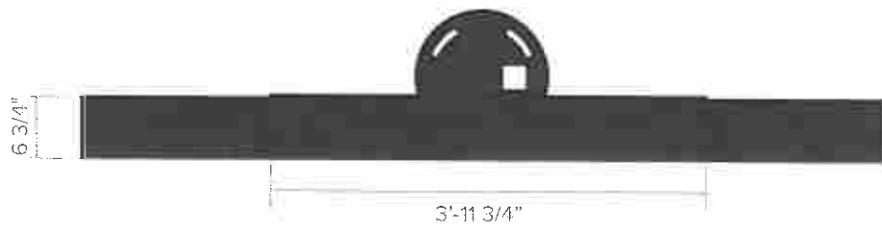
DT Menu Board - (Qty: 1)

Install new DT Menu Board. Refer to site plan for exact location.

COLOR SPECIFICATIONS

MAIN CABINET

Painted to match PMS Black 7C



Scale 1:20



Revisions:		
x		
x		
x		
x		

File Location:	STND	
Drive/Clients/	CSTM	
AS	CR	EN

Date:	10.30.2020
Designer:	PB PM: MC

City/State:	Willowbrook, IL
Address:	7505 S. Kingery

Drawing #	C59897
OE #	100100

D8

Non-Illuminated Directional - (Qty: 1)

Install new non-illuminated directional. Refer to site plan for exact location.

1'-0" x 1'-11" = 1.92 sq.ft.
 1.92 sq.ft. x 2 = **3.84 sq. ft. total**



Scale | 3/4" = 1'

COLOR SPECIFICATIONS

- PAINT**
- PMS Black 7C
 - Composite "Trex" Board
- VINYL**
- Arlon #4100-02 Reflective White Film

Reverse side



Revisions:	
Calculated face sq ft - KB - 119.21	x
	x
	x
	x

File Location:	STND
Drive/Clients/	CSTM
AS	CR
	EN

Date:	10.30.2020
Designer:	PB
PM:	MC

City/State:	Willowbrook, IL
Address:	7505 S. Kingery

Drawing #	C59897-1
OE #	100000

S9

Custom Illuminated Monument - (Qty: 1)

Install new internally illuminated double-faced monument sign.
Refer to site plan for exact location.
Sign is custom due to height.



COLOR SPECIFICATIONS

TOP CLADDING

 .080" Aluminum sign cladding
Color: To match PMS Black 7C

RETAINER

 .080" Routed aluminum ring with .040" welded return
Color: To match PMS Black 7C on return edge only
To match PMS 711C on face

FACE

 .125" Routed aluminum cladding
Color: To match PMS Black 7C

BASE/SKIRT

 Brick to match building

ILLUMINATED PANDA LOGO

 Bayer Makrolon .77"
#7328 White LD Polycarbonate

 3M Scotchcal #3630-33 Red film

 3M #3630-22 Black film

"DRIVE THRU" COPY

 Bayer Makrolon .77"
#7328 White LD Polycarbonate



Revisions:	
Changed to brick base - KB - 1.19.2021	X
X	X
X	X

File Location:	STND
Drive/Clients/	CSTM
<input type="checkbox"/> AS	<input checked="" type="checkbox"/> CR
	<input checked="" type="checkbox"/> EN

Date:	12.19.2018
Designer:	KB
PM:	MC

City/State:	Willowbrook, IL
Address:	7505 S. Kingery

Drawing #	C59897-1
OE #	100000

S10

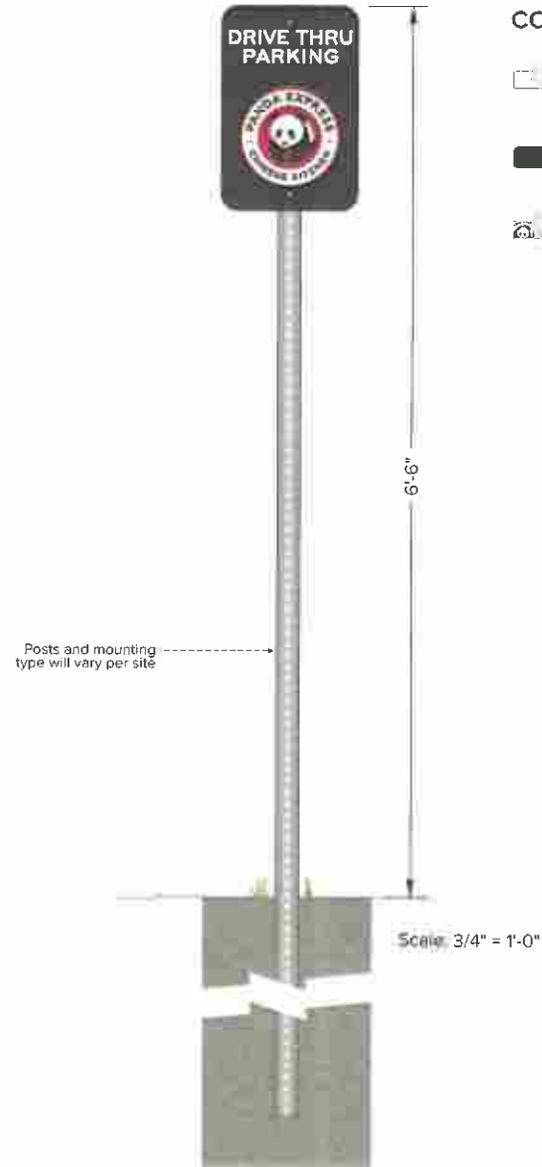
Aluminum Parking Panel - (Qty: 2)

Install new aluminum parking panels to existing posts OR with new posts.
 Refer to site plan for exact location.
 Verify ground mounting type.



Scale | 1:4

PE-Park-DT



COLOR SPECIFICATIONS

- 3M #680-10 White Scotchlite reflective vinyl
- Background printed to match PMS Black 7C
- Digitally printed graphics



Revisions:	
Revised copy / KB / 3.27.19	X
X	X
X	X

File Location:	STND
Drive/Clients/	CSTM
AS	CR
EN	

Date:	12.19.2018
Designer:	KB PM: MC

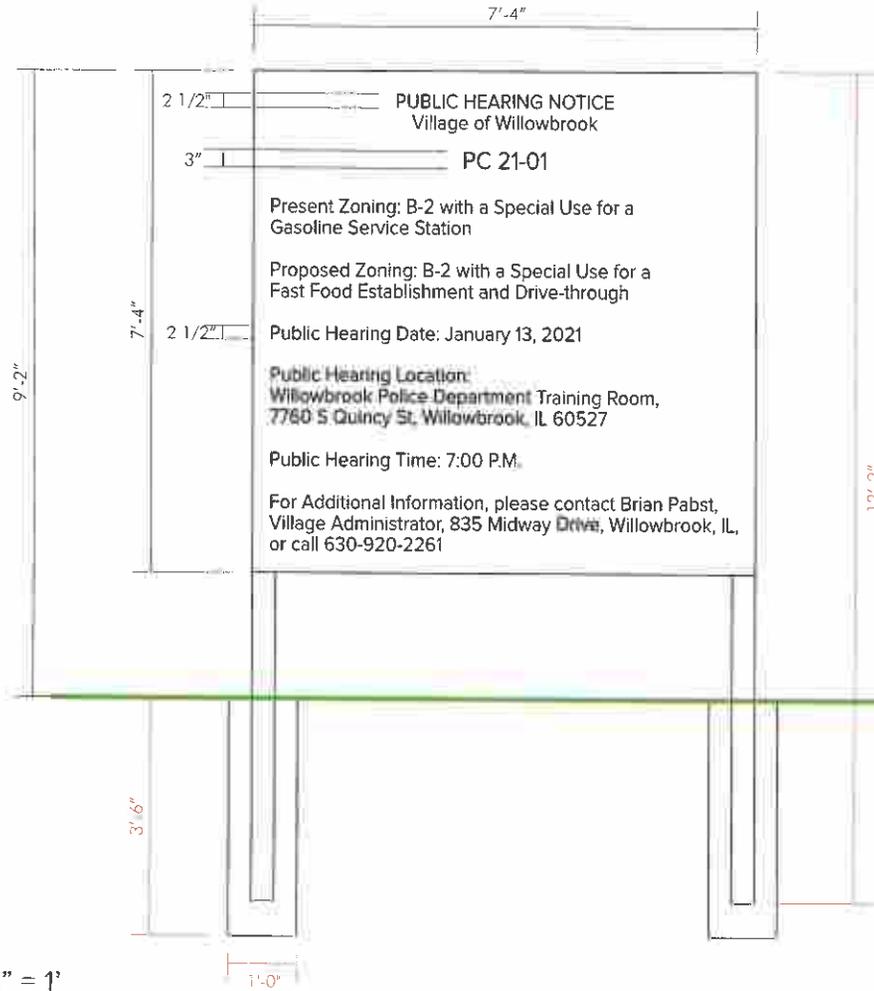
City/State:	Willowbrook, IL
Address:	7505 S. Kingery

Drawing #	C59897-1
OE #	120390

S11

Custom S/F Post and Panel Sign (Qty. 2)

Install new custom s/f post and panel signs as shown. 4" square wood posts painted white with 1/2" thick white MDO panel with black vinyl graphics applied. Faces to be screw mounted to posts. Direct bury.



Scale 1 3/8" = 1'



Revisions:	
Added to book KB - 12.16.2020	X
X	X
X	X

File Location:	STND
Drive/Clients/	CSTM
<input type="checkbox"/> AS	<input checked="" type="checkbox"/> CR
<input type="checkbox"/> EN	

Date:	12.19.2018
Designer:	KB PM: MC

City/State:	Willowbrook, IL
Address:	7505 S. Kingery

Drawing #	C59897
OE #	120100



WILLOWBROOK PANDA EXPRESS

PANDA PROJECT #D7058

7505 KINGERY HIGHWAY
WILLOWBROOK, IL 60527

FINAL ENGINEERING PLANS

DEVELOPMENT TEAM

CURRENT OWNER
TRUE NORTH ENERGY, LLC
10346 BRECKSVILLE ROAD
BRECKSVILLE, OH 44141
PHONE: (440) 792-4200
CONTACT: RYAN HOWARD

DEVELOPER
PANDA EXPRESS, INC.
1883 WALNUT GROVE AVENUE
ROSEMEAD, CA 91770
PHONE: (847) 477-5125
FAX: (626) 372-8280
CONTACT: HAKIM YALA

**CIVIL ENGINEER/SURVEYOR/
LANDSCAPE ARCHITECT**
ATWELL, LLC
1250 EAST DIEHL ROAD, SUITE 300
NAPERVILLE, IL 60563
PHONE: (630) 577-0800
FAX: (630) 577-0900
CONTACT: BRIAN STYCK, P.E.

ARCHITECT
NORR
150 W. JEFFERSON AVENUE, SUITE 1300
DETROIT, MI 48225
PHONE: (313) 324-3085
CONTACT: DINA MEKAWY

GOVERNING AGENCIES & UTILITY CONTACTS

MUNICIPALITY/ZONING
VILLAGE OF WILLOWBROOK
835 MIDWAY DRIVE
WILLOWBROOK, IL 60527
PHONE: (630) 920-2230
CONTACT: ANN CHOI

**SOIL EROSION/STORMWATER
ENGINEERING DEPARTMENT**
835 MIDWAY DRIVE
WILLOWBROOK, IL 60527
PHONE: (630) 920-2230
CONTACT: DAN LYNCH

WATER SEWER
VILLAGE OF WILLOWBROOK WATER
DIVISION
835 MIDWAY DRIVE
WILLOWBROOK, IL 60527
PHONE: (630) 920-2238
CONTACT: ANDREW PASSERO

SEWER
DUPAGE COUNTY PUBLIC WORKS
421 N. COUNTY FARM ROAD
WHEATON, IL 60187
PHONE: (830) 407-8800
CONTACT: EDWARD BUGA

ELECTRIC
COMMON WEALTH EDISON
PHONE: 1 (877) 429-6331

GAS
NICOR - WATCH AND PROTECT
REPRESENTATIVE
PHONE: (847) 344-0111
CONTACT: RAHUL

TELEPHONE
AT&T
PHONE: (630) 573-5887
CONTACT: ROBERT GILBERT

COMCAST
PHONE: (224) 229-5849
CONTACT: ROBERT STROLL

LEGAL DESCRIPTION

FIRST AMERICAN TITLE INSURANCE COMPANY
COMMITMENT NO.: NCS 968541-CLE
COMMITMENT DATE: AUGUST 28, 2020

THE NORTH 170.0 FEET OF THAT PART OF THE SOUTHEAST 1/4 OF SECTION 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING EAST OF THE EAST RIGHT OF WAY OF STATE ROUTE 83, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT 102.85 FEET EAST OF THE NORTHWEST CORNER OF SAID SOUTHEAST 1/4, THENCE EAST ALONG THE NORTH LINE OF SAID SOUTHEAST 1/4, A DISTANCE OF 256.24 FEET; THENCE SOUTH, ALONG A LINE PARALLEL WITH THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170 FEET; THENCE WEST, ALONG A LINE PARALLEL WITH THE NORTH LINE OF SAID SOUTHEAST 1/4, A DISTANCE OF 256.24 FEET TO THE INTERSECTION OF SAID PARALLEL LINE WITH THE EAST RIGHT OF WAY OF SAID STATE ROUTE 83, WHICH POINT IS 102.45 FEET EAST OF THE WEST LINE OF SAID SOUTHEAST 1/4; THENCE NORTH, ALONG THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170.0 FEET TO THE POINT OF BEGINNING, IN DUPAGE COUNTY, ILLINOIS.

BENCHMARKS:

SOURCE BENCHMARK:
NGS 0135 / PID (DK3296)

TO REACH THE STATION FROM THE INTERSECTION OF SR 83 AND PLAINFIELD ROAD, GO EAST ALONG PLAINFIELD ROAD FOR APPROXIMATELY 0.5 MI (0.8 KM) TO THE STATION ON THE RIGHT. THE STATION IS LOCATED AT THE SOUTHEAST CORNER OF THE INTERSECTION OF PLAINFIELD ROAD AND MADISON STREET.

THE STATION IS 7.0 FT (2.1 M) SOUTH OF THE CENTERLINE OF PLAINFIELD ROAD AND 38.5 FT (11.7 M) EAST OF THE CENTERLINE OF MADISON STREET. THE MONUMENT IS A 3 INCH (Ø CM) BRASS DISK ON THE BASE OF A TRAFFIC SIGNAL, 0.8 FT (Ø 2 M) ABOVE GRADE.

ELEVATION: 732.18 (NAVD 88)

SITE BENCHMARKS AND CONTROL
BM #204

SET BENCHMARK ON THE SOUTHERLY BOLT, UNDER THE WORD "CITY" ON A HYDRANT ON THE SOUTH SIDE OF 75TH STREET, 308' EAST OF THE INTERSECTION OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY
ELEVATION: 719.58 (NAVD 88)

CONTROL #1

SET CAPPED IRON ROD AT THE SOUTHEAST CORNER OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY, 15' SOUTHEAST OF A TRAFFIC SIGNAL AND 18' EAST OF THE BACK OF CURB OF ROUTE 83
N: 1852569.52 (NAD 83)
E: 1090379.82 (NAD 83)

CONTROL #2

SET CUT "X" IN A CONCRETE SIDEWALK ON THE EAST SIDE OF ROUTE 83, ROBERT KINGERY HIGHWAY, 235' SOUTH OF THE INTERSECTION OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY, 20' EAST OF THE BACK OF CURB OF ROUTE 83.
N: 1852417.41 (NAD 83)
E: 1090392.03 (NAD 83)

CONTROL #5

SET CUT "X" IN A CONCRETE SIDEWALK ON THE SOUTH SIDE OF 75TH STREET, 315' EAST OF THE INTERSECTION OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY, AND 7' NORTH OF A FIRE HYDRANT
N: 1852623.35 (NAD 83)
E: 1090603.32 (NAD 83)

SITE INFORMATION:

THE ABOVE DESCRIBED LOTS AND INTERESTS VARY FROM THE VILLAGE CODE AND WILL REQUIRE A VARIANCE

JURISDICTION: VILLAGE OF WILLOWBROOK
ZONING DISTRICT: B-2 COMMUNITY SHOPPING

SITE AREA CALCULATIONS:
TOTAL SITE AREA 1.0 ACRES
PERVIOUS AREA 0.29 ACRES
IMPERVIOUS AREA 0.71 ACRES

RESTAURANT BUILDING AREA 2,300 SF
TRASH ENCLOSURE AREA 307 SF
TOTAL BUILDING AREA 2,607 SF

	REQUIRED	PROVIDED
MIN. LOT AREA	2.0 ACRES	1.00 ACRES (43,561 SF)
MIN. LOT WIDTH	200'	170'
MIN. LOT DEPTH	200'	256.24'

MAX. LOT COVERAGE	50%	8.0%
MAX. BUILDING HEIGHT	30'	23'
MAX. FLOOR AREA RATIO	0.30	0.06

EXISTING BUILDINGS SETBACK	REQUIRED	PROVIDED
FRONT (WEST)	60'	61.5'
ROUTE 83 (WEST)	100'	61.5'
SIDE EXTERIOR (NORTH)	40'	52.2'
SIDE INTERIOR (SOUTH)	100'	58.2'
REAR (EAST)	30'	127.6'

PROPOSED BUILDING SETBACK	REQUIRED	PROVIDED
FRONT (WEST)	60'	50.0'
ROUTE 83 (WEST)	100'	50.0'
SIDE EXTERIOR (NORTH)	60'	43.4'
SIDE INTERIOR (SOUTH)	30'	63.6'
REAR (EAST)	40'	162.8'

PARKING SETBACK	REQUIRED	PROVIDED
FRONT (WEST)	25'	25'
SIDE EXTERIOR (NORTH)	15'	15'
SIDE INTERIOR (SOUTH)	10'	10'
REAR (EAST)	10'	10.3'

DRIVE AISLE WIDTH: 24' UNLESS OTHERWISE NOTED

FLOOD HAZARD: ZONE X
FIRM MAP NO. 17043C0189U, DATED 8/1/2019

EXISTING INFORMATION:
EXISTING SITE CONDITIONS ARE BASED ON AN ALTA/TPS LAND TITLE SURVEY BY ATWELL, LLC DATED 9/23/2020

SITE LIGHTING:
PHOTOMETRICS DESIGNED BY OTHERS. POLE LOCATIONS ARE SHOWN FOR REFERENCE ONLY. CONTRACTOR SHALL VERIFY FINAL LOCATIONS WITH PHOTOMETRICS PLAN AND OWNER PRIOR TO CONSTRUCTION.

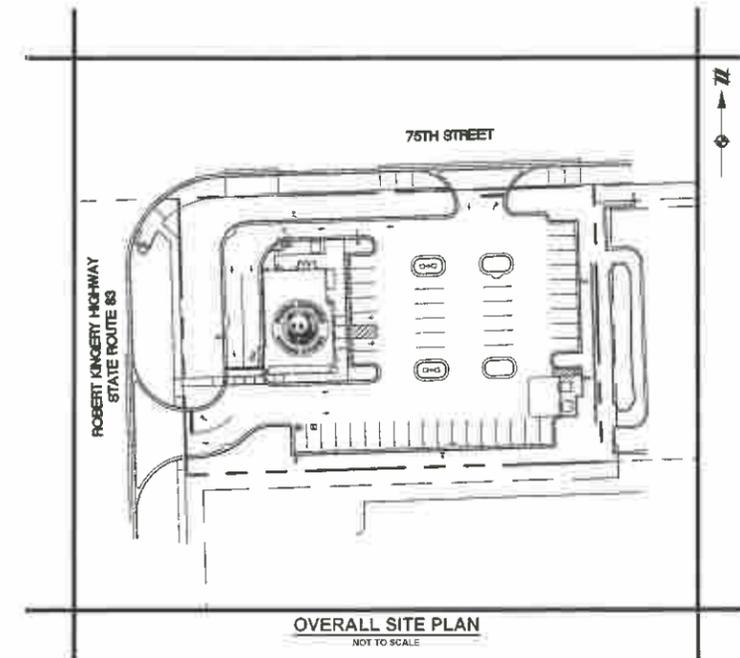
PARKING DATA

LOADING BERTHS	REQUIRED	PROVIDED
	0	0

REQUIRED PARKING CALCULATION:
1 PARKING SPACE PER 100 SF OF FLOOR AREA
1 SPACE/100 SF = 2,300 SF = 23 SPACES

TOTAL REQUIRED SPACES 23 SPACES

PROVIDED	REQUIRED
REGULAR (9' X 18')	42 SPACES
DRIVE THRU PICKUP (9' X 18')	2 SPACES
ADA (9' X 18')	2 SPACES
TOTAL PROVIDED SPACES	46 SPACES



SHEET INDEX

C01.0	COVER SHEET
C01.1	GENERAL NOTES
C01.2	GENERAL NOTES
C02.0	EXISTING CONDITIONS
C02.1	DEMOLITION PLAN
C03.0	SOIL EROSION AND SEDIMENTATION CONTROL PLAN
C03.1	SOIL EROSION AND SEDIMENTATION CONTROL NOTES AND DETAILS
C04.0	SITE LAYOUT PLAN
C04.1	DETAILED SITE LAYOUT PLAN
C05.0	GRADING PLAN
C05.1	DETAILED GRADING
C06.0	STORM SEWER PLAN
C07.0	UTILITY PLAN
C07.1	DETAILED UTILITY PLAN
C08.0	CONSTRUCTION DETAILS
C08.1	CONSTRUCTION DETAILS
C08.2	CONSTRUCTION DETAILS
C08.3	CONSTRUCTION DETAILS
C08.4	CONSTRUCTION DETAILS
L01.0	LANDSCAPE PLAN
L01.1	LANDSCAPE NOTES AND DETAILS

PANDA EXPRESS STANDARD NOTES:

- THE GEOTECHNICAL INVESTIGATION PREPARED BY TERRACON CONSULTANTS, INC. DATED 12/28/2020 AND ANY SUBSEQUENT ADDENDUMS IS CONSIDERED PART OF THE CONTRACT DOCUMENTS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE THE REPORT'S RECOMMENDATIONS AND FINDINGS WITH THE OWNER, ENGINEER AND ARCHITECT PRIOR TO CONSTRUCTION. IMPLEMENTATION OF THE REPORT'S RECOMMENDATIONS MAY REQUIRE THE CONTRACTOR TO PERFORM ADDITIONAL WORK NOT SHOWN ON THE CIVIL PLANS INCLUDING BUT NOT LIMITED TO EXCAVATION, REMEDIATION, DEWATERING, COMPACTION ETC.
- CONTRACTOR SHALL COORDINATE AND VERIFY LOCATION OF ALL SIGNAGE WITH OWNER PRIOR TO CONSTRUCTION.
- CONTRACTOR SHALL COORDINATE AND ADJUST LOCATION OF LOOP DETECTORS TO AVOID UTILITY CONFLICTS PRIOR TO CONSTRUCTION.
- CONTRACTOR SHALL ENSURE 100% COVERAGE OF ALL LANDSCAPED AREAS WITHIN LIMITS OF WORK, INCLUDING POTENTIAL OFFSITE AREAS. COVERAGE SHALL INCLUDE BOTH LANDSCAPING AND IRRIGATION.



THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE MANNER AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR THE CONTRACTOR. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MAY BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXCAVATE, LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

NOTICE:
CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. NEITHER THE OWNER NOR THE ENGINEER SHALL BE DEEMED TO ASSUME ANY RESPONSIBILITY FOR SAFETY OF THE WORK OF PERSONS ENGAGED IN THE WORK OF ANY HEAVY STRUCTURES, OR OF ANY OTHER PERSONS.
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24-HOUR CONTACT:
PANDA PM
HAKIM YALA
847-477-5125



PANDA EXPRESS, INC.
1683 Walnut Grove Ave.
Rosemead, California
91770
Telephone: 626 799 9858
Facsimile: 626 372 8288

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REVISIONS:

VILLAGE PLAN REV #1	01/19/2021
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ISSUE DATE:

1/19/2021	11/20/2020
-----------	------------

DRAWN BY: LEH

PANDA PROJECT #: D7058
ATWELL PROJECT #: 18003769



PANDA EXPRESS

WARM & WELCOME 2300
7505 KINGERY HIGHWAY
WILLOWBROOK, IL 60527

COVER SHEET

C01.0

TRUE WARM & WELCOME 2300

NOT FOR CONSTRUCTION

GENERAL CONSTRUCTION NOTES

- 1. ALL SITE WORK AND GRADING OPERATIONS WITHIN THE LIMITS OF THIS PROJECT SHALL BE DONE IN ACCORDANCE WITH THE CURRENT EDITION, AND ALL REVISIONS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION. THE CONTRACTOR SHALL ALSO FOLLOW ALL STATE, COUNTY AND LOCAL JURISDICTIONAL ORDINANCES AND REQUIREMENTS.

DEMOLITION NOTES

- 1. THE CONTRACTOR IS RESPONSIBLE FOR THE PROPER DISPOSAL OF ALL WASTE MATERIAL IN A LOCATION AND IN A MANNER APPROVED BY ALL GOVERNING AUTHORITIES INCLUDING, BUT NOT LIMITED TO, STRUCTURES, FOUNDATIONS, CONCRETE, ASPHALT, STEEL, UTILITIES, DRAINAGE STRUCTURES, ETC.

PAVEMENT

- 1. PAVEMENT DESIGN REQUIREMENTS FOR ALL PUBLIC AND PRIVATE STREETS, SHALL BE IN ACCORDANCE WITH ALL LOCAL/MUNICIPAL CODES AND THE CURRENT EDITION OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

STORM SEWER NOTES

- 1. ALL STORM SEWER CROSSING WATERMAIN SHALL BE CONSTRUCTED WITH "O" RING JOINTS PER ASTM C-443.

GRADING NOTES

- 1. EARTH EXCAVATION SHALL INCLUDE CLEARING, STRIPPING AND STOCKPILING TOPSOIL, REMOVING UNSUITABLE MATERIALS, THE CONSTRUCTION OF EMBANKMENTS, CONSTRUCTING NONSTRUCTURAL FILLS, AND FINAL SHAPING AND TRIMMING TO THE FINISH GRADES AND CROSS SECTIONS SHOWN IN THE PLANS. THIS WORK SHALL BE PERFORMED IN ACCORDANCE WITH APPLICABLE PROVISIONS OF DIVISION 200 OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

SANITARY SEWER NOTES

- 1. WORK AND MATERIAL SHALL BE IN ACCORDANCE WITH ALL STATE, COUNTY, AND LOCAL GOVERNING JURISDICTIONS AND IN ACCORDANCE WITH THE CURRENT EDITION OF "STANDARD SPECIFICATIONS FOR SEWER AND WATER MAIN CONSTRUCTION IN ILLINOIS".

WATER MAIN NOTES

- 1. WORK AND MATERIAL SHALL BE IN ACCORDANCE WITH ALL STATE, COUNTY, AND LOCAL GOVERNING JURISDICTIONS AND IN ACCORDANCE WITH THE CURRENT EDITION OF "STANDARD SPECIFICATIONS FOR SEWER AND WATER MAIN CONSTRUCTION IN ILLINOIS".



PANDA EXPRESS, INC.
1683 Walnut Grove Ave.
Rosemead, California
91770
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All ideas, designs, arrangements and plans indicated or represented by this drawing are the property of Panda Express Inc. and were created for use on this specific project.

Table with 2 columns: REVISIONS, VILLAGE PLAN REV. #1, 01/19/2021

Table with 2 columns: ISSUE DATE, PC SUBMITTAL, 11/20/2020

DRAWN BY: LEH

PANDA PROJECT # D7058
ATWELL PROJECT # 18003769



PANDA EXPRESS
WARM & WELCOME 2300
7505 KINERGY HIGHWAY
WILLOWBROOK, IL 60527

GENERAL NOTES

C01.1

TRUE WARM & WELCOME 2300



THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES SHOWN IN THIS DRAWING ARE BASED ON RECORD DRAWINGS AND FIELD SURVEY. THE CONTRACTOR SHALL VERIFY THE EXISTENCE AND DEPTHS OF ALL UTILITIES PRIOR TO ANY CONSTRUCTION.

24-HOUR CONTACT:
PANDA FM
HAKIM YALA
847-477-5125



DATE: 1/13/2021
EXP: 11/30/2021

Handwritten signature

EXISTING CONDITIONS LEGEND

	BOUNDARY LINE
	EXISTING ROW
	BOUNDARY ADJACENT LINE
	EXISTING EASEMENT LINE
	EXISTING CONTOUR
	EXISTING CURB AND GUTTER
	EXISTING EASEMENT LINE
	EXISTING SANITARY SEWER
	EXISTING STORM SEWER
	EXISTING WATER LINE
	EXISTING UNDERGROUND GAS
	EXISTING UNDERGROUND TELEPHONE
	EXISTING UNDERGROUND ELECTRIC
	EXISTING UNDERGROUND CABLE
	EXISTING CENTERLINE OF DITCH
	EXISTING GRAVEL
	EXISTING FENCE
	EXISTING VEGETATION LINE
	EXISTING MONITORING WELL
	SOIL BORING, REFER TO GEOTECHNICAL REPORT
	FOUND IRON ROD
	SITE CONTROL POINT
	EXISTING BOLLARD
	EXISTING FENCE POST
	EXISTING SIGN
	EXISTING TREE
	C.O.
	EXISTING FLARED END SECTION
	EXISTING STORM CATCH BASIN/MANHOLE
	EXISTING WATER SHUTOFF
	EXISTING WATER VALVE
	EXISTING HYDRANT WITH SHUTOFF
	EXISTING FIRE HYDRANT
	EXISTING WATER METER
	EXISTING IRRIGATION CONTROL VALVE
	UNDERGROUND GAS PIPE WARNING POST
	EXISTING TELEPHONE RISER
	EXISTING TRAFFIC SIGNAL
	EXISTING ELECTRIC TRANSFORMER
	EXISTING ELECTRIC METER
	EXISTING UTILITY POLE
	EXISTING LIGHT POLE
	EXISTING CABLE RISER
	EXISTING ASPHALT
	EXISTING CONCRETE
	EXISTING BUILDING

DEMOLITION LEGEND

	BOUNDARY LINE
	EXISTING ROW
	REMOVE CONCRETE CURB AND GUTTER
	FULL DEPTH SAWCUT LIMITS
	TREE PROTECTION FENCE, SEE DETAIL SHEET C03.1
	EXISTING FENCE/UTILITY TO BE REMOVED
	REMOVE EXISTING ASPHALT/CONCRETE PAVEMENT
	REMOVE GRAVEL
	REMOVE EXISTING VEGETATION
	EXISTING BUILDING (TO BE REMOVED BY OTHERS)

LAYOUT LEGEND

	BOUNDARY LINE
	EXISTING ROW
	EXISTING CURB AND GUTTER
	PROPOSED CURB AND GUTTER
	PROPOSED DEPRESSED CURB AND GUTTER
	EXISTING EASEMENT LINE
	PROPOSED LANDSCAPE BERM
	SETBACK LINE
	PARKING SPACE COUNT
	ADA RAMP
	DIMENSION TO BACK OF CURB
	DIMENSION TO PROPERTY LINE
	DIMENSION TO CENTER LINE
	DIMENSION TO EDGE OF PAVEMENT
	DIMENSION TO BUILDING
	PROPOSED LIGHT POLE/WALL PACK LIGHT
	PROPOSED SIGN
	PROPOSED ACCESSIBLE PARKING SPACE SIGN
	PROPOSED STOP SIGN
	PROPOSED DO NOT ENTER SIGN
	PROPOSED RIGHT TURN ONLY SIGN
	PROPOSED REINFORCED CONCRETE PAVEMENT, SEE DETAIL SHEET C02.2
	PROPOSED CONCRETE SIDEWALK, SEE DETAIL SHEET C02.2
	PROPOSED STANDARD DUTY ASPHALT PAVEMENT, SEE DETAIL SHEET C02.2
	PROPOSED HEAVY DUTY ASPHALT PAVEMENT, SEE DETAIL SHEET C02.2
	TRASH ENCLOSURE PAVEMENT, SEE DETAIL SHEET C02.2

SOIL EROSION AND SEDIMENTATION CONTROL LEGEND

	BOUNDARY LINE
	EXISTING ROW
	LIMITS OF DISTURBANCE
	EXISTING CONTOUR
	PROPOSED CONTOUR
	TEMPORARY SILT FENCE, SEE DETAIL SHEET C03.1
	PROPOSED RIDGE LINE
	TREE PROTECTION FENCE, SEE DETAIL SHEET C03.1
	EXISTING STORM SEWER
	PROPOSED STORM SEWER
	TEMPORARY CONSTRUCTION ENTRANCE, SEE DETAIL SHEET C03.1
	EROSION CONTROL BLANKET WITH PERMANENT STABILIZATION PER LANDSCAPE PLAN
	PERMANENT STABILIZATION PER LANDSCAPE PLAN
	TEMPORARY DROP-IN INLET PROTECTION, SEE DETAIL SHEET C03.1

GRADING LEGEND

	BOUNDARY LINE
	EXISTING ROW
	EXISTING CONTOUR
	PROPOSED CONTOUR
	PROPOSED RIDGE LINE
	EXISTING STORM SEWER
	PROPOSED STORM SEWER
	PROPOSED PITCH-OUT CURB AND GUTTER
	EXISTING STORM CATCH BASIN/MANHOLE
	PROPOSED STORM CATCH BASIN/MANHOLE
	PROPOSED SPOT ELEVATION
	PROPOSED RIM ELEVATION
	MATCH EXISTING ELEVATION
	PROPOSED FINISHED GRADE ELEVATION
	PROPOSED TOP OF CURB ELEVATION
	PROPOSED EDGE OF PAVEMENT ELEVATION
	PROPOSED FINISHED GRADE AT TOP OF WALL
	PROPOSED FINISHED GRADE AT BOTTOM OF WALL

STORM LEGEND

	BOUNDARY LINE
	EXISTING ROW
	EXISTING EASEMENT LINE
	EXISTING SANITARY SEWER
	PROPOSED SANITARY SEWER
	EXISTING STORM SEWER
	PROPOSED STORM SEWER
	EXISTING WATER LINE
	PROPOSED WATER LINE
	EXISTING UNDERGROUND GAS
	PROPOSED UNDERGROUND GAS
	EXISTING UNDERGROUND TELEPHONE
	PROPOSED UNDERGROUND TELEPHONE
	EXISTING UNDERGROUND ELECTRIC
	PROPOSED UNDERGROUND ELECTRIC
	EXISTING UNDERGROUND CABLE
	PROPOSED UNDERGROUND CABLE
	PROPOSED LIGHT POLE/WALL PACK LIGHT
	EXISTING MANHOLE/CATCH BASIN
	PROPOSED MANHOLE/CATCH BASIN/INLET
	PROPOSED CLEANOUT
	EXISTING END SECTION
	PROPOSED WATER VALVE
	PROPOSED FIRE HYDRANT
	PROPOSED FIRE DEPARTMENT CONNECTION
	PROPOSED UTILITY CROSSING, SEE UTILITY CROSSING CHART THIS SHEET

UTILITY LEGEND

	BOUNDARY LINE
	EXISTING ROW
	EXISTING EASEMENT LINE
	PROPOSED EASEMENT LINE
	EXISTING SANITARY SEWER
	PROPOSED SANITARY SEWER
	EXISTING STORM SEWER
	PROPOSED STORM SEWER
	EXISTING WATER LINE
	PROPOSED WATER LINE
	EXISTING UNDERGROUND GAS
	PROPOSED UNDERGROUND GAS
	EXISTING UNDERGROUND TELEPHONE
	PROPOSED UNDERGROUND TELEPHONE
	EXISTING UNDERGROUND ELECTRIC
	PROPOSED UNDERGROUND ELECTRIC
	EXISTING UNDERGROUND CABLE
	PROPOSED UNDERGROUND CABLE
	PROPOSED LIGHT POLE/WALL PACK LIGHT
	EXISTING MANHOLE/CATCH BASIN
	PROPOSED MANHOLE/CATCH BASIN/INLET
	PROPOSED CLEANOUT
	EXISTING END SECTION
	PROPOSED WATER VALVE
	PROPOSED FIRE HYDRANT
	PROPOSED FIRE DEPARTMENT CONNECTION
	PROPOSED TV PULL BOX
	PROPOSED UTILITY CROSSING, SEE UTILITY CROSSING CHART THIS SHEET

LANDSCAPE LEGEND

	BOUNDARY LINE
	EXISTING CONTOUR
	PROPOSED CONTOUR
	EXISTING ROW
	EXISTING EASEMENT LINE
	EXISTING SANITARY SEWER
	PROPOSED SANITARY SEWER
	EXISTING STORM SEWER
	PROPOSED STORM SEWER
	EXISTING WATER LINE
	PROPOSED WATER LINE
	EXISTING UNDERGROUND GAS
	PROPOSED UNDERGROUND GAS
	EXISTING UNDERGROUND TELEPHONE
	PROPOSED UNDERGROUND TELEPHONE
	EXISTING UNDERGROUND ELECTRIC
	PROPOSED UNDERGROUND ELECTRIC
	EXISTING UNDERGROUND CABLE
	PROPOSED UNDERGROUND CABLE
	PROPOSED IRRIGATION CONDUIT
	PROPOSED ALUMINUM EDGING
	EXISTING LIGHT POLE
	PROPOSED LIGHT POLE/WALL PACK LIGHT
	PROPOSED SHRUBS
	PROPOSED EVERGREEN TREES
	PROPOSED ORNAMENTAL TREES
	PROPOSED DECIDUOUS TREES
	PROPOSED KENTUCKY BLUE GRASS SOD
	PROPOSED STONE MULCH

811
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NOTICE: CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. NEITHER THE OWNER NOR THE ENGINEER SHALL BE EXPECTED TO ASSUME ANY RESPONSIBILITY FOR SAFETY OF THE WORK OF PERSONS ENGAGED IN THE WORK OF ANY NEARBY STRUCTURES OR OF ANY OTHER PERSONS.

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ROSEMARY E. HERRICK
082-081201
REGISTERED
PROFESSIONAL
ENGINEER
OF
ILLINOIS
DATE: 1/19/2021
EXP: 11/30/2021



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REVISIONS:

VILLAGE PLAN REV. #1	01/19/2021

ISSUE DATE:

PC SUBMITTAL	11/20/2020

DRAWN BY: LEH

PANDA PROJECT #: D7058
ATWELL PROJECT #: 18003769



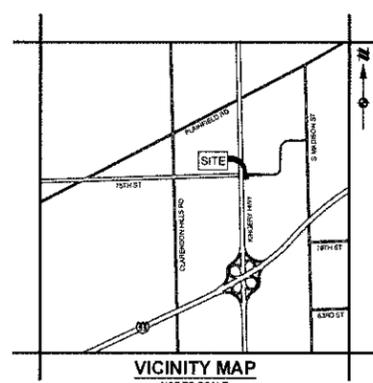
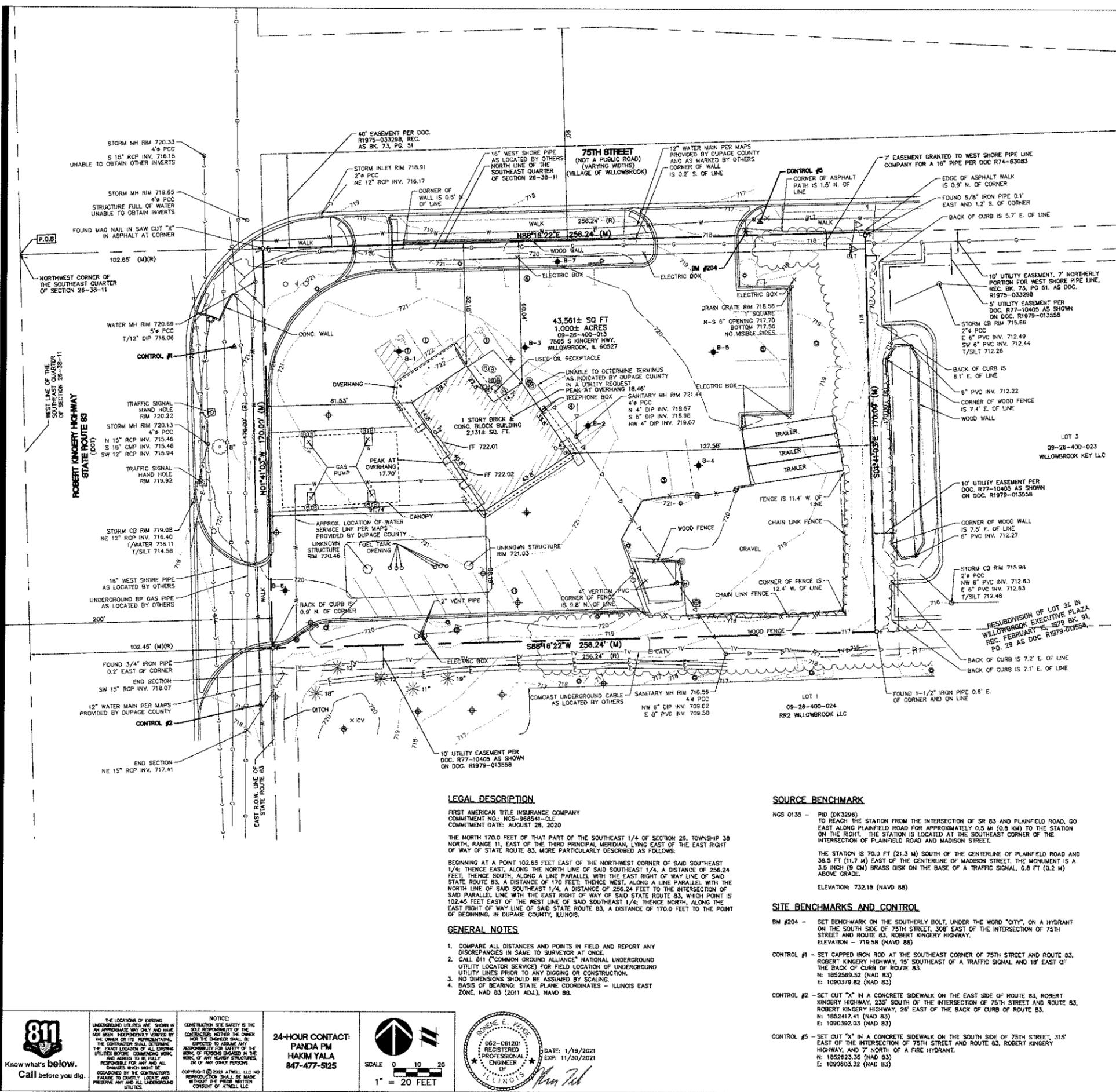
PANDA EXPRESS
WARM & WELCOME 2300
7505 KINGERY HIGHWAY
WILLOWBROOK, IL 60527

GENERAL NOTES

C01.2

TRUE WARM & WELCOME 2300

NOT FOR CONSTRUCTION



EXISTING CONDITIONS LEGEND

---	BOUNDARY LINE
---	EXISTING ROW
---	BOUNDARY ADJACENT LINE
---	EXISTING EASEMENT LINE
---	EXISTING CONTOUR
---	EXISTING CURB AND GUTTER
---	EXISTING SANITARY SEWER
---	EXISTING STORM SEWER
---	EXISTING WATER LINE
---	EXISTING UNDERGROUND GAS
---	EXISTING UNDERGROUND TELEPHONE
---	EXISTING UNDERGROUND ELECTRIC
---	EXISTING UNDERGROUND CABLE
---	EXISTING CENTERLINE OF DITCH
---	EXISTING GRAVEL
---	EXISTING FENCE
---	EXISTING VEGETATION LINE
---	EXISTING MONITORING WELL
---	SOIL BORING, REFER TO GEOTECHNICAL REPORT
---	FOUND IRON ROD
---	SITE CONTROL POINT
---	EXISTING BOLLARD
---	EXISTING FENCE POST
---	EXISTING SIGN
---	EXISTING TREE
---	EXISTING CLEANOUT
---	EXISTING SANITARY MANHOLE
---	EXISTING FLARED END SECTION
---	EXISTING STORM CATCH BASIN/MANHOLE
---	EXISTING WATER SHUTOFF
---	EXISTING WATER VALVE
---	EXISTING HYDRANT WITH SHUTOFF
---	EXISTING FIRE HYDRANT
---	EXISTING WATER METER
---	EXISTING IRRIGATION CONTROL VALVE
---	UNDERGROUND GAS PIPE WARNING POST
---	EXISTING TELEPHONE RISER
---	EXISTING TRAFFIC SIGNAL
---	EXISTING ELECTRIC TRANSFORMER
---	EXISTING ELECTRIC METER
---	EXISTING UTILITY POLE
---	EXISTING LIGHT POLE
---	EXISTING CABLE RISER
---	EXISTING ASPHALT
---	EXISTING CONCRETE
---	EXISTING BUILDING

REVISIONS:

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CONTRACTOR SHALL PROTECT ALL ITEMS OUTSIDE LIMITS OF CONSTRUCTION UNLESS OTHERWISE NOTED IN THE CONSTRUCTION PLANS OR SPECIFICATIONS.

CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITIES (LOCATIONS AND ELEVATIONS) PRIOR TO STARTING CONSTRUCTION AND ALERT ENGINEER TO ANY DISCREPANCIES IMMEDIATELY.

LEGAL DESCRIPTION

FIRST AMERICAN TITLE INSURANCE COMPANY
COMMITMENT NO.: NCS-968541-CLE
COMMITMENT DATE: AUGUST 28, 2020

THE NORTH 170.0 FEET OF THAT PART OF THE SOUTHEAST 1/4 OF SECTION 28, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING EAST OF THE EAST RIGHT OF WAY OF STATE ROUTE 83, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT 102.85 FEET EAST OF THE NORTHWEST CORNER OF SAID SOUTHEAST 1/4; THENCE EAST, ALONG THE NORTH LINE OF SAID SOUTHEAST 1/4, A DISTANCE OF 256.24 FEET; THENCE SOUTH, ALONG A LINE PARALLEL WITH THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170 FEET; THENCE WEST, ALONG A LINE PARALLEL WITH THE NORTH LINE OF SAID SOUTHEAST 1/4, A DISTANCE OF 256.24 FEET TO THE INTERSECTION OF SAID PARALLEL LINE WITH THE EAST RIGHT OF WAY OF SAID STATE ROUTE 83, WHICH POINT IS 102.45 FEET EAST OF THE WEST LINE OF SAID SOUTHEAST 1/4; THENCE NORTH, ALONG THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170.0 FEET TO THE POINT OF BEGINNING, IN DUPAGE COUNTY, ILLINOIS.

GENERAL NOTES

1. COMPARE ALL DISTANCES AND POINTS IN FIELD AND REPORT ANY DISCREPANCIES IN SAME TO SURVEYOR AT ONCE.
2. CALL 811 ("COMMON GROUND ALLIANCE" NATIONAL UNDERGROUND UTILITY LOCATOR SERVICE) FOR FIELD LOCATION OF UNDERGROUND UTILITY LINES PRIOR TO ANY DIGGING OR CONSTRUCTION.
3. NO DIMENSIONS SHOULD BE ASSUMED BY SCALING.
4. BASIS OF BEARING: STATE PLANE COORDINATES - ILLINOIS EAST ZONE, NAD 83 (2011 ADJ.), NAVD 83.

SOURCE BENCHMARK

NGS 0135 - PID (DK3298)
TO REACH THE STATION FROM THE INTERSECTION OF SR 83 AND PLAINFIELD ROAD, GO EAST ALONG PLAINFIELD ROAD FOR APPROXIMATELY 0.5 MI (0.8 KM) TO THE STATION ON THE RIGHT. THE STATION IS LOCATED AT THE SOUTHEAST CORNER OF THE INTERSECTION OF PLAINFIELD ROAD AND MADISON STREET.

THE STATION IS 70.0 FT (21.3 M) SOUTH OF THE CENTERLINE OF PLAINFIELD ROAD AND 38.5 FT (11.7 M) EAST OF THE CENTERLINE OF MADISON STREET. THE MONUMENT IS A 3.5 INCH (9 CM) BRASS DISK ON THE BASE OF A TRAFFIC SIGNAL, 0.8 FT (0.2 M) ABOVE GRADE.

ELEVATION: 732.19 (NAVD 88)

SITE BENCHMARKS AND CONTROL

BM #204 - SET BENCHMARK ON THE SOUTHERLY BOLT, UNDER THE WORD "CITY", ON A HYDRANT ON THE SOUTH SIDE OF 75TH STREET, 308' EAST OF THE INTERSECTION OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY. ELEVATION - 719.58 (NAVD 88)

CONTROL #1 - SET CAPPED IRON ROD AT THE SOUTHEAST CORNER OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY, 15' SOUTHEAST OF A TRAFFIC SIGNAL AND 18' EAST OF THE BACK OF CURB OF ROUTE 83. N: 1852417.41 (NAD 83) E: 1090379.82 (NAD 83)

CONTROL #2 - SET CUT "X" IN A CONCRETE SIDEWALK ON THE EAST SIDE OF ROUTE 83, ROBERT KINGERY HIGHWAY, 235' SOUTH OF THE INTERSECTION OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY, 26' EAST OF THE BACK OF CURB OF ROUTE 83. N: 1852417.41 (NAD 83) E: 1090392.03 (NAD 83)

CONTROL #5 - SET CUT "X" IN A CONCRETE SIDEWALK ON THE SOUTH SIDE OF 75TH STREET, 315' EAST OF THE INTERSECTION OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY, AND 7' NORTH OF A FIRE HYDRANT. N: 1852823.35 (NAD 83) E: 1090803.32 (NAD 83)



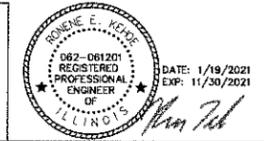
THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY AND HAVE NOT BEEN RESPONSIBLY LOCATED BY THE OWNER OR ITS REPRESENTATIVE. THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND ADHERE TO ALL RULES AND REGULATIONS OF THE UTILITIES AND TO ALL CHANGES WHICH MUST BE OBTAINED BY THE CONTRACTOR. FAILURE TO DO SO SHALL BE AT THE CONTRACTOR'S RISK AND ALL UNDERGROUND UTILITIES.

NOTICE: CONSTRUCTION FOR SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. NEITHER THE OWNER NOR THE DESIGNER SHALL BE HELD RESPONSIBLE FOR ANY WORK OF PERSONS ENGAGED IN THE WORK OF ANY HEAVY STRUCTURES OR OF ANY OTHER PERSONS.

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SCALE 0 10 20
1" = 20 FEET



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DRAWN BY: LEH

PANDA PROJECT #: D7058
ATWELL PROJECT #: 18003769



PANDA EXPRESS

WARM & WELCOME 2300
7505 KINGERY HIGHWAY
WILLOWBROOK, IL 60527

EXISTING CONDITIONS

C02.0

TRUE WARM & WELCOME 2300



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REVISIONS:

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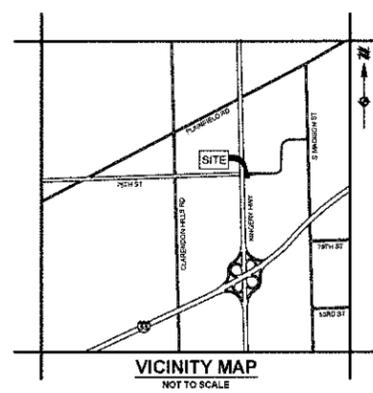
PANDA PROJECT # D7058
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DEMOLITION PLAN

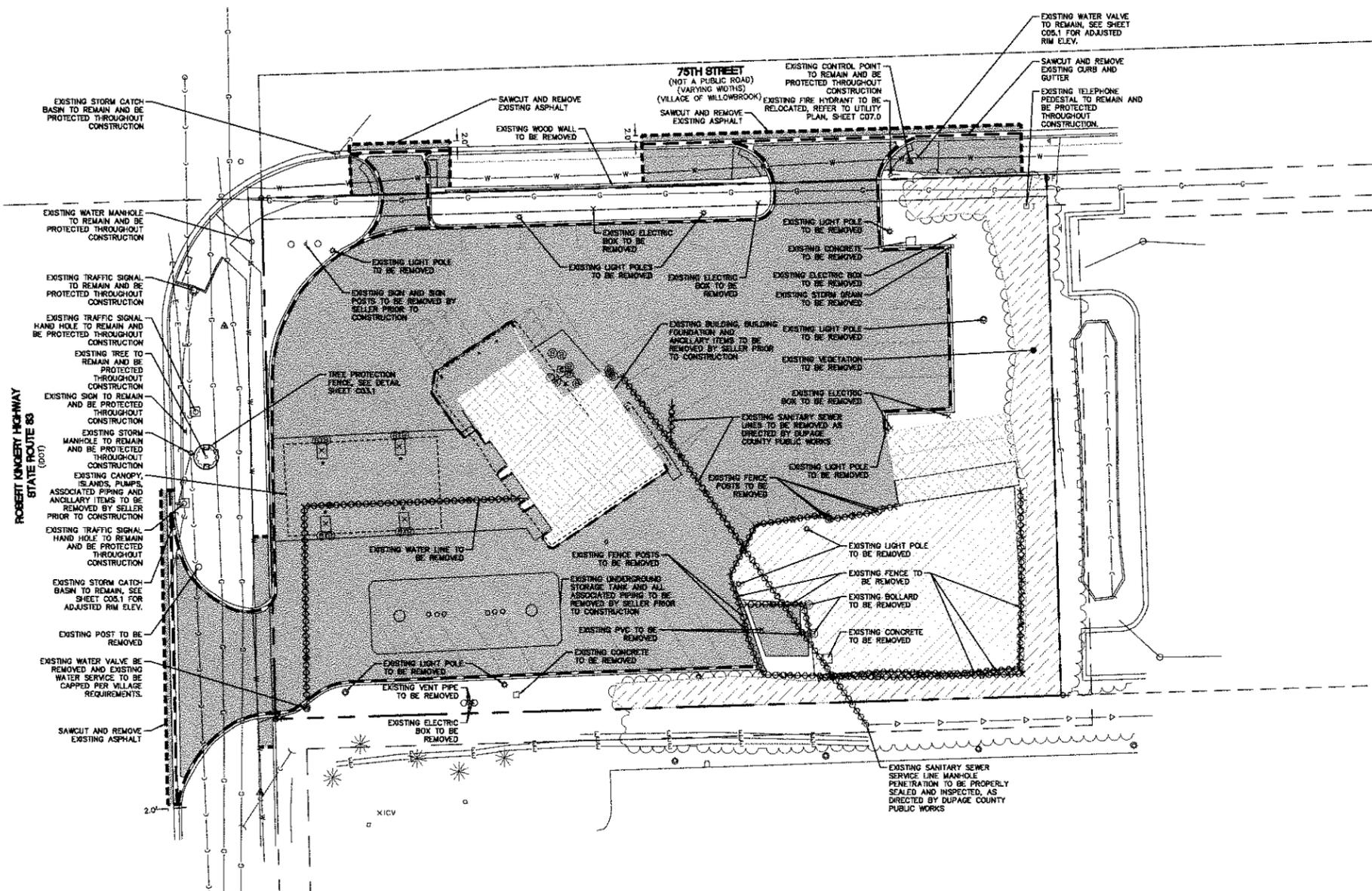
C02.1
TRUE WARM & WELCOME 2300



DEMOLITION LEGEND

---	BOUNDARY LINE
---	EXISTING ROW
---	REMOVE CONCRETE CURB AND GUTTER
---	FULL DEPTH SAWCUT LIMITS
---	TREE PROTECTION FENCE
---	SEE DETAIL SHEET C03.1
---	EXISTING FENCE/UTILITY TO BE REMOVED
---	REMOVE EXISTING ASPHALT/CONCRETE PAVEMENT
---	REMOVE GRAVEL
---	REMOVE EXISTING VEGETATION
---	EXISTING BUILDING (TO BE REMOVED BY OTHERS)

- NOTES:
- SEE SHEET C01.1 FOR DEMOLITION NOTES.
 - PROTECT ALL ITEMS DURING ALL PHASES OF CONSTRUCTION. THE CONTRACTOR SHALL ENSURE THE INTEGRITY OF ALL ITEMS DENOTED TO BE PROTECTED THAT ARE ADJACENT TO ITEMS DENOTED TO BE DEMOLISHED AND WILL SAFELY REPAIR ANY SUCH ITEMS TO THE REQUIRED JURISDICTIONAL STANDARDS.
 - SAWCUT AND REMOVAL OF EXISTING CONCRETE/ASPHALT PAVEMENT, SIDEWALK, CURB AND GUTTER AND ASSOCIATED APPURTENANCES SHALL INCLUDE, BUT NOT BE LIMITED TO, REINFORCEMENT AND STONE BASE.
 - REMOVAL OF EXISTING ON-SITE LANDSCAPE SHALL INCLUDE STUMPS.
 - ANY ITEM NOT INDICATED AS BEING REMOVED SHALL REMAIN.
 - CONTRACTOR SHALL COORDINATE ALL UTILITY REMOVAL AND ABANDONMENT ACTIVITIES WITH LOCAL GOVERNING AGENCY OR UTILITY COMPANY PRIOR TO STARTING DEMOLITION TO ENSURE COMPLIANCE WITH GOVERNING AGENCY AND UTILITY COMPANY REMOVAL AND ABANDONMENT STANDARDS.
 - TREES TO BE REMOVED ARE DESIGNATED ON THIS PLAN FOR REFERENCE. CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVAL OF ALL TREES AND VEGETATION NECESSARY TO ACCOMMODATE CONSTRUCTION.
 - CONTRACTOR MUST PROVIDE A BARRIER FROM PUBLIC ACCESS TO THE SITE PRIOR TO THE START OF DEMOLITION ACTIVITY AND UNTIL SITE CONSTRUCTION HAS BEEN COMPLETED.
 - REMOVAL OF DEMO ITEMS THAT REQUIRE EXCAVATION DEPTHS BELOW EXISTING TOPSOIL SHALL BE BACKFILLED ACCORDING TO THE GEOTECHNICAL REPORT.
 - PRIOR TO CONSTRUCTION OF THE PANDA EXPRESS SITE, THE SELLER SHALL REMOVE EXISTING ITEMS SHOWN ON THIS PLAN AND OBTAIN A "NO FURTHER REMEDIATION" (NFR) LETTER FROM THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY (EPA) THAT IS ACCEPTABLE TO PANDA EXPRESS FOR ITS INTENDED USE OF THE PROPERTY.



ROBERT KINERY HIGHWAY
STATE ROUTE 83
(000)

811
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THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROPRIATE MANNER AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVE. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREE TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MAY BE OCCURRED BY THE CONTRACTOR'S FAILURE TO PROPERLY LOCATE AND REMOVE ANY AND ALL UNDERGROUND UTILITIES.

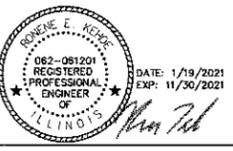
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ATWELL
1250 EAST DEER ROAD, SUITE 300
NAPERVILLE, IL 60563
TEL: 630.251.1000

24-HOUR CONTACT:
PANDA PM
HAKIM YALA
847-477-5125

SCALE 0 10 20
1" = 20 FEET

DATE: 1/19/2021
EXP: 11/30/2021



CONTRACTOR SHALL PROTECT ALL ITEMS OUTSIDE LIMITS OF CONSTRUCTION UNLESS OTHERWISE NOTED IN THE CONSTRUCTION PLANS OR SPECIFICATIONS.

CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITIES (LOCATIONS AND ELEVATIONS) PRIOR TO STARTING CONSTRUCTION AND ALERT ENGINEER TO ANY DISCREPANCIES IMMEDIATELY.

PROPOSED LIMITS OF DISTURBANCE, TYP.

TEMPORARY SILT FENCE, TYP. SEE DETAIL SHEET C03.1

TREE PROTECTION FENCE, SEE DETAIL SHEET C03.1

STOPWATER OUTFALL RUNOFF FROM THE SITE ENTERS THE PUBLIC STORM SEWER SYSTEM AND DISCHARGES INTO SANGAMON CREEK.

TEMPORARY DROP-IN INLET PROTECTION, TYP. SEE DETAIL SHEET C03.1

CONSTRUCTION SEQUENCE SCHEDULE

1. INSTALLATION OF SOIL EROSION AND SEDIMENT CONTROL SE/SC MEASURES
 A.) INSTALL STABILIZED CONSTRUCTION ENTRANCE
 B.) INSTALL SILT FENCE
 C.) INSTALL INLET FILTER PROTECTION AT EXISTING STORM STRUCTURES

2. TREE REMOVAL WHERE NECESSARY (CLEAR & GRUB).

3. REMOVE EXISTING BUILDING AND STRUCTURES.

4. CONTAIN STOCKPILE LOCATIONS WITHIN SITE AND INSTALL EROSION CONTROL MEASURES AS NECESSARY.

5. START CONSTRUCTION OF BUILDING PAD AND UTILITIES.

6. INSTALL INLET PROTECTION AS EACH STORM STRUCTURE IS INSTALLED.

7. INITIATE TEMPORARY SEEDING WITHIN ONE BUSINESS DAY OF INACTIVITY, THROUGHOUT CONSTRUCTION, DENUDE AREAS THAT WILL BE INACTIVE FOR 14 DAYS OR MORE.

8. INSTALL PAVEMENT.

9. PERMANENTLY STABILIZE ALL AREAS.

10. REMOVE ALL TEMPORARY SE/SC MEASURES AFTER THE SITE IS STABILIZED WITH VEGETATION.

* SOIL EROSION AND SEDIMENT CONTROL MAINTENANCE MUST OCCUR EVERY WEEK AND WITHIN 24 HOURS OF THE END OF A STORM OR BY THE END OF THE FOLLOWING BUSINESS DAY AFTER EVERY 0.5 IN. OR GREATER RAINFALL EVENT.

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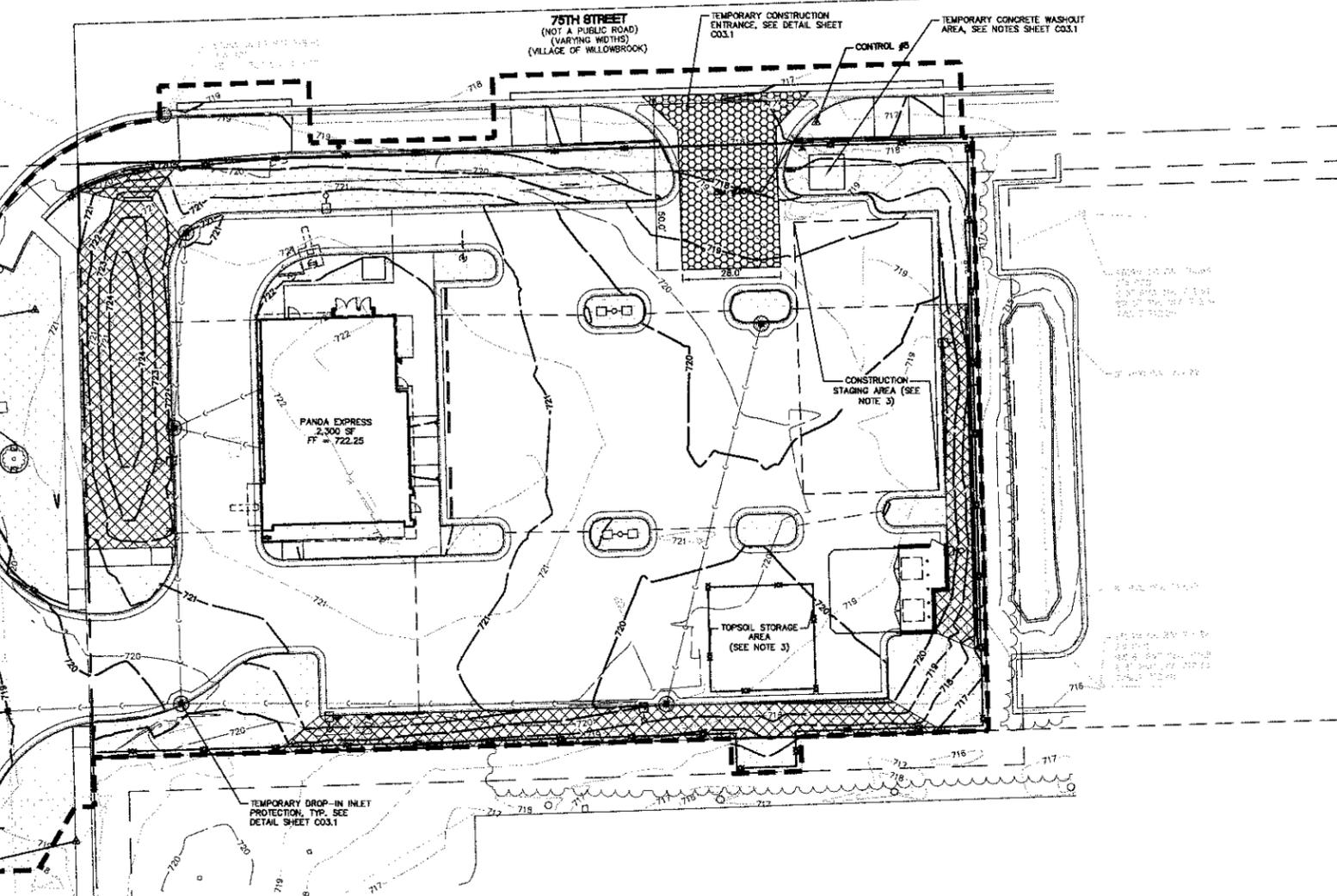
NOTICE: CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. NEITHER THE OWNER NOR THE ENGINEER SHALL BE DEEMED TO ASSUME ANY RESPONSIBILITY FOR SAFETY OF THE WORK, OF ANY NEARBY STRUCTURES, OR OF ANY OTHER PERSONS.

24-HOUR CONTACT:
 PANDA FM
 HAKIM YALA
 847-477-5125

SCALE 0 10 20
 1" = 20 FEET

DATE: 1/19/2021
 EXP: 11/30/2021

OWNER DATE



SOIL EROSION AND SEDIMENTATION CONTROL LEGEND

- BOUNDARY LINE
- EXISTING ROW
- LIMITS OF DISTURBANCE
- EXISTING CONTOUR
- PROPOSED CONTOUR
- TEMPORARY SILT FENCE, SEE DETAIL SHEET C03.1
- PROPOSED RIDGE LINE
- TREE PROTECTION FENCE, SEE DETAIL SHEET C03.1
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
- TEMPORARY CONSTRUCTION ENTRANCE, SEE DETAIL SHEET C03.1
- EROSION CONTROL BLANKET WITH PERMANENT STABILIZATION PER LANDSCAPE PLAN
- PERMANENT STABILIZATION PER LANDSCAPE PLAN
- TEMPORARY DROP-IN INLET PROTECTION, SEE DETAIL SHEET C03.1

NOTES:

1. SEE SHEET C03.1 FOR EROSION CONTROL NOTES AND DETAILS.
2. EROSION CONTROL BLANKET SHALL BE INSTALLED WHERE SHOWN IN AREAS DEPICTED ON THE LANDSCAPE PLAN AS SEED. EROSION CONTROL BLANKET IS NOT NECESSARY IN AREAS OF SOIL, MULCH, OR PLUGS.
3. THE LOCATIONS OF CONSTRUCTION STAGING AREA AND TOPSOIL STOCKPILE AREA ARE SUBJECT TO CHANGE PER CONTRACTOR. CONSTRUCTION TRAILERS, TEMPORARY PARKING, AND ITEMS SUCH AS SOLID WASTE RECEPTACLES, SANITARY FACILITIES, CONCRETE WASTE, FUEL TANKS, CONSTRUCTION MATERIALS, SUPPLIES, AND STOCKPILES SHALL BE PLACED IN THIS AREA IF NEEDED. TOPSOIL STOCKPILE AREA SHALL BE ENCLOSED WITH SILT FENCE.
4. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CLEAN VEHICLES PRIOR TO THEM EXITING THE SITE. ANY SEDIMENT OR DUST THAT HAS ACCUMULATED AT THE CONSTRUCTION EXIT, OR ON ANY OTHER EXISTING STABILIZED SURFACE WITHIN THE LIMITS OF DISTURBANCE, SHALL BE CLEANED IMMEDIATELY. CONSIDERATION SHALL BE GIVEN TO A WHEEL WASH SYSTEM WHERE APPROPRIATE. ANY WATER USED FOR CLEANING VEHICLES SHALL BE COLLECTED PRIOR TO LEAVING THE LIMITS OF DISTURBANCE OR BEFORE ENTERING AN EXISTING STORM DRAINAGE SYSTEM. SEE SHEET C03.1 FOR CONSTRUCTION EXIT DETAIL.
5. ALL SOIL DISTURBANCE SHALL BE LOCATED WITHIN THE LIMITS OF DISTURBANCE.

SITE DATA TABLE	
TOTAL PROJECT AREA (ACRES)	1.00
TOTAL DISTURBED AREA (ACRES)	1.3
EXISTING IMPERVIOUS AREA (ACRES)	0.8
PROPOSED IMPERVIOUS AREA (ACRES)	0.7

NOTE: THIS SOIL EROSION CONTROL PLAN DOES NOT CONSTITUTE A COMPLETE STORM WATER POLLUTION PREVENTION PLAN (SWPPP). THE CONTRACTOR IS RESPONSIBLE FOR DEVELOPING A COMPLETE STORM WATER POLLUTION PLAN IN ACCORDANCE WITH THE GENERAL NPDES PERMIT REQUIREMENTS INCLUDING BUT NOT LIMITED TO REPORTING, INSPECTIONS, MONITORING, ETC.

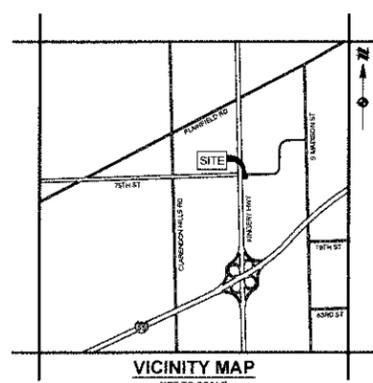
CONTRACTOR CERTIFICATION

I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (LRI01) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION.

GENERAL CONTRACTOR NAME _____
 GENERAL CONTRACTOR ADDRESS _____
 GENERAL CONTRACTOR TELEPHONE _____
 GENERAL CONTRACTOR SIGNATURE _____
 DATE _____
 SITE ADDRESS _____
 SUBCONTRACTOR NAME _____
 SUBCONTRACTOR ADDRESS _____
 SUBCONTRACTOR TELEPHONE _____
 SUBCONTRACTOR SIGNATURE _____
 DATE _____

OWNER CERTIFICATION

I CERTIFY UNDER PENALTY OF LAW THAT THIS DOCUMENT AND ALL ATTACHMENTS WERE PREPARED UNDER MY DIRECTION OR SUPERVISION IN ACCORDANCE WITH A SYSTEM DESIGNED TO ASSURE THAT QUALIFIED PERSONNEL PROPERLY GATHERED AND EVALUATED THE INFORMATION SUBMITTED, BASED ON MY INQUIRY OF THE PERSON OR PERSONS WHO MANAGE THE SYSTEM, OR THOSE PERSONS DIRECTLY RESPONSIBLE FOR GATHERING THE INFORMATION, THE INFORMATION SUBMITTED IS, TO THE BEST OF MY KNOWLEDGE AND BELIEF, TRUE, ACCURATE, AND COMPLETE. I AM AWARE THAT THERE ARE SIGNIFICANT PENALTIES FOR SUBMITTING FALSE INFORMATION, INCLUDING THE POSSIBILITY OF FINE AND IMPRISONMENT FOR KNOWING VIOLATIONS.



PANDA EXPRESS, INC.
 1683 Walnut Grove Ave.
 Rosemead, California
 91770
 Telephone: 626.799.9898
 Facsimile: 626.372.8288

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REVISIONS:
 VILLAGE PLAN REV. #1 01/19/2021

ISSUE DATE:
 PC SUBMITTAL 11/20/2020

DRAWN BY: LEH

PANDA PROJECT # D7058
 ATWELL PROJECT #: 18003769



PANDA EXPRESS
 WARM & WELCOME 2300
 7505 KINGERY HIGHWAY
 WILLOWBROOK, IL 60527

SOIL EROSION AND
 SEDIMENTATION
 CONTROL PLAN

C03.0

TRUE WARM & WELCOME 2300

CONTRACTOR SHALL PROTECT ALL ITEMS OUTSIDE LIMITS OF CONSTRUCTION UNLESS OTHERWISE NOTED IN THE CONSTRUCTION PLANS OR SPECIFICATIONS.

CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITIES (LOCATIONS AND ELEVATIONS) PRIOR TO STARTING CONSTRUCTION AND ALERT ENGINEER TO ANY DISCREPANCIES IMMEDIATELY.

NOT FOR CONSTRUCTION

S:\PROJECTS\2021\18003769\18003769-001\18003769-001-001.dwg 1/19/2021 4:30 PM LAMAR PAUL



Know what's below.
 Call before you dig.

THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE MANNER AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVE. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREE TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCURRED BY THE CONTRACTOR'S FAILURE TO CAREFULLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

NOTICE: CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. NEITHER THE OWNER NOR THE ENGINEER SHALL BE DEEMED TO ASSUME ANY RESPONSIBILITY FOR SAFETY OF THE WORK, OF ANY NEARBY STRUCTURES, OR OF ANY OTHER PERSONS.

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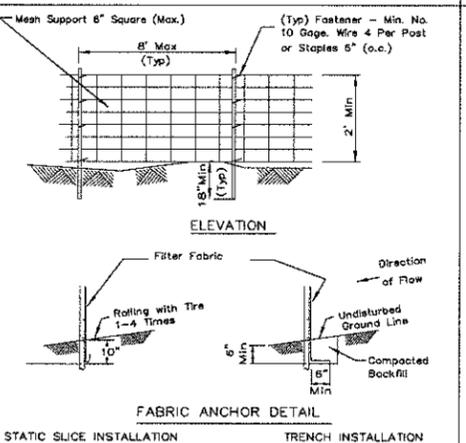
DATE: 1/19/2021
 EXP: 11/30/2021

OWNER DATE

SOIL EROSION AND SEDIMENTATION CONTROL NOTES

- ALL EROSION AND SEDIMENT CONTROLS SHALL BE INSPECTED IN ACCORDANCE WITH THE CONDITIONS OF APPLICABLE ILL. GENERAL NPDES PERMIT.
- EROSION AND SEDIMENT CONTROLS SHALL BE MAINTAINED AND REPLACED AS NECESSARY AT NO ADDITIONAL COST TO THE OWNER.
- THE EROSION CONTROL MEASURES INCLUDED IN THE SOIL EROSION AND SEDIMENTATION CONTROL PLAN SHALL BE INSTALLED PRIOR TO INITIAL LAND DISTURBANCE ACTIVITIES OR AS SOON AS PRACTICAL. SEDIMENT SHALL BE PREVENTED FROM DISCHARGING FROM THE PROJECT SITE BY INSTALLING AND MAINTAINING SILT FENCE, STRAW BALES, SEDIMENT BASINS, ETC. AS SHOWN ON THIS PLAN. IF SHOWN ON THESE PLANS, ENERGY-DISSIPATION DEVICES OR EROSION CONTROL AT THE OUTFALL OF THE STORM SEWER SYSTEM SHALL BE INSTALLED AT THE TIME OF THE CONSTRUCTION OF THE OUTFALL.
- THE CONTRACTOR SHALL CONTROL WASTES, GARBAGE, DEBRIS, WASTEWATER, AND OTHER SUBSTANCES ON THE SITE IN SUCH A MANNER THAT THEY SHALL NOT BE TRANSPORTED FROM THE SITE BY THE ACTION OF WINDS, STORM WATER RUNOFF, OR OTHER FORCES. PROPER DISPOSAL OR MANAGEMENT OF ALL WASTES AND UNUSED BUILDING MATERIAL, APPROPRIATE TO THE NATURE OF THE WASTE OR MATERIAL IS REQUIRED. COMPLIANCE IS REQUIRED WITH ALL STATE OR LOCAL REGULATIONS REGARDING WASTE DISPOSAL, SANITARY SEWER, OR SEPTIC SYSTEMS.
- PUBLIC OR PRIVATE ROADWAYS SHALL BE KEPT CLEARED OF ACCUMULATED SEDIMENT. IF SEDIMENT HAS BEEN TRACKED-OUT FROM THE SITE, ONTO PAVED ROADS, SIDEWALKS, OR OTHER PAVED AREAS OUTSIDE THE SITE, REMOVE THE DEPOSITED SEDIMENT BY THE END OF THE SAME BUSINESS DAY IN WHICH THE TRACK-OUT OCCURS OR BY THE END OF THE NEXT BUSINESS DAY IF TRACK-OUT OCCURS ON A NON-BUSINESS DAY. REMOVE TRACK-OUT BY SWEEPING, SHOVELING OR VACUUMING THESE SURFACES, OR BY USING OTHER SIMILAR EFFECTIVE MEANS OF SEDIMENT REMOVAL. THE HOUSING OR SWEEPING OF TRACKED-OUT SEDIMENT INTO ANY STORMWATER CONVEYANCE, STORM DRAIN INLET, OR WATER OF THE US IS PROHIBITED. CLEARED SEDIMENT SHALL BE RETURNED TO THE POINT OF LIKELY ORIGIN OR OTHER SUITABLE LOCATION.
- ALL DISCHARGES FROM DEWATERING ACTIVITIES, INCLUDING DISCHARGES FROM DEWATERING OF TRENCHES AND EXCAVATIONS, SHALL BE MANAGED BY APPROPRIATE CONTROL.
 - DEWATERING DISCHARGED SHALL BE TREATED OR CONTROLLED TO MINIMIZE DISCHARGES OF POLLUTANTS.
 - THE DISCHARGE SHALL NOT INCLUDE VISIBLE FLOATING SOLIDS OR FOAM.
 - AN OIL-WATER SEPARATOR OR SUITABLE FILTRATION DEVICE SHALL BE USED TO TREAT OIL, GREASE, OR OTHER SIMILAR PRODUCTS IF DEWATERING IS FOUND TO CONTAIN THESE MATERIALS.
 - TO THE EXTENT FEASIBLE, USE VEGETATED, UPLAND AREAS OF THE SITE TO INFILTRATE DEWATERING WATER BEFORE DISCHARGE.
 - BACKWASH WATER (WATER USED TO BACKWASH/CLEAN ANY FILTERS USED AS PART OF THE STORMWATER TREATMENT) MUST BE PROPERLY TREATED OR HAULED OFF-SITE FOR DISPOSAL.
 - DEWATERING TREATMENT DEVICES SHALL BE PROPERLY MAINTAINED.
- GENERAL CONTRACTOR SHALL DENOTE ON PLAN THE TEMPORARY PARKING AND STORAGE AREA WHICH SHALL ALSO BE USED AS THE EQUIPMENT MAINTENANCE AND CLEANING AREA, EMPLOYEE PARKING AREA, AND AREA FOR LOCATING PORTABLE FACILITIES, OFFICE, TRAILERS, AND TOILET FACILITIES.
- ALL WASH WATER (CONCRETE TRUCKS, VEHICLE CLEANING, EQUIPMENT CLEANING, ETC.) SHALL BE DETAINED AND PROPERLY TREATED OR DISPOSED.
- SUFFICIENT OIL AND GREASE ABSORBING MATERIALS AND FLotation ROOMS SHALL BE MAINTAINED ON SITE OR READILY AVAILABLE TO CONTAIN AND CLEAN-UP FUEL OR CHEMICAL SPILLS AND LEAKS.
- DUST ON THE SITE SHALL BE CONTROLLED. THE USE OF MOTOR OILS AND OTHER PETROLEUM BASED OR TOXIC LIQUIDS FOR DUST SUPPRESSION OPERATIONS IS PROHIBITED.
- ALL ON-SITE STORM DRAIN INLETS SHALL BE PROTECTED AGAINST SEDIMENTATION WITH STRAW BALES, FILTER FABRIC, OR EQUIVALENT BARRIERS AS SHOWN ON THESE PLANS.
- EXCEPT AS PREVENTED BY INCIDENT WEATHER CONDITIONS, STABILIZATION OF DISTURBED AREAS MUST BE INITIATED WITHIN ONE (1) WORKING DAY OF PERMANENT OR TEMPORARY CESSATION OF EARTH DISTURBING ACTIVITIES AND SHALL BE COMPLETED AS SOON AS POSSIBLE, BUT NO LATER THAN 14 DAYS FROM THE INITIATION OF THE STABILIZATION WORK IN AN AREA.
- THIS EROSION CONTROL PLAN SHALL BE IMPLEMENTED ON ALL DISTURBED AREAS WITHIN THE CONSTRUCTION SITE. ALL MEASURES INVOLVING EROSION CONTROL STRUCTURES SHALL BE INSTALLED UNDER THE GUIDANCE OF QUALIFIED PERSONNEL EXPERIENCED IN EROSION CONTROL, AND FOLLOWING THE PLANS AND SPECIFICATIONS INCLUDED HEREIN.
- ALL DISTURBED AREAS, AREAS USED FOR STORAGE OF MATERIALS THAT ARE EXPOSED TO PRECIPITATION, AND ALL AREAS WHERE STORMWATER TYPICALLY FLOWS WITHIN THE SITE SHALL BE INSPECTED FOR EVIDENCE OF, OR THE POTENTIAL FOR, POLLUTANTS ENTERING THE DRAINAGE SYSTEM. EROSION AND SEDIMENT CONTROL MEASURES IDENTIFIED IN THE PLAN SHALL BE OBSERVED TO ENSURE THAT THEY ARE STILL STABILIZED. WHERE DISCHARGE LOCATIONS OR POINTS ARE ACCESSIBLE, THEY SHALL BE INSPECTED TO ASCERTAIN WHETHER EROSION CONTROL MEASURES ARE EFFECTIVE IN PREVENTING SIGNIFICANT IMPACTS TO RECEIVING WATERS. LOCATIONS WHERE VEHICLES ENTER OR EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFF SITE SEDIMENT TRACKING.
- CONTRACTOR SHALL TAKE CORRECTIVE ACTIONS TO ADDRESS ANY STORMWATER CONTROL THAT NEEDS REPAIR OR REPLACEMENT AS SOON AS POSSIBLE AND DOCUMENT CORRECTIVE ACTIONS WITHIN SEVEN DAYS IN AN INSPECTION REPORT.
- CONTRACTOR SHALL INSTALL EROSION CONTROL BLANKET PER MANUFACTURER'S RECOMMENDATIONS ON ALL SLOPES 4:1 OR STEEPER.
- DURING THE PERIOD OF CONSTRUCTION ACTIVITY, ALL SEDIMENT BASINS AND OTHER EROSION CONTROL MEASURES SHALL BE MAINTAINED BY THE CONTRACTOR. AT COMPLETION OF CONSTRUCTION, THE CONTRACTOR SHALL COORDINATE THE PERIOD OF MAINTENANCE RESPONSIBILITIES, IF REQUIRED, WITH THE OWNER. MAINTENANCE SHALL BE IN ACCORDANCE WITH THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL, 1987, AND THE SOIL EROSION AND SEDIMENTATION CONTROL PLAN.
- ON-SITE AND OFF-SITE SOIL STOCKPILE AND BORROW AREAS SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION THROUGH IMPLEMENTATION OF BEST MANAGEMENT PRACTICES (I.E. SILT FENCE, TEMPORARY STABILIZATION, ETC.). STOCKPILE AND BORROW AREA LOCATIONS SHALL BE NOTED ON THE SITE MAP AND PERMITTED IN ACCORDANCE WITH GENERAL PERMIT REQUIREMENTS.
- EXISTING VEGETATION SHALL BE PROTECTED AS MUCH AS PRACTICAL.
- ALL TEMPORARY EROSION AND SEDIMENT CONTROL PRACTICES SHALL BE REMOVED AND DISPOSED OF WITHIN THIRTY DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY PRACTICES ARE NO LONGER NEEDED. TRAPPED SEDIMENT SHALL BE PERMANENTLY STABILIZED TO PREVENT FURTHER EROSION.
- CONTRACTOR SHALL REMOVE ANY ACCUMULATED SEDIMENT FROM DETENTION BASINS AND STORM SEWER SYSTEMS IN CONJUNCTION WITH THE FINAL STABILIZATION OF THE SITE.
- THIS EROSION CONTROL PLAN MUST BE RETAINED ON-SITE AT ALL TIMES DURING THE PERIOD OF CONSTRUCTION.

SILT FENCE WITH WIRE SUPPORT PLAN



- NOTES:**
- Silt Fence shall be installed prior to any grading work in the area to be protected. They shall be maintained throughout the construction period and removed in conjunction with the final grading and site stabilization. Silt fence shall be placed on the flattest area available.
 - Filter fabric shall meet the requirements of material specification 592 Geotextile Table 1 or 2, Class I with equivalent opening size of at least 30 for nonwoven and 40 for woven.
 - Fence posts shall be either standard steel post or wood post with a minimum cross-sectional area of 3.0 sq. in.

REFERENCE Project	DATE	DESIGNED	DATE	CHECKED	DATE	APPROVED	DATE

STANDARD DWG. NO. IUM-520A(W) SHEET 1 OF 2 DATE: 3-18-2012

MAINTENANCE:

SILT FENCE SHALL BE REMOVED ONCE UPSLOPE AREAS HAVE BEEN PERMANENTLY STABILIZED.

SILT FENCE SHALL BE INSPECTED NO LESS FREQUENTLY THAN EVERY WEEK DURING CONSTRUCTION. SHOULD THE FABRIC DECOMPOSE OR BECOME INEFFECTIVE PRIOR TO THE END OF THE EXPECTED USABLE LIFE, AND THE FENCE STILL IS NECESSARY, THE FABRIC OR THE ENTIRE SYSTEM SHALL BE REPLACED PROMPTLY.

SEDIMENT DEPOSITS MUST BE REMOVED WHEN THE LEVEL OF DEPOSITION REACHES APPROXIMATELY ONE-HALF THE HEIGHT OF THE SILT FENCE.

ANY SEDIMENT DEPOSITS REMAINING IN PLACE AFTER THE SILT FENCE IS NO LONGER REQUIRED SHALL BE DRESSED TO CONFORM TO THE EXISTING GRADE, A SEDGED PREPARED AND THE SITE VEGETATED.

CONCRETE WASHOUT:

INSTALLATION

PREFABRICATED WASHOUT SYSTEMS/CONTAINERS

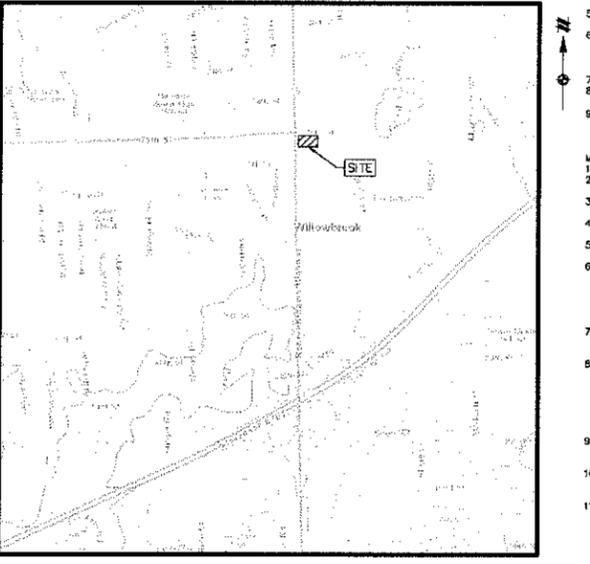
- INSTALL AND LOCATE ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.

DESIGNED AND INSTALLED SYSTEMS

- UTILIZE AND FOLLOW THE DESIGN IN THE STORM WATER POLLUTION PREVENTION PLAN TO INSTALL THE SYSTEM.
- DEPENDENT UPON THE TYPE OF SYSTEM, EITHER EXCAVATE THE PIT OR INSTALL THE CONTAINMENT SYSTEM.
- A BASE SHALL BE CONSTRUCTED AND PREPARED THAT IS FREE OF ROCKS AND OTHER DEBRIS THAT MAY CAUSE TEARS OR PUNCTURES IN THE POLYETHYLENE LINING.
- INSTALL THE POLYETHYLENE LINING. FOR EXCAVATED SYSTEMS, THE LINING SHOULD EXTEND OVER THE ENTIRE EXCAVATION. THE LINING FOR BERMED SYSTEMS SHOULD BE INSTALLED OVER THE POOLING AREA WITH ENOUGH MATERIAL TO EXTEND THE LINING OVER THE BERM OR CONTAINMENT SYSTEM. THE LINING SHOULD BE SECURED WITH PINS, STAPLES, OR OTHER FASTENERS.
- PLACE FLAGS, SAFETY FENCING, OR EQUIVALENT TO PROVIDE A BARRIER TO CONSTRUCTION EQUIPMENT AND OTHER TRAFFIC.
- PLACE A NON-COLLAPSING, NON-WATER HOLDING COVER OVER THE WASHOUT FACILITY PRIOR TO A PREDICTED RAINFALL EVENT TO PREVENT ACCUMULATION OF WATER AND POSSIBLE OVERFLOW OF THE SYSTEM (OPTIONAL).
- INSTALL SIGNAGE THAT IDENTIFIES CONCRETE WASHOUT AREAS.
- POST SIGNS DIRECTING CONTRACTORS AND SUPPLIERS TO DESIGNATED LOCATIONS.
- WHERE NECESSARY, PROVIDE STABLE INGRESS AND EGRESS (SEE TEMPORARY CONSTRUCTION INGRESS/EGRESS PAD) OR ALTERNATIVE APPROACH PAD FOR CONCRETE WASHOUT SYSTEMS.

MAINTENANCE:

- INSPECT DAILY AND AFTER EACH STORM EVENT.
- WHEN CONCRETE WASHOUT SYSTEMS ARE INSTALLED, THE INTERIORITY OF THE OVERFALL STRUCTURE INCLUDING, WHERE APPLICABLE, THE CONTAINMENT SYSTEM.
- INSPECT THE SYSTEM FOR LEAKS, SPILLS, AND TRACKING OF SOIL BY EQUIPMENT.
- INSPECT THE POLYETHYLENE LINING FOR FAILURE, INCLUDING TEARS AND PUNCTURES.
- ONCE CONCRETE WASTES HARDEN, REMOVE AND DISPOSE OF THE MATERIAL.
- EXCESS CONCRETE SHOULD BE REMOVED WHEN THE WASHOUT SYSTEM REACHES 50 PERCENT OF THE DESIGN CAPACITY. USE OF THE SYSTEM SHOULD BE DISCONTINUED UNTIL APPROPRIATE MEASURES CAN BE INITIATED TO CLEAN THE STRUCTURE. PREFABRICATED SYSTEMS SHOULD ALSO UTILIZE THIS CRITERION, UNLESS THE MANUFACTURER HAS ALTERNATE SPECIFICATIONS.
- UPON REMOVAL OF THE SOLIDS, INSPECT THE STRUCTURE. REPAIR THE STRUCTURE AS NEEDED OR CONSTRUCT A NEW SYSTEM.
- DISPOSE OF ALL CONCRETE IN A LEGAL MANNER. REUSE THE MATERIAL ON SITE, RECYCLE, OR HAUL THE MATERIAL TO AN APPROVED CONSTRUCTION/DEMOLITION LANDFILL SITE. RECYCLING OF MATERIAL IS ENCOURAGED. THE WASTE MATERIAL CAN BE USED FOR MULTIPLE APPLICATIONS INCLUDING BUT NOT LIMITED TO ROADBEDS AND BUILDING THE AVAILABILITY FOR RECYCLING SHOULD BE CHECKED LOCALLY.
- THE PLASTIC LINER SHOULD BE REPLACED AFTER EVERY CLEANING. THE REMOVAL OF MATERIAL WILL USUALLY DAMAGE THE LINING.
- THE CONCRETE WASHOUT SYSTEM SHOULD BE REPAIRED OR ENLARGED AS NECESSARY TO MAINTAIN CAPACITY FOR CONCRETE WASTE.
- CONCRETE WASHOUT SYSTEMS ARE DESIGNED TO PROMOTE EVAPORATION. HOWEVER, LIQUIDS DO NOT EVAPORATE, AND THE SYSTEM IS NEAR CAPACITY IT MAY BE NECESSARY TO VACUUM OR REMOVE THE LIQUIDS AND DISPOSE OF THEM IN AN ACCEPTABLE METHOD. DISPOSAL MAY BE ALLOWED AT THE LOCAL SANITARY SEWER AUTHORITY PROVIDED THEIR NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM PERMITS ALLOW FOR ACCEPTANCE OF THIS MATERIAL. ANOTHER OPTION WOULD BE TO UTILIZE A SECONDARY CONTAINMENT SYSTEM OR BASIN FOR FURTHER DEWATERING.
- PREFABRICATED UNITS ARE OFTEN PUMPED AND THE COMPANY SUPPLYING THE UNIT PROVIDES THIS SERVICE.
- INSPECT CONSTRUCTION ACTIVITIES ON A REGULAR BASIS TO ENSURE SUPPLIERS, CONTRACTORS, AND OTHERS ARE UTILIZING DESIGNATED WASHOUT AREAS. IF CONCRETE WASTE IS BEING DISPOSED OF IMPROPERLY, IDENTIFY THE VIOLATORS AND TAKE APPROPRIATE ACTION.
- WHEN CONCRETE WASHOUT SYSTEMS ARE NO LONGER REQUIRED, THE CONCRETE WASHOUT SYSTEMS SHALL BE CLOSED. DISPOSE OF ALL HARDENED CONCRETE AND OTHER MATERIALS USED TO CONSTRUCT THE SYSTEM.
- HOLES, DEPRESSIONS AND OTHER LAND DISTURBANCES ASSOCIATED WITH THE SYSTEM SHOULD BE BACKFILLED, GRADED, AND STABILIZED.



RECEIVING WATER: RUNOFF FROM THE SITE ENTERS THE PUBLIC STORM SEWER SYSTEM AND DISCHARGES INTO SAWMILL CREEK.

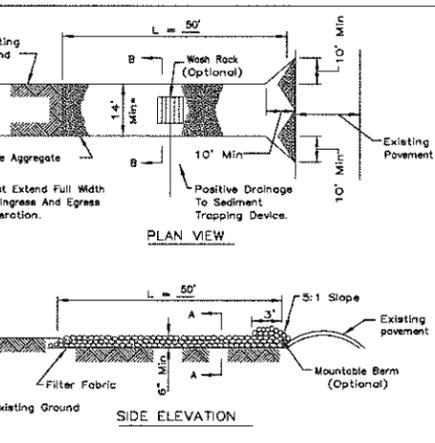
24-HOUR CONTACT:
PANDA PM
HAKIM YALA
847-477-5125

NOTICE: CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE OWNER OR ITS REPRESENTATIVE SHALL NOT BE RESPONSIBLE FOR THE SAFETY OF PERSONS ENGAGED IN THE WORK, OR ANY NEARBY STRUCTURES, OR OF ANY OTHER PERSONS.

DATE: 1/19/2021
 EXP: 11/30/2021

REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS

STABILIZED CONSTRUCTION ENTRANCE PLAN



- NOTES:**
- Filter fabric shall meet the requirements of material specification 592 Geotextile, Table 1 or 2, Class I, II, or IV and shall be placed over the cleared area prior to the placing of rock.
 - Rock or reclaimed concrete shall meet one of the following IDOT coarse aggregate gradation, CA-1, CA-2, CA-3 or CA-4 and be placed according to construction specification 25 ROCKFILL using placement Method 1 and Class III compaction.
 - Any drainage facilities required because of washing shall be constructed according to manufacturer's specifications.
 - If wash racks are used they shall be installed according to the manufacturer's specifications.

REFERENCE Project	DATE	DESIGNED	DATE	CHECKED	DATE	APPROVED	DATE

STANDARD DWG. NO. IUM-530 SHEET 1 OF 2 DATE: 8-18-04

CONCRETE WASHOUT:

INSTALLATION

PREFABRICATED WASHOUT SYSTEMS/CONTAINERS

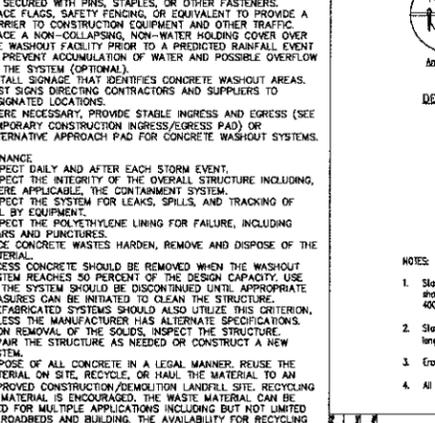
- INSTALL AND LOCATE ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.

DESIGNED AND INSTALLED SYSTEMS

- UTILIZE AND FOLLOW THE DESIGN IN THE STORM WATER POLLUTION PREVENTION PLAN TO INSTALL THE SYSTEM.
- DEPENDENT UPON THE TYPE OF SYSTEM, EITHER EXCAVATE THE PIT OR INSTALL THE CONTAINMENT SYSTEM.
- A BASE SHALL BE CONSTRUCTED AND PREPARED THAT IS FREE OF ROCKS AND OTHER DEBRIS THAT MAY CAUSE TEARS OR PUNCTURES IN THE POLYETHYLENE LINING.
- INSTALL THE POLYETHYLENE LINING. FOR EXCAVATED SYSTEMS, THE LINING SHOULD EXTEND OVER THE ENTIRE EXCAVATION. THE LINING FOR BERMED SYSTEMS SHOULD BE INSTALLED OVER THE POOLING AREA WITH ENOUGH MATERIAL TO EXTEND THE LINING OVER THE BERM OR CONTAINMENT SYSTEM. THE LINING SHOULD BE SECURED WITH PINS, STAPLES, OR OTHER FASTENERS.
- PLACE FLAGS, SAFETY FENCING, OR EQUIVALENT TO PROVIDE A BARRIER TO CONSTRUCTION EQUIPMENT AND OTHER TRAFFIC.
- PLACE A NON-COLLAPSING, NON-WATER HOLDING COVER OVER THE WASHOUT FACILITY PRIOR TO A PREDICTED RAINFALL EVENT TO PREVENT ACCUMULATION OF WATER AND POSSIBLE OVERFLOW OF THE SYSTEM (OPTIONAL).
- INSTALL SIGNAGE THAT IDENTIFIES CONCRETE WASHOUT AREAS.
- POST SIGNS DIRECTING CONTRACTORS AND SUPPLIERS TO DESIGNATED LOCATIONS.
- WHERE NECESSARY, PROVIDE STABLE INGRESS AND EGRESS (SEE TEMPORARY CONSTRUCTION INGRESS/EGRESS PAD) OR ALTERNATIVE APPROACH PAD FOR CONCRETE WASHOUT SYSTEMS.

MAINTENANCE:

- INSPECT DAILY AND AFTER EACH STORM EVENT.
- WHEN CONCRETE WASHOUT SYSTEMS ARE INSTALLED, THE INTERIORITY OF THE OVERFALL STRUCTURE INCLUDING, WHERE APPLICABLE, THE CONTAINMENT SYSTEM.
- INSPECT THE SYSTEM FOR LEAKS, SPILLS, AND TRACKING OF SOIL BY EQUIPMENT.
- INSPECT THE POLYETHYLENE LINING FOR FAILURE, INCLUDING TEARS AND PUNCTURES.
- ONCE CONCRETE WASTES HARDEN, REMOVE AND DISPOSE OF THE MATERIAL.
- EXCESS CONCRETE SHOULD BE REMOVED WHEN THE WASHOUT SYSTEM REACHES 50 PERCENT OF THE DESIGN CAPACITY. USE OF THE SYSTEM SHOULD BE DISCONTINUED UNTIL APPROPRIATE MEASURES CAN BE INITIATED TO CLEAN THE STRUCTURE. PREFABRICATED SYSTEMS SHOULD ALSO UTILIZE THIS CRITERION, UNLESS THE MANUFACTURER HAS ALTERNATE SPECIFICATIONS.
- UPON REMOVAL OF THE SOLIDS, INSPECT THE STRUCTURE. REPAIR THE STRUCTURE AS NEEDED OR CONSTRUCT A NEW SYSTEM.
- DISPOSE OF ALL CONCRETE IN A LEGAL MANNER. REUSE THE MATERIAL ON SITE, RECYCLE, OR HAUL THE MATERIAL TO AN APPROVED CONSTRUCTION/DEMOLITION LANDFILL SITE. RECYCLING OF MATERIAL IS ENCOURAGED. THE WASTE MATERIAL CAN BE USED FOR MULTIPLE APPLICATIONS INCLUDING BUT NOT LIMITED TO ROADBEDS AND BUILDING THE AVAILABILITY FOR RECYCLING SHOULD BE CHECKED LOCALLY.
- THE PLASTIC LINER SHOULD BE REPLACED AFTER EVERY CLEANING. THE REMOVAL OF MATERIAL WILL USUALLY DAMAGE THE LINING.
- THE CONCRETE WASHOUT SYSTEM SHOULD BE REPAIRED OR ENLARGED AS NECESSARY TO MAINTAIN CAPACITY FOR CONCRETE WASTE.
- CONCRETE WASHOUT SYSTEMS ARE DESIGNED TO PROMOTE EVAPORATION. HOWEVER, LIQUIDS DO NOT EVAPORATE, AND THE SYSTEM IS NEAR CAPACITY IT MAY BE NECESSARY TO VACUUM OR REMOVE THE LIQUIDS AND DISPOSE OF THEM IN AN ACCEPTABLE METHOD. DISPOSAL MAY BE ALLOWED AT THE LOCAL SANITARY SEWER AUTHORITY PROVIDED THEIR NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM PERMITS ALLOW FOR ACCEPTANCE OF THIS MATERIAL. ANOTHER OPTION WOULD BE TO UTILIZE A SECONDARY CONTAINMENT SYSTEM OR BASIN FOR FURTHER DEWATERING.
- PREFABRICATED UNITS ARE OFTEN PUMPED AND THE COMPANY SUPPLYING THE UNIT PROVIDES THIS SERVICE.
- INSPECT CONSTRUCTION ACTIVITIES ON A REGULAR BASIS TO ENSURE SUPPLIERS, CONTRACTORS, AND OTHERS ARE UTILIZING DESIGNATED WASHOUT AREAS. IF CONCRETE WASTE IS BEING DISPOSED OF IMPROPERLY, IDENTIFY THE VIOLATORS AND TAKE APPROPRIATE ACTION.
- WHEN CONCRETE WASHOUT SYSTEMS ARE NO LONGER REQUIRED, THE CONCRETE WASHOUT SYSTEMS SHALL BE CLOSED. DISPOSE OF ALL HARDENED CONCRETE AND OTHER MATERIALS USED TO CONSTRUCT THE SYSTEM.
- HOLES, DEPRESSIONS AND OTHER LAND DISTURBANCES ASSOCIATED WITH THE SYSTEM SHOULD BE BACKFILLED, GRADED, AND STABILIZED.



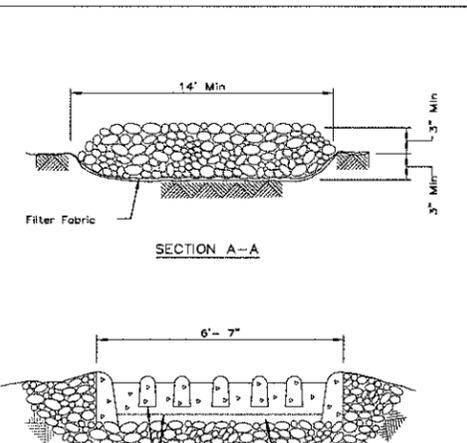
EROSION CONTROL BLANKET INSTALLATION DETAILS

- NOTES:**
- Staples shall be placed in a diamond pattern of 2 per sq. ft. for attached blankets. Non-stitched sheet use 4 staples per sq. ft. of material. This equates to 200 staples with attached blanket and 400 staples with non-stitched blanket per 100 sq. ft. of material.
 - Staple or push pin lengths shall be selected based on soil type and conditions. (minimum staple length is 6")
 - Erosion control material shall be placed in contact with the soil over a prepared seedbed.
 - All anchor stakes shall be staked at approximately 12" intervals.
- MAINTENANCE:**
- INSPECT ALL EROSION CONTROL BLANKETS PERIODICALLY AND AFTER RAINSTORMS TO CHECK FOR DAMAGE DUE TO WATER RUNNING UNDER THE BLANKET OR IF THE BLANKETS THAT HAVE BEEN DISPLACED BY WIND. ANY AREAS WHERE WATER SEEPED UNDER THE BLANKET, MORE STAPLES MAY BE NEEDED PER OVERFALL AREA OR MORE FREQUENT ANCHORING TRENCHES INSTALLED WITH BETTER COMPACTING. IF SIGNIFICANT EROSION HAS OCCURRED UNDER THE BLANKET THEN GRADING AND RESEEDING MAY ALSO BE NECESSARY. ANY EROSION CONTROL BLANKETS THAT HAVE BEEN DISPLACED WILL NEED TO BE REINSTALLED AND RE-STAPLED. THIS MAY INDICATE THAT THE WRONG TYPE OF BLANKET WAS CHOSEN. ONE MAY NEED TO REVISIT THE SITE CHARACTERISTICS AND THEN SELECT A DIFFERENT TYPE OF EROSION CONTROL BLANKET OR CHOOSE A DIFFERENT PRACTICE.

REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS

DATE: 1/19/2021
 EXP: 11/30/2021

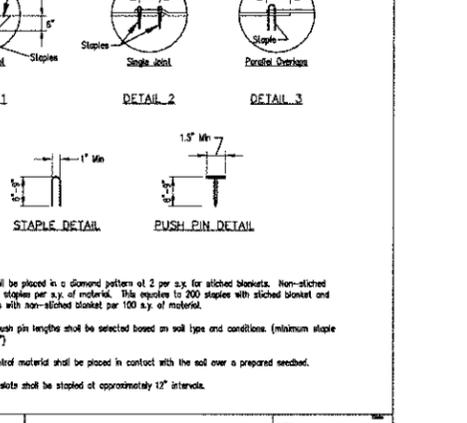
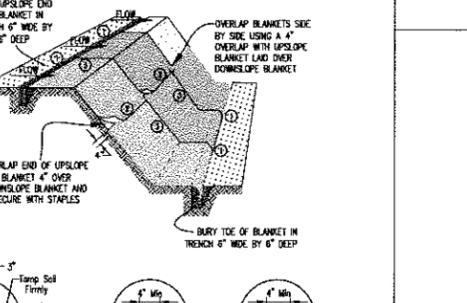
STABILIZED CONSTRUCTION ENTRANCE PLAN



- NOTES:**
- Filter fabric shall meet the requirements of material specification 592 Geotextile, Table 1 or 2, Class I, II, or IV and shall be placed over the cleared area prior to the placing of rock.
 - Rock or reclaimed concrete shall meet one of the following IDOT coarse aggregate gradation, CA-1, CA-2, CA-3 or CA-4 and be placed according to construction specification 25 ROCKFILL using placement Method 1 and Class III compaction.
 - Any drainage facilities required because of washing shall be constructed according to manufacturer's specifications.
 - If wash racks are used they shall be installed according to the manufacturer's specifications.

REFERENCE Project	DATE	DESIGNED	DATE	CHECKED	DATE	APPROVED	DATE

STANDARD DWG. NO. IUM-530 SHEET 2 OF 2 DATE: 8-18-04



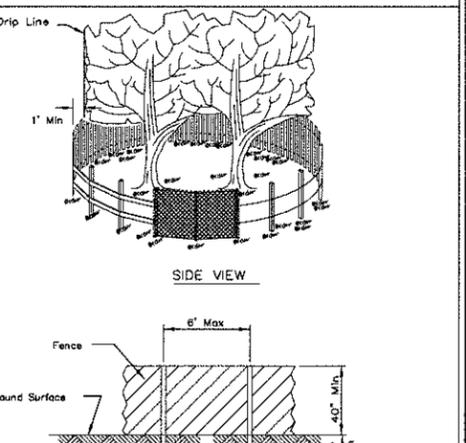
EROSION CONTROL BLANKET INSTALLATION DETAILS

- NOTES:**
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 - Erosion control material shall be placed in contact with the soil over a prepared seedbed.
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REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS

DATE: 1/19/2021
 EXP: 11/30/2021

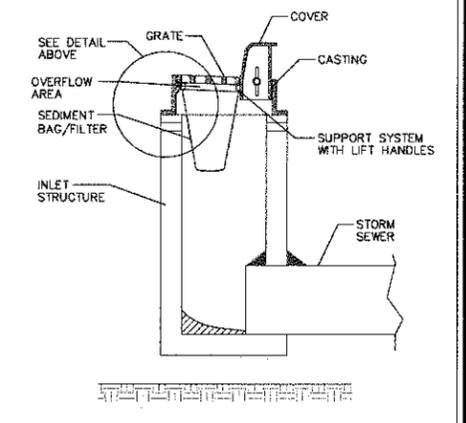
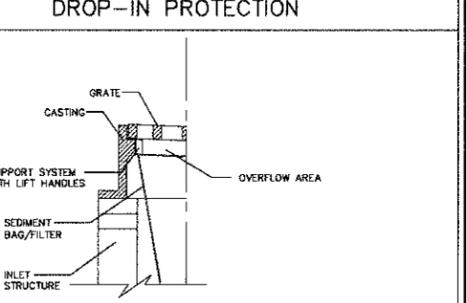
TREE PROTECTION - FENCING



- NOTES:**
- The fence shall be located a minimum of 1 foot outside the drip line of the tree to be saved and in no case closer than 5 feet to the trunk of any tree.
 - Fence posts shall be either standard steel posts or wood posts with a minimum cross sectional area of 3.0 sq. in.
 - The fence may be either 40" high snow fence, 40" plastic web fencing or any other material as approved by the engineer/inspector.

REFERENCE Project	DATE	DESIGNED	DATE	CHECKED	DATE	APPROVED	DATE

STANDARD DWG. NO. IUM-5610 SHEET 1 OF 1 DATE: 6-7-04



INLET PROTECTION - PAVED AREAS DROP-IN PROTECTION

- MAINTENANCE:**
- INSPECT ALL INLET PROTECTION PRACTICE OR DEVICE SHALL BE INSPECTED AFTER EVERY RUNOFF EVENT. ACCUMULATED SEDIMENT SHALL BE REMOVED PER MANUFACTURER'S DIRECTIONS BUT NOT LESS THAN WHEN THE CAPACITY FOR SEDIMENT STORAGE HAS BEEN REDUCED BY HALF. SEDIMENT THAT HAS BEEN REMOVED SHALL BE PLACED SUCH THAT IT WILL NOT REENTER THE STORM DRAIN SYSTEM.
- REPAIRS OR REPLACEMENT OF INLET PROTECTION DEVICES SHALL BE MADE IMMEDIATELY. FOR DEVICES TO BE KEPT IN PLACE IN THE WINTER SEASON, AREAS SHALL BE CLEARED OF ANY SEDIMENT ACCUMULATION AND PREPARED OR PROTECTED FOR SNOW REMOVAL OPERATIONS.
- INLET PROTECTION PRACTICES SHALL BE REMOVED UPON JOB COMPLETION.

REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS

DATE: 01-11-11

PANDA EXPRESS CHINESE KITCHEN

PANDA EXPRESS, INC.
 1683 Walnut Grove Ave.
 Rosemead, California
 91770
 Telephone: 628.799.8888
 Facsimile: 626.372.0288

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REVISIONS:

VILLAGE PLAN REV. #1	01/19/2021
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ISSUE DATE:

PC SUBMITTAL	11/20/2020
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DRAWN BY: LEH

PANDA PROJECT #: D7058
 ATWELL PROJECT #: 18003769

NORR

866.850.4200 www.atwell-group.com
 1150 EAST OGDEN ROAD, SUITE 300
 NAPERVILLE, IL 60563
 DESIGN TEAM #16-00018

ATWELL

PANDA EXPRESS

WARM & WELCOME 2300
 7505 KINGCERY HIGHWAY
 WILLOWBROOK, IL 60527

SOIL EROSION AND SEDIMENTATION CONTROL NOTES AND DETAILS C03.1

TRUE WARM & WELCOME 2300

811

Know what's below.
 Call before you dig.

THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE MANNER AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVE. THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND ACCESS TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES THAT MAY BE OCCURRED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

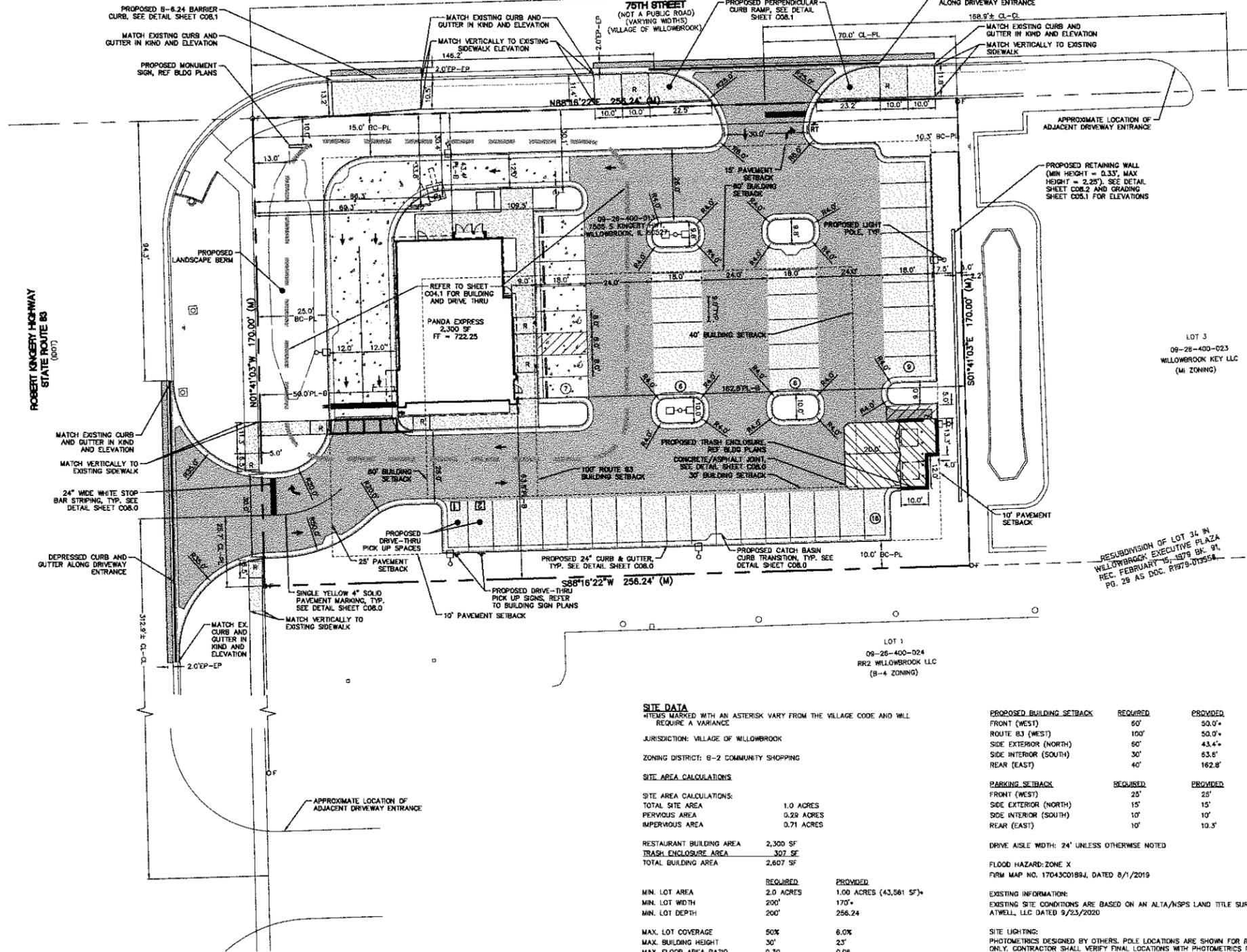
NOTICE: CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE OWNER OR ITS REPRESENTATIVE SHALL NOT BE RESPONSIBLE FOR THE SAFETY OF PERSONS ENGAGED IN THE WORK, OR ANY NEARBY STRUCTURES, OR OF ANY OTHER PERSONS.

REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS

DATE: 1/19/2021
 EXP: 11/30/2021

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DATE: 1/19/2021
 EXP: 11/30/2021



LAYOUT LEGEND

---	BOUNDARY LINE
- - - -	EXISTING ROW
- · - · -	EXISTING CURB AND GUTTER
- - - -	PROPOSED CURB AND GUTTER
- · - · -	PROPOSED DEPRESSED CURB AND GUTTER
- - - -	EXISTING EASEMENT LINE
- · - · -	PROPOSED LANDSCAPE BERM
- - - -	SETBACK LINE
○	PARKING SPACE COUNT
R	ADA RAMP
BC	DIMENSION TO BACK OF CURB
PL	DIMENSION TO PROPERTY LINE
CL	DIMENSION TO CENTER LINE
EP	DIMENSION TO EDGE OF PAVEMENT
BP	DIMENSION TO BUILDING
□	PROPOSED LIGHT POLE/WALL PACK LIGHT
+	PROPOSED SIGN
↓ ADA	PROPOSED ACCESSIBLE PARKING SPACE SIGN
↓ S	PROPOSED STOP SIGN
↓ DNE	PROPOSED DO NOT ENTER SIGN
↓ RT	PROPOSED RIGHT TURN ONLY SIGN
▨	PROPOSED REINFORCED CONCRETE PAVEMENT, SEE DETAIL SHEET C08.2
▩	PROPOSED CONCRETE SIDEWALK, SEE DETAIL SHEET C08.2
▧	PROPOSED STANDARD DUTY ASPHALT PAVEMENT, SEE DETAIL SHEET C08.2
▦	PROPOSED HEAVY DUTY ASPHALT PAVEMENT, SEE DETAIL SHEET C08.2
▤	TRASH ENCLOSURE PAVEMENT, SEE DETAIL SHEET C08.2

- NOTES:**
- SEE SHEET C01.1 FOR CONSTRUCTION NOTES.
 - ALL DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 - ALL RADII ARE 3.0' UNLESS OTHERWISE NOTED.
 - REFER TO ARCHITECTURAL PLANS FOR SITE LIGHTING ELECTRICAL PLANS.
 - ALL STRIPING SHALL BE 4" WHITE PAINT UNLESS OTHERWISE NOTED. ALL STRIPING SHALL BE APPLIED WITH TWO COATS OF PAINT AT MANUFACTURER'S RECOMMENDED RATE. SEE STRIPING DETAIL SHEET C08.0

INTERIOR LANDSCAPING
 SITE AREA EXCLUDING FRONT YARD, ROW, BUILDINGS, AND STRUCTURES: 36,576 SF
 REQUIRED INTERIOR LANDSCAPING: 3,658 SF (10%)
 PROVIDED INTERIOR LANDSCAPING: 9,408 SF (26%)

ADJACENT LAND USE DATA

ZONING DISTRICT	LAND USE	DEVELOPMENT NAME
NORTH: B2 COMMUNITY SHOPPING	SHOPPING CENTER	WILLOWBROOK PLAZA
SOUTH: B4 HWY & SERVICE BUSINESS	HOTEL	RED ROOF INN
EAST: M1 LIGHT MANUFACTURING	MANUFACTURING/SALES	WILLOWBROOK KEY LLC
WEST: B2 COMMUNITY SHOPPING	RESTAURANT	POTBELLY

SITE DATA
 ITEMS MARKED WITH AN ASTERISK VARY FROM THE VILLAGE CODE AND WILL REQUIRE A VARIANCE

JURISDICTION: VILLAGE OF WILLOWBROOK
 ZONING DISTRICT: B-2 COMMUNITY SHOPPING

SITE AREA CALCULATIONS

TOTAL SITE AREA	1.0 ACRES
PERVIOUS AREA	0.29 ACRES
IMPERVIOUS AREA	0.71 ACRES

RESTAURANT BUILDING AREA
 TRASH ENCLOSURE AREA: 307 SF
 TOTAL BUILDING AREA: 2,607 SF

MIN. LOT AREA
 MIN. LOT WIDTH: 200'
 MIN. LOT DEPTH: 200'

MAX. LOT COVERAGE
 MAX. BUILDING HEIGHT: 30'
 MAX. FLOOR AREA RATIO: 0.30

EXISTING BUILDING SETBACK

FRONT (WEST)	80'	PROVIDED 61.5'
ROUTE 83 (WEST)	100'	PROVIDED 61.5'
SIDE EXTERIOR (NORTH)	40'	PROVIDED 52.2'
SIDE INTERIOR (SOUTH)	100'	PROVIDED 58.2'
REAR (EAST)	30'	PROVIDED 127.8'

PROPOSED BUILDING SETBACK

FRONT (WEST)	60'	PROVIDED 50.0'
ROUTE 83 (WEST)	100'	PROVIDED 50.0'
SIDE EXTERIOR (NORTH)	60'	PROVIDED 43.4'
SIDE INTERIOR (SOUTH)	30'	PROVIDED 33.8'
REAR (EAST)	40'	PROVIDED 162.8'

PARKING SETBACK

FRONT (WEST)	25'	PROVIDED 25'
SIDE EXTERIOR (NORTH)	15'	PROVIDED 15'
SIDE INTERIOR (SOUTH)	10'	PROVIDED 10'
REAR (EAST)	10'	PROVIDED 10.3'

DRIVE AISLE WIDTH: 24' UNLESS OTHERWISE NOTED

FLOOD HAZARD: ZONE X
 FIRM MAP NO. 1704300189J, DATED 8/1/2019

EXISTING INFORMATION:
 EXISTING SITE CONDITIONS ARE BASED ON AN ALTA/NSP'S LAND TITLE SURVEY BY ATWELL, LLC DATED 9/23/2020

SITE LIGHTING:
 PHOTOMETRICS DESIGNED BY OTHERS. POLE LOCATIONS ARE SHOWN FOR REFERENCE ONLY. CONTRACTOR SHALL VERIFY FINAL LOCATIONS WITH PHOTOMETRICS PLAN AND OWNER PRIOR TO CONSTRUCTION.

PARKING DATA

LOADING BERTHS	0	PROVIDED 0
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REQUIRED PARKING CALCULATION:
 1 PARKING SPACE PER 100 SF OF FLOOR AREA
 1 SPACE/100 SF + 2,300 SF = 23 SPACES

TOTAL REQUIRED SPACES: 23 SPACES
 PROVIDED: 46 SPACES

REGULAR (9' X 18') 42 SPACES
 DRIVE THRU PICKUP (9' X 18') 2 SPACES
 ADA (9' X 18') 2 SPACES
 TOTAL PROVIDED SPACES: 46 SPACES



PANDA EXPRESS, INC.
 1683 Walnut Grove Ave.
 Rosemead, California 91770
 Telephone: 626.799.9998
 Facsimile: 626.372.8288

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REVISIONS:

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ISSUE DATE:

PC SUBMITTAL	11/20/2020
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DRAWN BY: LEH

PANDA PROJECT #: D7058
 ATWELL PROJECT #: 18003769



PANDA EXPRESS
 WARM & WELCOME 2300
 7505 KINGERY HIGHWAY
 WILLOWBROOK, IL 60527

SITE LAYOUT PLAN

C04.0

TRUE WARM & WELCOME 2300



THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN AS AN APPROXIMATE INDICATION ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVE. THE CONTRACTOR SHALL OBTAIN THE MOST CURRENT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND ACCESS TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESENCE ANY AND ALL UNDERGROUND UTILITIES.

24-HOUR CONTACT:
 PANDA PM
 HAKIM YALA
 847-477-5125

DATE: 1/19/2021
 EXP: 11/30/2021

ROSE E. HEAVY
 083-081301
 REGISTERED PROFESSIONAL ENGINEER
 ILLINOIS

SCALE 0 10 20
 1" = 20 FEET

CONTRACTOR SHALL PROTECT ALL ITEMS OUTSIDE LIMITS OF CONSTRUCTION UNLESS OTHERWISE NOTED IN THE CONSTRUCTION PLANS OR SPECIFICATIONS.

CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITIES (LOCATIONS AND ELEVATIONS) PRIOR TO STARTING CONSTRUCTION AND ALERT ENGINEER TO ANY DISCREPANCIES IMMEDIATELY.

CONTRACTOR SHALL PROTECT ALL ITEMS OUTSIDE LIMITS OF CONSTRUCTION UNLESS OTHERWISE NOTED IN THE CONSTRUCTION PLANS OR SPECIFICATIONS.

CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITIES (LOCATIONS AND ELEVATIONS) PRIOR TO STARTING CONSTRUCTION AND ALERT ENGINEER TO ANY DISCREPANCIES IMMEDIATELY.



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PANDA PROJECT # D7058
ATWELL PROJECT # 18003769

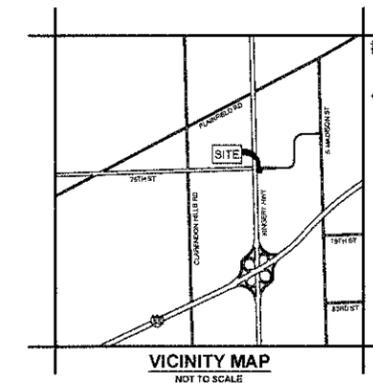


PANDA EXPRESS
WARM & WELCOME 2300
7505 KINGERY HIGHWAY
WILLOWBROOK, IL 60527

GRADING PLAN

C05.0

TRUE WARM & WELCOME 2300



GRADING LEGEND

---	BOUNDARY LINE
---	EXISTING ROW
---	EXISTING CONTOUR
---	PROPOSED CONTOUR
---	PROPOSED RIDGE LINE
---	EXISTING STORM SEWER
---	PROPOSED STORM SEWER
---	PROPOSED PITCH-OUT CURB AND GUTTER
---	EXISTING STORM CATCH BASIN/MANHOLE
---	PROPOSED STORM CATCH BASIN/MANHOLE
---	PROPOSED SPOT ELEVATION
---	PROPOSED RIM ELEVATION
---	MATCH EXISTING ELEVATION
---	PROPOSED FINISHED GRADE ELEVATION
---	PROPOSED TOP OF CURB ELEVATION
---	PROPOSED EDGE OF PAVEMENT ELEVATION
---	PROPOSED FINISHED GRADE AT TOP OF WALL
---	PROPOSED FINISHED GRADE AT BOTTOM OF WALL

NOTES:

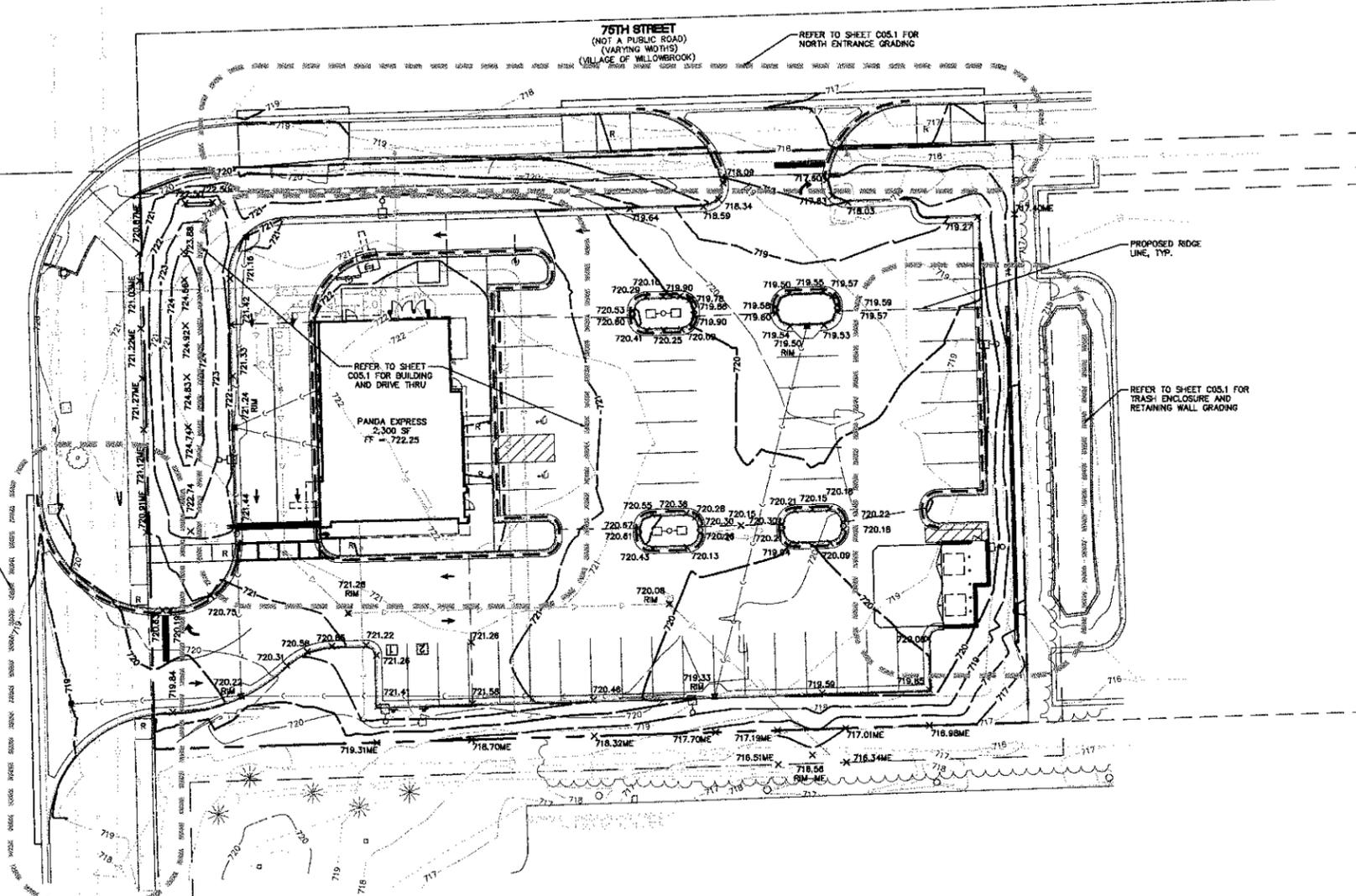
- SEE SHEET C01.1 FOR GENERAL NOTES.
- SEE LANDSCAPE PLAN FOR REQUIRED TREES AND GROUND COVER.
- SLOPE OF SURFACE GRADE WITHIN LANDSCAPED AREAS SHALL BE A MINIMUM OF 1.00%.
- MAXIMUM CUT OF FILL SLOPES IS 3H:1V.
- THE CONTRACTOR SHALL PROVIDE CLEAN, SUITABLE MATERIAL FOR REQUIRED FILL. SHOULD A SUFFICIENT QUANTITY OF SUITABLE MATERIAL NOT BE AVAILABLE FROM THE REQUIRED EXCAVATION ON THE SITE.
- ALL FILL SHOULD BE PLACED IN THIN, HORIZONTAL LOOSE LIFTS (MAXIMUM 9-INCH) AND COMPACTED TO AT LEAST 95 PERCENT OF THE MODIFIED PROCTOR MAXIMUM DRY DENSITY (ASTM D 1557). THE UPPER 8 INCHES OF SOIL BENEATH PAVEMENTS AND SLAB-ON-GRADE SHOULD BE COMPACTED TO AT LEAST 98 PERCENT. COMPACTOR MUST BE CERTIFIED BY AN ILLINOIS REGISTERED PROFESSIONAL SOILS ENGINEER PRIOR TO THE INSTALLATION OF PAVEMENTS, CURBS, SIDEWALKS OR FOOTINGS OF ANY TYPE.
- JURISDICTIONAL LAND DISTURBANCE PERMIT MUST BE DISPLAYED ON SITE AT ALL TIMES DURING CONSTRUCTION AND IN PLAIN VIEW FROM A PUBLIC ROAD OR STREET.

BUILDING AREA NOTES:

- MAINTAIN ACCESS FOR EMERGENCY VEHICLES AROUND AND TO ALL BUILDINGS UNDER CONSTRUCTION. IN TIMES OF RAIN OR MUD, ROADS SHALL BE PASSABLE TO EMERGENCY VEHICLES BY BEING PAVED OR HAVING A CRUSHED STONE BASE ETC. WITH A MINIMUM WIDTH OF 20 FEET. THE ACCESS TO BUILDINGS HAVING SPRINKLER OR STANDPIPE SYSTEMS SHALL BE TO WITHIN 40 FEET OF THE FIRE DEPARTMENT CONNECTION (NFPA 1141 3-1).
- CONTRACTOR TO PROVIDE POSITIVE DRAINAGE AWAY FROM BUILDING IN ALL AREAS AROUND BUILDING.

HYDROLOGY STATEMENT:

ON-SITE STORMWATER RUN-OFF WILL BE COLLECTED VIA PROPOSED CATCH BASINS. A PROPOSED STORM PIPE SYSTEM WILL CONVEY STORMWATER TO AN EXISTING STORM PIPE LOCATED ALONG ROUTE 83. THE EXISTING 18" PIPE WILL CONVEY THE STORMWATER SAWMILL CREEK.



LEGAL DESCRIPTION

FIRST AMERICAN TITLE INSURANCE COMPANY
COMMITMENT NO. NCS-985841-01E
COMMITMENT DATE: AUGUST 28, 2020

THE NORTH 170.0 FEET OF THAT PART OF THE SOUTHEAST 1/4 OF SECTION 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING EAST OF THE EAST RIGHT OF WAY OF STATE ROUTE 83, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT 102.65 FEET EAST OF THE NORTHWEST CORNER OF SAID SOUTHEAST 1/4; THENCE EAST, ALONG THE NORTH LINE OF SAID SOUTHEAST 1/4, A DISTANCE OF 256.24 FEET; THENCE SOUTH, ALONG A LINE PARALLEL WITH THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170 FEET; THENCE WEST, ALONG A LINE PARALLEL WITH THE NORTH LINE OF SAID SOUTHEAST 1/4, A DISTANCE OF 256.24 FEET TO THE INTERSECTION OF SAID PARALLEL LINE WITH THE EAST RIGHT OF WAY OF SAID STATE ROUTE 83, WHICH POINT IS 102.45 FEET EAST OF THE WEST LINE OF SAID SOUTHEAST 1/4; THENCE NORTH, ALONG THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170.0 FEET TO THE POINT OF BEGINNING, IN DUPAGE COUNTY, ILLINOIS.

SOURCE BENCHMARK

NGS 0135 - RID (DK3296)
TO REACH THE STATION FROM THE INTERSECTION OF SR 83 AND PLAINFIELD ROAD, GO EAST ALONG PLAINFIELD ROAD FOR APPROXIMATELY 0.5 MI (0.8 KM) TO THE STATION ON THE RIGHT. THE STATION IS LOCATED AT THE SOUTHEAST CORNER OF THE INTERSECTION OF PLAINFIELD ROAD AND MADISON STREET.

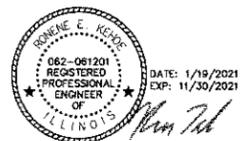
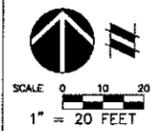
THE STATION IS 70.0 FT (21.3 M) SOUTH OF THE CENTERLINE OF PLAINFIELD ROAD AND 38.3 FT (11.7 M) EAST OF THE CENTERLINE OF MADISON STREET. THE MONUMENT IS A 3.5 INCH (9 CM) BRASS DISK ON THE BASE OF A TRAFFIC SIGNAL, 0.8 FT (0.2 M) ABOVE GRADE.

ELEVATION: 732.18 (NAVD 88)

SITE BENCHMARKS AND CONTROL

- BM #204 - SET BENCHMARK ON THE SOUTHERLY BOLT, UNDER THE WORD "CITY", ON A HYDRANT ON THE SOUTH SIDE OF 75TH STREET, 306' EAST OF THE INTERSECTION OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY. ELEVATION - 719.58 (NAVD 88)
- CONTROL #1 - SET GAPPED IRON ROD AT THE SOUTHEAST CORNER OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY, 15' SOUTHEAST OF A TRAFFIC SIGNAL AND 18' EAST OF THE BACK OF CURB OF ROUTE 83. N: 1852569.52 (NAD 83) E: 1090379.82 (NAD 83)
- CONTROL #2 - SET OUT "X" IN A CONCRETE SIDEWALK ON THE EAST SIDE OF ROUTE 83, ROBERT KINGERY HIGHWAY, 235' SOUTH OF THE INTERSECTION OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY, 26' EAST OF THE BACK OF CURB OF ROUTE 83. N: 1852417.41 (NAD 83) E: 10903392.03 (NAD 83)
- CONTROL #3 - SET OUT "X" IN A CONCRETE SIDEWALK ON THE SOUTH SIDE OF 75TH STREET, 315' EAST OF THE INTERSECTION OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY, AND 7' NORTH OF A FIRE HYDRANT. N: 1852623.35 (NAD 83) E: 1090603.32 (NAD 83)

24-HOUR CONTACT:
PANDA FM
HAKIM YALA
847-477-5125



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THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE MANNER AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE CONTRACTOR. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MAY BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXERCISE CARE AND PREVENT ANY AND ALL UNDERGROUND UTILITIES.

NOTICE: CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. NEITHER THE OWNER NOR THE ENGINEER SHALL BE EXPECTED TO ASSUME ANY RESPONSIBILITY FOR SAFETY OR THE WORK OF ANY OTHER PERSONS, OR OF ANY OTHER PERSONS.

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REVISIONS:
VILLAGE PLAN REV. #1 01/19/2021

ISSUE DATE:
PC SUBMITTAL 11/20/2020

DRAWN BY: LEH

PANDA PROJECT #: D7058
ATWELL PROJECT #: 18003769



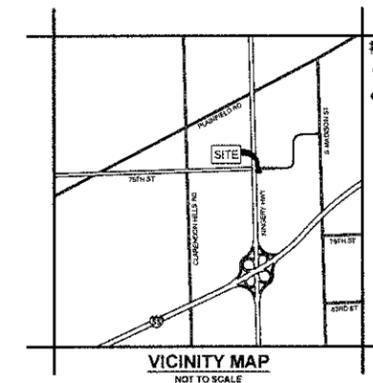
PANDA EXPRESS

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7505 KINGERY HIGHWAY
WILLOWBROOK, IL 60527

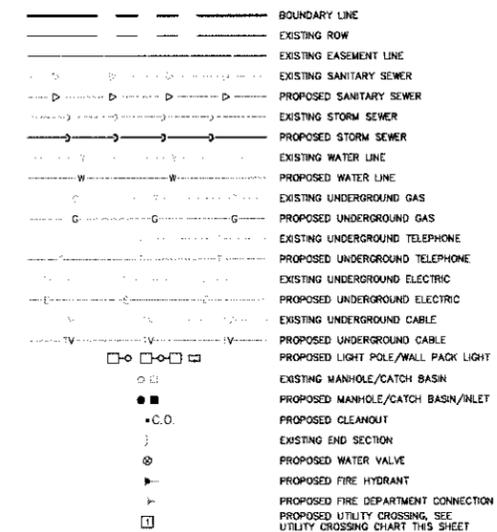
STORM SEWER PLAN

C06.0

TRUE WARM & WELCOME 2300



STORM LEGEND



CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION WITH APPROPRIATE UTILITY COMPANIES TO ENSURE THAT THE REQUIRED VERTICAL AND HORIZONTAL CLEARANCES AT ALL LOCATIONS WHERE ANY UTILITY LINE CROSSES AN EXISTING OR PROPOSED GAS, TELEPHONE OR ELECTRIC LINE ARE MET.

NOTES

- SEE SHEET C01.1 FOR GENERAL NOTES.
- SEE MEP PLANS FOR CONTINUATION OF ALL UTILITIES INTO BUILDING.
- CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITIES AND THEIR LOCATIONS AND ELEVATIONS PRIOR TO STARTING CONSTRUCTION.
- THE CONTRACTOR SHALL CONTACT PUBLIC UTILITIES INSPECTIONS AT LEAST 72 HOURS PRIOR TO ANY CONSTRUCTION ACTIVITY.
- ALL WORK TO BE DONE IN STRICT ACCORDANCE WITH LOCAL GOVERNING CODES.
- CONTROLLED BACK FILL TO BE PLACED IN 9" LOOSE LIFT AND COMPACTED PER GEOTECHNICAL REPORT RECOMMENDATIONS PRIOR TO STORM AND SANITARY SEWER CONSTRUCTION. BACK FILL SHALL BE PLACED TO A MINIMUM OF ±2' ABOVE THE CROWN ELEVATION OF THE PIPES.
- STORM SEWER AND SANITARY SEWER LENGTHS ARE MEASURED FROM CENTER LINE OF STRUCTURE TO CENTERLINE OF STRUCTURE OR FACE OF HEADWALL.
- ALL PIPE LENGTHS SHOWN ARE ROUNDED TO THE NEAREST FOOT.
- ALL STORM SEWER PIPING SHALL BE TRENCHED, BEDDED AND BACK FILLED ACCORDING TO DETAIL ON SHEET C08.3 UNLESS SPECIFICALLY NOTED OTHERWISE.
- UNFORESEEN SUBSURFACE CONDITIONS SHALL BE BROUGHT TO THE OWNER'S AND ENGINEER'S ATTENTION IMMEDIATELY IMPLEMENTATION OF CORRECTIVE BEDDING MEASURES WITHOUT THE OWNER'S APPROVAL. SHALL BE AT THE CONTRACTOR'S OWN RISK AND AT NO ADDITIONAL COMPENSATION.
- EXISTING GRADES SHOWN ARE APPROXIMATE AND DO NOT REFLECT TOP SOIL REMOVAL, CLEARING, AND GRUBBING OPERATIONS. THE CONTRACTOR SHALL ASCERTAIN FOR HIMSELF THE EXTENT OF DISTURBANCE FOR THESE ACTIVITIES.
- THE CONTRACTOR SHALL REFERENCE THE GEOTECHNICAL REPORT PREPARED FOR THE OWNER FOR SUBSURFACE CONDITIONS. THE GEOTECHNICAL REPORT IS NOT A PART OF THE CONTRACT DOCUMENTS.
- EXCAVATIONS FOR STRUCTURES SHALL BE TAKEN AS A TRENCHING EXCAVATION WITHOUT FURTHER COMPENSATION.

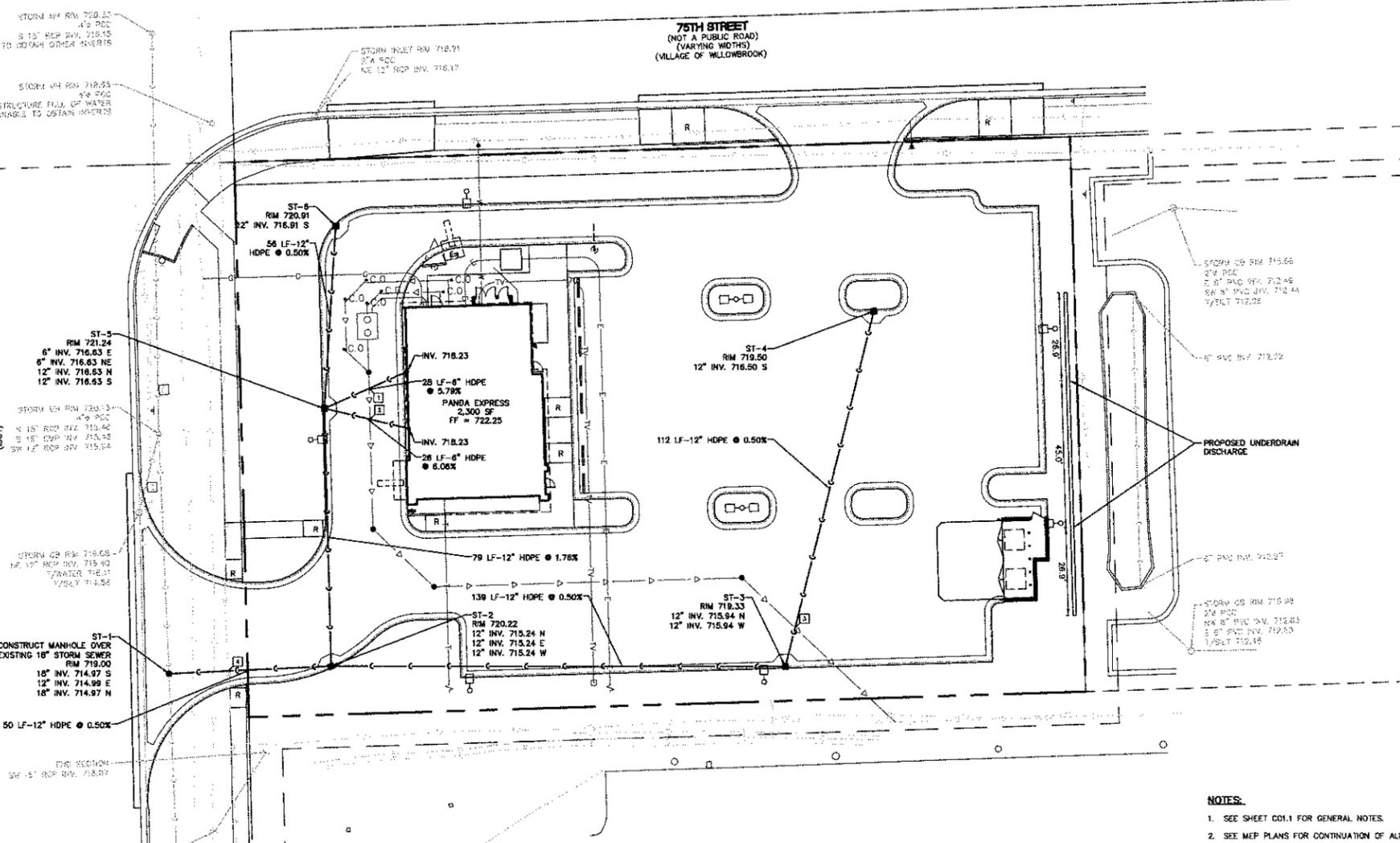
CONTRACTOR SHALL COORDINATE AND ADJUST LOCATION OF LOOP DETECTORS TO AVOID UTILITY CONFLICTS PRIOR TO CONSTRUCTION.

CONTRACTOR SHALL COORDINATE AND VERIFY LOCATION OF ALL SIGNAGE WITH OWNER PRIOR TO CONSTRUCTION.

CONTRACTOR SHALL INSTALL GENERAL UTILITY CONDUITS TO PLANTERS AROUND BUILDING AND PATIO. SEE ARCHITECTURAL/MEP PLANS FOR CONTINUATION.

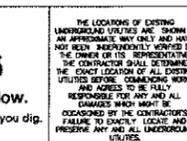
CONTRACTOR SHALL PROTECT ALL ITEMS OUTSIDE LIMITS OF CONSTRUCTION UNLESS OTHERWISE NOTED IN THE CONSTRUCTION PLANS OR SPECIFICATIONS.

CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITIES (LOCATIONS AND ELEVATIONS) PRIOR TO STARTING CONSTRUCTION AND ALERT ENGINEER TO ANY DISCREPANCIES IMMEDIATELY.



CROSSING NUMBER	FINISHED GROUND	TOP UTILITY	B.P. ELEVATION	BOTTOM UTILITY	TIP ELEVATION	CLEARANCE	COMMENT
1	721.84	8" STORM	717.32	8" SANITARY	715.73	1.59	
2	721.51	8" STORM	717.33	8" SANITARY	715.40	1.93	
3	719.53	12" STORM	715.93	8" SANITARY	712.17	3.00	
4	719.75	12" STORM	714.91	12" WATERMAIN	713.41	1.50	FIELD VERIFY DEPTH OF EXISTING WATER MAIN PRIOR TO CONSTRUCTION OF STORM SEWER. 12" WATER MAIN AS NEEDED TO MAINTAIN 1'-0" VERTICAL SEPARATION CLEARANCE.

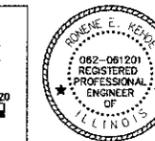
STRUCTURE NUMBER	STRUCTURE TYPE AND SIZE	GRATE TYPE	NOTES
ST-1	4" DIA. TYPE 1 MANHOLE	NEENAH R 1713	CLOSED LID
ST-2	4" DIA. TYPE A CATCH BASIN	NEENAH R 3281-A	
ST-3	4" DIA. TYPE A CATCH BASIN	NEENAH R 3281-A	
ST-4	4" DIA. TYPE A CATCH BASIN	NEENAH R 3281-A	
ST-5	4" DIA. TYPE A CATCH BASIN	NEENAH R 3281-A	
ST-6	4" DIA. TYPE A CATCH BASIN	NEENAH R 3281-A	



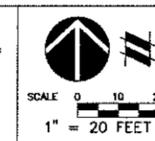
THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE MANNER AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVE. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREE TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MAY BE OCCURRED BY THE CONTRACTORS FAILURE TO CAREFULLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

NOTICE: CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. NEITHER THE OWNER NOR THE ENGINEER SHALL BE EXPECTED TO ASSUME ANY RESPONSIBILITY FOR SAFETY OF THE WORK OF PERSONS ENGAGED IN THE WORK OF ANY HEAVY CONSTRUCTION OR OF ANY OTHER PERSONS.

24-HOUR CONTACT:
PANDA PM
HAKIM YALA
847-477-5125



DATE: 1/15/2021
EXP: 11/30/2021



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ATWELL PROJECT # 18003769

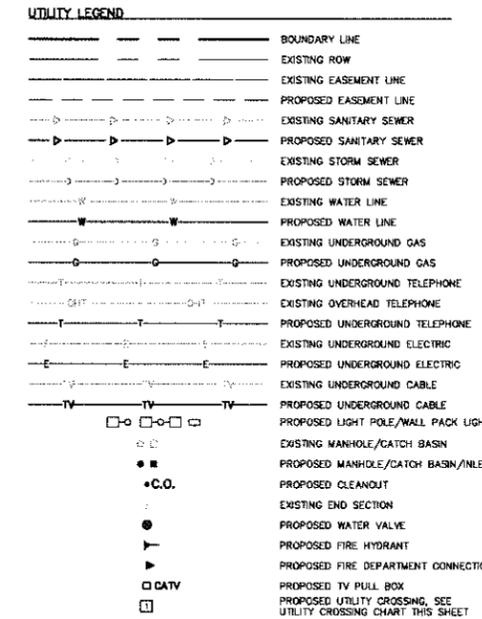
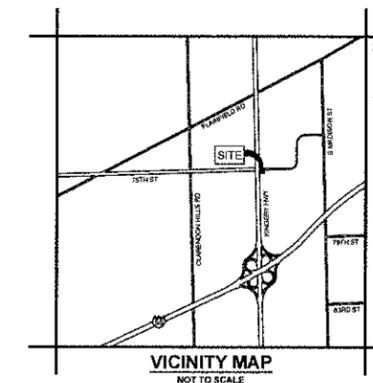


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UTILITY PLAN

C07.0

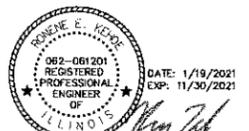
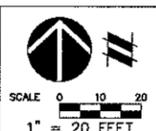
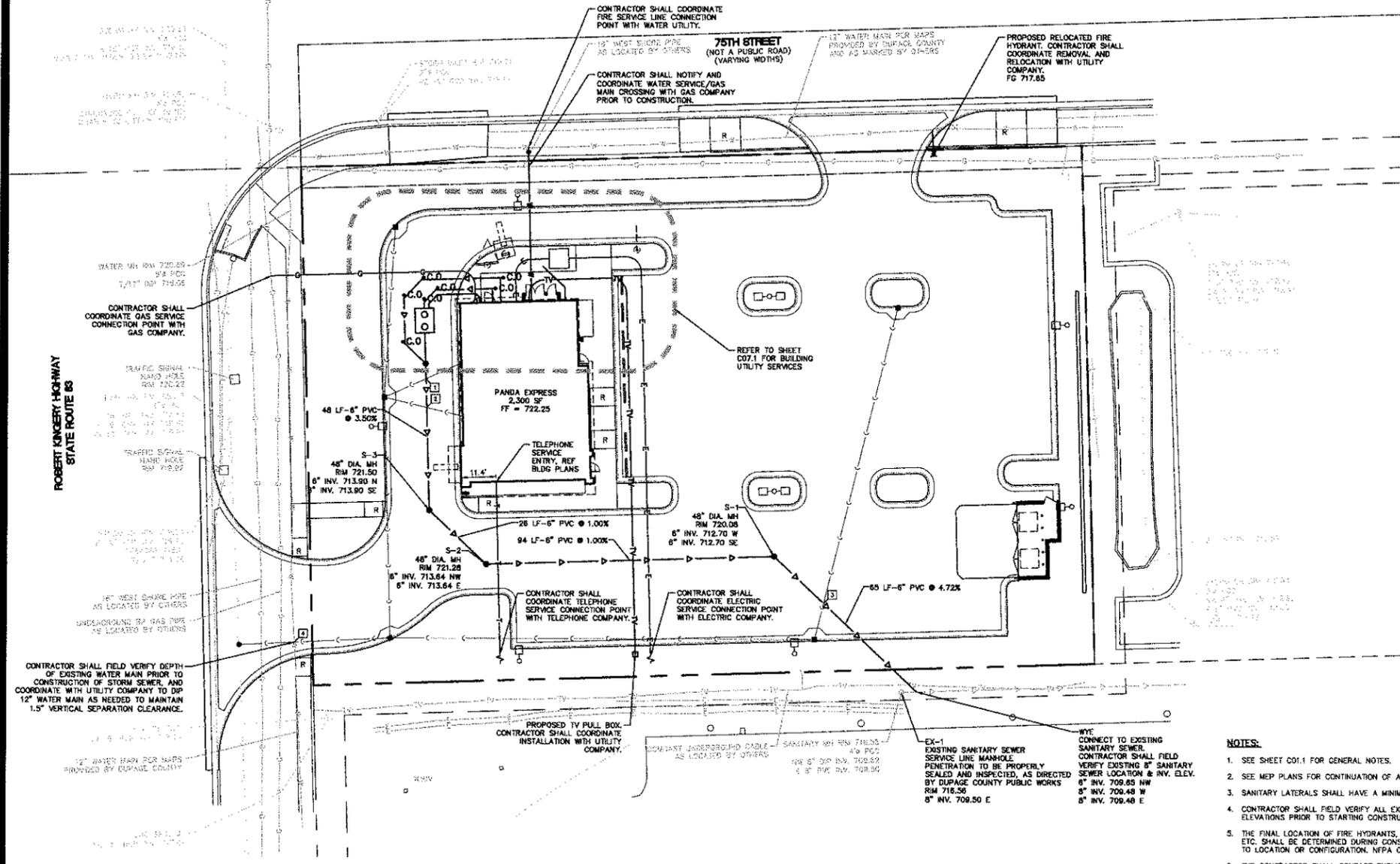
TRUE WARM & WELCOME 2300



CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION WITH APPROPRIATE UTILITY COMPANIES TO ENSURE THAT THE REQUIRED VERTICAL AND HORIZONTAL CLEARANCES AT ALL LOCATIONS WHERE ANY UTILITY LINE CROSSES AN EXISTING OR PROPOSED GAS, TELEPHONE OR ELECTRIC LINE ARE MET.

- NOTES:**
- SEE SHEET C01.1 FOR GENERAL NOTES.
 - SEE MEP PLANS FOR CONTINUATION OF ALL UTILITIES INTO BUILDING.
 - SANITARY LATERALS SHALL HAVE A MINIMUM FALL OF 1.00%.
 - CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITIES AND THEIR LOCATIONS AND ELEVATIONS PRIOR TO STARTING CONSTRUCTION.
 - THE FINAL LOCATION OF FIRE HYDRANTS, VALVES, WATER LINES, BACKFLOW PREVENTERS, ETC. SHALL BE DETERMINED DURING CONSTRUCTION. NOTIFY THE ENGINEER OF ANY CHANGES TO LOCATION OR CONFIGURATION. NFPA CODES SHALL BE ADHERED TO.
 - THE CONTRACTOR SHALL CONTACT PUBLIC UTILITIES INSPECTIONS AT LEAST 72 HOURS PRIOR TO ANY CONSTRUCTION ACTIVITY.
 - ALL WORK TO BE DONE IN STRICT ACCORDANCE WITH LOCAL GOVERNING CODES.
 - UTILITY CONDUIT MATERIAL FOR ELECTRIC, TELEPHONE, AND CABLE SHALL BE INSTALLED PER UTILITY PROVIDER SPECIFICATIONS.
 - CONTRACTOR TO BUILD CONCRETE TRANSFORMER PAD AND COORDINATE THE INSTALLATION OF THE CONDUITS WITH THE UTILITY COMPANY.
 - CONTROLLED BACK FILL TO BE PLACED IN 6" LOOSE LIFT AND COMPACTED PER GEOTECHNICAL REPORT RECOMMENDATIONS PRIOR TO STORM AND SANITARY SEWER CONSTRUCTION. BACK FILL SHALL BE PLACED TO A MINIMUM OF 1.2' ABOVE THE CROWN ELEVATION OF THE PIPES.
 - STORM SEWER AND SANITARY SEWER LENGTHS ARE MEASURED FROM CENTER LINE OF STRUCTURE TO CENTERLINE OF STRUCTURE OR FACE OF HEADWALL.
 - ALL PIPE LENGTHS SHOWN ARE ROUNDED TO THE NEAREST FOOT.
 - ALL SANITARY SEWER PIPING SHALL BE TRENCHED, BEDDED AND BACK FILLED ACCORDING TO DETAIL ON SHEET C08.3 UNLESS SPECIFICALLY NOTED OTHERWISE.
 - UNFORESEEN SUBSURFACE CONDITIONS SHALL BE BROUGHT TO THE OWNER'S AND ENGINEER'S ATTENTION IMMEDIATELY UPON IMPLEMENTATION OF CORRECTIVE BEDDING MEASURES WITHOUT THE OWNER'S APPROVAL SHALL BE AT THE CONTRACTOR'S OWN RISK AND AT NO ADDITIONAL COMPENSATION.
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1	721.54	6" STORM	717.32	6" SANITARY	715.73	1.59	
2	721.51	6" STORM	717.39	6" SANITARY	715.40	1.99	
3	719.53	12" STORM	716.63	6" SANITARY	712.17	3.99	
4	719.78	12" STORM	714.91	12" WATERMAIN	713.41	1.50	FIELD VERIFY DEPTH OF EXISTING WATER MAIN PRIOR TO CONSTRUCTION OF STORM SEWER. DIP 12" WATER MAIN AS NEEDED TO MAINTAIN 1.5" VERTICAL SEPARATION CLEARANCE.



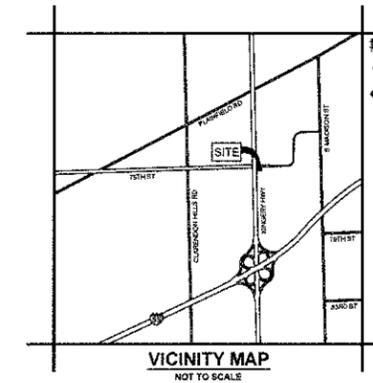
24-HOUR CONTACT:
PANDA PM
HAKIM YALA
847-477-5125

NOTICE:
CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE OWNER SHALL BE RESPONSIBLE TO ASSURE ANY RESPONSIBILITY FOR SAFETY OF THE WORK OF PERSONS EMPLOYED IN THE WORK OF ANY OTHER PERSONS, OR OF ANY OTHER PERSONS.

THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN AS APPROXIMATE. THEY MAY HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR HIS REPRESENTATIVE. THE CONTRACTOR SHALL OBTAIN THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND ASSURE TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MAY BE OCCURRED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PREPARE ANY AND ALL UNDERGROUND UTILITIES.



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ATWELL PROJECT # 18003769

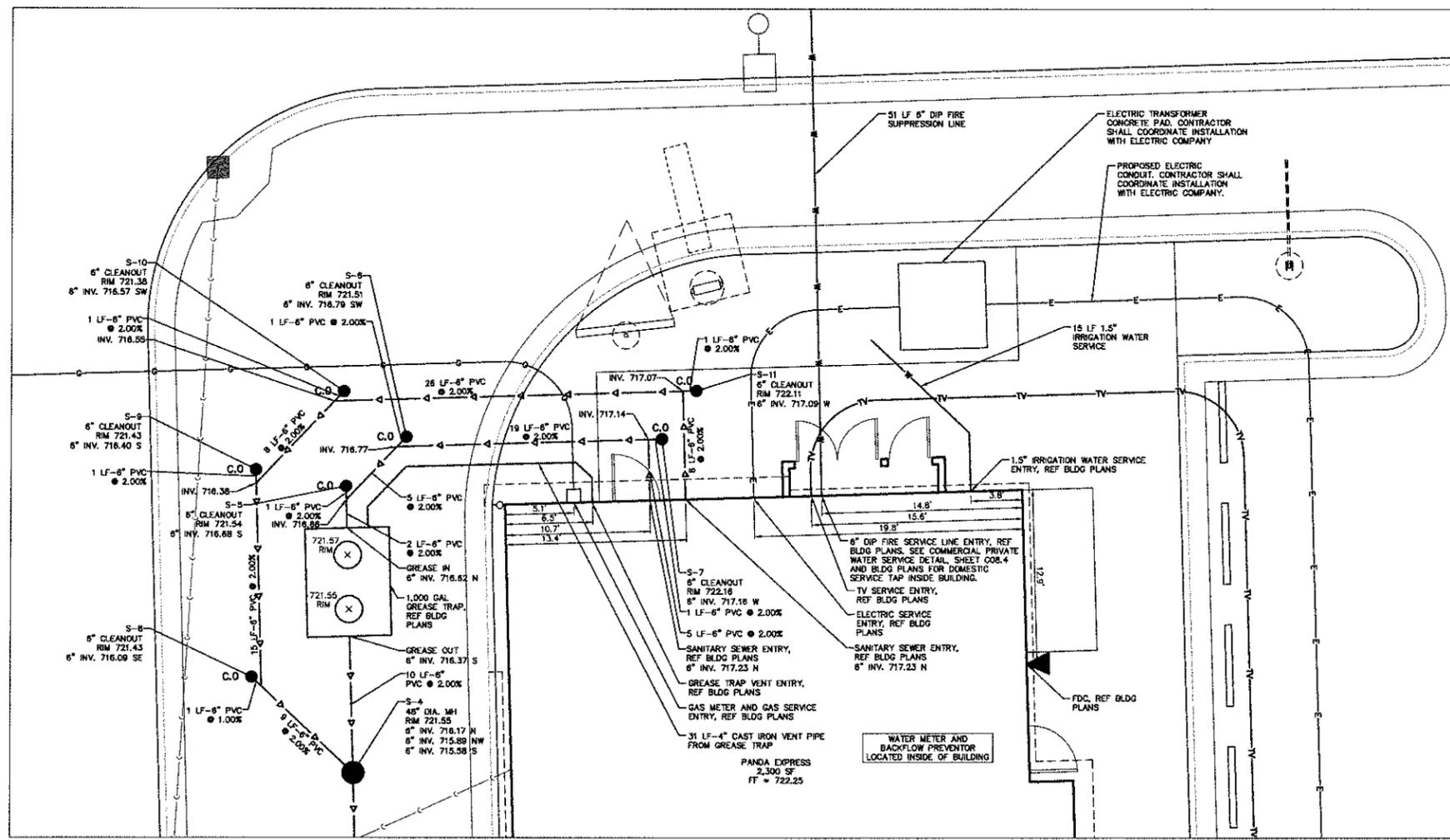


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DETAILED UTILITY
PLAN

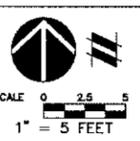
C07.1

TRUE WARM & WELCOME 2300



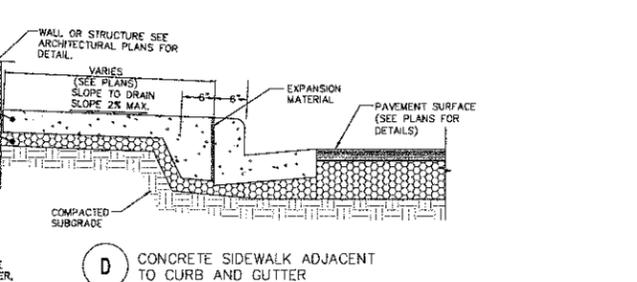
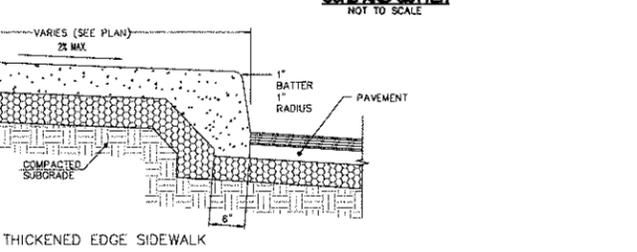
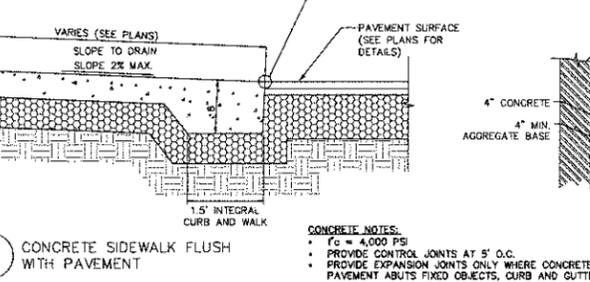
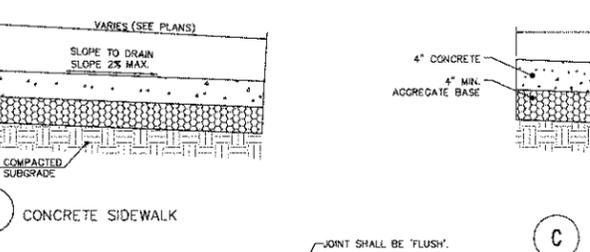
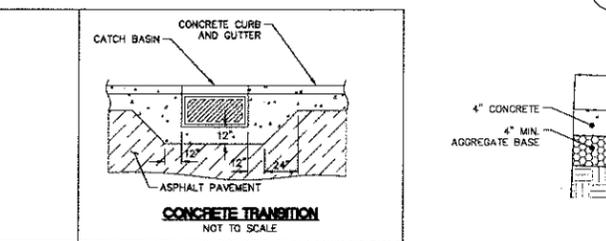
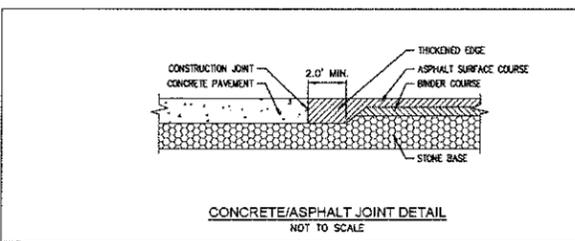
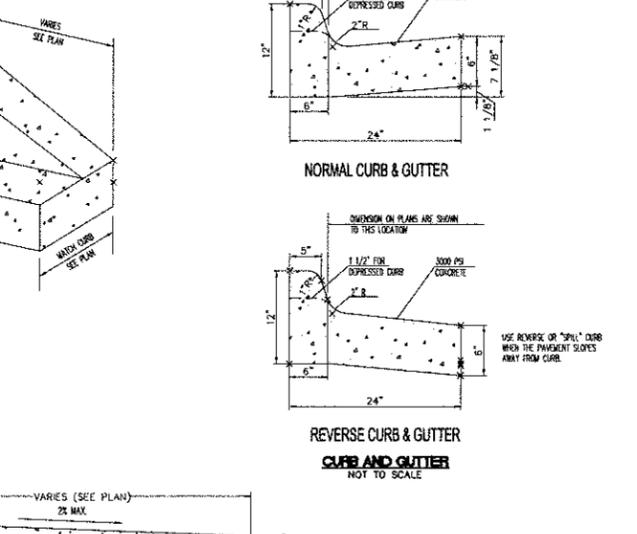
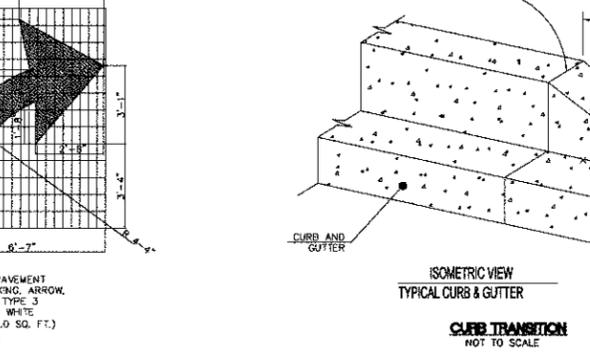
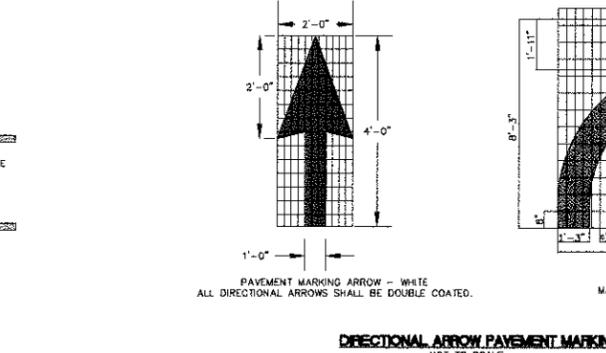
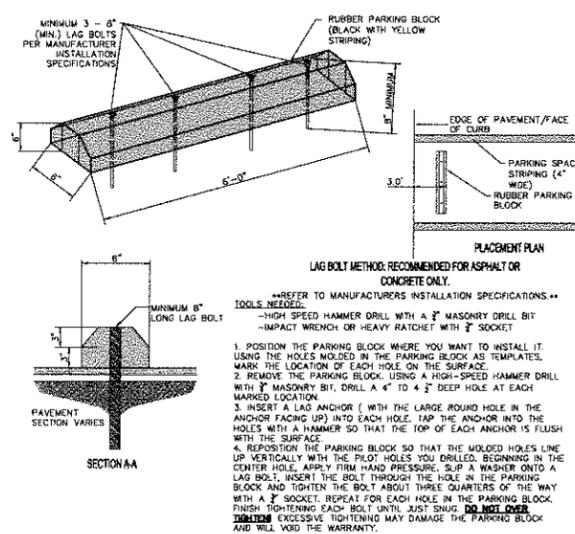
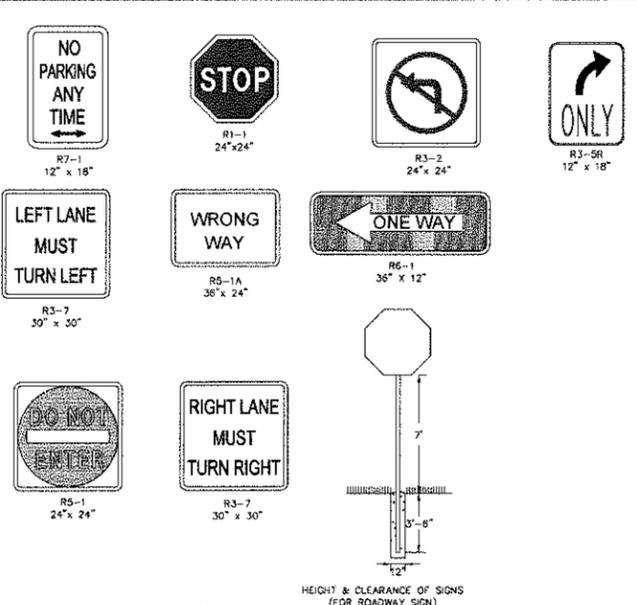
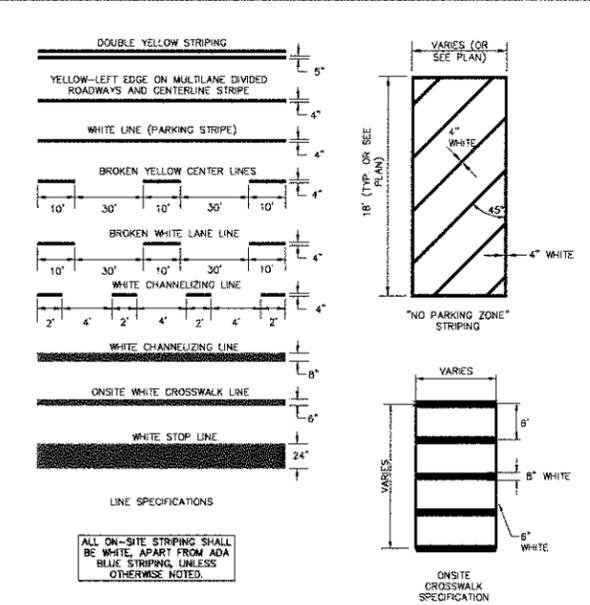
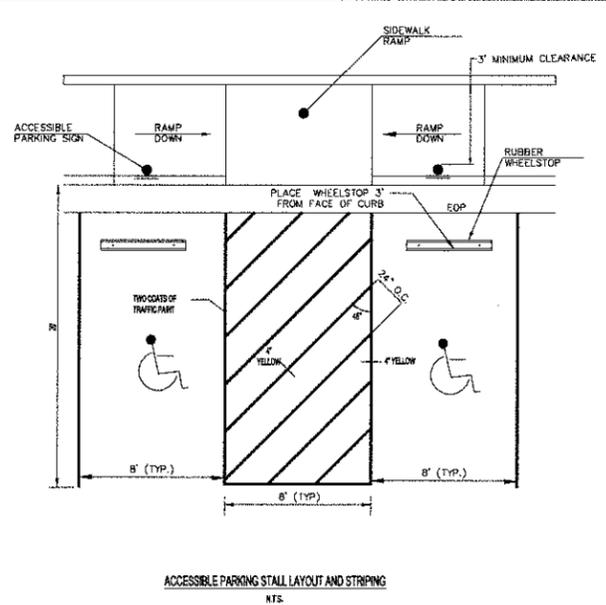
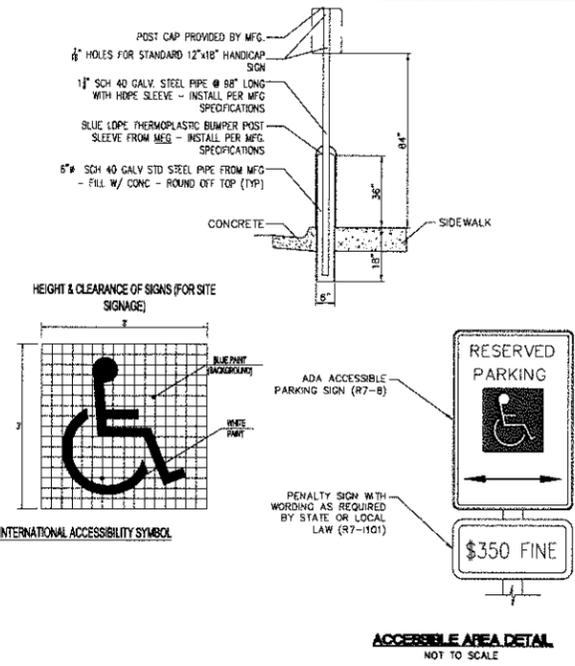
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24-HOUR CONTACT:
PANDA PM
HAKIM YALA
847-477-5125



ROSENE E. HEAR
082-081201
REGISTERED
PROFESSIONAL
ENGINEER
OF
ILLINOIS
DATE: 1/19/2021
EXP: 11/30/2021

S:\PROJECTS\2020\164-006816\DWG\164-006816-07.dwg 1/19/2021 9:24 AM LAUREN HILL



CONCRETE NOTES:

- Fc = 4,000 PSI
- PROVIDE CONTROL JOINTS AT 5' O.C.
- PROVIDE EXPANSION JOINTS ONLY WHERE CONCRETE PAVEMENT ABUTS FIXED OBJECTS, CURB AND GUTTER, AND OTHER PAVEMENT TYPES.
- 6"x8" W/M MAY BE SUBSTITUTED WITH EQUIVALENT STRENGTH FIBER MESH.

CONCRETE SIDEWALK
NOT TO SCALE

811
Know what's below.
Call before you dig.

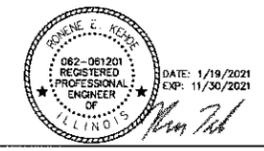
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REGISTERED PROFESSIONAL ENGINEER
DATE: 1/19/2021
EXP: 11/30/2021

24-HOUR CONTACT:
PANDA PM
HAKIM YALA
847-477-5125

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REVISIONS:

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ISSUE DATE:
PC SUBMITTAL 1/20/2020

DRAWN BY: LEH

PANDA PROJECT #: D7058
ATWELL PROJECT #: 18003769

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ARCHITECTS, PLANNERS & ENGINEERS

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DESIGN FIRM #164-000616

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CONSTRUCTION DETAILS

C08.0

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REVISIONS:
VILLAGE PLAN REV. #1 01/19/2021

ISSUE DATE:
PC SUBMITTAL 11/20/2020

DRAWN BY: LEH
PANDA PROJECT # D7058
ATWELL PROJECT # 18003769

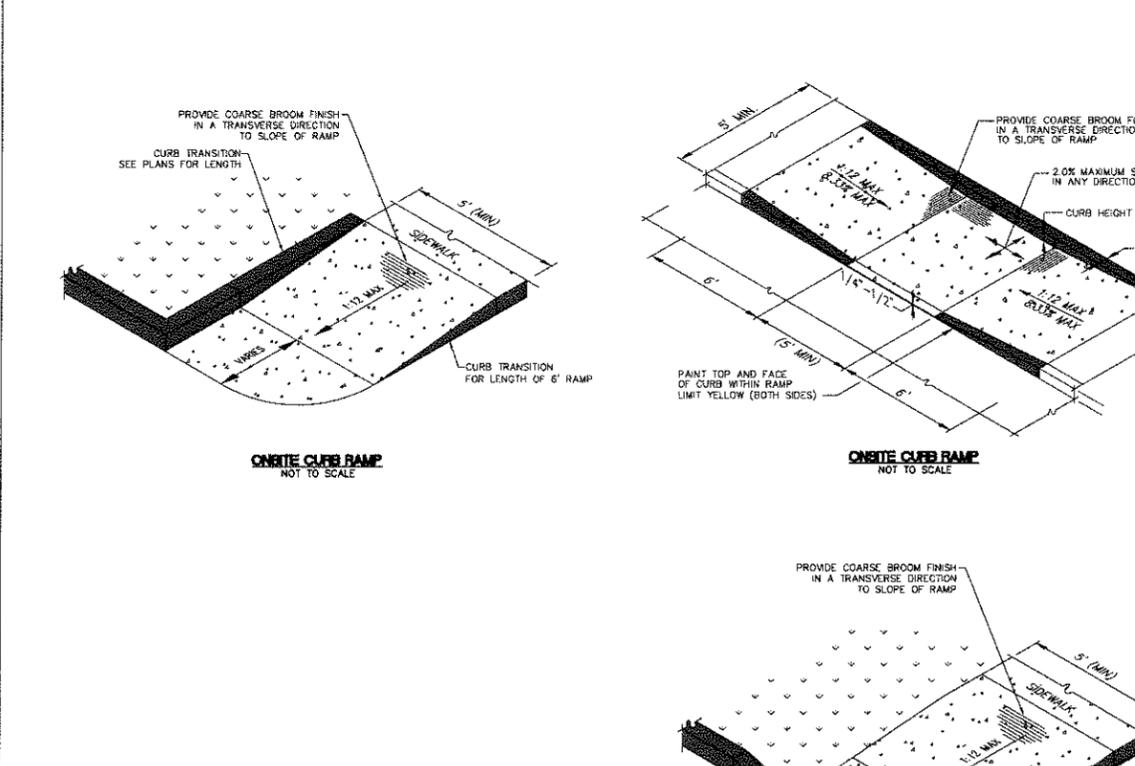
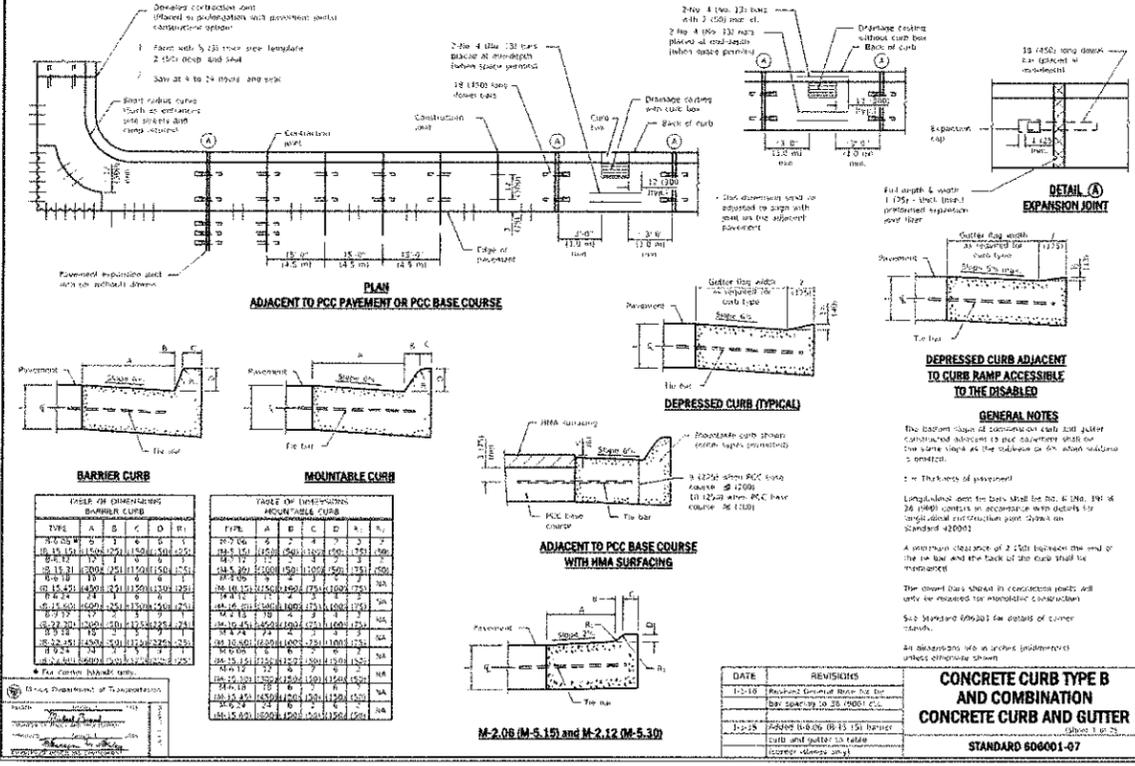
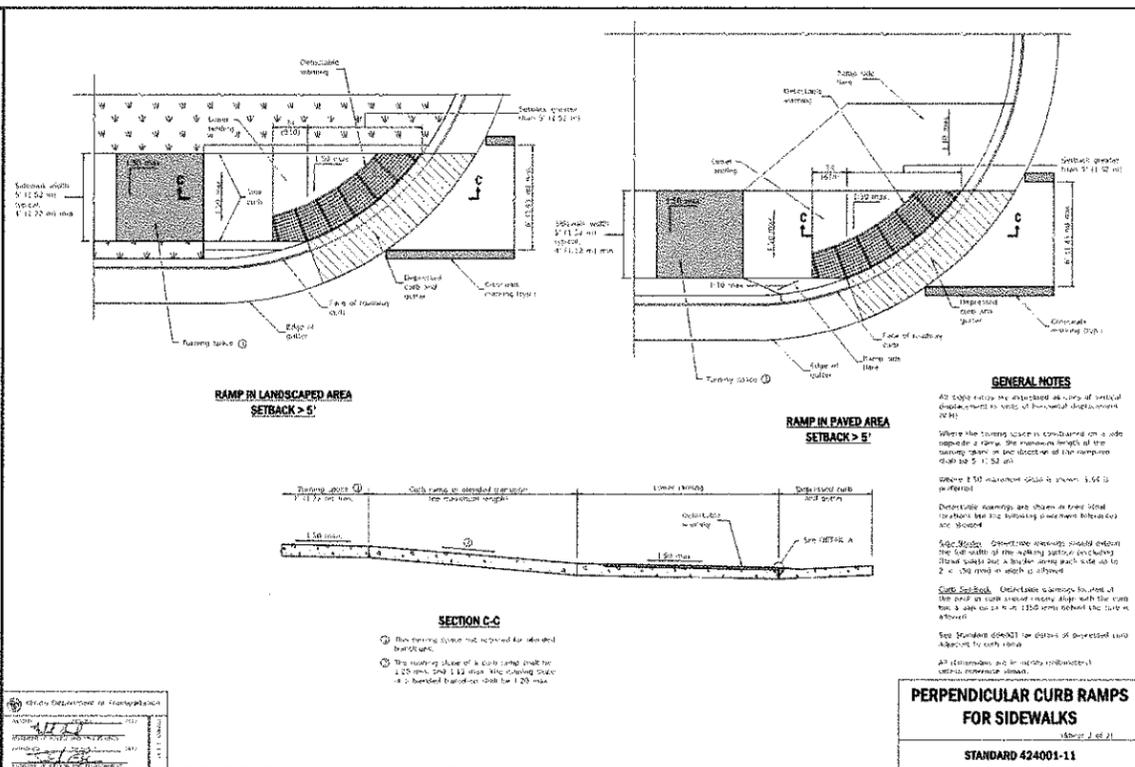
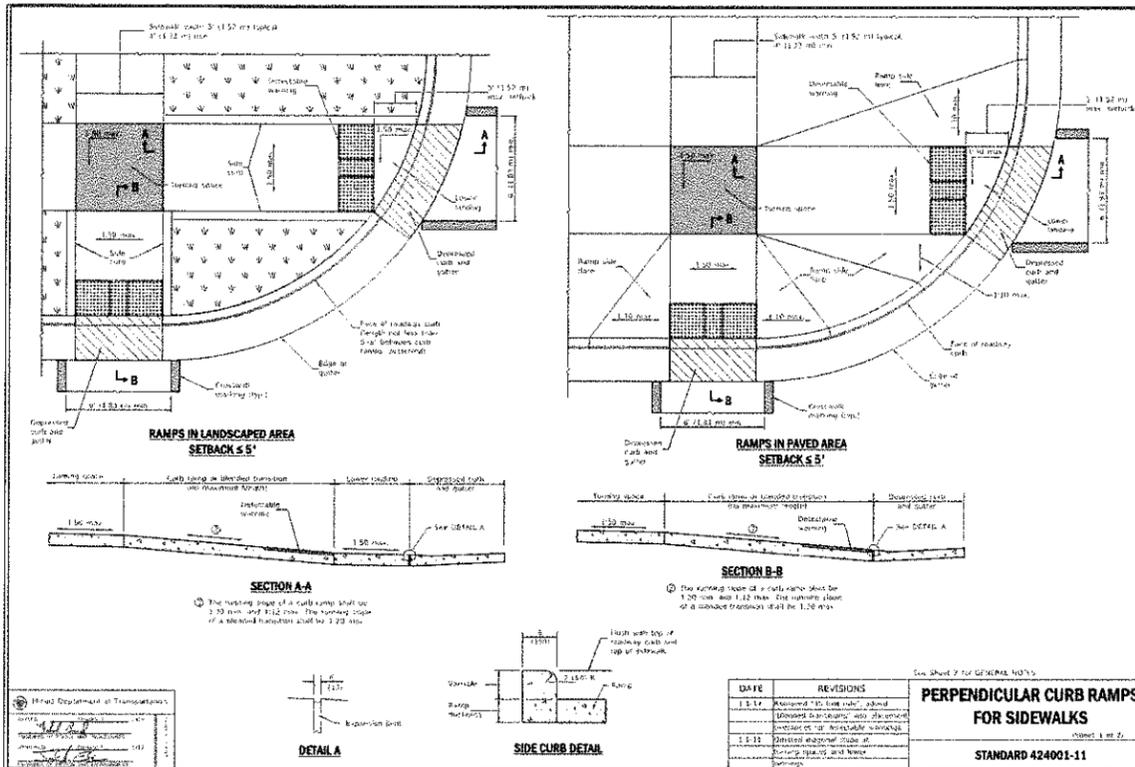


PANDA EXPRESS
WARM & WELCOME 2300
7505 KINGERY HIGHWAY
WILLOWBROOK, IL 60527

CONSTRUCTION
DETAILS

C08.1

TRUE WARM & WELCOME 2300



811
Know what's Below.
Call before you dig.

THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVE. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND ACCEPT TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MAY BE OCCURRED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

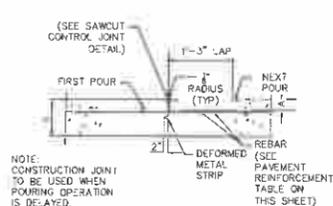
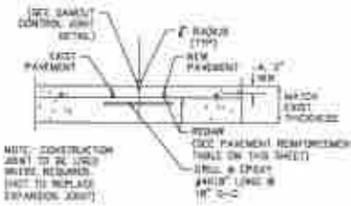
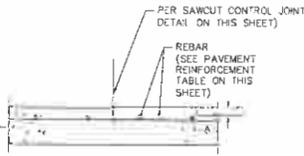
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24-HOUR CONTACT:
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HAKIM YALA
847-477-5125

DATE: 1/19/2021
EXP: 11/30/2021

ROSEMEAD, CALIFORNIA
11/19/2020
PROFESSIONAL ENGINEER
HAKIM YALA

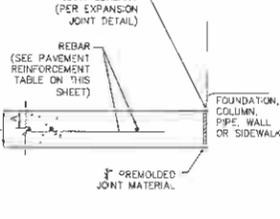
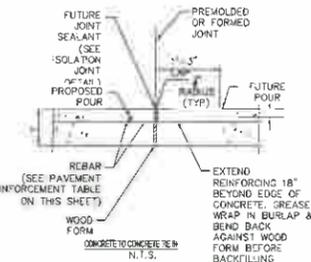
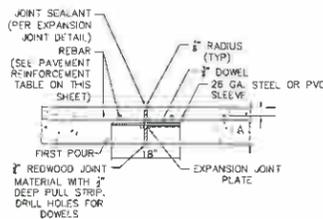
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SAWCUT JOINT
N.T.S.

CONCRETE TO CONCRETE
N.T.S.

CONSTRUCTION JOINT
N.T.S.



SAWCUT JOINT
N.T.S.

CONCRETE TO CONCRETE
N.T.S.

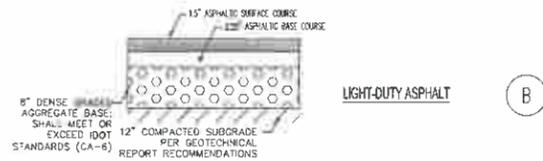
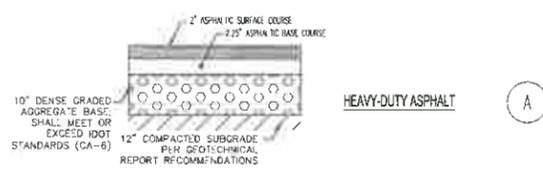
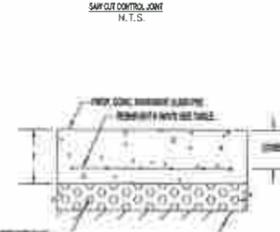
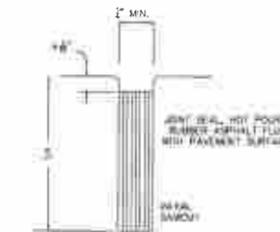
CONSTRUCTION JOINT
N.T.S.

- NOTES:**
1. REINFORCING STEEL BAR SIZE/SPECIFICATIONS IN SECTION REPORT SHALL SUPERSEDE ABOVE TABLE.
 2. REINFORCING STEEL BAR SPACING IS BASED ON MIN 8000 PSI TENSILE COMPRESSIVE STRENGTH STEEL AS SHOWN.
 3. CONCRETE PAVING MIX DESIGN SHALL HAVE MINIMUM 4000 PSI COMPRESSIVE STRENGTH AT 28 DAYS. GEOTECHNICAL REPORT CONCRETE PAVING MIX DESIGN SHALL SUPERSEDE VALUES HEREIN.
 4. BASELINE JOINT SPACING SHALL BE PER JOINT LAYOUT PLAN IF PROVIDED BUT SHALL NOT EXCEED VALUES IN TABLE.
 5. MAXIMUM JOINT SPACING IN GEOTECHNICAL REPORT SHALL SUPERSEDE VALUES IN ABOVE TABLE.
 6. USE STATE DOT STANDARD PRACTICES OTHERWISE SPECIFIED BY GEOTECHNICAL REPORT.
 7. ALL JOINTS IN PAVING SHALL BE REFLECTED IN CURBS AND SHALL HAVE ALL THEIR RESPECTIVE JOINTED MATERIALS PERFORMED I.E. EXPANSION JOINTS SHALL HAVE THEIR RESPECTIVE FILLER BOARD AND COLLAR REPLACED.
 8. CURB EXPANSION JOINTS: IF THERE IS AN EXPANSION JOINT IN THE PAVING, THE EXPANSION JOINT MUST FOLLOW THROUGH THE CURB. THE RESPECTIVE SIZE MUST ALSO BE CUT AT THE EXPANSION JOINT AND NOT ALLOWED TO RUN THROUGH THE JOINT CONTINUOUSLY. A SAW CUT EXPANSION JOINT IS NOT ACCEPTABLE BECAUSE NORMAL EXPANSION AND CONTRACTION WILL CAUSE THE CONCRETE TO PUSH AGAINST THE JOINT SECTION AND THE JOINT WILL EVENTUALLY FAIL. IF AN EXPANSION JOINT IS LEFT OUT AND MUST BE SAW CUT IN, THE CURB SHOULD BE CUT TWICE AND A 1/2" PIECE OF CONCRETE IS REMOVED. IN ALL CASES THE JOINT SHOULD BE CAULKED WITH NPL.
 9. CONCRETE TOUCHING THE BACK OF CURBS: ANY CONCRETE THAT TOUCHES THE BACK OF A CURB INCLUDING SIDEWALKS, ISLAND NOSINGS AND PAVEMENT PADS SHALL BE ISOLATED FROM THE CURB USING A BLACK EXTERNALLY APPLIED POLYURETHANE COMPOSITION SMALL GEL REMOVABLE STRIP OR A 2" STRIP AND SEAL THE JOINT WITH ALL THE JOINT SEALANT IF THE ISLAND NOSINGS ARE POURED MONOLITHICALLY WITH THE CURB AND PAVED SIDE.
 10. CURBS AT THE BUILDING FOUNDATION: CURBS TO BE ISOLATED WITH EXPANSION JOINT MATERIAL JUST LIKE THE PAVING. IF AN EXPANSION JOINT IS LEFT OUT AND MUST BE SAW CUT IN, A 1/2" PIECE OF CONCRETE SHOULD BE REMOVED. THE JOINT SHOULD BE CAULKED WITH NPL.
 11. EXPANSION JOINTS AT ISLAND NOSINGS: IF THE ISLAND NOSINGS ARE POURED MONOLITHICALLY WITH THE CURB AND PARKING LOT, THEN PAVING EXPANSION JOINTS SHOULD CONTINUE THROUGH THE NOSINGS.

REINFORCEMENT TABLE

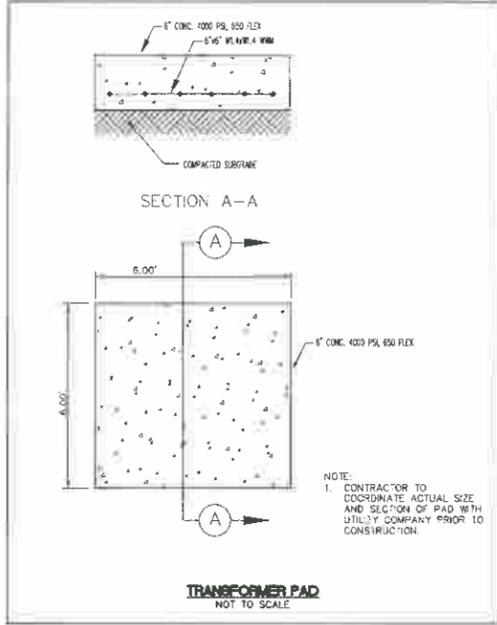
CONCRETE SECTION DESIGNATION	(1) SLAB THICKNESS (IN)	(2) COVER (IN) MIN	(3) MAX SPACING (FT)	(4) #8@24" CC REINFORCING STEEL BAR SIZE & SPACING
PARKING STALLS	5	2	15	#8@24" CC
DRIVE TRAY	6	2	15	#8@24" CC
TRASH COMPACTOR	7	2	15	#8@24" CC

CONCRETE PAVEMENT SECTION
N.T.S.

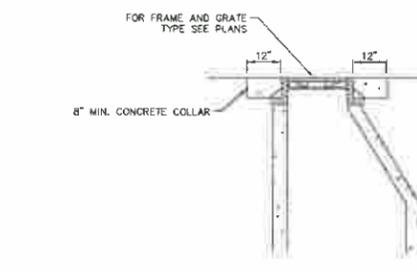


- ASPHALT NOTES:**
- 1. THE ASPHALT SURFACE COURSE SHOULD CONFORM TO THE MOST RECENT EDITION OF THE LINDS DEPARTMENT OF TRANSPORTATION (DOT) STANDARD SPECIFICATION FOR ROAD AND BRIDGE CONSTRUCTION FOR HOT MIX ASPHALTIC CONCRETE SURFACE COURSE.
 - 2. THE BASE COURSE SHOULD CONFORM TO THE DOT STANDARDS FOR BASE COURSE.
 - 3. TACK COAT SHOULD BE PROVIDED ATOP EACH PAVEMENT SECTION.

ASPHALT PAVEMENT SECTION
NOT TO SCALE



TRANSFORMER PAD
NOT TO SCALE

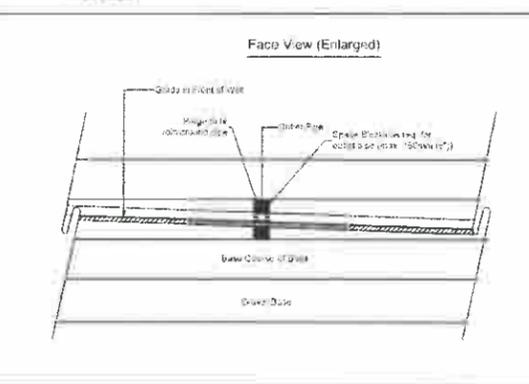
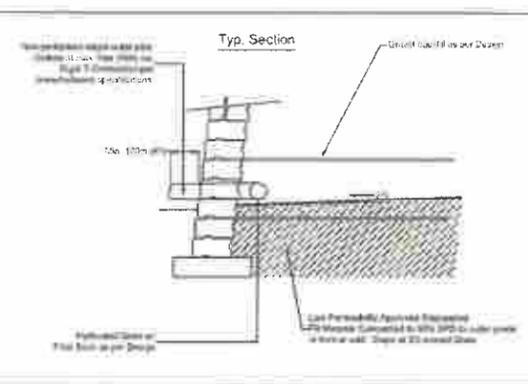
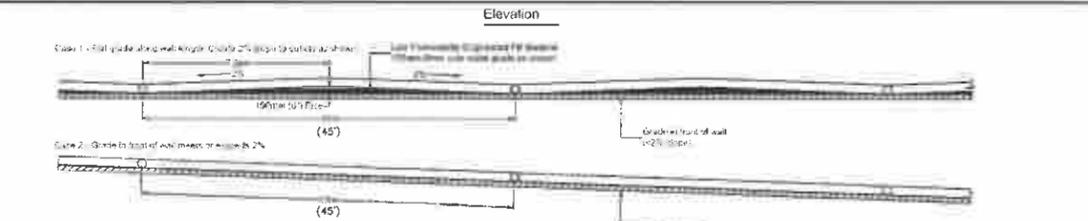


- NOTES:**
1. CONCRETE COLLARS SHALL BE INSTALLED AROUND ALL UTILITY STRUCTURE TOPS (CATCH BASINS, MAN-HOLES, ETC.)
 2. CONCRETE COLLAR SHALL BE PAINTED BLACK IF STRUCTURE IS WITHIN ASPHALT PAVEMENT.

CONCRETE COLLAR DETAIL
NOT TO SCALE

Design Specific Soil Information

Description (see notes)	Soil Region			
	First Region	Foundation	Grade	Drainage
Disturbance	SP	CL	GW	see 10M
Bedding	Angular (for concrete and steel)	Angular (for concrete and steel)	Angular (for concrete and steel)	see 10M
Subgrade	36"	28"	24"	MR
Vertical Subgrade	10 (119)	20 (227)	22 (244)	MR
Subgrade	MR	13 (278)	MR	NR
Notes	<p>1. This design was prepared in accordance with the minimum design requirements of the design standards listed above. Final design was prepared in accordance with the National Electrical Safety Association (NESA) Manual for Segregated Working Areas, Third Edition. This is a typical design for a standard design.</p> <p>2. No analysis of global stability, lateral or differential settlement, or seismic effects has been performed.</p> <p>3. The design is for a standard design. It is not intended to be used for any other design or application without the written consent of ATWELL.</p> <p>4. The design is for a standard design. It is not intended to be used for any other design or application without the written consent of ATWELL.</p>			



Product Schedule Characteristics

Characteristic	Value	Material Type and Manufacture	Value
Drainage (100) <td>190mm</td> <td>Concrete</td> <td>190mm</td>	190mm	Concrete	190mm
Minimum Height (mm)	190	Concrete	190mm
Maximum Slope (mm)	190	Concrete	190mm
Minimum Slope (mm)	190	Concrete	190mm
Minimum Slope (mm)	190	Concrete	190mm
Minimum Slope (mm)	190	Concrete	190mm

RisiStone
retaining wall systems

Pisa[®] / Roman[®]
Drain Outlet Through Face of Wall

P21DET003.DWG

TRUE WARM & WELCOME 2300



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91770
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Facsimile: 626.372.8286

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REVISIONS

NO.	DESCRIPTION	DATE
1	VILLAGE PLAN REV #1	01/19/2021

ISSUE DATE:

NO.	DESCRIPTION	DATE
1	PC SUBMITTAL	11/20/2020

DRAWN BY: LEH
PANDA PROJECT #: D7058
ATWELL PROJECT #: 18003769



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WILLOWBROOK, IL 60527

CONSTRUCTION DETAILS

C08.2

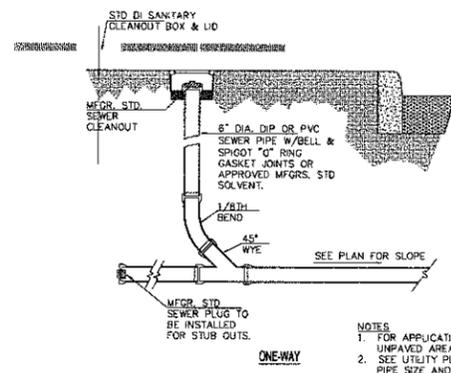
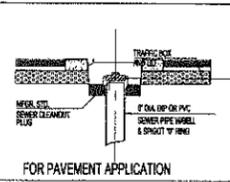
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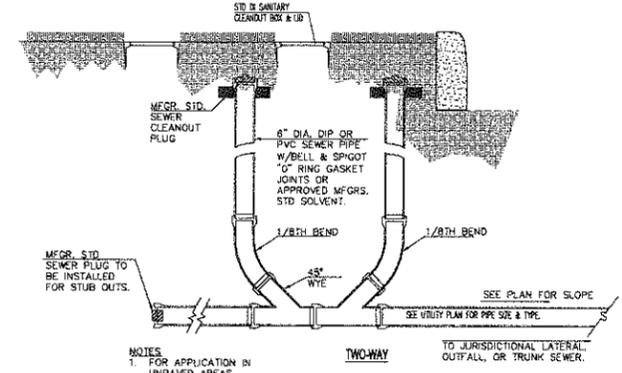
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24-HOUR CONTACT:
PANDA PM
HAKIM YALA
847-477-5125



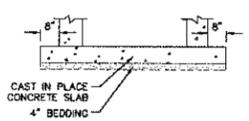


NOTES:
1. FOR APPLICATION IN UNPAVED AREAS.
2. SEE UTILITY PLAN FOR PIPE SIZE AND MATERIAL.

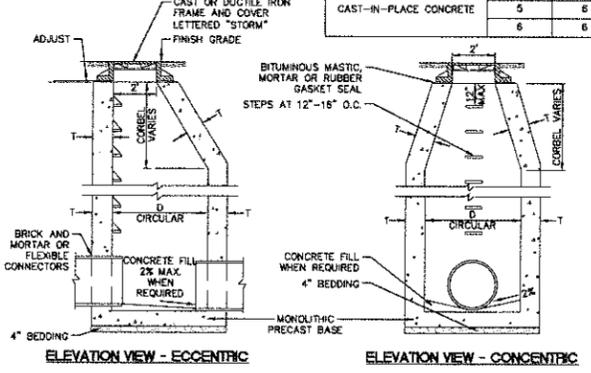


NOTES:
1. FOR APPLICATION IN UNPAVED AREAS.
2. SEE UTILITY PLAN FOR PIPE SIZE AND MATERIAL.

SANITARY SEWER CLEANOUT
NOT TO SCALE



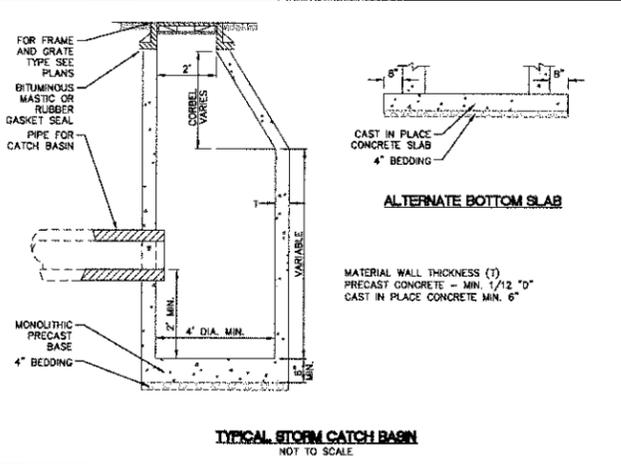
ALTERNATE BOTTOM SLAB



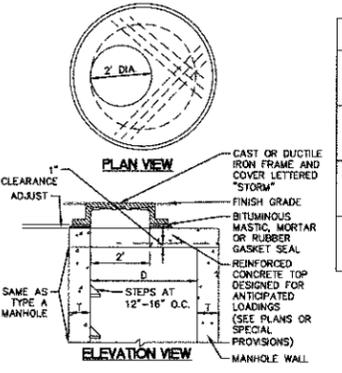
NOTES:
1. MANHOLE STEPS WHEN REQUIRED SHALL BE INSTALLED AS SHOWN ON THE PLANS.

TYPICAL STORM MANHOLE 'A'
NOT TO SCALE

ALT. MATERIALS FOR WALLS	D (FEET)	T (MIN.) (INCHES)
CONCRETE MASONRY UNITS	4	5
CONCRETE MASONRY UNITS	5	5
CONCRETE MASONRY UNITS	6	5
BRICK MASONRY	4	6
BRICK MASONRY	5	6
BRICK MASONRY	6	6
PRECAST REIN. CONCRETE SECT.	4	4
PRECAST REIN. CONCRETE SECT.	5	5
PRECAST REIN. CONCRETE SECT.	6	6
CAST-IN-PLACE CONCRETE	4	6
CAST-IN-PLACE CONCRETE	5	6
CAST-IN-PLACE CONCRETE	6	6



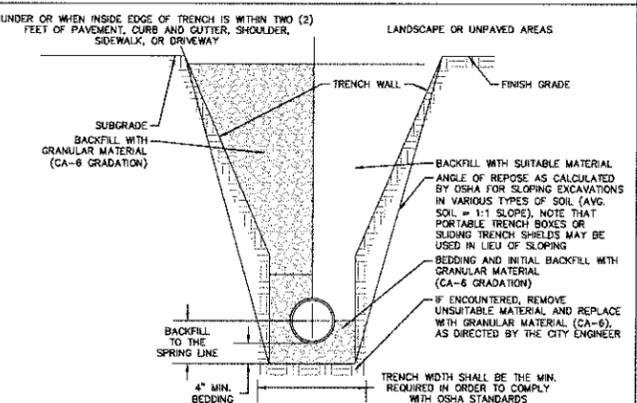
TYPICAL STORM CATCH BASIN
NOT TO SCALE



NOTES:
1. DETAIL OF M.H. TOP TO BE USED WHERE RESTRICTED HEAD ROOM WILL NOT PERMIT TAPERED WALLS.
2. MANHOLE STEPS WHEN REQUIRED SHALL BE INSTALLED AS SHOWN ON THE PLANS.

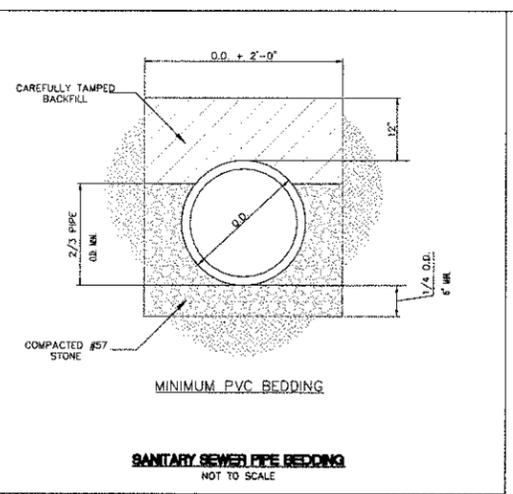
FLAT TOP MANHOLE/CATCH BASIN
NOT TO SCALE

ALT. MATERIALS FOR WALLS	D (FEET)	T (MIN.) (INCHES)
CONCRETE MASONRY UNITS	4	5
CONCRETE MASONRY UNITS	5	5
CONCRETE MASONRY UNITS	6	5
BRICK MASONRY	4	6
BRICK MASONRY	5	6
BRICK MASONRY	6	6
PRECAST REIN. CONCRETE SECT.	4	4
PRECAST REIN. CONCRETE SECT.	5	5
PRECAST REIN. CONCRETE SECT.	6	6
CAST-IN-PLACE CONCRETE	4	6
CAST-IN-PLACE CONCRETE	5	6
CAST-IN-PLACE CONCRETE	6	6

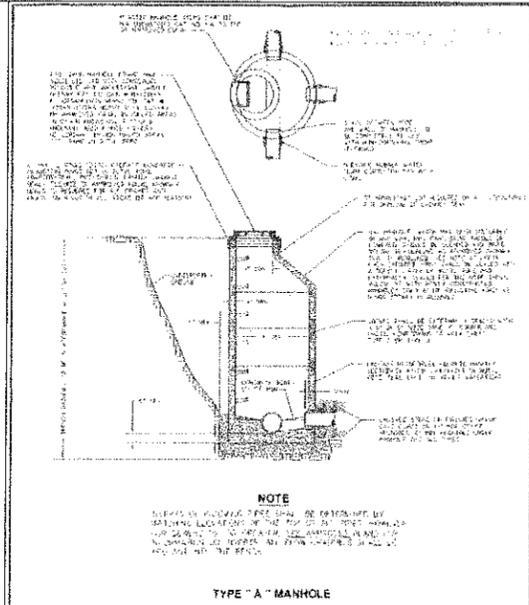


NOTES:
1. CONTRACTOR SHALL CONDUCT HIS OPERATIONS IN A SAFE MANNER AT ALL TIMES AND SHALL COMPLY WITH ALL APPLICABLE GOVERNING REGULATIONS, INCLUDING BUT NOT LIMITED TO OSHA SAFETY STANDARDS.
2. ALL BACKFILL MATERIAL UP TO A HEIGHT OF 12 INCHES ABOVE THE PIPE SHALL BE CAREFULLY DEPOSITED IN UNIFORM LAYERS NOT EXCEEDING 8 INCHES THICK (LOOSE MEASURE). THE MATERIAL IN EACH LAYER SHALL BE FIRMLY COMPACTED BY RAMMING OR TAMPING WITH TOOLS APPROVED BY THE CITY ENGINEER IN SUCH A MANNER AS NOT TO DISTURB OR INJURE THE PIPE. THE BACKFILLING ABOVE THIS HEIGHT SHALL BE DONE AS NOTED BELOW.
3. GRANULAR BACKFILL MATERIAL SHALL BE PLACED AND COMPACTED AS SPECIFIED IN NOTE 2, ABOVE. THE USE OF JETTING SHALL NOT BE ALLOWED UNLESS AUTHORIZED IN WRITING BY THE CITY ENGINEER. IT SHALL BE THE DESIGN ENGINEER OR CONTRACTOR'S RESPONSIBILITY TO PROVIDE APPROPRIATE JUSTIFICATION AND DOCUMENTATION (SOIL INVESTIGATION REPORTS, ETC.) TO THE CITY ENGINEER WITH THE REQUEST FOR APPROVAL OF JETTING.
4. BACKFILL MATERIAL CONSISTING OF SUITABLE EXCAVATED MATERIAL SHALL BE PLACED IN LIFTS NOT EXCEEDING TWELVE (12) INCHES THICK (LOOSE MEASURE) AND EACH LAYER SHALL BE COMPACTED BY RAMMING OR TAMPING TO ACHIEVE THE REQUIRED COMPACTION. JETTING OF THIS MATERIAL MAY BE PERMITTED WHEN AUTHORIZED IN WRITING BY THE CITY ENGINEER. IT SHALL BE THE DESIGN ENGINEER OR THE CONTRACTOR'S RESPONSIBILITY TO SUBMIT APPROPRIATE JUSTIFICATION AND DOCUMENTATION (SOILS INVESTIGATION REPORTS, ETC.) TO THE CITY ENGINEER WITH THE REQUEST FOR APPROVAL OF JETTING.
5. GRANULAR MATERIAL FOR BACKFILL AND BEDDING SHALL BE GRAVEL, CRUSHED GRAVEL OR STONE MEETING THE REQUIREMENTS OF THE IDOT "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" FOR COURSE AGGREGATE (CA-6 GRADATION).
6. THE BEDDING THICKNESS SHALL BE EQUAL TO ONE-QUARTER (1/4) OF THE OUTSIDE DIAMETER OF THE PIPE BUT NOT LESS THAN FOUR (4) INCHES.

STORM SEWER TRENCH SECTION
NOT TO SCALE



SANITARY SEWER PIPE BEDDING
NOT TO SCALE



NOTE:
1. MANHOLE STEPS WHEN REQUIRED SHALL BE INSTALLED AS SHOWN ON THE PLANS.

TYPICAL MANHOLE 'A'
NOT TO SCALE

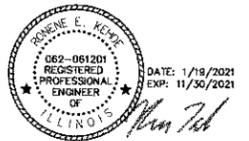
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2	1/19/2021	ISSUED FOR PERMIT
3	1/19/2021	ISSUED FOR PERMIT
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5	1/19/2021	ISSUED FOR PERMIT
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7	1/19/2021	ISSUED FOR PERMIT
8	1/19/2021	ISSUED FOR PERMIT
9	1/19/2021	ISSUED FOR PERMIT
10	1/19/2021	ISSUED FOR PERMIT



THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN ALL APPROXIMATE WAY ONLY AND HAVE NOT BEEN INDICATED BY THE OWNER OR ITS REPRESENTATIVE. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MAY BE OCCURRED BY THE CONTRACTORS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

NOTICE:
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REVISIONS:
VILLAGE PLAN REV. #1 01/19/2021

ISSUE DATE:
PC SUBMITTAL 11/20/2020

DRAWN BY: LEH

PANDA PROJECT # D7058
ATWELL PROJECT # 18003769



PANDA EXPRESS
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7505 KINGERY HIGHWAY
WILLONBROOK, IL 60527

CONSTRUCTION
DETAILS

C08.3

TRUE WARM & WELCOME 2300



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91770
Telephone: 628 799 8888
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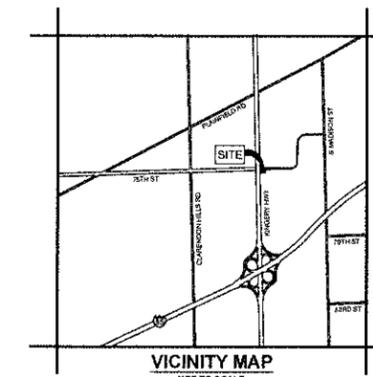


PANDA EXPRESS
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7505 KINGSLEY HIGHWAY
WILLOWBROOK, IL 60527

LANDSCAPE PLAN

L01.0

TRUE WARM & WELCOME 2300

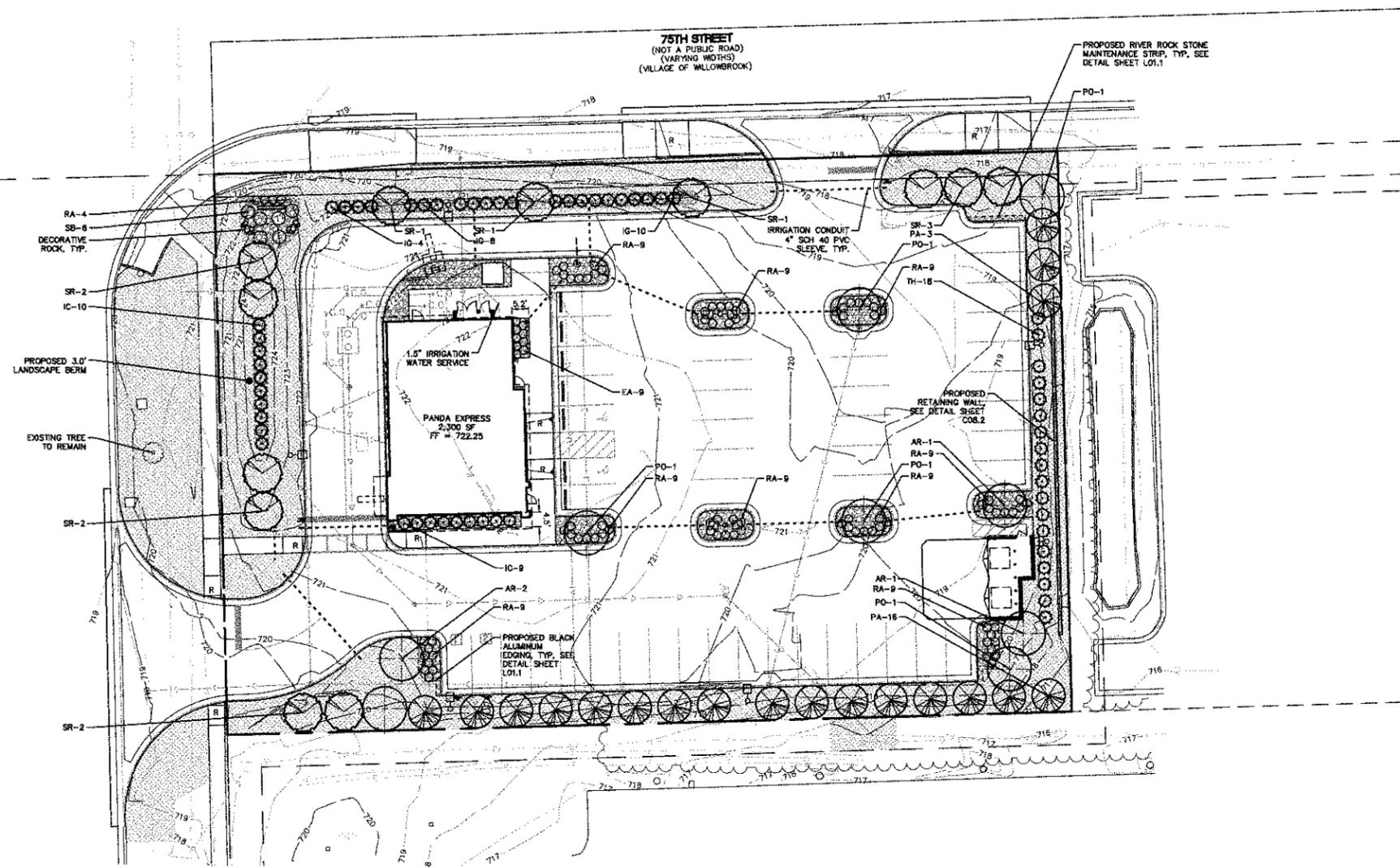


LANDSCAPE LEGEND

---	BOUNDARY LINE
---	EXISTING CONTOUR
---	PROPOSED CONTOUR
---	EXISTING ROW
---	EXISTING EASEMENT LINE
---	EXISTING SANITARY SEWER
---	PROPOSED SANITARY SEWER
---	EXISTING STORM SEWER
---	PROPOSED STORM SEWER
---	EXISTING WATER LINE
---	PROPOSED WATER LINE
---	EXISTING UNDERGROUND GAS
---	PROPOSED UNDERGROUND GAS
---	EXISTING UNDERGROUND TELEPHONE
---	EXISTING UNDERGROUND ELECTRIC
---	PROPOSED UNDERGROUND ELECTRIC
---	EXISTING UNDERGROUND CABLE
---	PROPOSED UNDERGROUND CABLE
---	PROPOSED IRRIGATION CONDUIT
---	PROPOSED ALUMINUM EDGING
---	EXISTING LIGHT POLE
---	PROPOSED LIGHT POLE/WALL PACK LIGHT
---	PROPOSED SHRUBS
---	PROPOSED EVERGREEN TREES
---	PROPOSED ORNAMENTAL TREES
---	PROPOSED DECIDUOUS TREES
---	PROPOSED KENTUCKY BLUE GRASS SOD
---	PROPOSED STONE MULCH

PLANT SCHEDULE

KEY	QTY.	BOTANICAL NAME	COMMON NAME	SIZE	REMARKS/SPEC.
AR	4	Acer rubrum	Red Maple	2 1/2" Cal.	B&B
PO	5	Platanus x acerifolia 'Bloodgood'	Bloodgood London Planetree	2 1/2" Cal.	B&B
EA	9	Euconymus alata 'Compacta'	Dwarf Burningbush	36" H., No. 5 Cont.	Trim to hedge
RA	85	Rhus aromatica 'Grp-Low'	Grp-Low Fragrant Sumac	No. 3 Cont.	
SB	8	Spiraea x bumalda 'Gold Mound'	Gold Mound Spiraea	No. 3 Cont.	
SR	12	Syringa reticulata 'Ivory Silk'	Ivory Silk Japanese Lilac	1 1/2" Cal. (6" Ht. min)	B&B, Multi-stem, 3 stems
EA	9	Euconymus alata 'Compacta'	Dwarf Burningbush	36" H., No. 5 Cont.	Trim to hedge
RA	85	Rhus aromatica 'Grp-Low'	Grp-Low Fragrant Sumac	No. 3 Cont.	
SB	8	Spiraea x bumalda 'Gold Mound'	Gold Mound Spiraea	No. 3 Cont.	
IC	19	Ilex crenata 'Green Lustre'	Green Lustre Japanese Holly	24"-36" H., No. 5 Cont.	
IG	22	Ilex glabra 'Stamrock'	Shamrock Inkberry	48" H., No. 5 Cont.	
TH	18	Taxus x media 'Hydax'	Hicks Yew	No. 3 Cont.	Trim to hedge



PLANT CALCULATION

ORDINANCE COMPLIANCE MATRIX

Zoning: B-2 COMMUNITY SHOPPING	Acres: 1.00	spaces: 46
Street Frontage (Route 83): 176.00	feet: 284.00	feet: 174.00
Street Frontage (75th St): 284.00	feet: 174.00	square feet: 174.00
Sign Surface Area: 174.00	square feet: 174.00	USDA Zone: S (-20 degrees to -10 degrees)

Section 9-4-1, BUSINESS DISTRICT LANDSCAPING REQUIREMENTS
Section 9-4-1(D) - FRONT AND EXTERIOR SIDE YARD LANDSCAPE PLANTING REQUIREMENTS

requirement	required	provided
1. Landscape plantings shall be provided within any yard adjoining a street within a business district, in conformance with the standards contained within section 9-14-2.23 of the zoning ordinance. The quantity of plantings required within any yard adjacent to a street, in addition to required parkway trees, shall be based upon a point system ratio according to five (5) times the linear length of the specific yard frontage, measured at the property line.	850 Points (Route 83) 1280 Points (75th St)	900 Points 1280 Points
Route 83	Points	Quantity
Shade Tree	100	2
Conifer Tree	50	1
Ornamental Tree	50	6
Evergreen Shrub	15	14
Deciduous Shrub	10	19
Total	225	20
75th street	Points	Quantity
Shade Tree	100	2
Conifer Tree	50	3
Ornamental Tree	50	6
Evergreen Shrub	15	22
Deciduous Shrub	10	27
Total	225	27

3. Earth berms constructed in a continuous and/or undulating configuration which perform a screening function and are within a minimum height range of two and one-half feet (2 1/2') to three feet (3'), may be credited up to a maximum of fifty percent (50%) of the total landscape planting points required within a yard. The maximum percentage of points credited for berms shall be equal to ten (10) times the average berm height where said berm extends across a minimum of seventy five percent (75%) of the yard frontage.

Section 9-10-94 and 9-10-5, OFF STREET LOADING & PARKING LANDSCAPING REQUIREMENTS
Section 9-10-5(D) - DESIGN AND MAINTENANCE

requirement	required	provided
1. Front Yards and Exterior Side Yards: Permanent peripheral screening at least four (4') high shall be constructed and maintained in interior side and rear yards adjacent to parking areas. This screen shall consist of a planted earth berm, densely planted.	4' berm/screen	6' evergreen screen
3. Interior Parking Lot Landscaping: Landscaped areas shall occupy not less than ten percent (10%) of the remaining site area excluding areas occupied by buildings or other structures, the required front yard and all street rights of way. End parking islands of a minimum one foot (1') width shall be provided at each aisle end of all parking bays. Intermediate islands of a minimum six foot (6') width shall be provided between every twenty (20) parking spaces. Center dividing islands of a minimum ten foot (10') width shall be provided between every three (3) parking bays. Each island shall be effectively landscaped with appropriately sized plantings at a minimum ratio of one shade tree and nine (9) dwarf variety shrubs per island and/or fifty (50) linear feet of parking area. Shade tree distribution shall provide a minimum of one tree in the area occupied by each twenty (20) parking spaces.	10 %	26 %
9 trees	9 trees	
81 shrubs	81 shrubs	

Section 9-11-14, MONUMENT SIGN LANDSCAPING REQUIREMENTS
Section 9-11-14(A)

requirement	required	provided
1. For every square foot of sign surface area, there shall be provided one and one-half (1 1/2) plants.	261 SF	261 SF

Section 9-14-2.23 GENERAL LANDSCAPING REQUIREMENTS
Section 9-14-2.23(B) - DESIGN AND MAINTENANCE

requirement	required	provided
1. Minimum Plantings: The applicant shall minimally provide one approved planting or each seven hundred twenty five (725) square feet of gross lot area. For each required planting, the applicant shall provide a minimum of one (1) approved planting (i.e. front and side yard, screening, off street parking, monument sign, and foundation requirements) and satisfies the minimum requirement of 80 plantings.	80 Plantings	199 Plantings
2. Approved Plantings and Ground Cover	600 SF ground cover	2625 SF ground
3. Border Plantings And Foundation Plantings: A landscaped area having a minimum width of seven feet (7') shall be provided around all buildings except for building entrances and the front of retail business facilities to permit appropriate foundation plantings. Border plantings and foundation plantings shall be provided and shall comprise the remainder of the required landscape plantings, of which a minimum of one-half (1/2) shall be deciduous shrubs at a minimum height of three feet (3') and the remaining one-half (1/2) shall be evergreen shrubs at a minimum height of two feet (2').	5% Min.	15%
Shade Trees: minimum of 20% of the required plantings, excluding parkway trees. Shall not be less than 5 feet in height and shall comprise a minimum of 10% of the required plantings. Shall be at least 1 1/2" caliper or less than 6" in height, and comprise at minimum 20% of the landscape plantings.	10% Min.	32%
Ornamental Trees: Shall be at least 1 1/2" caliper or less than 6" in height, and comprise at minimum 20% of the landscape plantings.	20% Min.	20%
7 Foot foundation planting width	4.5" Foot foundation planting width	
50% Deciduous Shrubs	50% Deciduous Shrubs	
50% Evergreen Shrubs	50% Evergreen Shrubs	

4.5" Foot foundation planting width
50% Deciduous Shrubs
50% Evergreen Shrubs

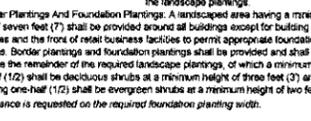
ROBERT KINGSLEY HIGHWAY
STATE ROUTE 83
(001)



THE LOCATION OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE MANNER ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVE. THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MAY BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

NOTICE: CONSTRUCTION SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SAFETY OF THE WORKER AND THE PUBLIC. NO COMPENSATION SHALL BE MADE FOR REPRODUCTION OF THIS DRAWING WITHOUT THE WRITTEN CONSENT OF ATWELL, LLC.

24-HOUR CONTACT:
PANDA FM
HAKIM YALA
847-477-5125



SCALE 0 10 20
1" = 20 FEET

CONTRACTOR SHALL PROTECT ALL ITEMS OUTSIDE LIMITS OF CONSTRUCTION UNLESS OTHERWISE NOTED IN THE CONSTRUCTION PLANS OR SPECIFICATIONS.

CONTRACTOR RESPONSIBLE FOR MAINTENANCE OF LANDSCAPE FOR 90 DAYS FOLLOWING INSTALLATION.

CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITIES (LOCATIONS AND ELEVATIONS) PRIOR TO STARTING CONSTRUCTION AND ALERT ENGINEER TO ANY DISCREPANCIES IMMEDIATELY.

C:\WORKSPACE\PROJECTS\LANDSCAPE\LANDSCAPE\LANDSCAPE.DWG, 07/22/2020 4:38:38 PM, LAMAR, JESSE

GENERAL LANDSCAPE NOTES

- 1. LANDSCAPE CONTRACTOR (CONTRACTOR) SHALL VISIT SITE, INSPECT EXISTING CONDITIONS AND REVIEW PROPOSED PLANTINGS AND RELATED WORK...
2. PRIOR TO ANY LAND CLEARING OR CONSTRUCTION, TREE PROTECTION FENCING IS TO BE INSTALLED BY THE CONTRACTOR...
3. LANDSCAPE CONTRACTOR SHALL COORDINATE THE PHASES OF CONSTRUCTION AND PLANTING INSTALLATIONS WITH OTHER CONTRACTORS WORKING ON SITE...
4. WHERE EXISTING TREES AND/OR SIGNIFICANT SHRUB MASSINGS ARE FOUND ON SITE, WHETHER SHOWN ON THE DRAWING OR NOT, THEY SHALL BE PROTECTED AND SAVED UNLESS NOTED TO BE REMOVED AND/OR ARE IN AN AREA TO BE GRADED...
5. ALL EXISTING TREES TO REMAIN TO BE FERTILIZED AND PRUNED TO REMOVE DEAD WOOD AND DAMAGED OR RUBBING BRANCHES...
6. NO PLANT MATERIAL SUBSTITUTIONS WILL BE ACCEPTED UNLESS APPROVAL IS REQUESTED OF THE LANDSCAPE ARCHITECT, OWNER, AND THE VILLAGE ZONING ADMINISTRATOR BY THE LANDSCAPE CONTRACTOR PRIOR TO INSTALLATION...
7. ALL PLANT MATERIAL SHALL COMPLY WITH THE LATEST EDITION OF THE AMERICAN STANDARD FOR NURSERY STOCK, AMERICAN NURSERY AND LANDSCAPE ASSOCIATION...
8. CONTRACTOR WILL SUPPLY FINISHED GRADE AND EXCAVATE AS NECESSARY TO SUPPLY 4" TOPSOIL DEPTH IN ALL PLANTING BEDS AND 4" TOPSOIL DEPTH IN ALL LAWN AREAS...
9. ACCEPTANCE OF GRADING AND SOO/SEED SHALL BE BY LANDSCAPE ARCHITECT AND/OR PROJECT REPRESENTATIVE...
10. PLANT MATERIAL LOCATIONS SHOWN ARE DIAGRAMMATIC AND MAY BE SUBJECT TO CHANGE IN THE FIELD AS REQUIRED...
11. REPAIR ALL DAMAGE TO PROPERTY FROM PLANTING OPERATIONS AT NO COST TO THE OWNER...
12. OWNER OR OWNER'S REPRESENTATIVE SHALL INSPECT LANDSCAPE INSTALLATION AND HAVE THE RIGHT TO REJECT AND WITHHOLD PAYMENT ON ANY PLANT MATERIAL(S) OF DAMAGED OR POOR QUALITY OR NOT MEETING SPECIFICATIONS...
13. LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEAN-UP OF SITE AT THE COMPLETION OF LANDSCAPING EACH DAY...
14. LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR SOIL EROSION AND DUST CONTROL MEASURES PRIOR TO AND DURING CONSTRUCTION...

GENERAL PLANTING NOTES

- 1. NO PLANTING TO BE INSTALLED UNTIL GRADING AND CONSTRUCTION HAS BEEN COMPLETED IN THE IMMEDIATE AREA...
2. SEE CIVIL/SITE PLAN FOR ALL SITE DIMENSIONS, SQUARE FOOTAGES, PARKING CALCULATIONS, AND DETAILS OF ALL SITE IMPROVEMENTS...
3. IF THE LANDSCAPE CONTRACTOR PERCEIVES ANY DEFICIENCIES IN THE PLANT SELECTIONS, SOIL CONDITIONS, OR ANY OTHER SITE CONDITION WHICH MIGHT NEGATIVELY AFFECT PLANT MATERIAL ESTABLISHMENT, SURVIVAL, OR GUARANTEE, THEY SHALL BRING THESE DEFICIENCIES TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION...
4. PRIOR TO ANY LAND CLEARING OR CONSTRUCTION, TREE PROTECTION FENCING IS TO BE INSTALLED BY THE CONTRACTOR...
5. ALL PLANTS TO BE INSTALLED AS PER PLANTING DETAILS. PLANT MATERIALS ARE TO BE PLANTED IN THE SAME RELATIONSHIP TO GRADE AS WAS GROWN IN NURSERY CONDITIONS...
6. ONE SHRUB PER TYPE AND SIZE IN EACH PLANTING BED AND EVERY TREE SHALL BE CLEARLY IDENTIFIED (COMMON OR LATIN NOMENCLATURE) WITH A PLASTIC TAG WHICH SHALL NOT BE REMOVED PRIOR TO OWNER ACCEPTANCE...
7. SEED AND/OR SOO ALL AREAS DISTURBED DUE TO GRADING AND CONSTRUCTION ACTIVITIES, WHERE SOO/SEED ABUTS PAVED SURFACES, FINISHED GRADE OF SOO/SEED SHALL BE HELD 1" BELOW SURFACE ELEVATION OF TRAIL, SLAB, CURB, ETC...
8. PRUNE, THIN AND SHAPE TREES AND SHRUBS ACCORDING TO STANDARD HORTICULTURAL PRACTICES...
9. EXISTING LAWN AREAS TO BE SAVED AND AREAS THAT ARE DAMAGED DURING CONSTRUCTION MUST BE INSPECTED TO DETERMINE VIABILITY...
10. CONVERSION OF ALL ASPHALT AND GRAVEL AREAS TO LANDSCAPE SHALL BE DONE IN THE FOLLOWING MANNER:

- A. REMOVE ALL ASPHALT, GRAVEL AND COMPACTED EARTH TO A DEPTH OF 24" - 30" DEPENDING ON THE DEPTH OF SUB BASE AND DISPOSE OF OFF SITE...
B. REPLACE EXCAVATED MATERIAL W/ GOOD, MEDIUM TEXTURED PLANTING SOIL (LOAM OR LIGHT YELLOW CLAY) TO A MIN. OF 2" ABOVE TOP OF CURB AND SIDEWALK...
EXISTING LAWN FOUND TO BE IN POOR CONDITION MUST FIRST BE SPRAYED WITH ROUND-UP (OR EQUAL) TO KILL THE EXISTING LAWN AND WEEDS...
EXISTING LAWN FOUND TO BE IN GOOD CONDITION, BUT WITH BARE, SPARSE OR WEEDY AREAS MUST BE RENOVATED BY FILLING IN LOW AREAS, RAKING, OVERSEEDING AND TOP DRESSING ALL SPARSE AND BARE SPOTS AND BY INITIATING A WEED AND FEED PROGRAM...
IF CONVERSION TO LANDSCAPE OCCURS IN AN EXISTING (OR BETWEEN) LANDSCAPE AREAS, REPLACE EXCAVATED MATERIAL TO 4"-6" BELOW ADJACENT EXISTING GRADE W/ GOOD MEDIUM TEXTURED PLANTING SOIL (LOAM OR LIGHT YELLOW CLAY) AND ADD 4"-6" OF TOPSOIL TO MEET EXISTING GRADES AFTER EARTH SETTLING...
11. ALL TREE PITS MUST BE TESTED FOR PROPER DRAINAGE PRIOR TO PLANTING TREES...
12. ALL LANDSCAPE AREAS SHALL HAVE PROPER DRAINAGE THAT PREVENTS EXCESS WATER FROM STANDING ON LAWN AREAS OR AROUND TREES & SHRUBS...
13. ALL MULCH RINGS AND SHRUB BEDS IN LAWN AREAS SHALL BE EDGED WITH A MANICURED EDGE OR WITH MANUFACTURED EDGING AS INDICATED...
14. MULCHING AND WATERING OF ALL PLANTS & TREES SHALL BE IMMEDIATELY OR WITHIN 16 HOURS AFTER INSTALLATION.

LANDSCAPE PLANTING SPECIFICATIONS

- 1. PLANTING BED PREPARATION: ALL MASS PLANTING BEDS SHALL BE TILED TO A MINIMUM DEPTH OF 10"...
2. BACKFILL SOIL: USE SOIL EXCAVATED FROM PLANTING HOLES AND PROVIDE AMENDMENTS (1 PART LEAF MOLD OR SPHAGNUM PEAT MOSS AND 3 PARTS EXCAVATED SOIL)...
3. FERTILIZATION: ALL PLANT MATERIALS SHALL BE FERTILIZED UPON INSTALLATION WITH DRIED BONE MEAL OR OTHER SPECIFIED FERTILIZER MIXED IN WITH THE PLANTING SOIL...
4. MULCH MATERIAL: STONE SHALL BE RIVER ROCK AS SPECIFIED ON THE LANDSCAPE PLANS...
5. TREE STAKING: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO STAKE AND/OR CUY THE TREES ACCORDING TO THE DETAILS...
6. TREE WRAPPING: WRAPPING MATERIAL SHALL BE QUALITY, HEAVY WATERPROOF CREPE PAPER MANUFACTURED FOR THIS PURPOSE...
7. EDGING: EDGING SHALL BE SPADE EDGED...
8. FERTILIZER: JUMP-SHOT ROOT STIMULATOR AS MANUFACTURED BY AGRIE, OR APPROVED EQUAL, SHALL BE APPLIED TO THE SOIL BACKFILL OF EACH PLANT DURING INSTALLATION...
9. PLANT SIZING: MEASURE TREES AND SHRUBS ACCORDING TO ANSI Z60.1 STANDARDS...
10. PLANTING PLAN: ALL PROPOSED PLANTS SHALL BE LOCATED CAREFULLY AS SHOWN ON THE PLANS...
11. WEED BARRIER: CONTRACTOR SHALL APPLY 4.2 OZ. NON-WOVEN POLYPROPYLENE, NEEDLE-PUNCHED FABRIC WEED BARRIER AT ALL INTERIOR LANDSCAPE ISLANDS AND STONE MULCH STRIPS.

PLANT MATERIAL NOTES

- 1. PROVIDE PLANTS OF QUANTITY, SIZE, GENUS, SPECIES, AND VARIETY SHOWN AND SCHEDULED AND IN CONFORMANCE WITH THE REQUIREMENTS OF ANSI Z60.1 "AMERICAN STANDARD FOR NURSERY STOCK"...
2. ALL PLANTS SHALL BE FULL, WELL-BRANCHED PLANTS CHARACTERISTIC OF THE SPECIES...
3. PLANT STOCK SHALL HAVE BEEN GROWN UNDER CLIMATIC CONDITIONS SIMILAR TO CONDITIONS IN THE LOCALITY OF THE PROJECT...
4. LABEL AT LEAST ONE PLANT OF EACH KIND WITH A SECURELY ATTACHED WATERPROOF TAG BEARING LEGIBLE DESIGNATION OF BOTANICAL AND COMMON NAME...
5. PROVIDE FRESHLY DUG BALLED & BURLAPPED PLANT MATERIALS...
6. DO NOT REMOVE CONTAINER-GROWN STOCK FROM CONTAINERS UNTIL PLANTING TIME...
INSTALLATION OF PLANT MATERIAL NOTES
1. INSTALL TREES AND SHRUBS ACCORDING TO STANDARD DETAILS SHOWN ON THE PLAN...
2. ALL TREE SAUCERS SHALL BE SOAKED WITH WATER AND MULCHED IMMEDIATELY FOLLOWING PLANTING...
3. ALL TREE SAUCERS AND SHRUB BEDS SHALL BE MULCHED WITH A 3-INCH LAYER OF ORGANIC TRIPLE SHREDED HARDWOOD BARK MULCH...
4. TREE GUYING SHALL BE REMOVED AFTER ONE FULL GROWING SEASON...
5. APPLY 12 CUBIC FEET OF PEAT MOSS PER 100 SQUARE FEET AND 20 POUNDS OF 8-8-8 FERTILIZER PER 100 SQUARE FEET OF GROUND COVER PLANTING BEDS...

LAWN NOTES

- 1. KENTUCKY BLUE GRASS SOO SHALL BE PROMOTED IN AREAS WHERE SEEDING IS NORMALLY UNSUCCESSFUL...
2. SOO SHALL BE TIGHTLY-FITTED TOGETHER, ENDS AND EDGES SHALL MEET WITHOUT OVERLAP AND JOINTS SHALL BE STAGGERED WITH ADJACENT ROWS...
3. MAINTENANCE OF PLANT MATERIALS AND LAWN AREAS SHALL BEGIN IMMEDIATELY AFTER INSTALLATION AND SHALL CONTINUE UNTIL FINAL ACCEPTANCE...
LAWN AREAS: 60 DAYS AFTER SUBSTANTIAL COMPLETION

PLANT MATERIAL MAINTENANCE AND WARRANTY NOTES

- 1. MAINTENANCE OF PLANT MATERIALS AND LAWN AREAS SHALL BEGIN IMMEDIATELY AFTER INSTALLATION AND SHALL CONTINUE UNTIL FINAL ACCEPTANCE...
2. AFTER REQUIRED MAINTENANCE PERIOD, THE OWNER, UPON REQUEST, WILL MAKE AN INSPECTION TO DETERMINE ACCEPTABILITY...
3. A WRITTEN WARRANTY SHALL BE PROVIDED TO THE OWNER GUARANTEEING THAT ALL PLANT MATERIALS, SOO, AND/OR SEEDED AREAS WILL BE THRIVING FOR THE FOLLOWING STATED PERIODS...
4. THE CONTRACTOR SHALL PROVIDE THE OWNER WITH WRITTEN INSTRUCTIONS REGARDING MAINTENANCE OF EACH TYPE OF VEGETATION...
5. THE CONTRACTOR IS NOT RESPONSIBLE FOR ACTS OF NATURE INCLUDING ABNORMAL WEATHER CONDITIONS, EROSION, VANDALISM, NOR DAMAGES BY OTHERS...

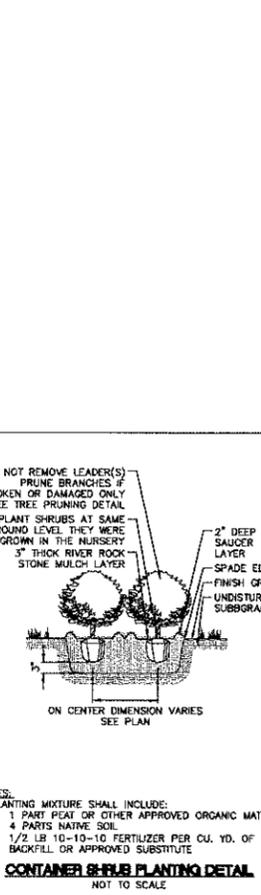
IRRIGATION SYSTEM NOTES (DESIGN BUILD)

- 1. THE CONTRACTOR SHALL DESIGN AND INSTALL AN IRRIGATION SYSTEM FOR ALL LANDSCAPING ON-SITE...
2. RAIN BIRD SYSTEM OR EQUIVALENT SHALL BE USED...
3. SEE KITCHEN ELEVATION FOR LOCATION OF THE IRRIGATION CONTROL BOX...
4. DRIP IRRIGATION IS REQUIRED IN BUILDING PLANTERS...
5. RAIN BIRD RSD SERIES RAIN SENSOR, OR EQUIVALENT, SHALL BE INCLUDED IN THE DESIGN AND INSTALLATION...
6. LOCATE ALL UTILITIES BEFORE DIGGING...
7. PLACE IRRIGATION LINES WITHIN PLANTING AND TURF AREAS UNLESS SLEEVING IS INDICATED...
8. PROVIDE SLEEVING UNDER PAVEMENT AND SIDEWALKS TO ACCOMMODATE LATERAL LINES, MAIN LINES, AND/OR CONTROL WIRE...
9. PROVIDE A MINIMUM 12" COVER OVER LATERAL LINES AND A MINIMUM OF 18" COVER OVER MAINLINES...
10. INSTALL ALL ELECTRICAL SERVICE TO MEET LOCAL CODES...
11. PROVIDE POP-UP SPRINKLERS ONLY...
12. MOUNT HEADS FLUSH WITH FINISHED GRADE...
13. INSTALL VALVES IN 15" BOXES WITH TAMPER RESISTANT LIDS...
14. ALL IRRIGATION CONTROL WIRE TO BE 14-GAUGE WIRE...
15. MAKE ALL WIRE SPICES WITH WATERPROOF SPLICE KITS...
16. ADJUST HEADS TO MAXIMIZE COVERAGE AND MINIMIZE OVERSPRAY.

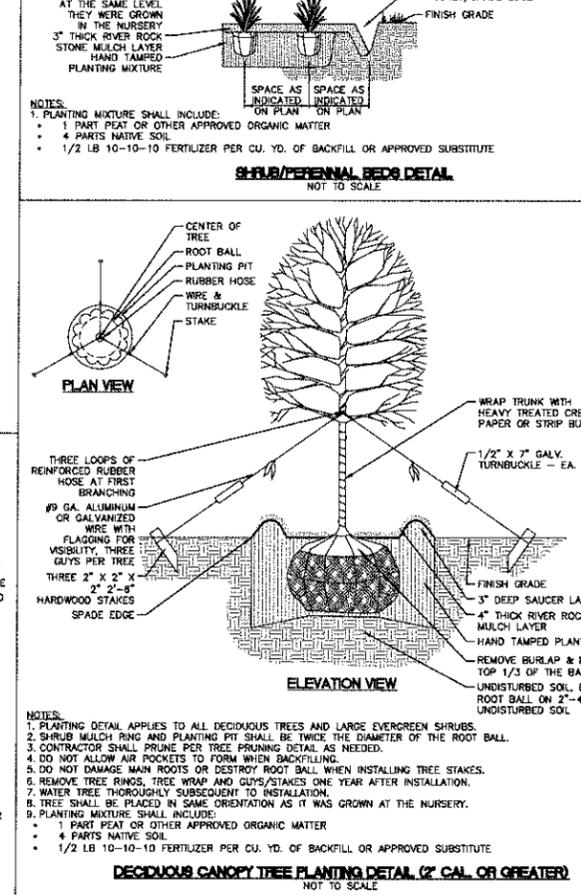
CONCRETE AND FINISHING NOTES

- 1. THE CONTRACTOR SHALL DESIGN AND INSTALL AN IRRIGATION SYSTEM FOR ALL LANDSCAPING ON-SITE...
2. RAIN BIRD SYSTEM OR EQUIVALENT SHALL BE USED...
3. SEE KITCHEN ELEVATION FOR LOCATION OF THE IRRIGATION CONTROL BOX...
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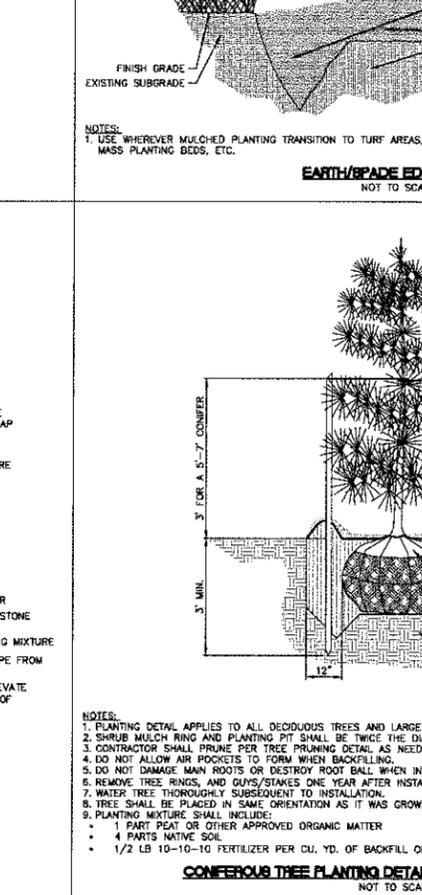
CONTAINER SHRUB PLANTING DETAIL



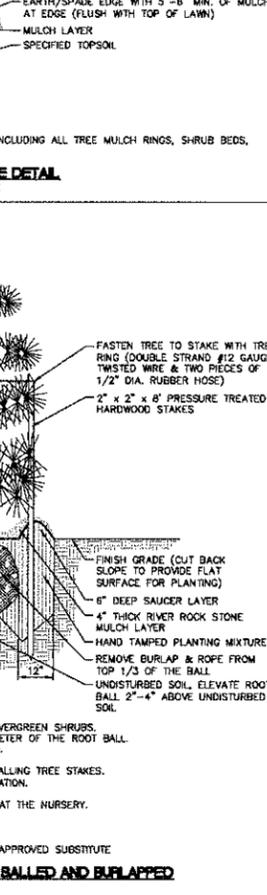
SHRUB/PERENNIAL BEDS DETAIL



DECIDUOUS CANOPY TREE PLANTING DETAIL



CONIFEROUS TREE PLANTING DETAIL



All ideas, design, arrangement and plans indicated or represented by this drawing are the property of Panda Express Inc. and were created for use on this specific project.

Table with 2 columns: REVISIONS, and 3 columns: VILLAGE PLAN REV. #, DATE, and DESCRIPTION.

STONE MULCH WITH WEED BARRIER DETAIL

EARTH/SPADE EDGE DETAIL

SHRUB/PERENNIAL BEDS DETAIL

CONIFEROUS TREE PLANTING DETAIL

DECIDUOUS CANOPY TREE PLANTING DETAIL



NOTICE: CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE OWNER AND/OR DESIGNER SHALL BE RESPONSIBLE FOR THE SAFETY OF THE WORKERS AND THE PUBLIC...

24-HOUR CONTACT: PANDA PM HAKIM YALA 847-477-5125

CONTAINER SHRUB PLANTING DETAIL

DECIDUOUS CANOPY TREE PLANTING DETAIL

CONIFEROUS TREE PLANTING DETAIL



PANDA EXPRESS WARW & WELCOME 2300 7505 KINGERY HIGHWAY WILLOWBROOK, IL 60527

LANDSCAPE NOTES AND DETAILS L01.1 TRUE WARM & WELCOME 2300



Project: Willowbrook, IL - Panda Express
 Location: 7505 S. Kingery Hwy.
 Willowbrook, IL 60527

Date: 11/20/2020

1250 East Diehl Road, Suite 300
 Naperville, IL 60563
 Ph: 630-577-0800

Engineer's Opinion of Probable Construction Cost

This EOPC was prepared in accordance with Final Site Plans for Willowbrook, IL Panda Express, prepared by Atwell dated 11/20/2020.

	Quantity	Unit	Unit Cost	Total
SITWORK				
Erosion Control				
Construction Entrance	1	EACH	\$5,000.00	\$5,000.00
Concrete Washout	1	EACH	\$500.00	\$500.00
Tree Protection Fence	12	LF	\$4.00	\$48.00
Silt Fence (Wire Back)	880	LF	\$2.50	\$2,200.00
Inlet Filter	7	EACH	\$200.00	\$1,400.00
Erosion Control Sub-Total				\$9,148.00
Demolition and Earthwork				
Clear and Grub	0.08	ACRES	\$5,000.00	\$400.00
Remove Existing Sanitary Sewer Pipe	154	LF	\$10.00	\$1,540.00
Remove Existing Water Service Pipe	138	LF	\$10.00	\$1,380.00
Relocate Existing Hydrant	1	EACH	\$1,750.00	\$1,750.00
Remove Existing Wood Wall	99	LF	\$2.00	\$198.00
Remove Existing Light Pole	11	EACH	\$225.00	\$2,475.00
Remove Existing Bollard	2	EACH	\$100.00	\$200.00
Remove Existing Fence	346	LF	\$3.00	\$1,038.00
Remove Existing Concrete	17	SF	\$1.50	\$25.50
Topsoil Stripping and Stockpiling	199	CY	\$2.25	\$447.75
Topsoil Respread (4")	1772	SY	\$1.50	\$2,658.00
Excavation to Stockpile	211	CY	\$3.00	\$633.00
Fill from Stockpile	211	CY	\$3.00	\$633.00
Fill from Import	396	CY	\$18.00	\$7,128.00
Fine Grade Pad	256	SY	\$2.50	\$640.00
Subgrade Preparation - Rough Grade	2910	SY	\$1.00	\$2,910.00
Remove Existing Pavement	4437	SY	\$5.25	\$23,294.25
Sawcut Pavement, Full Depth	341	LF	\$3.50	\$1,193.50
Remove Existing Curb and Gutter	1195	LF	\$5.00	\$5,975.00
Demolition & Earthwork Sub-Total				\$54,519.00
Utilities				
Sanitary Sewer				
6" PVC Service	348	LF	\$32.00	\$11,136.00
6" Clean Out	10	EACH	\$500.00	\$5,000.00
4'-0" Diameter Manhole	1	EACH	\$4,500.00	\$4,500.00
Connect to Existing Sanitary Sewer Manhole	1	EACH	\$800.00	\$800.00
Grease Trap (1,000 gal - precast concrete)	1	EACH	\$5,500.00	\$5,500.00
Testing & Inspections	1	EACH	\$1,000.00	\$1,000.00
Sanitary Sewer Sub-Total				\$27,936.00

	Quantity	Unit	Unit Cost	Total
Water				
1.5" Irrigation Water Service	10	LF	\$18.00	\$180.00
6" DIP Fire Water Service	73	LF	\$30.00	\$2,190.00
Dip Exiting Water Main	1	EACH	\$4,000.00	\$4,000.00
Connection to Existing Water Main	1	EACH	\$1,000.00	\$1,000.00
Water Sub-Total				\$7,370.00
Storm Sewer				
6" HDPE Storm Sewer Pipe	54	LF	\$20.00	\$1,080.00
12" HDPE Storm Sewer Pipe	436	LF	\$38.00	\$16,568.00
4'-0" Diameter Type "A" Catch Basin	5	EACH	\$3,800.00	\$19,000.00
4'-0" Diameter Manhole	1	EACH	\$4,500.00	\$4,500.00
Storm Sewer Sub-Total				\$41,148.00
Other Utilities				
Area Lights	8	EACH	\$1,500.00	\$12,000.00
Other Utilities Sub-Total				\$12,000.00
Paving				
Subgrade Preparation - Fine Grading	2959	SY	\$1.50	\$4,438.50
Aggregate Base Course (CA-6), 4" (Curb & Gutter)	24	SY	\$5.50	\$132.00
Aggregate Base Course (CA-6), 5" (Drive Thru)	285	SY	\$6.00	\$1,710.00
Aggregate Base Course (CA-6), 5" (PCC Parking)	121	SY	\$6.00	\$726.00
Aggregate Base Course (CA-6), 5" (Dumpster)	83	SY	\$6.00	\$498.00
Aggregate Base Course (CA-6), 8" (Asphalt Parking)	680	SY	\$7.75	\$5,270.00
Aggregate Base Course (CA-6), 10" (Asphalt drive Aisles)	1766	SY	\$8.50	\$15,011.00
Hot Mix Asphalt Binder Course, N50, 2-1/2" (Parking)	680	SY	\$11.75	\$7,990.00
Hot Mix Asphalt Binder Course, N50, 2-1/2" (Drive Aisles)	1766	SY	\$11.75	\$20,750.50
Hot Mix Asphalt Surface Course, 1-1/2" (Parking)	680	SY	\$9.00	\$6,120.00
Hot Mix Asphalt Surface Course, 2" (Drive Aisles)	1766	SY	\$11.25	\$19,867.50
PCC Pavement, 5" (Parking)	121	SY	\$28.00	\$3,388.00
PCC Pavement, 6" (Drive thru)	285	SY	\$32.00	\$9,120.00
PCC Pavement, 7" (Dumpster)	83	SY	\$36.00	\$2,988.00
Concrete Curb and Gutter Type B-6:12	1512	LF	\$12.00	\$18,144.00
PCC Sidewalk with Subbase, 4"	294	SY	\$45.00	\$13,230.00
Pavement Marking, ADA Parking Symbol	2	EACH	\$250.00	\$500.00
Pavement Marking, ADA Striping	1	EACH	\$350.00	\$350.00
Pavement Marking, Crosswalk	1	EACH	\$750.00	\$750.00
Pavement Marking, Drive Thru Parking Space	2	EACH	\$350.00	\$700.00
Pavement Marking, 4" Solid White	913	LF	\$3.00	\$2,739.00
Pavement Marking, 24" Solid White	54	LF	\$9.50	\$513.00
Rubber Wheel Stops	7	EACH	\$325.00	\$2,275.00
Signage	5	EACH	\$200.00	\$1,000.00
Paving Sub-Total				\$138,210.50
Landscaping				
Retaining Wall	248	SF	\$27.50	\$6,820.00
Sod	1472	SY	\$5.00	\$7,360.00
Mulch	36	CY	\$50.00	\$1,800.00
Tree	46	EACH	\$350.00	\$16,100.00
Shrub	203	EACH	\$40.00	\$8,120.00
Landscaping Sub-Total				\$40,200.00
TOTAL				\$330,531.50

Quantity	Unit	Unit Cost	Total
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This Engineer's Opinion of Probable Cost is made on the basis of Engineer's experience and qualifications using estimated quantities and represents Engineer's best judgment as an experienced and qualified professional Engineer generally familiar with the construction industry. However, since Engineer has no control over the cost of labor, materials, equipment, or services furnished by others, or over the Contractor's methods of determining prices, or over competitive bidding or market conditions, or over quantities of work actually performed, Engineer cannot and does not guarantee that proposals, bids, or actual construction cost will not vary from Opinions of Probable Construction Cost prepared by Engineer. This Opinion of Probable Construction Cost is limited to those items stated herein.

Notes:

1. The EOCP includes site work only and does not include the building, monument sign, or drive-thru equipment.
2. Demolition of the existing building, underground storage tank, fuel pumps, fuel piping, canopy and monument sign are to be completed by the Seller and are not included in this EOCP.



CONSULTING. ENGINEERING. CONSTRUCTION.

To: Hakim Yala
Panda Express

From: Brian A. Styck, P.E.
Project Manager

Michael Keith, P.E.
Project Manager

Date: January 5, 2021

Subject: Willowbrook, IL Panda Express
Site Traffic Study
Special Use Permit

This memorandum report summarizes the findings and recommendations of the Site Traffic Study performed by Atwell, LLC for the Site Plan Review, Special Use Permit and Variations requested for a proposed Panda Express located at 7505 Kingery Highway (IL Route 83), Willowbrook, IL. The proposed project includes demolition of an existing 2,131 SF Shell Gasoline and Service Station with Convenience Mart and construction of a 2,300 SF Panda Express Restaurant. A Special Use Permit is requested to allow for a proposed drive-thru within the Village of Willowbrook B-2 Community Shopping Zoning District.

EXISTING CONDITIONS

Location

The subject property is a ±1.0 acre parcel located at the southeast corner of the Kingery Highway and 75th Street intersection. The site address is 7505 Kingery Highway, Willowbrook, IL 60527.



Figure 1 – Site Location Aerial View

Site Accessibility

The subject property is currently accessed from both Kingery Highway (IL Route 83) and 75th Street (see Figure 1).

Kingery Highway (IL Route 83) is a north-south principle arterial roadway under the jurisdiction of the Illinois Department of Transportation (IDOT). Adjacent to the site, Kingery Highway is a four-lane divided roadway section. At the signalized intersection with 75th Street, there are two north bound left turn lanes, one north bound right turn lane, one south bound left turn lane and one south bound right turn lane. Kingery Highway has a median consequently requiring right in/right out access at the site driveway. Based on IDOT's Traffic Data Map (year: 2019), Kingery Highway carries approximately 50,000 average daily trips.

75th Street is an east-west major collector roadway under the jurisdiction of the Village of Willowbrook. East of the project site, 75th Street is a two-lane undivided roadway section. West of the project site (west of the Kingery Highway intersection), 75th Street is a four-lane undivided roadway section. At the signalized intersection with Kingery Highway, there is an eastbound left and right turn lane and a westbound left and right turn lane. The current site does allow a full movement access on 75th Street. Based on IDOT's Traffic Data Map (year: 2019), 75th Street carries approximately 3,000 average daily trips (east of the Kingery Highway intersection).

Existing Land Use Traffic Generation

The existing site land use is a developed Shell Gasoline and Service Station with Convenience Mart (ITE Code 945). In accordance with the ITE Trip Generation Manual 10th Edition, the existing site generates the following traffic counts:

- Average Daily Trips = 2,131 SF x 1,440.02 Trips/KSF = 3,069 Trips

- AM Peak Hour Total Trips = 2,131 SF x 75.99 Trips/KSF = 162 Trips
- PM Peak Hour Total Trips = 2,131 SF x 88.35 Trips/KSF = 188 Trips

PROPOSED CONDITIONS

Site Accessibility

The Panda Express site layout proposes constructing new access driveways in the approximate location of the existing access driveways, with the exception of the westernmost right-in only site access drive on 75th Street which will be closed off and curbed. The access on 75th Street will be a full movement for inbound traffic and limited to a right-out only. The access on Kingery Highway will remain to be a right-in/right-out access.

75th Street – The proposed site access driveway on 75th Street meets the Village requirement of 70' from the nearest lot line, however, it does not meet the minimum spacing from the ROW intersection of 500' (or outside the functional area), therefore a variation will be required. This variation was previously granted for the existing site access driveway.

Kingery Highway – The proposed site access driveway on Kingery Highway neither meets the Village requirement of 70' from the nearest lot line, nor the minimum spacing from the ROW intersection of 500' (or outside the functional area), therefore variations will be required for both requirements. These variations were previously granted for the existing site access driveway.

Proposed Land Use Traffic Generation

The proposed site land use is a Fast Food Restaurant with Drive-Thru (ITE Code 934). In accordance with the ITE Trip Generation Manual 10th Edition, the proposed site will generate the following traffic counts:

- Average Daily Trips = 2,300 SF x 470.95 Trips/KSF = 1,083 Trips
- AM Peak Hour Total Trips = 2,300 SF x 40.19 Trips/KSF = 92 Trips*
 - *NOTE: Panda Express typical hours of operation are 10 AM to 10 PM, therefore, the AM peak hour total trips listed above are not anticipated for this site. The AM Peak Hour Total Trips information for a Fast Food Restaurant with Drive-Thru is provided for reference only.
- PM Peak Hour Total Trips = 2,300 SF x 32.67 Trips/KSF = 75 Trips

Based on trip calculations from the ITE Trip Generation Manual 10th Edition, the proposed site land use will generate approximately 60%-65% less traffic (trips) than the existing site land use.

	Existing Land Use Shell Gasoline/Service Station with Convenience Mart (ITE Code 945)	Proposed Land Use Panda Express Fast Food Restaurant with Drive-Thru (ITE Code 934)	Total Trip Decrease	% Decrease
Average Daily Trips	3,069	1,083	-1,986	-64.71%
AM Peak Hour Trips	162	92*	-70	-43.21%
PM Peak Hour Trips	188	75	-113	-60.11%

*NOTE: Panda Express typical hours of operation are 10 AM to 10 PM, therefore, the AM peak hour total trips listed above are not anticipated for this site. The AM Peak Hour Total Trips information for a Fast Food Restaurant with Drive-Thru is provided for reference only.

Drive-Thru Queueing

Data was collected from three (3) similar nearby Panda Express drive-thru restaurants for the purpose of estimating peak drive-thru queueing for the proposed Willowbrook, IL Panda Express. The three similar nearby Panda Express drive-thru restaurants include the following locations:

Panda Express #1054
185 Countryside Plaza
Countryside, IL 60525

Panda Express #2685
6501 S Cicero Ave
Chicago, IL 60638

Panda Express #2969
1010 Ogden Ave
Downers Grove, IL 60515



Figure 2 – Similar Nearby Panda Express Drive-Thru Restaurants

The data collected from three (3) similar nearby Panda Express drive-thru restaurants included seven (7) consecutive days of drive-thru transaction counts, divided into 30-minute time spans. Copies of the drive-thru transaction information for each nearby location is included in the attachments. At the request of Panda Express, the net sales information has been removed from the drive-thru transaction tables. An average service time of 5 minutes (information provided by Panda Express) starting with the food order at the menu board and ending with food pickup at the drive-thru window was used for the drive-thru queueing analysis.

	Panda #1054	Panda #2685	Panda #2969
Avg. Daily Total (10 AM - 10 PM)	345	327	174
Max. One Day Total (10 AM - 10 PM)	397	386	201
Peak 7 Day Avg./30 Min.	22	18	12
Peak Total/30 Min.	25	23	15
Estimated Max Queueing	5	4	3

Panda Express #1054 had the highest drive-thru traffic counts in all categories. Both the peak 7-day average transactions/30 minutes and the peak total transactions/30-minutes occurred during lunch hours (11 AM – 2 PM). The peak total transactions/30-minute span (25 cars) occurred on three (3) separate occasions during

the one-week data collection. The maximum drive-thru queueing estimate for the proposed Willowbrook, IL Panda Express, based on counts from Panda Express #1054, is not expected to exceed five (5) cars during peak business hours.

The Willowbrook, IL Panda Express proposed site layout is designed to provide a total storage length of seven (7) vehicles for the drive-thru window queue, with placement of the menu/order board at the fifth car. Furthermore, two (2) drive-thru parking pick up spaces are proposed which will help reduce drive-thru queueing as needed during peak business hours. A copy of the Traffic Plan showing the proposed drive-thru queueing and drive-thru parking pick up spaces is provided in the attachments.

Parking Evaluation

The Village of Willowbrook Ordinance 9-10-5(K) requires 1 parking space for each 100 square feet of floor area which equals 23 parking spaces (2,300 SF x 1 parking space/100 SF). For comparison, the nearby similar Panda Express Drive-Thru Restaurant #2969 also requires 23 parking spaces (2,300 SF x 10 parking spaces/1,000 SF) in accordance with the Village of Downers Grove Municipal Code (Sec 28.7.030). The proposed Panda Express site layout provides a total of 46 parking spaces which is double the Village of Willowbrook parking requirement and should be more than adequate to meet current and future parking needs. The 46 total parking spaces are broken down as follows:

- Regular = 42 Spaces
- Drive-Thru Pickup = 2 Spaces
- ADA (Van Accessible) = 2 Spaces

Truck Turn Analysis

A truck turn analysis was completed for the proposed Panda Express site layout with the following vehicles:

- Fire Truck: 40' Overall Length (22' wheel base)
- Front Load Garbage Truck: 33.75' Overall Length (22.167' wheel base)
- Delivery Truck: 45.5' Overall Length (40' wheel base)

No issues are anticipated with truck access in and out of the site. A copy of the Truck Turn Analysis Exhibit is provided in the Attachments.

CONCLUSION

Conclusion

Based on the proposed Panda Express Drive-Thru Restaurant development plans and the preceding findings, the following conclusions and recommendations are made:

- The proposed site access is more than adequate to serve the site and will provide for safer traffic flow by eliminating the westernmost driveway on 75th Street and requiring a right turn only onto 75th Street.
- The proposed Panda Express Drive-Thru Restaurant is estimated to generate less traffic than the existing Shell Gasoline and Service Station with Convenience Mart; therefore, the proposed development will have no additional impact on adjacent roadways and/or land uses.
- The proposed drive-thru is designed to provide a total storage length that exceeds the estimated peak drive-thru queueing. Furthermore, two drive-thru parking pick up spaces are proposed which will help to reduce drive-thru queueing as needed during peak business hours.
- The proposed site layout is designed to provide sufficient parking spaces that exceed the Village of Willowbrook off street parking requirements.

ATTACHMENTS

Attachments

1. Traffic Plan
2. Nearby Panda Express Drive-Thru Transaction Counts:
 - a. Panda #1054 (Countryside)
 - b. Panda #2685 (Chicago)
 - c. Panda #2969 (Downers Grove)
3. Truck Turn Analysis

Panda Express #1054 (Countryside, IL)

Store	Daypart Detail	Sun		Mon		Tue		Wed		Thu		Fri		Sat		Average Daily			
		DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)
1054	Breakfast 10:00 - 10:29		0		0		1		1		1		0		0		0		0
1054	Breakfast 10:30 - 10:59		4		5		9		6		8		7		3		6		6
1054	Lunch 11:00 - 11:29		10		14		18		17		14		14		12		14		14
1054	Lunch 11:30 - 11:59		13		21		21		19		21		23		15		21		18
1054	Lunch 12:00 - 12:29		18		25		22		25		22		23		17		22		22
1054	Lunch 12:30 - 12:59		16		21		22		22		20		25		19		21		21
1054	Lunch 13:00 - 13:29		16		22		18		20		19		23		19		20		20
1054	Lunch 13:30 - 13:59		15		18		16		19		17		23		20		18		18
1054	Afternoon 14:00 - 14:29		16		18		15		18		16		20		21		18		18
1054	Afternoon 14:30 - 14:59		16		15		11		14		14		19		19		16		16
1054	Afternoon 15:00 - 15:29		20		16		14		14		12		16		20		16		16
1054	Afternoon 15:30 - 15:59		18		13		17		16		17		16		17		16		16
1054	Afternoon 16:00 - 16:29		18		14		12		15		14		17		20		17		17
1054	Afternoon 16:30 - 16:59		18		13		17		16		17		17		18		17		17
1054	Dinner 17:00 - 17:29		21		21		17		19		19		21		18		19		19
1054	Dinner 17:30 - 17:59		17		18		22		21		19		20		20		20		20
1054	Dinner 18:00 - 18:29		19		17		18		18		17		20		19		19		19
1054	Dinner 18:30 - 18:59		16		18		15		18		22		20		20		18		18
1054	Dinner 19:00 - 19:29		12		15		14		19		18		20		16		16		16
1054	Dinner 19:30 - 19:59		8		11		14		13		16		18		12		13		13
1054	Evening 20:00 - 20:29		7		11		12		15		13		16		14		12		12
1054	Evening 20:30 - 20:59		4		6		5		9		6		11		7		7		7
1054	Evening 21:00 - 21:29		1		3		3		2		2		8		6		3		3
1054	Evening 21:30 - 21:59		1		1		1		1		1		3		1		1		1
1054	Evening 22:00 - 22:29		0		0		0		0		0		1		1		0		0
1054	Evening 22:30 - 22:59		0		0		0		0		0		0		0		0		0
1054	Evening 23:00 - 23:29		0		0		0		0		0		0		0		0		0
1054	Evening 23:30 - 23:59		0		0		0		0		0		0		0		0		0
			300		330		332		361		341		397		351		345		345

Panda Express #2969 (Downers Grove, IL)

Store	Day of Week	Daypart Detail	Sun		Mon		Tue		Wed		Thu		Fri		Sat		Average Daily		
			DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales
2969	Breakfast	10:00 - 10:29	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2969	Breakfast	10:30 - 10:59	3	3	3	5	5	5	5	5	5	5	5	5	5	5	5	5	5
2969	Lunch	11:00 - 11:29	4	7	7	10	10	10	10	10	10	10	10	10	10	10	10	10	10
2969	Lunch	11:30 - 11:59	7	10	10	11	11	11	11	11	11	11	11	11	11	11	11	11	11
2969	Lunch	12:00 - 12:29	8	13	13	14	14	14	14	14	14	14	14	14	14	14	14	14	14
2969	Lunch	12:30 - 12:59	8	11	11	13	13	13	13	13	13	13	13	13	13	13	13	13	13
2969	Lunch	13:00 - 13:29	7	12	12	11	11	11	11	11	11	11	11	11	11	11	11	11	11
2969	Lunch	13:30 - 13:59	7	10	10	11	11	11	11	11	11	11	11	11	11	11	11	11	11
2969	Afternoon	14:00 - 14:29	7	7	7	8	8	8	8	8	8	8	8	8	8	8	8	8	8
2969	Afternoon	14:30 - 14:59	8	7	7	6	6	6	6	6	6	6	6	6	6	6	6	6	6
2969	Afternoon	15:00 - 15:29	7	10	10	7	7	7	7	7	7	7	7	7	7	7	7	7	7
2969	Afternoon	15:30 - 15:59	6	8	8	4	4	4	4	4	4	4	4	4	4	4	4	4	4
2969	Afternoon	16:00 - 16:29	6	7	7	6	6	6	6	6	6	6	6	6	6	6	6	6	6
2969	Afternoon	16:30 - 16:59	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7
2969	Dinner	17:00 - 17:29	9	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10
2969	Dinner	17:30 - 17:59	8	9	9	9	9	9	9	9	9	9	9	9	9	9	9	9	9
2969	Dinner	18:00 - 18:29	10	10	10	8	8	8	8	8	8	8	8	8	8	8	8	8	8
2969	Dinner	18:30 - 18:59	9	9	9	10	10	10	10	10	10	10	10	10	10	10	10	10	10
2969	Dinner	19:00 - 19:29	10	8	8	8	8	8	8	8	8	8	8	8	8	8	8	8	8
2969	Dinner	19:30 - 19:59	5	5	5	7	7	7	7	7	7	7	7	7	7	7	7	7	7
2969	Evening	20:00 - 20:29	4	4	4	6	6	6	6	6	6	6	6	6	6	6	6	6	6
2969	Evening	20:30 - 20:59	4	3	3	4	4	4	4	4	4	4	4	4	4	4	4	4	4
2969	Evening	21:00 - 21:29	0	3	3	1	1	1	1	1	1	1	1	1	1	1	1	1	1
2969	Evening	21:30 - 21:59	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1
2969	Evening	22:00 - 22:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2969	Evening	22:30 - 22:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2969	Evening	23:00 - 23:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2969	Evening	23:30 - 23:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
			145	175	183	183	183	183	183	193	201	141	174						



PANDA EXPRESS, INC.
1833 Walnut Grove Ave.
Rosemead, CA 91768
Telephone: 626 796 8608
Facsimile: 626 972 5282

ATWELL, NORR, ARCHITECTS AND ENGINEERS, INC.
is licensed by the State of California to provide the professional services of a registered professional engineer and a registered professional architect. The services provided herein are those of a registered professional engineer and a registered professional architect. The services provided herein are those of a registered professional engineer and a registered professional architect.

REVISIONS:

ISSUE DATE: 11/02/00
PROJECT: SUBMITTAL

DESIGNED BY: LEH

PANDA PROJECT #: 07068
ATWELL PROJECT #: 1800708

NORR

ATWELL

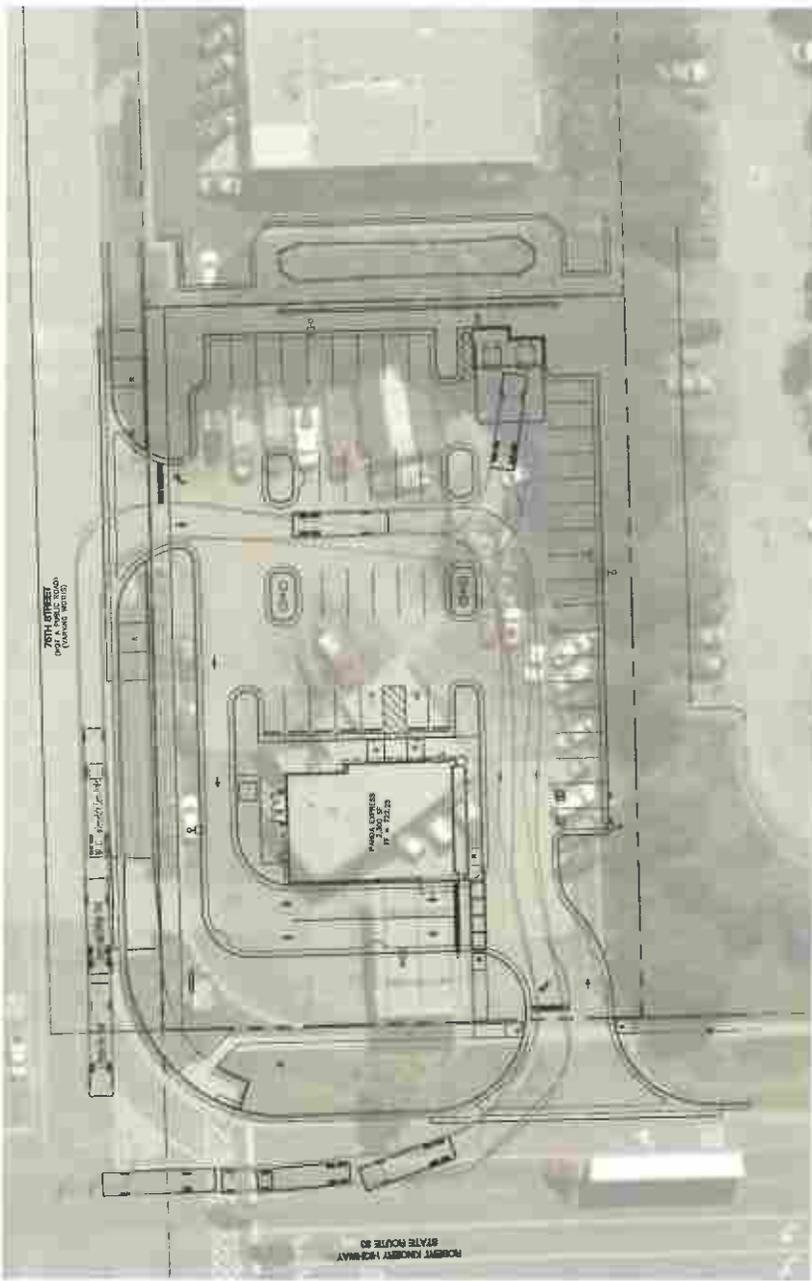
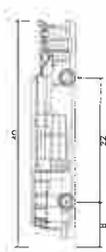
PANDA EXPRESS
17820 KIMBERLY HIGHWAY
WILLOWBROOK, IL 60097

TRUCK TURN ANALYSIS

EX-01

TRUE NORTH & WELCOME DRG

NOT FOR CONSTRUCTION



LEGEND
PUMPER FIRE TRUCK
FRONT-LOAD GARBAGE TRUCK
WB-40 DELIVERY TRUCK



24-HOUR CONTACT:
PANDA, INC.
HARRIS YALOW
847-477-5068

NOTES:
1. THIS TRUCK TURN ANALYSIS IS FOR INFORMATIONAL PURPOSES ONLY. IT IS NOT TO BE USED FOR CONSTRUCTION. THE USER OF THIS ANALYSIS SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES. THE USER OF THIS ANALYSIS SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES.





PANDA EXPRESS, INC.
1483 Westland Centre Ave.
Fosterland, California
Telephone: 650.799.8808
Facsimile: 650.372.8208

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REVISIONS:

NO.	DATE	DESCRIPTION
1	11/20/2020	ISSUE DATE

DRAWN BY: LEH
PANDA PROJECT # D7068
ATWELL PROJECT # 1003769

NORR

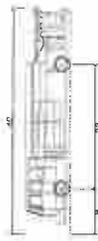


PANDA EXPRESS
1483 W. WILLOWBROOK, L. 69227
1483 WILLOWBROOK AVE.
WILLOWBROOK, IL 60092

TRUCK TURN ANALYSIS
EX-01

TRUE NORTH & WELCOME 2000

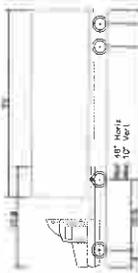
NOT FOR CONSTRUCTION



Pumper Fire Truck
Overall Length 32
Overall Width 8
Min Body Height 10.656ft
Min Body Ground Clearance 8.000ft
Lock-to-Lock Time 145.00°
Max Wheel Angle



Front-load Garbage Truck
Overall Length 33.750ft
Overall Width 8.375ft
Min Body Height 10.568ft
Min Body Ground Clearance 8.000ft
Lock-to-Lock Time 8.000s
Max Wheel Angle 29.300ft



WS-40 Intermediate Semi-Trailer
Overall Length 45.499ft
Overall Width 8.000ft
Min Body Height 13.300ft
Min Body Ground Clearance 8.000ft
Lock-to-Lock Time 20.30°



LEGEND:
PUMPER FIRE TRUCK
FRONT-LOAD GARBAGE TRUCK
WS-40 DELIVERY TRUCK

181
None who's below.
Call before you dig.

24-HOUR CONTACT
HANK YALA
847-677-5925

SCALE: 1" = 20 FEET



PANDA EXPRESS, INC.
1881 Walnut Grove Ave
Rosemead, California
Tel: 626-286-7600
Fax: 626-286-7601

THIS PLAN IS THE PROPERTY OF THE ENGINEER. IT IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREON. ANY REUSE OR MODIFICATION OF THIS PLAN WITHOUT THE WRITTEN CONSENT OF THE ENGINEER IS STRICTLY PROHIBITED.

REVISIONS:

NO.	DATE	DESCRIPTION
1	10/20/2011	ISSUE DATE

DATE: 10/20/2011

DRAWN BY: LEH

PANDA PROJECT # D7068

ATWELL PROJECT # 1000769

NORR
NORR ENGINEERING, INC.
10000 WILLOW BROOK AVENUE
WILLOW BROOK, ILLINOIS 60091
TEL: 630-584-1000
WWW.NORRENG.COM



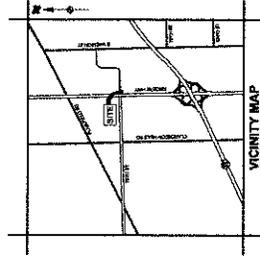
PANDA EXPRESS
1881 WALNUT GROVE AVE
ROSEMEAD, CALIFORNIA 91768

TRAFFIC PLAN

EX-02

TRUE NORTH & WELCOME 7500

NOT FOR CONSTRUCTION



LEGEND:

- ROADWAY USE
- EXISTING SIDEWALK
- PARKING SPACE COUNT
- PROPOSED SIDEWALK
- PROPOSED ACCESSIBLE PARKING SPACE SIGN
- POST, STOP SIGN

SITE TRAFFIC DATA

BASED ON PEAK HOUR TRAFFIC VOLUMES AND PEAK HOUR TRAFFIC SIGNAL CYCLES (75 SECS PH)

PEAK HOUR TRAFFIC VOLUME (PEAK HOUR TRAFFIC SIGNAL CYCLES)

PEAK HOUR TRAFFIC SIGNAL CYCLES (75 SECS PH)

PEAK HOUR TRAFFIC SIGNAL CYCLES (75 SECS PH)

PEAK HOUR TRAFFIC SIGNAL CYCLES (75 SECS PH)

SITE OPERATIONAL DATA

OPERATIONAL DATA

OPERATIONAL DATA

OPERATIONAL DATA

OPERATIONAL DATA

PARKING DATA

PARKING DATA

PARKING DATA

PARKING DATA

PARKING DATA

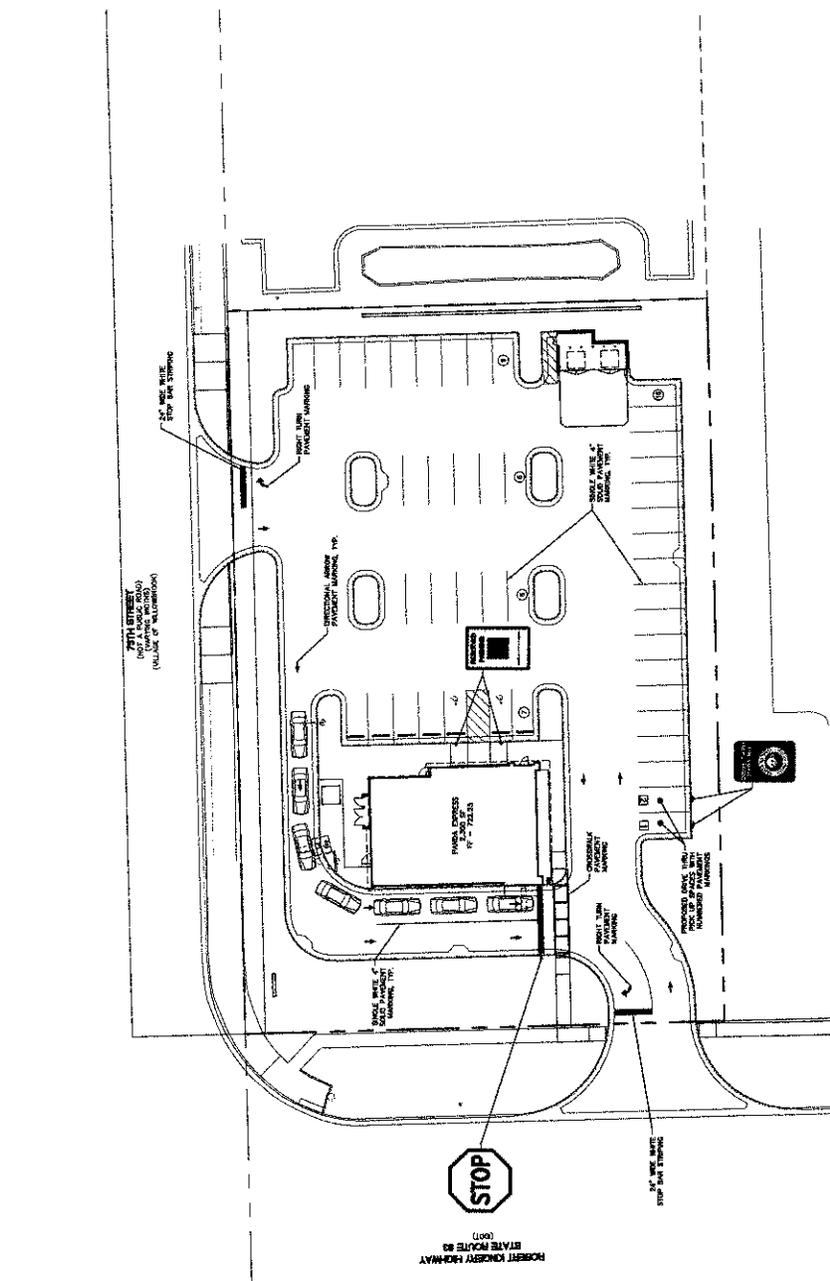
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1818

Robert Koenig & Partners, Inc.
Civil Engineers & Architects

28 HANCOCK CONTACT
PANDA EXPRESS
JANET YALA
647-677-8855

SCALE
1" = 20 FEET



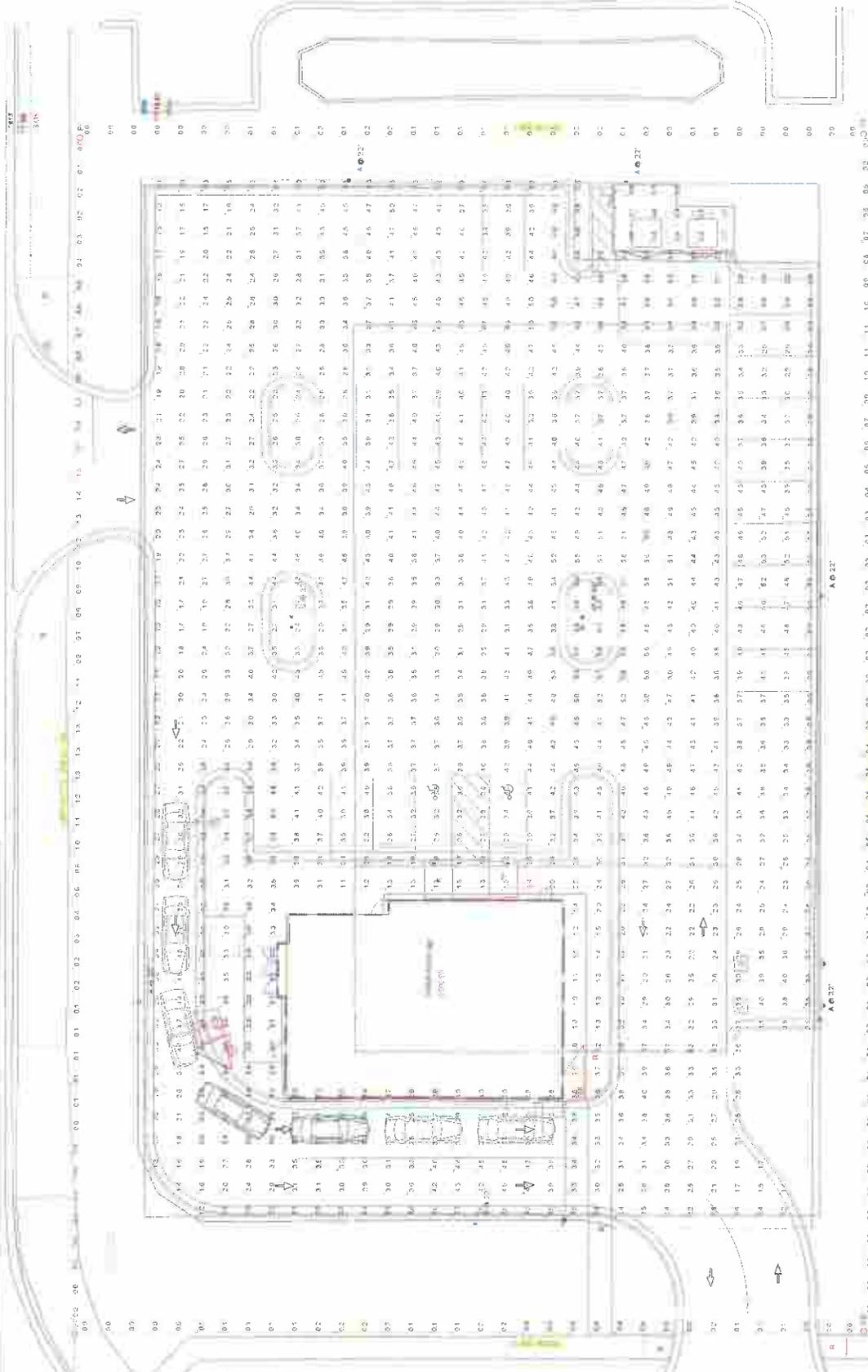
PANDA EXPRESS, IL
WILLOWBROOK, IL

Designer
R. ZINSELMIEER
Date
10/29/2020
Scale
NOTED
Drawing No.
Summary

- Note**
1. MOUNTING HEIGHT OF 22' (20 POLE)
 2. CALCULATIONS TAKEN AT GROUND LEVEL
 3. CONTACT VILA LIGHTING - RYAN ZINSELMIEER
RYAN@VILALIGHTING.COM 314-531-2600

TYPE	DESCRIPTION	AMOUNT	UNIT PRICE	TOTAL
1	34' G. 1.1 K 1.0 K	6.11	3.41	
2	0.3 G. 1.5 K 0.0 G.	N/A	N/A	
3	13.7 FEET 1/2" 2.6 K 1.8 I	1.41		

QTY	DESCRIPTION	UNIT PRICE	TOTAL
1	SUN-LED-18L-7000-340 CONTACT RYAN ZINSELMIEER-314-531-2600	148.5	
1	SUN-LED-18L-516-340 CONTACT RYAN ZINSELMIEER-314-531-2600	148.5	
1	SUN-LED-18L-516-PT-40 CONTACT RYAN ZINSELMIEER-314-531-2600	297	



Plan View
Scale: 1" = 16'



WILLOWBROOK, IL PANDA EXPRESS – PROPOSED CONSTRUCTION SCHEDULE

Item	Description	2021											
		Jun	Jul	Aug	Sep	Oct	Nov	Dec					
1	Mobilization / Begin Construction		X										
2	Demolition		X										
3	Earthwork		X										
4	Building - Foundation/Slab		X										
5	Site Utilities			X									
6	Building - Vertical Construction			X									
7	Site Concrete/Paving				X								
8	Landscaping					X							
9	Site Signage / Parking Lot Striping						X						
10	Final Completion / Store Opening									X			



Attachment 3
Findings of Fact
Standards for Special Use Permits (2 pages)

9-14-5(B): Standards for Special Use

The Plan Commission shall not recommend and the Board of Trustees shall not grant a Special Use Permit from the regulation of this title unless affirmative findings of fact shall be made as to all of the standards hereinafter set forth, which findings of fact shall be based upon evidence adduced upon the hearing held thereon, that:

- (A) **That the establishment, maintenance, or operation of the special use will not be detrimental to or endanger the public health, safety, morals, comfort or general welfare.**

Applicant Finding: Panda Express is a nationwide, experienced restaurateur that has developed many sites prior to this one. Establishment, maintenance, or operation of the proposed Panda Express Drive-Thru Restaurant will not be detrimental to or endanger the public health, safety, morals, comfort or general welfare.

- (B) **That the special use will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish and impair property values within the neighborhood.**

Applicant Finding: The proposed Panda Express Drive-Thru Restaurant will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish and impair property values within the neighborhood. The proposed improvements made to an old site will be a major upgrade to what exists there now.

- (C) **That the establishment of the special use will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.**

Applicant Finding: Establishment of the proposed Panda Express Drive-Thru Restaurant will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district. The entire surrounding area is already developed.

- (D) **That adequate utilities, access roads, drainage and/or other necessary facilities have been or are being provided.**

Applicant Finding: Adequate utilities, access (drives), drainage, and/or other necessary facilities will be engineered, permitted and constructed in accordance with local requirements for the proposed Panda Express Drive-Thru Restaurant.

- (E) **That adequate measures have been or will be taken to provide ingress and egress so designed as to minimize traffic congestion in the public streets.**

Applicant Finding: Site layout, access drives, parking, signage, pavement markings and estimated drive-thru queueing were analyzed to provide ingress and egress so designed as to minimize traffic congestion in the public streets for the proposed Panda Express Drive-Thru Restaurant. Furthermore, in accordance with the ITE Trip Generation 10th Edition, traffic generated from the proposed land use will be substantially lower than existing traffic generated from the current land use.



- (F) That the special use shall in all other respects conform to the applicable regulations of the district in which it is isolated, except as such regulations may in each instance be modified by the Village Board pursuant to the recommendation of the Plan Commission.**

Applicant Finding: The proposed Panda Express Drive-Thru Restaurant shall conform to the applicable regulations of the "B-2" Zoning District with the exception of Variations approved by the Village Board pursuant to the recommendation of the Plan Commission.

- (G) Conditions in the area have substantially changed, and at least one year has elapsed since any denial by the Village Board of any prior application for a special use permit that would have authorized substantially the same use of all or part of the site. (Ord. 97-O-05, 1-27-1997).**

Applicant Finding: The proposed Panda Express Drive-Thru Restaurant has not previously been submitted for a Special Use Permit.



Attachment 4
Findings of Fact
Standards for Variations (2 pages)

9-14-4(E): Standards for Variations

The Plan Commission shall not recommend and the Board of Trustees shall not grant variations from the regulation of this title unless affirmative findings of fact shall be made as to all of the standards hereinafter set forth, which findings of fact shall be based upon evidence adduced upon the hearing held thereon, that:

- (A) The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located.**

Applicant Finding: The subject property for the proposed Panda Express Drive-Thru Restaurant is a substandard size lot for the B-2 district and cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. The requested variations are needed to provide a site which has economic viability and can be operated safely with good access and parking.

- (B) The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district.**

Applicant Finding: The requested variations will not merely serve as a convenience for the proposed Panda Express Drive-Thru Restaurant, but rather are necessary for any business development on the existing substandard lot. The small size of this lot creates a hardship that can only be remedied with the variations requested so that a safe efficient operation can be maintained.

- (C) The alleged hardship has not been created by any person presently having a proprietary interest in the premises.**

Applicant Finding: The alleged hardships were not created by any person presently having a proprietary interest in the premises and proposed Panda Express Drive-Thru Restaurant. Previous Variations approved for the site under Ordinance 94-O-16 are similar to the variations being requested herein so that the site could yield a reasonable return. The size and dimensions of the lot were preexisting.

- (D) The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.**

Applicant Finding: Proposed Variations for the proposed Panda Express Drive-Thru Restaurant will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood as they were previously granted for the existing uses on the property with no negative impacts on surrounding property.



- (E) **The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, increase the danger of fire, or endanger the public safety.**

Applicant Finding: Proposed Variations for the proposed Panda Express Drive-Thru Restaurant will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.

- (F) **The proposed variation will not alter the essential character of the locality. (Ord. 77-O-4, 2-14-1977)**

Applicant Finding: Proposed Variations for the proposed Panda Express Drive-Thru Restaurant will not alter the essential character of the locality as the restaurant will conform to the existing surrounding area uses. As they were previously granted for this site so the new development is consistent with was previously existing as far as variations.

- (G) **The proposed variation is in harmony with the spirit and intent of this title. (Ord. 97-O-05, 1-27-1997)**

Applicant Finding: Proposed Variations for the proposed Panda Express Drive-Thru Restaurant are in harmony with the spirit and intent of Title 9 of this Code. The subject property cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. Previous variations approved for the subject site under Ordinance 94-O-16 changed the driveways and also noted that a portion of the lot was taken by the widening of Route 83. Variations are necessary for development on the existing substandard lot.



WILLOWBROOK, IL PANDA EXPRESS – VARIATIONS REQUESTED

#	Village Code	Description	Requirement	Existing Conditions (Shell Gas/Service Station; Ordinance 94-O-16)	Requested Variation
1	9-3-3(C)	Minimum Lot Area, Two Uses	2 Acres	1 Acre	1 Acre
2	9-3-7(A)1	Route 83 Setback	100'	58' (Service Station Building) 5'-4" (Overhead Canopy)	50' (Building) 62.4' (Order Canopy) 56.6' (Menu Board)
3	9-6-1(A)	Minimum Lot Area, Two Uses	2 Acres	1 Acre	1 Acre
4	9-6B-3(A)	Minimum Lot Area, (B-2 Bulk Regulations)	2 Acres	1 Acre	1 Acre
5	9-6B-3(C)1	Minimum Lot Width	200'	170'	170'
6	9-6B-3(E)1	Minimum Front Yard Setback (Route 83)	60'	58' (Service Station Building) 5'-4" (Overhead Canopy) 15' (Gas Pump)	50' (Building) 62.4' (Order Canopy) 56.6' (Menu Board) 43.4' (Building)
7	9-6B-3(E)3	Minimum Exterior Side Yard Setback (75th St.)	60'	43'	25.0' (Order Canopy) 32.7' (Menu Board)
8	9-10-5(L)2(b)(2)A	Minimum Driveway Radius Connecting to 75th Street	35'	16'	25'
9	9-10-5(L)2(d)	Driveway Location to Nearest Lot Line	70'	17'-6"	25.7'
10	9-10-5(L)2(e)	Minimum Spacing Between Separate Driveways	400'	±171' (75th St.) ±306' (Route 83)	±169' (75th St.) ±313' (Route 83)
11	9-10-5(L)2(f)	Minimum Spacing Between Driveway & ROW Intersection	500' or Outside Functional Area (whichever is less)	154.3' (75th St.) 101.4' (Route 83)	146.2' (75th St.) 94.3' (Route 83)
12	9-10-5(L)2(g)	Access Driveway Concrete Median & Driveway Storage	Concrete Median & 100' Storage Area	No Concrete Median, 0' Storage Area	No Concrete Median, 0' Storage Area
13	9-14-2(D)2(c)(4)	Minimum Landscape Foundation Plantings Width	7'	0' (none provided)	4.5' at South Façade 5.2' at Northern East Façade 0' at Western Façade & Remaining East Façade

EXHIBIT A

Staff Report to the Plan Commission
21-01 7505 Kingery Highway
Panda Express Special Use Permit

February 3, 2021
Ann Choi, Planning Consultant



Attachment 5
Public Hearing Notice (3 pages)

NOTICE OF PUBLIC HEARING
ZONING HEARING CASE NO. 21-01

NOTICE IS HEREBY GIVEN that the Plan Commission of the Village of Willowbrook, DuPage County, Illinois, will conduct a public hearing at a regular meeting of the Plan Commission on the 13th of January, 2021 at the hour of 7:00 P.M. This meeting would typically take place in the Willowbrook Police Department Training Room, 7760 S. Quincy St, Willowbrook, IL 60527. However, due to the current circumstances concerning Covid-19, this meeting will be held virtually. Internet address and access instructions will be provided on the Village of Willowbrook's Plan Commission website once available:

<https://www.willowbrookil.org/Archive.aspx?AMID=44>

The purpose of this public hearing shall be to consider a petition requesting approval of a special use permit for a fast food establishment and a special use permit for a drive-through in the B-2 Community Shopping District, including certain variations from Title 9 of the Village Code. The applicant seeks to demolish the existing gas/service station and construct a one-story building and drive-through with associated site improvements on the property legally described as follows:

THE NORTH 170.0 FEET OF THAT PART OF THE SOUTHEAST QUARTER OF SECTION 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING EAST OF THE EAST RIGHT OF WAY OF STATE ROUTE 83, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT 102.65 FEET EAST OF THE NORTHWEST CORNER OF SAID SOUTHEAST QUARTER; THENCE EAST, ALONG THE NORTH LINE OF SAID SOUTHEAST QUARTER, A DISTANCE OF 256.24 FEET; THENCE SOUTH, ALONG A LINE PARALLEL WITH THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170 FEET; THENCE WEST, ALONG A LINE PARALLEL WITH THE NORTH LINE OF SAID SOUTHEAST QUARTER, A DISTANCE OF 256.24 FEET TO THE INTERSECTION OF SAID PARALLEL LINE WITH THE EAST RIGHT OF WAY OF SAID STATE ROUTE 83, WHICH POINT IS 102.45 FEET EAST OF THE WEST LINE OF SAID SOUTHEAST QUARTER; THENCE NORTH, ALONG THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170.0 FEET TO THE POINT OF BEGINNING, IN DUPAGE COUNTY, ILLINOIS.

PIN: 09-26-400-013

ADDRESS: 7505 S. Kingery Highway, Willowbrook IL 60527.

The applicant for this petition is Hakim Yala of Panda Express, Inc., 1683 Walnut Grove Avenue, Rosemead CA 91770. The property owner is True North Energy, LLC, 10346 Brecksville Road, Brecksville OH 44141.

A copy of the Village's petition is on file in the Office of the Village Planner, Village of Willowbrook, 835 Midway, Willowbrook, Illinois, and is available for public inspection. Any individual with a disability requiring a reasonable accommodation in order to participate in any public meeting held under the authority of the Village of Willowbrook should contact Ann Choi, Village of Willowbrook, 835 Midway, Willowbrook, IL 60527, or call (630) 920-2233, Monday through Friday, between 8:30 A.M. and 4:30 P.M. All persons desiring to be heard in support or opposition to the application shall be afforded an opportunity and may submit their statements orally, in written form, or both. Written comments may be submitted up to the hour of 6:00pm on January 13, 2021 to planner@willowbrook.il.us. This hearing may be recessed to another date if not concluded on the evening scheduled.

/s/ Brian Pabst
Village Administrator
(630) 920-2261

Published in the December 24, 2020 edition of *The Doings* Newspaper.

EQUAL HOUSING UNITIES

is advertised in is subject to the Housing Act. illegal to ad- ference, limita- tion based religion, sex, ial status, or in the sale, ing of housing. the Illinois Act prohibits based on age, status, sexual unfavorable je.

It not knowingly arising for real in violation of ons are hereby all dwellings available on an y basis. we you have ted against in the sale, rental ouling. Call:

Suburbs: using Center 4-8800
Suburbs: Housing Center 7-4874
Suburbs: using Center
Suburbs: 1-8780

LEGAL NOTICES

Notice of Public Sale of Personal Property Metro Self Storage

Notice is hereby given that the undersigned self storage units will be sold at a public sale by competitive bidding, in their entirety to the highest bidder to satisfy the lien of the Lessor, with Metro Storage LLC as managing agent for Lessor, for rental and other charges due from the undersigned. The said property has been stored and is located at the respective address below. The sale will be conducted under the guidance of Christopher Rosa (441.002059) on behalf of the facility's management. Units will be available for viewing and bidding five (5) days prior to the sale on www.StorageResumes.com. The bidding will close on January 20, 2021 beginning at 9:30 AM. The terms of the sale will be cash only to the highest bidder. A 10% buyer's premium will be charged per unit. All sales are final. Metro Self Storage LLC reserves the right to withdraw any or all units, partial or entire, from the sale at any time before the sale or to refuse any bids. The property to be sold is described as "general household items" unless otherwise noted. All contents must be removed completely from the property within 48 hours or sooner.

Metro Self Storage - 3220 West Touhy Ave, Skokie, IL 60076, 847-265-1006
Unit 783 Amado Guillarta.

Metro Self Storage - 2121 Sherman Road, Northbrook, IL 60062, 847-265-0200
Unit 586 Carl Dunham.

12/24, 12/31/2020 6840829

Notice of Public Sale

Notice is hereby given that the undersigned will sell, to satisfy lien of the owner, according to (770 ILCS 95) Self-Service Storage Facility Act, at public sale by competitive bidding on www.storage-resumes.com ending on January 6, 2021 at 10:00 AM or after for units located at:

Compass Self Storage
3000 N River Rd.
River Grove, IL 60171

The personal goods stored therein by the following may include, but are not limited to general household, furniture, boxes, clothes and appliances. Purchases must be made with cash only and paid at the time of sale. All goods are sold as is and must be removed at time of purchase. Compass Self Storage reserves the right to refuse any bid. Sale is subject to adjournment.

Unit# Tenants Name
1015 Angela F Roberts
163 Anna Heese
224 Jordan
365 Heier Lemus
461 Tamell Richard

12/17, 12/24/20 6834208

APARTMENT FOR RENT?

The search begins here! Many apartment and home hunters check the Classifieds before looking for a new place to live. Advertise your rental units with us to get a jump on the competition! Call 866-399-0537 or visit placeanad.tribunesuburbs.com

ONLINE

Go to placeanad.tribunesuburbs.com to order and pay for your classified ad. Online. Anytime. It's fast! It's easy!

LEGAL NOTICES

Public Hearings

NOTICE OF PUBLIC HEARING ZONING HEARING CASE NO. 21-01

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<https://www.willowbrookil.org/Archive.aspx?AMID=44>

The purpose of this public hearing shall be to consider a petition requesting approval of a special use permit for a fast food establishment and a special use permit for a drive-through in the B-2 Community Shopping District including certain variations from Title 9 of the Village Code. The applicant seeks to demolish the existing gas/service station and erect a one-story building and drive-through with associated site improvements on the property legally described as follows:

THE NORTH 170.0 FEET OF THAT PART OF SECTION 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING EAST OF THE EAST RIGHT OF WAY OF STATE ROUTE 83, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT 102.65 FEET EAST OF THE NORTHWEST CORNER OF SAID SOUTHWEST QUARTER; THENCE EAST ALONG THE NORTH LINE OF SAID SOUTHWEST QUARTER, A DISTANCE OF 266.24 FEET; THENCE SOUTH ALONG A LINE PARALLEL WITH THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170 FEET; THENCE WEST ALONG A LINE PARALLEL WITH THE NORTH LINE OF SAID SOUTHWEST QUARTER, A DISTANCE OF 266.24 FEET TO THE INTERSECTION OF SAID PARALLEL LINE WITH THE EAST RIGHT OF WAY OF SAID STATE ROUTE 83, WHICH POINT IS 102.45 FEET EAST OF THE WEST LINE OF SAID SOUTHWEST QUARTER; THENCE NORTH ALONG THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170.0 FEET TO THE POINT OF BEGINNING, IN DUPAGE COUNTY, ILLINOIS.

PIN: 09-26-400-013

ADDRESS: 7305 S. Kingsley Highway, Willowbrook IL 60527.

The applicant for this petition is Hakim Yala of Panda Express, Inc., 1683 Walnut Grove Avenue, Rosemead CA 91770. The property owner is True North Energy LLC, 10346 Brecksville Road, Brecksville OH 44141.

A copy of the Village's petition is on file in the Office of the Village Planner, Village of Willowbrook, 835 Midway, Willowbrook, Illinois, and is available for public inspection. Any individual with a disability requiring a reasonable accommodation in order to participate in any public meeting held under the authority of the Village of Willowbrook should contact Ann Choi, Village of Willowbrook, 835 Midway, Willowbrook, IL 60527, or call (630) 920-2233, Monday through Friday between 8:30 A.M. and 4:30 P.M. All persons desiring to be heard in support of or opposition to the application shall be afforded an opportunity and may submit their statements orally, in written form, or both. Written statements may be submitted up to the hour of 6:00pm on January 13, 2021 to planner@willowbrook.il.us. This hearing may be recessed to another date if not concluded on the evening scheduled.

/s/ Brian Pabst
Village Administrator
(630) 920-2261

12/24/2020 6839354

Public Hearings

NOTICE OF PUBLIC HEARING ZONING HEARING CASE NO. 21-02

NOTICE IS HEREBY GIVEN that the Plan Commission of the Village of Willowbrook, DuPage County, Illinois, will conduct a public hearing at a regular meeting of the Plan Commission on the 13th of January, 2021 at the hour of 7:00 P.M. This meeting would typically take place in the Willowbrook Police Department Training Room, 7760 S. Quincey St., Willowbrook, IL 60527. However, due to the current circumstances concerning Covid-19, this meeting will be held virtually. Internet address and access instructions will be provided on the Village of Willowbrook's Plan Commission website once available.

<https://www.willowbrookil.org/Archive.aspx?AMID=44>

The purpose of this public hearing shall be to consider a petition for approval of a variation from Section 9-5B-3(K) to increase the maximum FAR from 0.30 to 0.39 to allow construction of a 1,081 square foot addition to house an indoor swimming pool and other such relief from Title 9 of the Village Code necessary, on the property legally described as follows:

LOT 176 IN GALLAGHER AND HENRY'S WATERFORD UNIT NO. 2, A SUBDIVISION OF PART OF THE NORTH WEST QUARTER OF SECTION 24, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED NOVEMBER 9, 1978 AS DOCUMENT R78-108411, IN DUPAGE COUNTY ILLINOIS.

PIN: 09-24-112-035

ADDRESS: 6401 Meadow Lane, Willowbrook, Illinois 60527

The applicant for this petition is Pete Barfui (and Jain Bhagwan as property owner), 25341 Radcliffe Road, Naperville, Illinois 60563.

A copy of the Village's petition is on file in the Office of the Village Planner, Village of Willowbrook, 835 Midway, Willowbrook, Illinois, and is available for public inspection. Any individual with a disability requiring a reasonable accommodation in order to participate in any public meeting held under the authority of the Village of Willowbrook should contact Ann Choi, Village of Willowbrook, 835 Midway, Willowbrook, IL 60527, or call (630) 920-2233, Monday through Friday between 8:30 A.M. and 4:30 P.M.

/s/ Brian Pabst
Village Administrator
(630) 920-2261

12/24/2020 6839375

PUBLIC NOTICE

FLAG CREEK WATER RECLAMATION DISTRICT 2021 BOARD MEETING SCHEDULE

All meetings are scheduled to start at 10:00 A.M. (CST) on the dates stated below at the District's main office located at 7001 North Frontage Road, Burr Ridge, Illinois 60527.

January 29, 2021 (Friday)
February 26, 2021 (Friday)
March 26, 2021 (Friday)
April 16, 2021 (Friday) - Budget Study Session
April 30, 2021 (Friday)
May 28, 2021 (Friday)
June 25, 2021 (Friday)
July 30, 2021 (Friday)
August 27, 2021 (Friday)
September 24, 2021 (Friday)
October 29, 2021 (Friday)
November 19, 2021 (Friday)
December 17, 2021 (Friday)

Any changes in meeting dates, times or location of the meeting will be published on the District's website, posted on the front door of the District's main office and published in the Hinsdale Doings per the Open Meeting Act requirements. This schedule was approved at the District's regularly scheduled Board meeting on September 25, 2020.

12/24/2020 6841716

AUTOMOTIVE

Cars for Sale

Hyundai Elantra 2003 109k Miles, Runs Great, \$700. Call: 847-297-2546

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Call: 866-399-0537



Notice of Sign on Illinois Route 83/Kingery Highway frontage (posted on December 30, 2020)



Notice of Sign on 75th Street frontage (posted on December 30, 2020)



Staff Report to the Plan Commission
21-01 7505 Kingery Highway
Panda Express Special Use Permit

February 3, 2021
Ann Choi, Planning Consultant



Attachment 6
Traffic Regulation Agreement (11 pages)

Return Recorded Document

to:

Brian Pabst
Village Administrator
Village of Willowbrook
835 Midway Drive
Willowbrook, IL 60527

TRAFFIC REGULATION AGREEMENT

THIS TRAFFIC REGULATION AND ENFORCEMENT AGREEMENT (the "Agreement") is made as of this _____ day of _____, 2019, by and between True North Energy, LLC, a Delaware limited liability company authorized to conduct business in the State of Illinois (the "Owner"), and the Village of Willowbrook, an Illinois home-rule municipal corporation (the "Village"), within which the "Complex" (as defined in Section 1.1 below) is located, for the regulation of traffic and other matters within the Complex and for the enforcement of said regulations by the assigned traffic law enforcement personnel of the Village.

WITNESSETH:

ARTICLE 1. DEFINITIONS: As used in this Agreement, the following definitions apply:

- 1.1 Complex: The land, buildings and other improvements commonly known as 7505 S. Kingery Highway, situated in the Village of Willowbrook, DuPage County, Illinois, and legally described in the attached Exhibit "A".
- 1.2 Permanent Index Numbers (PINs): 09-26-400-013.

- 1.3 Manager: Those persons or entity employed or retained by Owner from time to time with authority to administer, manage and operate the Complex for the purposes of this Agreement.
- 1.4 Owner: True North Energy, LLC.
- 1.5 Village: Village of Willowbrook, DuPage County, Illinois.

ARTICLE 2. RECITAL OF FACTS: The following recitals of fact are an integral part of this Agreement.

- 2.1 Owner holds record title to the Complex.
- 2.2 The Complex is located within the corporate jurisdiction of the Village.
- 2.3 It is the mutual desire of the parties hereto that the Village shall have the authority, but not the obligation, to regulate the parking of vehicles, traffic, roller skating, bicycle riding, skateboarding and/or other recreational activities within the Complex, and to enforce said regulations by the assigned traffic law enforcement personnel of the Village.
- 2.4 The Illinois Vehicle Code (625 ILCS 5/11-209), the Illinois Municipal Code (65 ILCS 5/1-1-7) and the Village's home rule authority provide authority for such agreement between the Village and the Owner and said statutory authorization enumerated on those matters which may be included in such agreement. Further, additional matters may be included in such agreements pursuant to Article VII, Section 6 and Section 10 of the Illinois Constitution.
- 2.5 Manager, in its capacity with Owner, is empowered to enter into this Agreement.

ARTICLE 3. COVENANTS: In consideration of the foregoing premises and of the covenants and conditions hereinafter contained, the adequacy and sufficiency of which the parties hereto hereby stipulate, each of the respective parties hereto covenants and agrees as follows:

- 3.1 The Owner will cause the Manager or its designated representative to cooperate with the Chief of Police of the Village or his designated representative in inspecting the parking area of the Complex to determine what, if any, stop signs, yield signs, person with disabilities parking area signs, or any other traffic markers are to be erected, and to determine what, if any, areas are to be marked as stop intersections, yield intersections, person with disabilities parking areas or pedestrian crossings, in order to provide for the safe and efficient trafficking of the parking area of the Complex. Further, such determination may include the regulation and/or prohibition of roller skating, the riding of bicycles, the riding or

operation of skateboards and/or other recreational activities in and upon the Complex, and the posting of the signs with the respect thereto.

- 3.2 If it be determined, pursuant to Section 3.1 of this Agreement, that stop signs, yield signs, person with disabilities parking area signs, or any other markers are to be erected or that specified intersections are to be marked as stop intersections, yield intersections, or pedestrian crossings, the Owner agrees to erect such signs and markers at the indicated places on the Complex, in accordance with all applicable regulations and specifications promulgated by the State of Illinois. The Owner shall bear the costs and expenses of obtaining, erecting and maintaining any and all such signs and markers necessary for enforcement of the regulations agreed to by the parties herein.
- 3.3 The Owner shall cause the Manager to mark such fire lanes as the local Fire Protection District Chief or his designated representatives shall recommend as necessary for effective movement of Fire Department and other emergency vehicles.
- 3.4 Signs or other devices providing for the regulation of traffic and parking, or the regulation or prohibition of roller skating, bicycle riding, skateboarding and/or other recreational activities, within the Complex, as well as a designation of the exact regulations to be imposed thereon, shall be installed as shown in the Traffic Regulation Plan for (Project Name) Willowbrook, IL Panda Express as prepared by (Consultant) Atwell, LLC, (Address) 1250 East Diehl Road, Suite 300, Naperville, IL 60563, referenced as Project No. 18003769.01, consisting of 1 sheet, dated November 16, 2020 and revised through N/A, _____ a copy of which is attached hereto as Exhibit "B" and by this reference incorporated herein.
- 3.5 The Village has the authority, but not the obligation, to enforce all regulations in the parking areas of the Complex by use of assigned traffic enforcement personnel of the Village; to issue citations to any and all violators of such regulations; and to adopt and enforce any additional reasonable rules and regulations with respect to traffic and parking in the parking area as local conditions may require or the safety and convenience of the public or the users of the parking area.
- 3.6 The Owner hereby agrees to provide for the removal of vehicles that are abandoned or parked in areas where stopping, standing or parking is prohibited. Such removal shall be done pursuant to a towing agreement and in accordance with the requirements of 625 ILCS 5/4-203, including, but not limited to, posting of the notice required therein.
- 3.7 Neither the Owner nor the Manager shall permit any person to park a vehicle at any location in the Complex for the purpose of displaying such vehicle for sale, lease or ride-share. The owner of any such vehicle may be ticketed by the Village.

- 3.8 The Owner has named the Village as an additional insured on a primary non-contributory basis on its Comprehensive General Liability Insurance Policy and on their Excess Liability Insurance Policy and a copy of said Certificate of Insurance is attached hereto as Exhibit "C" and incorporated herein. Such liability insurance shall provide that the Village, its officers, agents, agencies, employees, and departments shall be additional insured under such insurance. Said insurance shall be in the minimum amount of one million dollars (\$1,000,000) combined single limit or in such amounts acceptable to the Village and shall be in such form and with such Company as shall be approved by the corporate authorities.

The Owner hereby agrees to keep said policies in full force and effect throughout the terms of this Agreement. A mandatory written notice must be provided upon the cancellation of any policy as outlined under the Certificate of Insurance evidencing the coverage provided for herein.

- 3.9 In the event the Owner changes Managers, the Owner shall notify in writing the Village within ten (10) days of such change.

ARTICLE 4. TERM:

- 4.1 This Agreement shall be in full force and effect from and after the date of its execution for a period of twenty (20) years of the date thereof, and may, by further agreement of the parties, be continued for additional periods of like duration.

Notwithstanding any provision contained herein to the contrary, after this Agreement has been in effect for a term of one (1) year, this Agreement may be canceled upon the giving of thirty (30) days prior written notice by either party hereto, except to the extent that the Owner may be required to maintain this Agreement pursuant to any zoning relief granted by the Village.

- 4.2 The sole remedy available to the Owner, upon any breach of this Agreement by the Village, shall be the cancellation of the Agreement under its terms. It is of the essence of this Agreement that the Village shall not be liable in money damages for any breach of this Agreement.

ARTICLE 5. SUCCESSORS: This Agreement shall be binding upon and inure to benefit the respective assigns, successors and personal representatives of each of the parties hereto, and this Agreement shall run with title to the Complex and the obligations of the Owner shall be and are binding on future owners of any portion of the Complex.

ARTICLE 6. GOVERNING LAW: This Agreement shall be governed by the laws of the State of Illinois.

ARTICLE 7. EXECUTION AND RECORDING: This Agreement shall be executed by the parties' prior to the commencement of operations of the Complex. A fully executed copy of the Agreement shall be recorded in the Office of the Recorder of Deeds of the County of DuPage of the State of Illinois against title to the Complex, and it is agreed, pursuant to the statutes set forth above, that no regulation made pursuant to this Agreement shall be effective or enforceable until three (3) days after this Agreement is recorded.

ARTICLE 8. NOTICES: All notices hereunder shall be in writing and sent by Certified Mail, addressed to the Manager at EC Developments II, LLC, 1120 North Town Center Drive, Suite 150, Las Vegas, NV 89144 and, if to the Village, at the Office of the Village Administrator of Willowbrook, 7760 Quincy Street, Willowbrook, Illinois 60527.

IN WITNESS WHEREOF, the parties hereto have caused this Agreement to be duly executed, as of the date and year first above mentioned.

OWNER True North Energy, LLC
[Signature]

BY:

MARK E LYONS, President/CEO
(Managing Agent or Owner)

VILLAGE OF WILLOWBROOK

BY:

President

ATTEST:

Village Clerk

EXHIBIT "A"

LEGAL DESCRIPTION OF COMPLEX

THE NORTH 170.0 FEET OF THAT PART OF THE SOUTHEAST 1/4 OF SECTION 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING EAST OF THE EAST RIGHT OF WAY OF STATE ROUTE 83, MORE PARTICULARLY DESCRIBED AS FOLLOWS: BEGINNING AT A POINT 102.65 FEET EAST OF THE NORTHWEST CORNER OF SAID SOUTHEAST 1/4; THENCE EAST, ALONG THE NORTH LINE OF SAID SOUTHEAST 1/4, A DISTANCE OF 256.24 FEET; THENCE SOUTH, ALONG A LINE PARALLEL WITH THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170 FEET; THENCE WEST, ALONG A LINE PARALLEL WITH THE NORTH LINE OF SAID SOUTHEAST 1/4, A DISTANCE OF 256.24 FEET TO THE INTERSECTION OF SAID PARALLEL LINE WITH THE EAST RIGHT OF WAY OF SAID STATE ROUTE 83, WHICH POINT IS 102.45 FEET EAST OF THE WEST LINE OF SAID SOUTHEAST 1/4; THENCE NORTH, ALONG THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170.0 FEET TO THE POINT OF BEGINNING, IN DUPAGE COUNTY, ILLINOIS.

EXHIBIT "B"

TRAFFIC REGULATION PLAN

Pursuant to the attached agreement, the Willowbrook Police Department has the authority, but not the obligation, to enforce the following areas as designated on the traffic control plan attached hereto.

1. Traffic control signs including posted speed limit signs, stop signs, yield signs, and one-way signs.
2. No parking within 15 feet of hydrant locations as marked.
3. Handicap parking areas as marked with an official sign.
4. Prohibition of parking along all streets between 2:00 a.m. and 6:00 a.m.
5. Enforcement of yellow curb markings with adjacent "No Parking" signs indicating no parking areas.
6. Enforcement of posted, "No Trespassing" signs.
7. Posted "No Parking" zones.
8. Parking of vehicles for the purpose of being displayed for sale.
9. Enforcement of Village ordinance violations.
10. Prohibition of roller skating, bicycle riding, skateboarding and/or other recreational activities within the complex as posted.

Any future signs, crosswalks, and so forth may be agreed upon at a later date.

Tango North Energy, LLC

By: [Signature]

Owners' Representative

Chief of Police

[ATTACH PLAN]

EXHIBIT "C"

CERTIFICATE OF INSURANCE



Attachment 7
Village Review Letters (20 pages)



EST. 1960

Willowbrook

835 Midway Drive
Willowbrook, IL 60527-5549

Phone: (630) 323-8215 Fax: (630) 323-0787 www.willowbrookil.org

Mayor

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Village Clerk

Deborah Hahn

Village Trustees

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Paul Oggerino

Gregory Ruffolo

Village Administrator

Brian Pabst

Chief of Police

Robert Schaller

Director of Finance

Carrie Dittman



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January 13, 2021

Atten: Hakim Yala
Panda Express, Inc.
1683 Walnut Grove Avenue
Rosemead, CA 91770

Brian A. Styck
Atwell, LLC

Re: **7505 Kingery (Panda Express & Drive-Through) Planning Review #1**

Mr. Yala and Styck,

Planning staff has reviewed the submitted application, submitted on November 25, 2020, for the Panda Express Fast-Food Restaurant and Drive-Through development on the lot currently occupied by Shell Oil gas station on 7505 Kingery Highway in Willowbrook, IL 60527. The following plan has been reviewed per the requirements of the Village of Willowbrook Zoning Ordinance:

1. **"Legal Description"** – 1 page, undated and prepared by Atwell Group.
2. **"ALTA-NSPS Land Title Survey"** – 2 sheets (Job. No. 18003769), dated 8/10/2020 and bearing the latest revision date of 9/23/2020, and prepared by NORR LLC.
3. **"Site Layout Plan"** – 1 sheet, Sheet C04.0, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, and prepared by NORR Architects Engineers Planners.
4. **"Architectural Drawings"** – 5 sheets, Sheets A-101, A-200, A-201, A-202, A-300, Panda Project #: D7058, Arch. Project #: JCDT180453, bearing an issue date of 11-04-2020, and prepared by NORR Architects Engineers Planners.
5. **"Proposed Sign package"** – 16 pages, Project #: C59897, dated 12.19.2018, bearing the latest revision date of 11.02.2020, and prepared by Priority Sign.
6. **"Final Engineering Plans"** – 21 sheets, Panda Project #: D7058, Arch. Project #: JCDT180453, bearing an issue date of 11-20-2020, and prepared by NORR Architects Engineers Planners.
7. **"Engineer's Opinion of Probable Construction Cost"** – 3 pages, dated 11/20/2020, and prepared by Atwell Group.
8. **"Traffic Turn Analysis"** – 1 sheet, Sheet EX-01, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, and prepared by NORR Architects Engineers Planners.
9. **"Traffic Plan"** – 1 sheet, Sheet EX-02, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, and prepared by NORR Architects Engineers Planners.
10. **"Traffic Regulation Agreement"** – 9 pages, dated 11/20/2020, and prepared by Atwell Group.
11. **"Landscape Plan"** – 1 sheet, Sheet L01.0, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, and prepared by NORR Architects Engineers Planners.
12. **"Photometric Plan"** – 1 sheet, Sheet L01.0, dated 10/29/2020, and prepared by Villa Lighting.
13. **"Lighting Cut Sheets"** – 1 sheet, Sheet A-400, Panda Project #: D7058, Arch. Project #: JCDT180453, dated 10/29/2020, and prepared by NORR Architects Engineers Planners.



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14. "Construction Schedule" – 1 page, dated 11/17/2020, and prepared by Atwell Group.
15. "Standards for Special Use Permits" – 2 pages, prepared by Atwell Group.
16. "Standards for Variations" – 2 pages, prepared by Atwell Group.
17. "Variations Requested" – 1 page, dated 11/22/2020, and prepared by Atwell Group.
18. "Kane-DuPage Soil and Water Conservation District (SWCD) Land Use Opinion" – 1 page, dated December 14, 2020.
19. "Illinois Department of Conservation, Endangered Species Consultation" – 3 pages, dated 11/17/2020.

Staff offers the following comments:

General Comments

1. Revise Standards for Variations according to the new list of variations noted at the end of this review letter.

ALTA-NSPS Land Title Survey

2. The survey should be signed by a registered Illinois Professional Land Surveyor (missing signature).

Site Layout Plan

3. Include the distances from the center of the driveway on Route 83 to the center of the driveway on the adjacent property to the south.
4. Include the distances from the center of the driveway on 75th Street to the center of the driveway on the adjacent property to the east.
5. Label the width of all parking islands.
6. Identify and label the land uses adjacent to the subject property to the north, south, east, and west. Include the name of development and the zoning district.
7. Label the three shapes indicated in dashed and solid lines located north of the proposed building. (Are these menu boards?) Add dimensions from the north and west lot lines.

Traffic

Per your request, Gewalt Hamilton Associates, Inc. (GHA) has reviewed the following material provided for the above captioned project pertaining to traffic, parking, and on-site circulation:

- A. Site Traffic Study, prepared by Atwell dated January 5, 2021, which includes Traffic Plan, Sheet EX-02.
- B. Response to Comments Letter, prepared by Atwell dated January 5, 2021.
- C. Truck Turn Analysis, Sheet EX-01, prepared by Atwell dated November 20, 2020.

Conclusions of this effort and recommendations for consideration are presented below:

8. The TIS prepared by Atwell is generally in conformance with the guidelines published by the Institute of Transportation Engineers (ITE).
9. GHA has found the documentation and/or responses provided, in general, to adequately address the comments made and concerns raised in our previous Project Traffic Review dated September 30, 2020. An updated Site Traffic Study is not required.



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10. Employees should be encouraged to park in remote spaces (furthest from the site entrance) to provide convenient parking for customers.
11. The Site Traffic Study references ITE Land Use Code (LUC) 945, Gasoline/Service Station with Convenience Market using the building size to estimate traffic generated by the existing site use. Based on the existing site parameters (8 vehicle fueling positions, a convenience store less than 2,000 SF and ancillary vehicle service center), LUC 944, Gasoline/Service Station would be a more appropriate use. However, the trip generation estimates for the existing site use based on multiple independent variables (building size and number of vehicle fueling positions) would still result in a traffic estimate that is greater than the proposed Panda Express, fast-food restaurant with drive thru. Accordingly, no modifications to the study are required.
12. 75th Street to the west of Kingery Highway is under the jurisdiction of the DuPage County Division of Transportation.
13. Kingery Highway (IL Route 83) is also designated as a Strategic Regional Arterial (SRA) route. SRA routes are designed to carry higher traffic volumes at higher travel speeds through access control and increased access and traffic signal installation/spacing requirements.
14. A Certificate of Insurance will be required as an attachment to the Traffic Regulation Agreement.

Bulk Regulations & Landscaping

15. The Landscape Plan, Sheet L01.0, incorrectly labels the nine shrubs in the parking island immediately to the southeast of the buildings as "HR-9" when it should be labeled "RA-9". Revise to correct the typo.
16. Revise the "Ordinance Compliance Matrix" under the "Plant Calculation" table in the Landscape Plan to reflect that the Route 83 frontage is 170 feet and the 75th Street frontage is 256 feet.
17. Revise the "Section 9-6-1(D)" requirement 1. table in the Landscape Plan to reflect that 1,280 points is required for 75th Street and that 850 points is required for Route 83.
18. Differentiate between the evergreen and the deciduous shrubs under the Plant Schedule.
19. Section 9-6-1(D) of the Village Code provides that the front yard (Route 83) requires a minimum of 850 points. The Landscape Plan indicates 2 shade trees = 200 pts, 5 conifer trees = 300 pts, 6 ornamental trees = 300 pts, 19 evergreen shrubs = 285 pts and 76 deciduous shrubs = 760 pts, for a total of 1,845 pts. The total of 1845 pts. Exceeds the minimum requirement of 850 points.
20. Section 9-6-1(D) of the Village Code provides that the exterior side yard (75th Street) provides a minimum of 1,280 points. The Landscape Plan indicates 1 shade trees = 100 pts, 4 conifer trees = 240 pts, 6 ornamental trees = 300 pts, 16 evergreen shrubs = 240 pts, 24 deciduous shrubs = 240 pts, for a total of 1,120 points. The exterior side yard setback is deficient 160 points. **A variation from this requirement will be required unless you can provide more plantings.** A berm may be credited up to a maximum of fifty percent (50%) of the total landscape planting points required within a yard pursuant to Section 9-6-1(D)3.
21. The existing conditions plan (Sheet C02.0) calls out a 16-inch diameter underground gas pipe within a 7-foot easement along the north lot line. Typically, when a gas main is this large, Nicor Gas would send Watch and Protect when anyone is digging in close proximity to the main. Since it is such a large main, coordination with the gas company should be



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initiated during the design phase to understand how close any trees or landscape plantings with large root balls should be planted.

Photometric Plan

22. The photometric plans, consisting of two sheets and prepared by Villa Lighting and dated October 29, 2020 are in general compliance with Village Code and standard engineering methods. Please note that the Village reserves the right to require glare shields to be installed, should it deem appropriate.

Loading & Parking

23. Section 9-10-5(G) of the Village code requires that a landscaped earth berm of a minimum three-foot (3') height shall be required and appropriately landscaped with densely planted evergreen trees or shrubs to an effective minimum height of four feet (4') upon installation. **Staff requests that the Site Plan (Sheet C04.0) and Landscape Plan (Sheet L01.0) be revised to reflect this requirement.**
24. In addition, Section 9-10-5(G) requires that in all non-residential districts, parking shall be permitted in required front, side and/or rear yards, provided that the following screening and landscaping standards are met:
1. Front Yards and Exterior Side Yards: Permanent peripheral screening at least four feet (4') high shall be constructed and maintained in front yard and exterior side yard areas adjacent to parking areas. This screening shall consist of a planted earth berm, densely planted evergreen shrubs or trees or a combination of both. **Staff requests that the Site Plan (Sheet C04.0) and Landscape Plan (Sheet L01.0) be revised to reflect a berm within the front yard and exterior side yard to comply with this requirement and Section 9-6-1(D)3 (since the exterior side yard area was deficient by 160 points).**
 2. Interior Side and Rear Yards: Permanent peripheral screening at least five feet (5') high shall be constructed and maintained in interior side and rear yards adjacent to parking areas. This screening shall consist of a planted earth berm, densely planted evergreen shrubs or trees, or a combination of both. In interior side and rear yards, the screening shall be so located and constructed as not to interfere or conflict with the use of any utility easement or utility installations existing or planned to be installed in these easement areas. **The proposed Chadwick Yews in the rear yard only grow to a maximum height of 3' to 4'. Staff requests that these be replaced to comply with Section 9-10-5(G)2.**
 3. Interior Parking Lot Landscaping: Section 9-10-5(G)3 requires that each island shall be effectively landscaped with appropriately sized plantings at a minimum ratio of one shade tree and nine (9) dwarf variety shrubs per island and/or fifty (50) linear feet of parking area. Shade tree distribution shall provide a minimum of one tree in the area occupied by each twenty (20) parking spaces. **Add landscape plantings to the end island located to the northeast of the proposed building.**
25. Section 9-10-5(L)2(e) provides for spacing between separate driveway entrances requirements. **Staff requests to include these on the revised site plan to verify. A variation from this code section will be required.**



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26. Section 9-10-5(L)2(f) provides for spacing between driveway entrance and right-of-way of an adjacent intersecting street requirements. **Staff requests to include these on the revised site plan to verify or to indicate the functional area of the intersection. A variation from this code section may be required.**

Signage

27. Section 9-11-12(B)1(a) permits 1.25 square feet of sign surface area for each 1' of business site frontage up to a maximum of 350 square feet of sign surface area. In the case of a corner lot, "Business Site Frontage" is defined under Section 9-2-2 as the length of the exterior side lot line, measured in feet. The permitted sign surface area is 320 square feet.
28. Section 9-11-12(B)1(b)1 permits a total of four (4) wall signs.
29. Section 9-11-12(B)1(d)6 further reduces the sign surface area by seven and one-half (7 1/2) square feet per each foot of sign height for any freestanding or ground sign constructed in excess of eight feet (8') above average surrounding grade. The proposed ground sign S9 is twelve feet (12') tall and therefore in excess of 4 feet (4') above average surrounding grade. Therefore, the allowable sign surface area is reduced to 290 square feet.
30. Private traffic direction signs directing traffic movement onto a premises or within a premises, not exceeding four (4) square feet in area and four feet (4') in height for each sign shall be exempt under Section 9-11-6(L). Illumination of these signs shall be permitted in accordance with the regulations contained in this chapter. Sign D8 has two faces and the sum of these two faces is 4.33 square feet which exceeds the 4 square foot maximum. Please include a table that indicates compliance with the minimum area requirement for private directional signs.
31. Signs must contain brick or other similar material in keeping with the design of the principal structure under Section 9-11-12(B)1(d)1.

Fences

32. Section 9-12-4(D)2 requires that fences not greater than 3' in height and at least eighty percent (80%) open may be located anywhere on a lot. Based on Sheet C0.82, Staff calculates that the retaining wall located within the rear yard has a maximum height of 1.68' up to a height of 33". **Staff requests that the applicant confirm this dimension by providing a note to this effect on the Site Plan.**

Trash Enclosure

33. No comments.

Zoning Certificates and Occupancy Certificates

34. Section 9-14-2(D)2(a) requires that all areas of the development not proposed to be improved with structures, paved areas, walks, retention or detention areas, lighting or other approved landscaping shall be improved with Kentucky blue grass sod or seed. Sheet L01.1, Lawn Note No.1 indicates locally grown sod. **Please specify Kentucky blue grass sod or seed on the Landscape Plan (Sheet L01.0).**
35. Section 9-14-2(D)2(c)4 requires that a landscaped area having a minimum width of seven feet (7') shall be provided around all buildings except for building entrances and the front of retail business facilities to permit appropriate foundation plantings. Border plantings and



EST. 1960

Willowbrook

835 Midway Drive
Willowbrook, IL 60527-5549

Phone: (630) 323-8215 Fax: (630) 323-0787 www.willowbrookil.org

Mayor

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Village Clerk

Deborah Hahn

Village Trustees

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Umberto Davi

Michael Mistele

Gayle Neal

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Gregory Ruffolo

Village Administrator

Brian Pabst

Chief of Police

Robert Schaller

Director of Finance

Carrie Dittman



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foundation plantings shall be provided and shall comprise the remainder of the required landscape plantings, of which Please revise the sizes of the evergreen and deciduous shrubs in compliance with 9-14-2(D)2(c)4 which requires a minimum of one-half of the required landscape plantings shall be deciduous shrubs at a minimum height of three feet (3') and the remaining one-half (1/2) shall be evergreen shrubs at a minimum height of two feet (2').

36. Consider adding foundation planting along the north portion of the east façade to comply with Section 9-14-2(D)2(c)4. A variation for a reduced foundation planting width will be required along the west façade.

37. Add the dimension of a 4.5' wide foundation planting with on the Landscape Plan.

Engineering

38. Sheet C01.0: No comments.

39. Sheet C01.1:

- a. In Storm Sewer Note 4, add that the grates must have the "Dump No Waste - Drains to Creek" emblem.
- b. Watermain Note 11 should be revised to state that the contractor shall verify with the Public Services Department for specific make and model of fire hydrants, however there are no proposed hydrants on the plans.
- c. Add to the Watermain Notes "The contractor shall coordinate with the Public Services Department at least two working days prior to making the service tap to coordinate schedule, verify materials, and schedule required inspections."

40. Sheet C01.2: No comments.

41. Sheet C02.0: No comments.

42. Sheet C02.1

- a. Add to the note regarding the sanitary service note "as directed by Du Page County Public Works". Additional comments may be generated as more information is provided.

43. Sheet C03.0, L03.1: No comments.

44. Sheet C04.0, 4.1:

- a. The path along 75th Street is an asphalt path with concrete curb ramps at the driveways. Modify how the new pavement is shown at the proposed 75th Street driveway to only show the curb ramps to be concrete, with the remainder to be asphalt (up to first joint shown).
- b. We presume that the signs shown at the two handicapped accessible stalls are the required signs although not matching the legend.

45. Sheet C05.0, 5.1: No comments.

46. Sheet C06.0, 7.0, 7.1:

- a. The proposed water service should be as close as possible to a direct route from the main to the point of entry into the building.
- b. The Building Department will review the details of the water service, as it is considered plumbing under the Illinois Plumbing Code. Hydrocarbon resistant gaskets may be required since the service will cross a petroleum pipeline.
- c. Modify the note for the connection of the sanitary service to the existing manhole to refer to Du Page County requirements, not Village requirements.
- d. Show where the retaining wall underdrain will discharge.

47. Sheet C08.0 - 8.4:



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- a. For the handicapped sign detail, specify that the fine is \$350. The signage must be in compliance with the Illinois Accessibility Code standards.
48. Sheet L01.0, 1.1: Refer to Bulk Regulations & Landscaping, Loading & Parking and Zoning Certificates and Occupancy Certificates sections above.
49. General Comments:
 - a. An abbreviated Stormwater Submittal is required. We foresee this document being one to two pages with a project narrative and statements that the site does not contain floodplain or wetlands, and the development does not reach the net new impervious thresholds to trigger Best Management Practices or Detention.
 - b. A permit is required from IDOT prior to any work in the Route 83 right of way.
 - c. A permit is required from the Du Page County Public Works Department for the sanitary sewer connection.
 - d. The applicant must file a Notice of Intent with the IEPA prior to the start of construction.

Tri-State Fire Protection District

50. The Bureau of Fire Prevention has received a resubmitted copy of the proposed site, traffic, and auto turn plans for the above listed project. After review, the plans are found to be in apparent compliance with applicable standards relative to fire prevention and life safety.
51. Necessary inspections are to be performed along with any outstanding fees paid before occupancy is granted.
52. The Bureau of Fire Prevention has been asked to review the plans, specifications or other documents submitted to see if compliance has been made with the Fire Prevention Codes and Ordinances of the Tri-State Fire Protection District. Errors or omissions by representatives of the Bureau of Fire Prevention do not constitute permission to cancel, set aside or waive any provision of any applicable Code or Ordinance of the Tri-State Fire Prevention District. Approvals by the Bureau of Fire Prevention will be in writing only.

Illinois Department of Conservation. Endangered Species Consultation

53. The Illinois Natural Heritage Database contains no record of State-listed threatened or endangered species, Illinois Natural Area Inventory sites, dedicated Illinois Nature Preserves, or registered Land and Water Reserves in the vicinity of the project location.

Zoning Requests

Approval of a Special Use Permit for the construction of a one-story restaurant in the B-2 zoning district including a drive-through lane and associated site improvements. The approval of the special use permit will include, but is not limited to, the following variations:

1. A variation from Section 9-3-3(C) to decrease the minimum lot area for two (2) or more special uses from two acres (2 AC) to one acre (1 AC).
2. A variation from Section 9-3-7(A)1 to decrease the minimum special setback on Illinois Route 83 from one hundred feet (100') to fifty feet (50') for the proposed building.
3. A variation from Section 9-6-1(A) to decrease the minimum lot area for two (2) or more special uses (fast food establishment and drive-through) from two acres (2 AC) to one acre (1 AC).
4. A variation from Section 9-6-1(D)1 to decrease the required exterior side yard landscape planting requirements from 1,280 points to 1,120 points. **STAFF RECOMMENDS REVISING THE LANDSCAPE PLAN TO ELIMINATE THIS VARIATION**



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5. A variation from Section 9-6B-3(A) to decrease the minimum lot area for all uses from two acres (2 AC) to one acre (1 AC).
6. A variation from Section 9-6B-3(C)1 to decrease the minimum lot width of 200 feet for restaurants from two hundred feet (200') to one hundred seventy feet (170'). *(Existing Condition)*
7. A variation from Section 9-6B-3(E)1 to decrease the minimum front yard setback on Illinois Route 83 from sixty feet (60') to fifty feet (50').
8. A variation from Section 9-6B-3(E)3 to decrease the minimum exterior side yard setback on 75th Street from sixty feet (60') to forty-three point four feet (43.4') for the proposed building.
9. *A variation from Section 9-10-5(G) to eliminate the requirement of a 3' tall berm within the front yard adjacent to Illinois Route 83. (May not need if plan is revised)*
10. *A variation from Section 9-10-5(G)2 to eliminate the permanent peripheral screening at least five feet (5') high to be constructed and maintained in interior side and rear yards adjacent to parking areas. (May not need if plan is revised)*
11. A variation from Section 9-10-5(L)2(b)2)a to decrease the radius connecting street pavement edge and driveway edge on 75th street from 35' to 25'. *(Existing Condition)*
12. A variation from Section 9-10-5(L)2(d) to decrease the driveway location on Route 83 from seventy feet (70') to twenty-five point seven feet (25.7'). *(Existing Condition)*
13. A variation from Section 9-10-5(L)2(e) to decrease the minimum spacing between separate driveway entrances from four hundred feet (400') to approximately three hundred thirteen feet (+/- 313') on Illinois Route 83 and to approximately one hundred sixty-nine feet (+/-169') on 75th Street. *(Existing Condition)* **APPLICANT TO PROVIDE DIMENSIONS ON SITE PLAN**
14. A variation from Section 9-10-5(L)2(f) to decrease the minimum spacing between driveway entrance and right-of-way of an adjacent intersecting street from five hundred feet (500') to ninety-four point three feet (94.3') on Illinois Route 83 and to one hundred forty-six point two feet (146.2') on 75th Street. *(Existing Condition)*
15. A variation from Section 9-10-5(L)2(g) to eliminate the requirement of a concrete median separation and to reduce the driveway storage from one hundred feet (100') to zero feet on Illinois Route 83 and 75th Street.
16. A variation from Section 9-14-2(D)2(c)4 to decrease the minimum foundation landscape area from seven feet (7') to four point five feet (4.5') along the southern façade, and from seven feet (7') to zero feet along the western building façade and the northern portion of the eastern building façade.



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If you have any questions, you may contact me at planner@willowbrook.il.us or 630.920.2233.

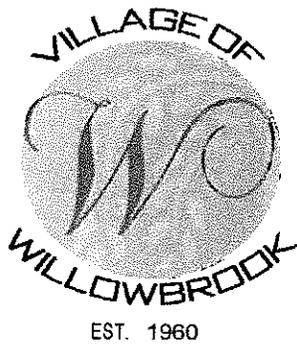
Sincerely,

Ann Choi
Village Planning Consultant
WBK Engineering, LLC

Cc: Brian Pabst
Michael S. Mertens
Dan Lynch
Roy Giuntoli



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January 22, 2021

Atten: Hakim Yala
Panda Express, Inc.
1683 Walnut Grove Avenue
Rosemead, CA 91770

Brian A. Styck
Atwell, LLC

Re: **7505 Kingery (Panda Express & Drive-Through) Planning Review #2**

Mr. Yala and Styck,

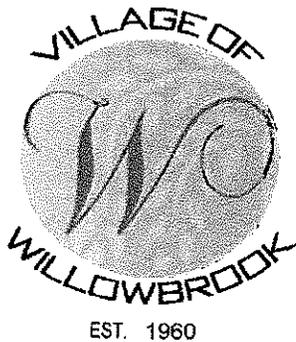
Planning staff has reviewed the revised application materials submitted on January 19, 2021, for the Panda Express Fast-Food Restaurant and Drive-Through development on the lot currently occupied by Shell Oil gas station on 7505 Kingery Highway in Willowbrook, IL 60527. The following plan has been reviewed per the requirements of the Village of Willowbrook Zoning Ordinance:

1. "ALTA-NSPS Land Title Survey" – 2 sheets (Job. No. 18003769), dated 8/10/2020, bearing the latest revision date of 9/23/2020, resubmitted on January 19, 2021 and prepared by NORR LLC.
2. "Site Layout Plan" – 1 sheet, Sheet C04.0, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, bearing the latest revision date of 01/19/2021 and prepared by NORR Architects Engineers Planners.
3. "Final Engineering Plans" – 18 sheets, Panda Project #: D7058, Atwell Project #: 18003769, bearing an issue date of 11/20/2020, bearing the latest revision date of 01/19/2021 and prepared by NORR Architects Engineers Planners.
4. "Stormwater Management Summary" – 3 pages, bearing an issue date of January 19, 2021, and prepared by Atwell, LLC.
5. "Traffic Regulation Agreement" – 11 pages, resubmitted by Atwell Group on 01/19/2021.
6. "Landscape Plan" – 2 sheets, Sheets L01.0 and L01.1, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, bearing the latest revision date of 01/19/2021 and prepared by NORR Architects Engineers Planners.
7. "Proposed Sign Package" – 17 pages, Project #: C59897, dated 12.19.2018, bearing the latest revision date of 01.19.2020, and prepared by Priority Sign.
8. "Standards for Variations" – 3 pages, submitted by Atwell Group on 01/21/2021.

Staff offers the following comments:

Site Layout Plan

1. Label the width of all parking islands. End parking islands closest to proposed building are missing dimensions.



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Traffic

GHA has reviewed the response letter and supporting documentation prepared by Atwell dated January 19, 2021 and found it to adequately address our comments, questions, and concerns. No further response is needed.

Loading & Parking

2. In addition, Section 9-10-5(G) requires that in all non-residential districts, parking shall be permitted in required front, side and/or rear yards, provided that the following screening and landscaping standards are met:
 1. Front Yards and Exterior Side Yards: Permanent peripheral screening at least four feet (4') high shall be constructed and maintained in front yard and exterior side yard areas adjacent to parking areas. This screening shall consist of a planted earth berm, densely planted evergreen shrubs or trees or a combination of both. **To comply with Section 9-10-5(G)1, add a row of Shamrock Inkberry shrubs on the east side of the 75th Street driveway within the exterior side yard.**
 2. Interior Parking Lot Landscaping: Section 9-10-5(G)3 requires that each island shall be effectively landscaped with appropriately sized plantings at a minimum ratio of one shade tree and nine (9) dwarf variety shrubs per island and/or fifty (50) linear feet of parking area. Shade tree distribution shall provide a minimum of one tree in the area occupied by each twenty (20) parking spaces. **Add one shade tree to each of the two (2) interior parking lot islands.**

Engineering

Our previous comments have been addressed, and we have no objection to the project moving forward subject to the following items:

- a. A permit is required from IDOT prior to any work in the Route 83 right of way.
- b. A permit is required from the Du Page County Public Works Department for the sanitary sewer connection.
- c. The applicant must file a Notice of intent with the IEPA prior to the start of construction.
- d. It is our understanding that the property owner will provide the Village with a letter of No Further Remediation (NFR) from the IEPA prior to a building permit being issued.
- e. Prior to building permit, the Village will require a Plat of Easement to grant access to the B-Box at the building. This can be a 10 foot wide non-exclusive easement over the water service from 75th Street to the building.

Zoning Requests

Approval of a Special Use Permit for the construction of a one-story restaurant in the B-2 zoning district including a drive-through lane and associated site improvements. The approval of the special use permit will include, but is not limited to, the following variations:

1. A variation from Section 9-3-3(C) to decrease the minimum lot area for two (2) or more special uses from two acres (2 AC) to one acre (1 AC).



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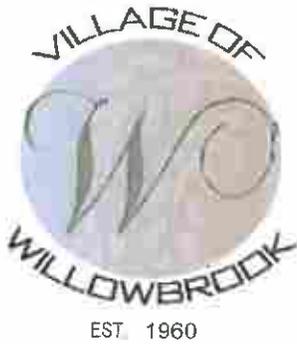
Director of Finance

Carrie Dittman

2. A variation from Section 9-3-7(A) to decrease the minimum special setback on Illinois Route 83 from one hundred feet (100') to fifty feet (50') for the proposed building, to fifty-six and six tenths feet (56.6') for the proposed menu board, and to sixty-two and four tenths feet (62.4') to the order canopy.
3. A variation from Section 9-6-1(A) to decrease the minimum lot area for two (2) or more special uses (fast food establishment and drive-through) from two acres (2 AC) to one acre (1 AC).
4. A variation from Section 9-6B-3(A) to decrease the minimum lot area for all uses from two acres (2 AC) to one acre (1 AC).
5. A variation from Section 9-6B-3(C)1 to decrease the minimum lot width of 200 feet for restaurants from two hundred feet (200') to one hundred seventy feet (170').
6. A variation from Section 9-6B-3(E)1 to decrease the minimum front yard setback on Illinois Route 83 from sixty feet (60') to fifty feet (50') for the proposed building and to fifty-six and six tenths feet (56.6') for the proposed menu board.
7. A variation from Section 9-6B-3(E)3 to decrease the minimum exterior side yard setback on 75th Street from sixty feet (60') to forty-three and four tenths feet (43.4') for the proposed building, to thirty-two and seven tenths feet (32.7') for the proposed menu board, and to twenty-five feet (25') to the order canopy.
8. A variation from Section 9-10-5(L)2(b)2a to decrease the radius connecting street pavement edge and driveway edge on 75th street from thirty-five (35') to twenty-five (25').
9. A variation from Section 9-10-5(L)2(d) to decrease the driveway location on Route 83 from seventy feet (70') to twenty-five and seven tenths feet (25.7').
10. A variation from Section 9-10-5(L)2(e) to decrease the minimum spacing between separate driveway entrances from four hundred feet (400') to approximately three hundred thirteen feet (+/- 313') on Illinois Route 83 and to approximately one hundred sixty-nine feet (+/- 169') on 75th Street.
11. A variation from Section 9-10-5(L)2(f) to decrease the minimum spacing between driveway entrance and right-of-way of an adjacent intersecting street from five hundred feet (500') to ninety-four and three tenths feet (94.3') on Illinois Route 83 and to one hundred forty-six and two tenths feet (146.2') on 75th Street.
12. A variation from Section 9-10-5(L)2(g) to eliminate the requirement of a concrete median separation and to reduce the driveway storage from one hundred feet (100') to zero feet on Illinois Route 83 and 75th Street.
13. A variation from Section 9-14-2(D)2(c)4 to decrease the minimum foundation landscape area from seven feet (7') to four and five tenths feet (4.5') along the southern façade, to five and two tenths feet (5.2') along the northern portion of the eastern building façade and from seven feet (7') to zero feet along the western building façade and remaining portion of the eastern building façade.



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If you have any questions, you may contact me at annchoi@willowbrookil.org or 630.920.2233.

Sincerely,

Ann Choi
Village Planning Consultant
WBK Engineering, LLC

Cc: Brian Pabst
Michael S. Mertens
Dan Lynch
Roy Giuntoli



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FOUNDED IN 1946



TRI-STATE
FIRE PROTECTION DISTRICT

419 PLAINFIELD ROAD • DARIEN, ILLINOIS 60561 • (630) 323-6445

December 8, 2020

Village of Willowbrook
Attn: Ann Choi
835 Midway Drive
Willowbrook, IL 60527
630-323-8215

planner@willowbrook.il-us

RE: Site Plan Review #2
7505 Kingery Highway
Willowbrook, IL 60527

To whom it may concern,

We have received a resubmitted copy of the proposed site, traffic, and auto turn plans for the above listed project. After review, we find the plans to be in apparent compliance with applicable standards relative to fire prevention and life safety.

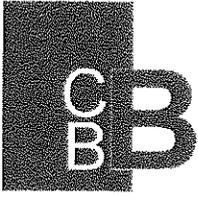
Necessary inspections are to be performed along with any outstanding fees paid before occupancy is granted.

The Bureau of Fire Prevention has been asked to review the plans, specifications or other documents submitted to see if compliance has been made with the Fire Prevention Codes and Ordinances of the Tri-State Fire Protection District. Errors or omissions by representatives of the Bureau of Fire Prevention do not constitute permission to cancel, set aside or waive any provision of any applicable Code or Ordinance of the Tri-State Fire Prevention District. Approvals by the Bureau of Fire Prevention will be in writing only.

Sincerely,

A handwritten signature in black ink, appearing to read "Chris Drews".

Chris Drews
Plan Review Specialist
Bureau of Fire Prevention
cdrews@tristatefd.com
630-654-6284



CHRISTOPHER B. BURKE ENGINEERING, LTD.

9575 W Higgins Road, Suite 600 Rosemont, Illinois 60018-4920 Tel (847) 823-0500 Fax (847) 823-0520

December 21, 2020

Village of Willowbrook
835 Midway Drive
Willowbrook, IL 60527

Attention: Ann Choi

Subject: 7505 Kingery – Final Engineering Plans
First Review
(CBBEL Project No. 900144.H212)

Dear Ann:

As requested on December 4, 2020, we have reviewed the Final Engineering Plans and supporting documents for the above property prepared by Atwell, and dated November 20, 2020. The following comments must be addressed before we can recommend approval:

Engineering Plans

Sheet C01.0

1. No Comments

Sheet C01.0

1. In Storm Sewer Note 4, add that the grates must have the "Dump No Waste – Drains to Creek" emblem.
2. Watermain Note 11 should be revised to state that the contractor shall verify with the Public Services Department for specific make and model of fire hydrants, however there are no proposed hydrants on the plans.
3. Add to the Watermain Notes "The contractor shall coordinate with the Public Services Department at least two working days prior to making the service tap to coordinate schedule, verify materials, and schedule required inspections."

Sheet C01.2

1. No Comments

Sheet C02.0

1. No Comments

Sheet C02.1

1. Add to the note regarding the sanitary service note "as directed by Du Page County Public Works".

Sheet C03.0, L03.1

1. No Comments

Sheet C04.0, 4.1

1. The path along 75th Street is an asphalt path with concrete curb ramps at the driveways. Modify how the new pavement is shown at the proposed 75th Street driveway to only show the curb ramps to be concrete, with the remainder to be asphalt (up to first joint shown).
2. We presume that the signs shown at the two handicapped accessible stalls are the required signs although not matching the legend.

Sheet C05.0, 5.1

1. No comments

Sheet C06.0, 7.0, 7.1

1. The proposed water service should be as close as possible to a direct route from the main to the point of entry into the building.
2. The Building Department will review the details of the water service, as it is considered plumbing under the Illinois Plumbing Code. Hydrocarbon resistant gaskets may be required since the service will cross a petroleum pipeline.
3. Modify the note for the connection of the sanitary service to the existing manhole to refer to Du Page County requirements, not Village requirements.
4. Show where the retaining wall underdrain will discharge.

Sheet C08.0 – 8.4

1. For the handicapped sign detail, specify that the fine is \$350. The signage must be in compliance with the Illinois Accessibility Code standards.

Sheet L01.0, 1.1

1. No Comments

Photometric Plans

The photometric plans, consisting of two sheets and prepared by Villa Lighting and dated October 29, 2020 are in general compliance with Village Code and standard engineering methods. Please note that the Village reserves the right to require glare shields to be installed, should it deem appropriate.

General Comments

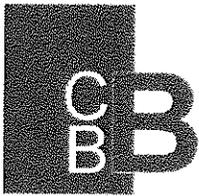
1. An abbreviated Stormwater Submittal is required. We foresee this document being one to two pages with a project narrative and statements that the site does not contain floodplain or wetlands, and the development does not reach the net new impervious thresholds to trigger Best Management Practices or Detention.
2. A permit is required from IDOT prior to any work in the Route 83 right of way.
3. A permit is required from the Du Page County Public Works Department for the sanitary sewer connection.
4. The applicant must file a Notice of Intent with the IEPA prior to the start of construction.

If there are any questions, please do not hesitate to contact me.

Sincerely,

A handwritten signature in cursive script, appearing to read "Daniel Lynch".

Daniel L. Lynch, PE, CFM
Head, Municipal Engineering Department



CHRISTOPHER B. BURKE ENGINEERING, LTD.

9575 W Higgins Road, Suite 600 Rosemont, Illinois 60018-4920 Tel (847) 823-0500 Fax (847) 823-0520

January 20, 2021

Village of Willowbrook
835 Midway Drive
Willowbrook, IL 60527

Attention: Ann Choi

Subject: 7505 Kingery – Final Engineering Plans
Second Review
(CBBEL Project No. 900144.H212)

Dear Ann:

As requested on January 19, 2021, we have reviewed the Final Engineering Plans and supporting documents for the above property prepared by Atwell, and dated January 19, 2021. Our previous comments have been addressed, and we have no objection to the project moving forward subject to the following items:

1. A permit is required from IDOT prior to any work in the Route 83 right of way.
2. A permit is required from the Du Page County Public Works Department for the sanitary sewer connection.
3. The applicant must file a Notice of Intent with the IEPA prior to the start of construction.
4. It is our understanding that the property owner will provide the Village with a letter of No Further Remediation (NFR) from the IEPA prior to a building permit being issued.
5. Prior to building permit, the Village will require a Plat of Easement to grant access to the B-Box at the building. This can be a 10 foot wide non-exclusive easement over the water service from 75th Street to the building.

If there are any questions, please do not hesitate to contact me.

Sincerely,

Daniel L. Lynch, PE, CFM
Head, Municipal Engineering Department

Project Traffic Review #2

To: Ms. Ann Choi
Planning Consultant, Village of Willowbrook

625 Forest Edge Drive, Vernon Hills, IL 60061
TEL 847.478.9700 ■ FAX 847.478.9701

From: Lynn M. Means, P.E., PTOE
Senior Transportation Engineer

www.gha-engineers.com

Date: January 13, 2021

Subject: Panda Express Restaurant with Drive-Thru
7505 Kingery Highway (IL Route 83)
Willowbrook, Illinois

Per your request, Gewalt Hamilton Associates, Inc. (GHA) has reviewed the following material provided for the above captioned project pertaining to traffic, parking and on-site circulation:

1. Site Traffic Study, prepared by Atwell dated January 5, 2021, which includes Traffic Plan, Sheet EX-02.
2. Response to Comments Letter, prepared by Atwell dated January 5, 2021.
3. Truck Turn Analysis, Sheet EX-01, prepared by Atwell dated November 20, 2020.

Conclusions of this effort and **recommendations** for consideration are presented below:

1. The TIS prepared by Atwell is generally in conformance with the guidelines published by the Institute of Transportation Engineers (ITE).
2. GHA has found the documentation and/or responses provided, in general, to adequately address the comments made and concerns raised in our previous Project Traffic Review dated September 30, 2020. An updated Site Traffic Study is not required.
3. Employees should be encouraged to park in remote spaces (furthest from the site entrance) to provide convenient parking for customers.
4. The Site Traffic Study references ITE Land Use Code (LUC) 945, Gasoline/Service Station with Convenience Market using the building size to estimate traffic generated by the existing site use. Based on the existing site parameters (8 vehicle fueling positions, a convenience store less than 2,000 SF and ancillary vehicle service center), LUC 944, Gasoline/Service Station would be a more appropriate use. However, the trip generation estimates for the existing site use based on multiple independent variables (building size and number of vehicle fueling positions) would still result in a traffic estimate that is greater than the proposed Panda Express, fast-food restaurant with drive thru. Accordingly, no modifications to the study are required.
5. 75th Street to the west of Kingery Highway is under the jurisdiction of the DuPage County Division of Transportation.
6. Kingery Highway (IL Route 83) is also designated as a Strategic Regional Arterial (SRA) route. SRA routes are designed to carry higher traffic volumes at higher travel speeds through access control and increased access and traffic signal installation/spacing requirements.

* * * * *

We hope you find these comments helpful in your assessment of the proposed development. Should you have any questions please do not hesitate to contact GHA at 847-478-9700.

5503.900_Panda Express_7505 Kingery_Traffic Review 2_01.13.21

Project Traffic Review #3

To: Ms. Ann Choi
Planning Consultant, Village of Willowbrook

From: Lynn M. Means, P.E., PTOE
Senior Transportation Engineer

Date: January 22, 2021

Subject: Panda Express Restaurant with Drive-Thru
7505 Kingery Highway (IL Route 83)
Willowbrook, Illinois



CONSULTING ENGINEERS

625 Forest Edge Drive, Vernon Hills, IL 60061

TEL 847.478.9700 ■ FAX 847.478.9701

www.gha-engineers.com

GHA has reviewed the response letter and supporting documentation prepared by Atwell dated January 19, 2021 and found it to adequately address our comments, questions and concerns. No further response is needed.

Should you have any questions please do not hesitate to contact GHA at 847-478-9700.

5503.900_Panda Express_7505 Kingery_Traffic Review 3_1.22.21



Village of Willowbrook
Staff Report to the Plan Commission

Plan Commission Date:	February 3, 2021															
Prepared By:	Ann Choi, Village Planning Consultant															
Case Title:	Zoning Hearing Case No. 21-03: "735 Plainfield Road PUD" – Planned Unit Development for a car wash, bank with drive-through, and fast food restaurant with drive-through.															
Applicant:	GW Property Group, LLC															
Property Owner:	Viren-Gill Ltd. (Louis Viren)															
Action Requested:	Consideration of a petition requesting approval of a special use permit for a planned unit development, including a financial institution with drive through, fast-food establishment with drive through, and an automobile washing and cleaning facility, including certain relief, exceptions and variations from Title 9 and Title 10 of the Village Code; approval of a Preliminary Plat of Subdivision; and approval of a Preliminary Plat of PUD.															
Applicable Regulations:	Comprehensive Plan, Zoning Ordinance, Subdivision Regulations															
Location:	735 Plainfield Road, Willowbrook IL 60527															
PINs:	09-23-406-003															
Existing Zoning:	B-2 Community Shopping															
Proposed Zoning:	B-2 Community Shopping with a Special Use for a PUD															
Existing Land Use:	Willowbrook Bowl (not currently operational)															
Property Size:	2.79 Acres															
Surrounding Land Use:	<table><thead><tr><th></th><th>Use</th><th>Zoning</th></tr></thead><tbody><tr><td>North</td><td>Chase Bank/Office Buildings</td><td>LOP</td></tr><tr><td>South</td><td>WB Town Center/Lock-Up Storage</td><td>B-2</td></tr><tr><td>East</td><td>TCF Bank & WB Orthodontics</td><td>OR</td></tr><tr><td>West</td><td>WB Town Center</td><td>B-2</td></tr></tbody></table>		Use	Zoning	North	Chase Bank/Office Buildings	LOP	South	WB Town Center/Lock-Up Storage	B-2	East	TCF Bank & WB Orthodontics	OR	West	WB Town Center	B-2
	Use	Zoning														
North	Chase Bank/Office Buildings	LOP														
South	WB Town Center/Lock-Up Storage	B-2														
East	TCF Bank & WB Orthodontics	OR														
West	WB Town Center	B-2														

Necessary Action by Plan Commission: Conduct public hearing. Continue hearing, if necessary, to continue discussion and vote to a future meeting to be determined as plans are completed, or complete and close hearing, and forward a **recommendation**, if ready. **Sample motions are provided on pages 25-27.**



Documents Attached:

Attachment 1: Legal Description

Attachment 2: Submitted Plans

- **"ALTA-NSPS Land Title Survey"** as prepared by Compass Surveying Group Ltd., Project No. 20-0006-01, Sheet 1 of 1, and dated 2/28/20.
- **"Preliminary Plat of PUD"** as prepared by Kimley-Horn and Associates, Inc., KHA Project No. 168726009, Sheet No. C1.0, bearing an original issue date of 12/15/2020, and bearing the latest revision date of 01/21/21.
- **"Car Wash First Floor Plan"** as prepared by Mark Shively Architecture, Sheet No. A2.1, and bearing a revision date of 01.14.2021.
- **"Car Wash Exterior Elevations"** as prepared by Mark Shively Architecture, Sheet No. A4.0, and bearing a revision date of 12.01.2020.
- **"Vacuum Canopy Plans, Elevations, Sections"** as prepared by Mark Shively Architecture, Sheet No. A1.1, and bearing a plot date of 01.25.2021.
- **"Vacuum Canopy Plan Diagram"** as prepared by Mark Shively Architecture, Sheet No. A1.2, and bearing a plot date of 01.25.2021.
- **"Pay Station & Trash Enclosure Elevations for Car Wash"** as prepared by Mark Shively Architecture, Sheet No. A1.2, and bearing a revision date of 01.14.2021.
- **"Preliminary Floor Plan" for Fast Food Establishment** as prepared by Interplan LLC, Project No. 2020.0919, 1 page, Sheet PFP, and dated 01.20.21.
- **"Architectural Elevations with Signs" for Fast Food Establishment** as prepared by Interplan LLC, 2 pages, and dated 01.20.21.
- **"Master Sign Key Plan" for Fast Food Establishment** – 1 page, undated and provided by GW Properties on January 24, 2021.
- **"Freestanding Sign" for Fast Food Establishment** – 1 page, Sheet ST1.1, bearing the latest revision date of 09.28.20, and provided by GW Properties on January 21, 2021.
- **"Exterior Perspective" for Fast Food Establishment** as prepared by Interplan LLC, 1 page, and dated 01.20.21.
- **"Floor Plan" for Chase Bank** as prepared by The Architects Partnership, TAP Project No. 17073, 1 page, and dated 01.22.2021.
- **"Architectural Elevations" for Chase Bank** as prepared by The Architects Partnership, TAP Project No. 19108, 2 pages, and dated 1.18.2021.
- **"Sign Package" for Chase Bank** as prepared by Signtech, Drawing No. 18-01613, Project No. CHASE_730_1, 41 pages, bearing an original issue date of 09/14/18, and bearing the latest revision date of 01.19.2021.
- **"Car Wash Rendering"** – 1 page, untitled, undated, and provided by GW Properties.



- **"PUD Plans"** as prepared by Kimley-Horn and Associates, Inc., KHA Project No. 168726009, 9 Sheets, bearing an original issue date of 12/15/2020, and bearing the latest revision date of 01/21/21. **(Includes Landscape Plan)**
- **"Photometric Plan"** as prepared by Kimley-Horn and Associates, Inc., KHA Project No. 168726009, Sheet No. E1.0, and bearing an original issue date of 12/15/2020.
- **"Traffic Control Plan"** as prepared by Kimley-Horn and Associates, Inc., KHA Project No. 168726009, 9 Sheets, Sheet No. 1 of 1, and bearing an original issue date of 12/15/2020.
- **"Traffic Impact Study"** as prepared by KLOA, 130 pages, and dated January 20, 2021.
- **"Traffic Regulation Agreement"** – 9 pages.
- **"Preliminary Plat of Subdivision"** as prepared by Compass Surveying Group Ltd., Project No. 20.0006, Sheet 1 of 1, and bearing the latest revision date of 01/19/21.
- **"Draft Declaration of Covenants"** – 20 pages (still undergoing review by Village Attorney).

Attachment 3: Standards for Special Use Permits

Attachment 4: Standards for Variations

Attachment 5: Standards for Planned Unit Developments and Findings of Fact for Planned Unit Developments

Attachment 6: Public Hearing Notice

Attachment 7: Village of Willowbrook Review Letter(s)



Development Proposal

Location & Zoning Request

The subject property is located on the south side of Plainfield Road, between Illinois Route 83 to the west and Adams Street to the east. The site contains about 2.79 acres and is currently improved with one vacant building, formerly occupied by the Willowbrook Bowling Alley. Adjacent developments include Pete's Fresh Market/Willows Shopping Center to the northwest, the existing Chase Bank to the north, Willowbrook Kindercare to the northeast, TCF Bank to the east, and the Willowbrook Town Center ("Town Center") to the west and south. The existing bowling alley building on the subject property has non-conforming setbacks as it is situated at the far southeast end of the lot, proximate to the south and east property lines.

Existing access to the subject property includes one driveway on Plainfield Road and one cross-access driveway along the western property line that connects to the Town Center. Nearly all of the lot is paved with a parking lot. Immediately east of subject property line is TCF Bank and currently, there is no direct access from the subject property to TCF Bank or to Adams Street, which has a higher elevation than the subject property.

GW Properties, LLC, (the "Applicant") proposes to demolish the existing building and is requesting approval of a special use for the proposed Planned Unit Development (PUD) that includes a car wash, restaurant with drive-through, and a bank. The PUD proposal also requests approval of certain relief, exceptions and variations from Title 9 and Title 10 of the Village Code; a Preliminary and Final Plat of Subdivision; and a Preliminary Plat of PUD. The Applicant intends to redevelop the subject property into a three-tenant mixed use development that will include a 5,850 square foot automobile washing and cleaning facility (Encore Car Wash) on the southern portion of the property, a 6,660 square foot financial institution (Chase Bank) with drive-through on the northwest portion of the property, and a 2,830 square foot fast food establishment with drive-through (Guzman Y Gomez) on the northeast portion of the property.

Exhibit 2: Location Map





Exhibit 3: Street View of Subject Property



Exhibit 4: Zoning Map (B-2 Community Shopping)



Summary of Requests:

1. Special Use Permit for a Planned Unit Development with an automobile washing and cleaning facility, fast food establishment with drive-through, and a financial institution with drive-through including certain relief, exceptions and waivers from Title 9 and Title 10 of the Village Code.
2. Subdivision of the subject property into three lots.

History/Background

The subject property was developed in 1963 by Louis Viren's Father, Lou Senior, as a bowling alley and restaurant. In 1973, ten years later, eight (8) additional bowling lanes were added towards the west. The property owner, Lou Viren, took over the ownership and operational management in 2003 and has run the business since. The Applicant, GW Properties, has been working with the property owner and the Village on potential redevelopment concepts for the property. The Plan Commission first saw concepts for the subject property in September 2018 for the reuse and repurposing of the existing bowling alley building (now vacant) for an entertainment venue consisting of boutique bowling, laser tag, video arcade and a restaurant bar. The previous concept also included a Chase Bank as an outlet.



Business Narrative

Car Wash

GW Property Group, LLC will be selling Lot 1, the 1.20-acre lot located on the southern end of the subject property, to Encore Car Wash who will be developing the property and operating the car wash. Encore Car Wash is marketed as a high-end, self-service automatic car wash. The owners of Encore Car Wash have experience primarily in the landscape architecture industry, predominantly for single-family homes throughout the country, but are currently developing two other car wash facilities in the Midwest (Plainfield, Illinois and Hammond, Indiana) since they first ventured into the car wash industry approximately two years ago.



Rendering of Encore Car Wash (left) and Photo of Guzman Y Gomez in Naperville, IL (right)

Restaurant with Drive-through

On the northeast corner of the subject property, GW Property Group will be developing a one-story, approximately 2,830 square-foot, free-standing restaurant with one drive-through lane on the 0.795-acre northeast lot (Lot 2). The fast casual restaurant operator is Guzman Y Gomez, an Australian-licensed, casual-dining restaurant chain that also operates internationally. The franchise specializes in Mexican cuisine dishes such as burritos, fires, nachos, tacos, and other specialty items. The franchise operates internationally in Singapore, Japan, and in January of 2020, opened their first location in the United States in Naperville, Illinois (1519 North Naper Boulevard). An aggressive expansion is currently taking place in the United States, especially in Illinois. Currently there are 135 locations globally.

The typical hours of operation at the Naperville location are Mondays – Thursdays from 6am to 10pm, and Fridays – Sundays from 6am to 11pm.

Financial Institution with Drive-Through

Chase Bank wishes to purchase Lot 3, the 0.792-acre parcel located at the northwest portion of the subject property. Chase Bank will be relocating from across the street into a brand new 6,660 square-foot property with three drive-through lanes. Chase Bank will be purchasing their portion of the property from GW Property Group, LLC and self-developing their new branch. The Chase Bank located across the street is currently open on Mondays – Fridays from 9am to 5pm, on Saturdays from 9am to 2pm, and are closed on Sundays.

Staff Analysis

Comprehensive Plan/Appropriateness of Use

Adjacent to one of the major commercial intersections in the Village, the Comprehensive Plan designates this property as "Community Commercial". The Comprehensive Plan includes several pages of development objectives and design guidelines that have been incorporated into the proposed project including the attention to landscaping, signage, pedestrian connection, and façade materials. Conformance with relevant



development and design policies and parameters are listed below. Community Commercial areas are intended to provide a full range of retail and service uses in the community with emphasis on community convenience related goods and services. The proposed development presents some challenges as the site will need to accommodate three high intensity uses on less than three acres. A traffic study has been provided and is discussed in more detail in the Traffic and Circulation section of this staff report. The subject property is zoned B-2 Community Shopping and is within the Route 83/Plainfield Road Business District. The proposed B-2 zoning complies with the Comprehensive Plan. The financial institution is a permitted use, the fast food establishment and automobile washing and cleaning facility are permitted as special uses in the B-2 Community Shopping District. There are also two drive-throughs proposed which are also allowed as special uses for permitted uses, in the B-2 Community Shopping District. Although the B-2 Community Shopping District is typically geared more towards retail uses and two of the three proposed uses will not generate retail sales tax revenue for the Village, the Village will benefit from a places of eating tax from the proposed fast food establishment with drive-through. The proposed uses will also occupy a key vacancy along Plainfield Road. The bank and the car wash may bring additional foot traffic and consumers to the adjacent shopping center and surrounding area.

Commercial Development Policies

The proposed redevelopment of the Willowbrook Bowling Alley into a planned unit development is consistent with the development policies set forth in the Comprehensive Plan. The existing site will be improved and upgraded with a bank, a fast food establishment and a car wash. Improvements to overall access, parking and landscaping are proposed through the relocation of the existing driveway long Plainfield Road, the proposed relocation of the shared access driveway between the subject property and the Town Center, and the incorporation of a dedicated left-turn lane into the subject property within the existing the Town Center drive. A future shared access drive is also proposed between the subject property and the lot to the east. These measures have been taken to reduce the negative impact on adjacent land uses. The proposed uses have also been sized and located to serve specific needs within the community and are clustered in small groupings with some shared parking areas, and an internal access drive serving all three proposed lots. The overall image and appearance of the now vacant lot will be upgraded and improved through new commercial area lighting, landscaping, signage, and new pedestrian connections.

Route 83/Plainfield Road Business District

The Route 83/Plainfield Road Business District Plan is generally bounded by 69th Street to the north, 72nd Court to the south, Illinois Route 83 to the west, and Adams Street and Willow Way Lane to the east. The Route 83/Plainfield Road Business District Plan was created in 2016 to serve as a funding mechanism for required off-site improvements, including necessary traffic improvements, for commercial properties located near Illinois Route 83 and Plainfield Road. These commercial properties were faced with challenges in finalizing their re-development project plans given the high cost of these off-site improvements. It was agreed that a public subsidy was warranted and resulted in the creation of a new Business District Sales Tax to fund these subsidies. The sales tax rate within the boundaries of the Route 83/Plainfield Road Business District was established at a 1% increase over those properties located beyond the boundaries of the Business District Plan. This 1% sales tax increment is collected from the state, forwarded to the Village, and deposited into a separate Village fund. Eligible project expenses are reimbursed to the developers from this fund to offset extraordinary development costs. The development may be subject to





the 1% Business District Tax of 1%. Village staff will defer to a Tax Impact Analysis, which was not included in the planning application, but one is required and has been included as a condition of approval. It should also be noted that the Applicant has not requested any assistance to offset their development costs.

Relief and Waivers Requested

PUDs may depart from the strict conformance with the required density, dimension, area, height, bulk and other regulations for the underlying zoning district and other provisions of the Zoning Ordinance to the extent specified in the preliminary plat and document authorizing the PUD so long as it will not be detrimental to or endanger the public health, safety, and general welfare, except that there are no exceptions for complying with seven (7) of the twelve (12) PUD Standards. The departures are identified as “waivers” and outlined in any approving ordinance. In order to maintain compliance with the Village Zoning Code, this PUD amendment and plat of subdivision require certain relief, exceptions, and waivers as outlined in the “Certain Relief Waivers Requested” in the last section of the staff report. The project as proposed includes nearly 60 waivers, which may be an indication that the site may not accommodate a combination of uses of this intensity.

Bulk Regulations

The subject property is zoned B-2; however, PUDs can allow for flexibility to the bulk requirements if authorized by the Village. A detailed discussion of important bulk exceptions and variations is provided below.

B-2 Zoning District Bulk Regulations						
Item	Code Section	Code Requirement	Proposed Lot 1 Car Wash	Proposed Lot 2 Restaurant	Proposed Lot 3 Bank	Departure
Min. Lot Area	9-6B-3(A)	Lot 1 – 1 acre Lot 2 – 2 acres Lot 3 – 2 acres	1.2 acres	0.795 acres	0.792 acres	Yes – Lots 2 and 3
Min. Lot Width	9-6B-3(C)2	Restaurants: 200' (Lot 2) Other uses: 150' (Lots 1 & 3)	289.86'	122.7'	161.3'	Yes – Lot 2
Min. Lot Depth	9-6B-3(D)	200'	180.3'	279.8'	212.7'	Yes – Lot 1
Min. Front Yard Setback (Plainfield Road)	9-6B-3(E)1	60'	N/A	116.6'	71.8'	Yes
Min. Interior Side Yard Setback	9-6B-3(E)2	30'	54.4' (west) 70.5' (east)	55.9' (west) 15.5' (east)	74.5' (west) 7' (east)	Yes – Lot 2 and 3
Min. Rear Yard Setback (south lot line)	9-6B-3(E)4	40'	58.9'	82.4'	62.7'	None
Max. Lot Coverage	9-6B-3(F)	50%	14.3%	9.2%	20.9%	None
Max. Height	9-6B-3(G)	30'	40'	28'	21'-6"	Yes – Lot 1
Max. Floor Area Ratio	9-6B-3(H)	0.30	0.112	0.082	0.193	None



- Height.** Maximum height in a B-2 district is thirty feet (30'). The proposed car wash building on Lot 1 has a maximum height of forty feet (40'), which is to the peak of the proposed water tower architectural feature. The proposed fast food establishment on Lot 2 has a maximum height of twenty-eight feet (28') and the proposed bank on Lot 3 has a maximum height of twenty-one feet six inches (21'-6").
- Yards.** Per Section 9-13-6(F) Standards for a PUD, Yards, building setbacks are required to be equal to the required setback of the adjoining zoning district, and not less than the height of the building (shown in the 4th column in the table below). The required and proposed minimum setbacks are shown in the table below. The number of building proposed on the overall site limits the placements of buildings on the subject property, so setback relief from the interior side yard requirements are being requested for Lots 2 and 3.

Yard	Description	Zoning Ordinance	Based on Height	Minimum Provided*			Departure
				Lot 1	Lot 2	Lot 3	
Front	Varies	60' front yard setback (and 100' specific setback)	Lot 1 is 40' Lot 2 is 28' Lot 3 is 21'-6"	85.4'	116.6'	73.9	None
Interior Side (east)	OR district to the east	20'		70.5'	15.5'	7'	Yes – Lots 2 & 3
Interior Side (west)	B-2 district to the west	30'		54.4'	59.9'	74.5'	None
Rear	B-2 district to the west	30'		58.9'	82.4'	62.7'	None

* Numbers represent minimums based on proposed design, not "build to" setbacks.

- Parking Area/Pavement Setbacks.** Parking area/pavement setbacks are regulated in the Off-Street Parking Section of the Zoning Ordinance under Section 9-10-5(G). The required and proposed minimum setbacks are shown in the table below.

Yard	Description	Zoning Ordinance	Minimum Provided*			Departure
			Lot 1	Lot 2	Lot 3	
Front	From Plainfield Road	25'	NA	24'	24'	Yes – Lots 2 & 3
Front	Between Lot 1 and Lots 2 & 3	15'	Zero feet	NA	NA	Yes – Lot 1
Interior Side	Varies	10'	4.4' (west) 5' (east)	Zero feet (west) 2.4' (east)	8.5' (west) Zero feet (east)	Yes
Rear	Varies	10'	5'	Zero feet	Zero feet	Yes

- Foundation Setbacks.** Section 9-14-2(D)2(c)4 of the Zoning Ordinance requires a minimum foundation landscape width of seven feet (7'). A few areas meet this requirement around the buildings, excluding areas along the north building facade of the car wash where a detached canopy is located close to the building's north wall and would therefore hinder the growth of any foundation plantings in that area; along the south and west building facades of the proposed fast food



establishment; and along the west, north and east building facades of the proposed bank. **Waivers from the code section are being requested as part of the PUD.**

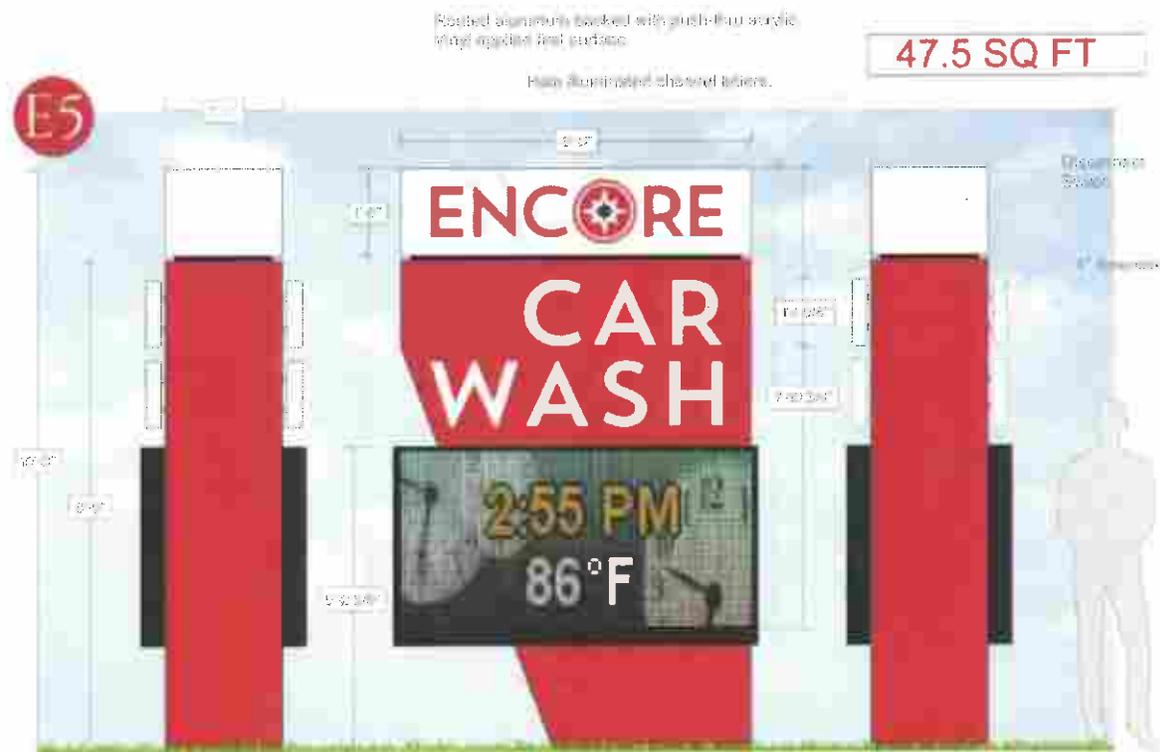
Signage

Signage variations are not allowed under the existing B-2 zoning district, and although relief is possible through a PUD, it is generally supported only to the extent needed to adequately promote the proposed business. On-going discussions about signage on this property have resulted in improvements, however, it is staff's opinion that additional modifications should be required before final consideration before the Village Board. The requested waivers to allow the prohibited roof signs, digital sign, painted signs, as well as the number of waivers from the total permitted sign surface area, location, and number of signs, should be carefully evaluated by the Plan Commission.

Freestanding Signs

Three freestanding signs are proposed for the overall development. Each lot will be required to accommodate a freestanding sign promoting only the business on that lot. The proposed freestanding signs that staff last reviewed are shown below and are included as **Attachment 2** of the staff report.

Proposed Freestanding Sign with Digital Display for Lot 1 (Encore Car Wash)



The freestanding sign on Lot 1 is located on Lot 3. Staff has recommended that this sign be relocated to Lot 1 and has included this as a condition of approval. All freestanding or ground signs are required to contain only the advertisement related to the identity of the business. The proposed freestanding sign has a maximum height of ten feet (10') and a maximum width of six feet (6'). The total sign surface area of the freestanding

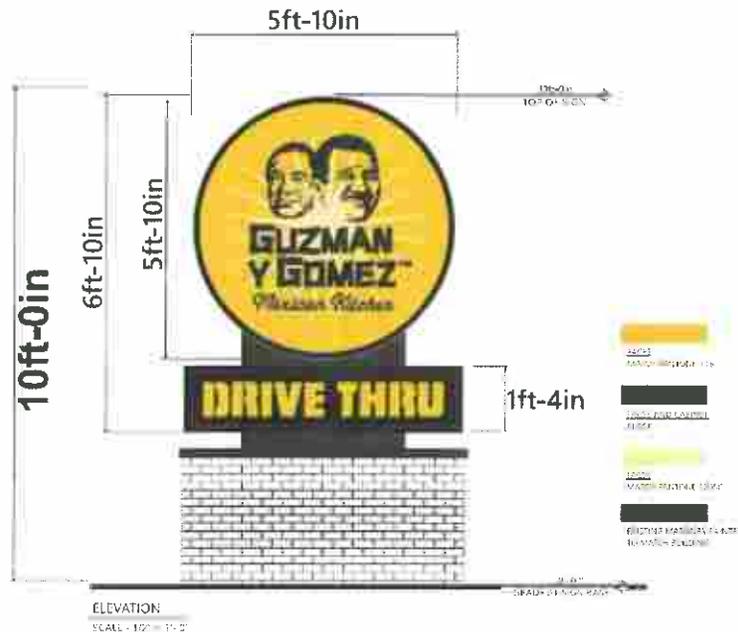


sign is ninety-five square feet (95 SF) including both faces. Because the proposed ground sign is in excess of eight feet (8') in height, the total sign surface area for Lot 1 is reduced by fifteen square feet (15 SF). Therefore, the total sign surface area for Lot 1 is a maximum of three hundred thirty-five square feet (335 SF).

The proposed freestanding sign incorporates digital technology, which is currently not allowed by the Sign Ordinance. The applicant would like to incorporate this technology as part of their PUD consideration, much like the shopping center approved for a digital sign on 63rd Street west of Route 83 and for Pete's Fresh Market. Similar to these approvals, language will be incorporated into the ordinance that will restrict brightness, how messages are displayed, and the length of time required between changing messages. These include restrictions that permit only static messages (no animation of any kind), and dwell time that will be consistent with whatever provision is in place at the time a recommendation is forwarded to the Village Board. It is recommended that all digital technology be treated the same unless and until a comprehensive amendment is made to the code to regulate them village wide, as opposed to regulating them as part of PUD approvals.

Please note that the proposed freestanding sign also does not contain brick in keeping with the design of the principal structure. A number of waivers from Section 9-11-4(R), Prohibited Signs, and Section 9-11-12(B)1(d)1, Sign Surface Area, Number, Sign Types and Height Limitations, are included in the list of waivers for the proposed ground sign on Lot 1.

Proposed Freestanding Sign for Lot 2 (Guzman Y Gomez)

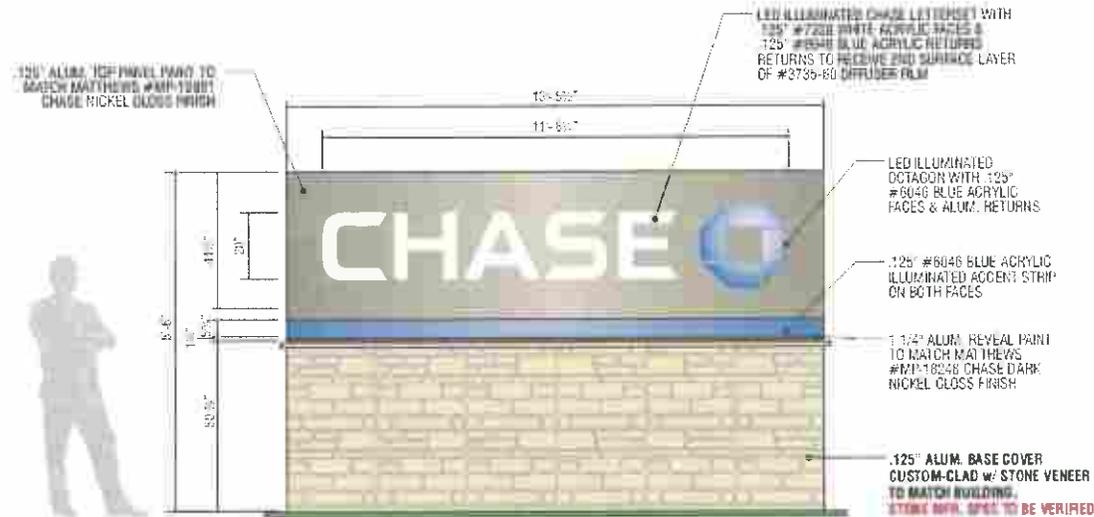


The freestanding ground sign for Lot 2 is located on Lot 3. For the same reason stated above, staff has recommended that this sign be relocated on its own lot, with a suggested location closer to the Plainfield Road access drive, and Staff has included this as a condition of approval. The proposed freestanding sign has a maximum height of ten feet (10') and a maximum width of five feet ten inches (5'-10"). The total sign surface area of the freestanding sign is sixty-eight square feet (68 SF) including both faces. Because the



proposed ground sign is in exceed of eight feet (8') in height, the total sign surface area for Lot 2 is reduced by fifteen square feet (15 SF). Therefore, the total sign surface area for Lot 2 is a maximum of one hundred thirty-eight and four tenths square feet (138.4 SF).

Proposed Freestanding Sign with Digital Display for Lot 3 (Chase Bank)



The freestanding ground sign for Lot 3 is located at the northeast corner of the lot. The proposed freestanding sign has a maximum height of eight feet six inches (8'-6") and a maximum width of thirteen feet five and a half inches (13'-5 1/2"). The total sign surface area of the freestanding sign is two hundred thirty-three and three tenths square feet (233.3 SF). Because the proposed ground sign is in excess of eight feet (8') in height, the total sign surface area for Lot 2 is reduced by three and seventy-five hundredths square feet (3.75 SF). Therefore, the total sign surface area for Lot 3 is a maximum of one hundred ninety-seven and eighty-five hundredths square feet (197.85 SF).

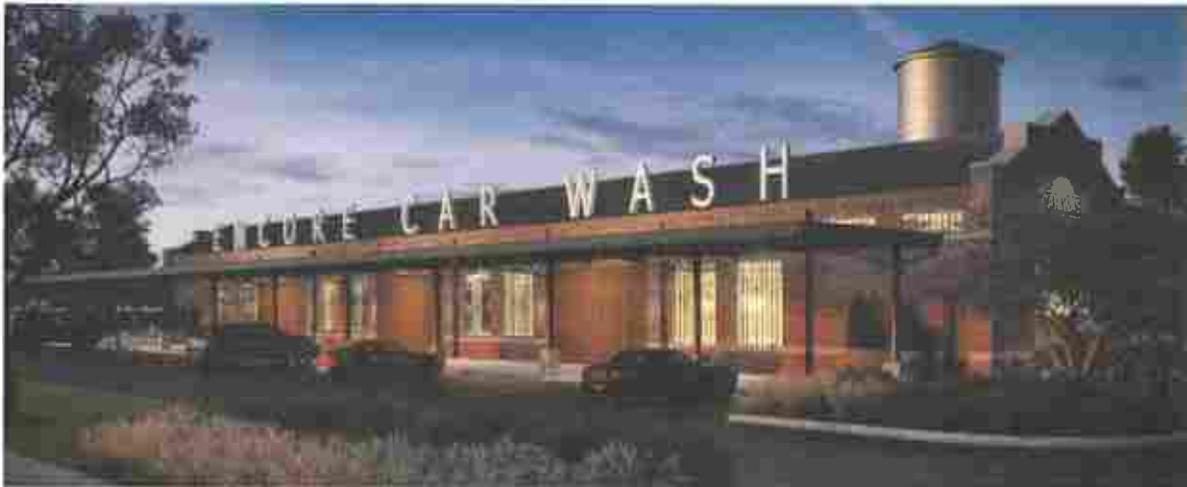
Please note that the latest sign package for Lot 2 submitted for this review, has proposed a multi-tenant sign located just north of the shared access drive with the Town Center (technically on Lot 3) and that has not been discussed with staff. Staff does not support this multi-tenant for several reasons. First, staff has not received any drawings to review the sign surface area, height, location, etc., and has not had sufficient time to evaluate what waivers would be required. Second, each lot already proposes the maximum number of ground signs permitted. Third, there is a concentration of signs located in that general vicinity: the proposed freestanding sign with digital display for the car wash and the existing freestanding sign for the Lock-Up self-storage facility are both located south of the shared access drive with the Town Center. Staff recommends eliminating the multi-tenant sign as proposed in the sign package for Lot 2 and has included this as a condition of approval.

Wall Signs

Wall signs are generally limited by the sum of all gross square foot measurements; meaning the height and width dimensions used are to the farthest edges of the sign, which includes "dead" space. The use of capital letters in signs or graphics (such as the Encore Car Wash roof signs) that extend beyond letters can limit overall signage based on the "gross square footage" calculation. As part of a PUD, however, signage can be



evaluated as to the quality of its design and whether it is effectively incorporated into the building's architecture. Staff has evaluated wall signage as can be seen in the elevation drawings and generally finds the appearance to be acceptable; however, a number of waivers are requested because the sign surface area of the proposed signs do not meet the code requirements. Staff has also found inconsistencies in the wall sign drawings and have noted these in the planning review letter that are included as **Attachment 7** of the staff report. Staff will require the provision of correct sign surface area information on revised plans, with future approvals referencing these revised numbers. A description of the requested signs and recommended modifications is provided in the table below.



Proposed Signs for Lot 1 (Encore Car Wash)				
Sign	Description	Location	Size	Comments
Lot 1 Wall Sign (1)	Sign E.1 Individual illuminated channel letters flush mounted to wall.	Along the lower pitch of roof on the north building façade.	[[[<ul style="list-style-type: none"> Higher than allowed by code. Waiver reasonable given location – rear of the site and not highly visible from Plainfield Rd. Sign surface area needs to be confirmed.
Lot 1 Painted Signs (2)	Signs E.7 Non-illuminated painted sign on brick.	On east and west building facades.	[[[<ul style="list-style-type: none"> Prohibited by code. Waiver is reasonable given location – rear of the site and not highly visible from Plainfield Rd. Sign surface area needs to be confirmed.
Lot 1 Roof Signs (3)	Signs E.2, E.3, E.4	On detached canopies over the vacuum stations and pay canopy.	E.2: [[[E.3: 20.6 SF E.4: 15.1 SF	<ul style="list-style-type: none"> Prohibited by code. Waiver is reasonable given location – rear of the site and not highly visible from Plainfield Rd.
Lot 1 Freestanding Sign (1)	Sign E.5	On Lot 3	95 SF	<ul style="list-style-type: none"> Needs to relocate to Lot 1 Reduces total SSA by 15 SF
Total Sign Surface Area Requested		[[[
Total Sign Surface Area Allowed		335 SF		
Excess Wall Signage		[[[



Waivers to permit an increase in the maximum total sign surface area for private traffic direction signs are also included for Lots 1 and 2.

Proposed Signs for Lot 2 (Guzman Y Gomez)				
Sign	Description	Location	Size	Comments
Lot 2 Wall Sign (3)	Signs A, D and F UL Listed channel letters.	On north, south, and west building facades.	A: 46.7 SF D: 46.7 SF F: 75 SF	<ul style="list-style-type: none"> Higher than allowed by code. Waiver reasonable given they are located on the taller "tower" elements. Min. separation distance lower than permitted by code.
Lot 2 Wall Signs (3)	Signs B, C and E Illuminated logo plex face/LED internal illumination	On north, east, and west building facades.	B: 49 SF C: 49 SF E: 49 SF	<ul style="list-style-type: none"> Higher than allowed by code. Waiver reasonable given they are located on the taller "tower" elements. Min. separation distance lower than permitted by code.
Lot 2 Freestanding Sign (1)	Sign G	Proposed on southwest corner of Lot 3	G: 68 SF	<ul style="list-style-type: none"> Needs to relocate to Lot 2 as it conflicts with Sign E.1 on Lot 3 Reduces total SSA by 15 SF
Total Sign Surface Area Requested		383.4 SF		
Total Sign Surface Area Allowed		335 SF		
Excess Wall Signage		48.4 SF		
Number of Wall Signs Requested		6		
Number of Wall Signs Allowed		3		

Proposed Signs for Lot 3 (Chase Bank)				
Sign	Description	Location	Size	Comments
Lot 3 Wall Sign (3)	Signs E.2, E.3 and E.4 24" White Channel Letters and Logo	On north, east, and west building facades.	E.2: 36.9 SF E.3: 36.9 SF E.4: 36.9 SF	<ul style="list-style-type: none"> Higher than allowed by code. Waiver reasonable given they are located on the taller "tower" elements. Min. separation distance lower than permitted by code.
Lot 3 Window Signs (2)	Signs I.30 and I.45 Universal Thin-Profile ATM Surround and Illuminated Interior Blue Octagon Ceiling- Hung	In vestibule of west building façade and in window of east building façade.	I.30: 8.34 SF I.45: 4 SF	<ul style="list-style-type: none"> Waiver for Sign I.30 reasonable given they are located within the building and serves as an ATM sign.
Lot 3 Freestanding Sign (1)	Sign E.1	Proposed on northwest corner of Lot 3	E.1 233.3 SF	<ul style="list-style-type: none"> Sign surface area needs to be confirmed. Reduces total SSA by 3.75 SF
Total Sign Surface Area Requested		344 SF		
Total Sign Surface Area Allowed		197.9 SF		
Excess Wall Signage		146.1 SF		
Number of Wall Signs Requested		3		
Number of Wall Signs Allowed		4		



Parking, Site Access & Circulation

The subject property is currently served by an existing eastbound right-in, right-out only access along Plainfield Road. The proposed development proposes constructing a new access driveway in the approximate location of the existing access driveway and to maintain this right-in/right-out access. This access has been shifted approximately twenty feet to the east to better align with the flow of traffic from the fast food establishment's drive-through.

A two-way internal driveway runs between each of the three proposed lots and provides the main circulation through the subject property. There is also a shared driveway between the subject property and the Town Center along the western portion of the subject property. The proposed site plan requires the relocation of this existing driveway between the two properties to be shifted slightly to the south more than twenty feet (20'). The proposed site plan also incorporates a dedicated left turn lane from the existing Town Center drive aisle for vehicles turning into the subject property from Plainfield Road. The proposed site plan as included in **Attachment 2** indicates sufficient parking with a total of one hundred five (105) parking spaces, including five (5) parallel employee parking spaces and six (6) accessible spaces. The parking requirement is fifty-four (54) parking spaces.

Bank

The bank is located on Lot 3 towards the northwest portion of the site. Three drive-through lanes are proposed along the south wall of the bank building and includes an additional bypass lane. The bank provides a total of thirty (30) parking spaces (including two handicap accessible spaces) and is accessible via the right-in/right-out driveway on Plainfield Road, where vehicles would travel in a clockwise rotation around the bank building. The bank is also accessible via the existing shared driveway between the Town Center and the subject property. The Traffic Impact Study (TIS) has indicated that approximately three to four vehicles will be accommodated within each of the drive-through lanes without blocking the access drives or internal circulation, and that this should be sufficient to accommodate the peak demand of the proposed bank. The TIS also included a recommendation that exiting movements from the drive-throughs should be under stop sign control, which has been included as a condition of approval.

Fast Food Establishment

The proposed restaurant and drive-through is located on Lot 2 and the drive-through facility for the proposed quick-serve restaurant will extend along the west side of the building. Vehicles will enter through the Plainfield Road driveway and make an immediate right turn and then an immediate left turn to either park the car in the restaurant's parking lot or drive through the parking lot to enter the single-lane drive-through. A separate by-pass lane is not proposed. A secondary path of travel is provided by the existing cross access driveway between the Town Center and the subject property, and via the internal drive aisle between all three uses. Vehicles will approach the menu/order board to place their order and then pick up items before exiting the drive-through. After exiting the drive-through, the vehicles will resume straight out to the Plainfield Road driveway. The restaurant provides a total of thirty (30) parking spaces including, two (2) handicap accessible spaces. Approximately eleven (11) vehicles can be accommodated within the drive-through lane without blocking the access drives or internal circulations. The TIS has determined that the stacking area will be sufficient to accommodate the peak demand of the proposed fast food establishment.

Car Wash

The single-lane automatic car wash tunnel will be an exterior-only car wash system and will have a clockwise rotation around Lot 3. The entrance to the car wash drive-through system is located at the southeast portion of Lot 1. Patrons to the car wash will enter from the Town Center driveway and vehicles will proceed east through the internal driveway that runs between each of the three proposed lots. Vehicles will then make two right turns (south) to enter the car wash drive aisle that leads patrons directly to the vacuum stations/parking lot that provide a total of thirty-seven (37) parking spaces, or vehicles can proceed forward



to three queue lanes that lead to the entrance of the car wash tunnel. At the entrance to the car wash tunnel, the driver will remain in the vehicle and the car wash will automatically pull the vehicle through the tunnel.

After exiting the tunnel, the vehicles will turn west and then either proceed straight to the vacuum positions and then exit the car wash by making a left-turn back to the Town Center driveway. If patrons of the car wash wish to use the bank or the restaurant, vehicles can make a right-turn out into the internal driveway.

There is stacking for approximately thirty (30) vehicles to queue before cars exit onto the internal drive aisle. Based on the experience with similar car washes, the TIS has determined that the proposed site plan provides for efficient circulation and adequate stacking for the proposed car wash. The Village Traffic Engineer, however, has indicated that the supporting car wash queue data provided is insufficient to verify the projected queue lengths since the TIS references a study that was performed in Minnesota in 2012, where 5 of the 6 carwash locations surveyed were at gasoline station facilities. The referenced study also did not include average daily traffic for a measure of comparison. The Village has therefore requested that the Applicant provide supporting operational characteristics (e.g., transaction information), as available, pertaining to peak operations and/or supporting operations / documentation from similar Chicagoland area existing sites during average, typical conditions. This has been noted in the Project Traffic Review #2 dated January 26, 2021. Please note that the Village Traffic Engineer does not believe the comments noted above will materially impact the findings / recommendations of the TIS.

Willowbrook Town Center with Internal Access Drive

The TIS has revealed that when a southbound exclusive left-turn lane is not provided at the access drive, the results of a simulation that was conducted indicate that vehicles are queueing back toward Plainfield Road, especially during the weekday evening and Saturday midday peak hours. Therefore, the TIS recommends the provision of an exclusive southbound left-turn lane to keep the southbound through lane clear and minimize the queueing back toward Plainfield Road. In addition, "Do Not Block Intersection" signage was recommended to prevent vehicles from blocking the access drive. The proposed site plan incorporates both of these recommendations.

Cross Access Agreements and Amendment to the Town Center PUD

The relocation of the existing driveway between the subject property and the Town center will require an amendment of the recorded plat for the Town Center PUD. The Applicant would be required to seek authorization from the Town Center to amend the Town Center PUD and the existing development agreement between the Town Center and the Village of Willowbrook as a condition of approval. Representatives of GW Properties indicated they had met with the Harlem Irving Group to discuss the amendment to the Town Center PUD. The response from Harlem Irving appears to have been positive and it appears as if full cooperation will be achieved.

The subject property also has an existing cross access agreement in place with the Town Center along the western portion of the lot. The proposed development will further require an additional cross access agreement since interior vehicular circulation is dependent upon shared access between all three lots. The proposed development also proposes an access drive between Lot 2 and the lot to the east of the property. The topography needs further study to see if this cross access is feasible and has included a condition of approval that this be further studied, and if feasible, to be incorporated as part of the Final PUD and Final Plat of Subdivision processes. Staff notes that Lot 3 will be landlocked due to the subdivision of the three lots and will not have frontage along a public street. Lot 3 will only be accessible via the existing drive aisle that is part of the Town Center. A waiver from the subdivision regulations that require lots to front on a public street will be required.



Pedestrian Access

Sidewalks along Plainfield are being added and/or reconstructed and additional sidewalks within the subject property are proposed to increase pedestrian connectivity between the proposed lots.

Parking Requirements

Parking for the proposed uses is regulated by the Zoning Ordinance as the sum of the individual uses planned. The following table summarizes the proposed uses and the associated parking requirements. Note that there appears to be a parking surplus of forty-nine (49) parking spaces and can be attributed to the number of parking spaces in Lot 1. Lot 1 also does not provide the minimum required number of stacking spaces and this is noted as a waiver as part of the PUD.

Item	Code Section	Code Requirement	Proposed			Departure
			Lot 1	Lot 2	Lot 3	
Required Spaces	9-10-5(K)	Car Wash: 1/1.5 employees = 2 spaces Stacking for 50 vehicles required Restaurant: 1/100 SF= 29 spaces Bank: 1/300 SF = 22 spaces	2 spaces Stacking for 30 vehicles	31 spaces (include 2 accessible spaces) Shared parking of 3 spaces from bank	30 spaces (include 2 accessible spaces)	Yes – Lot 1

Loading Requirements

No loading berths are proposed within the planned unit development. Since only businesses with a gross floor area of over 5,000 square feet are required to provide one loading berth, only the proposed car wash and bank are each required to provide one loading berth. However, since the automated car wash operations and the proposed bank do not anticipate any loading requirements to operate, Village staff considers the waivers from Section 9-10-4(H) to reduce the loading requirement to zero berths as reasonable requests.

Trash Enclosure

To comply with the current zoning ordinance, the Applicant has proposed two areas for trash enclosures on Lots 2 and 3. One trash enclosure would serve the car wash exclusively, and the other trash enclosure would be shared between the bank and the restaurant. Staff raised concerns that the location of the enclosure located on Lot 3 could be problematic as trash vehicles might block access to the restaurant’s drive-through or to the drive aisle leading up to the car wash. The Applicant explained that the trash bins would be on rollers and trash pickup would be scheduled off hours to avoid conflicts in circulation.

Stormwater Management/Engineering

Stormwater control on redevelopment sites is not required if the net new impervious area is less than 25,000 square feet. The threshold for Best Management Practices (BMPs) is 2,500 square feet of net new impervious surface. As the subject property is nearly one hundred percent impervious, and the proposed redevelopment would provide more pervious area, stormwater control is not anticipated.

Tri-State Fire Protection District Preliminary Comments

The Fire Protection District offered the following comments:

1. Drive width of 20’ clear aisles. This includes the drive adjacent to the car wash. The curb radius and drive space is less than 20’ and our fire apparatus will have a difficult time accessing the building.



2. Fire truck dimensions are not an inner city bus and shall be for a vehicle that is 47.68' long with a clear height of 13'6" and a width of 10'4".
3. The trash enclosures located in that area make it impossible for a fire vehicle to access the drive that is adjacent to the building. Prove that the turn radius is available.
4. The canopies for the vacuum stations may not be an issue if they don't extend into the drive area. This needs to maintain a 20' clear path in the drive with a 13'6" clear height.
5. The exit from the car wash is less than 20' and the fire vehicles will not be able to exit without hitting the curbs and causing damage.
6. All the other canopies will not be an issue. There needs to be a verification with the car wash.

Staff recommends that the comments provided by Tri-State Fire Protection District be addressed prior to the consideration by the Village Board.

Police Department Comments

Police Chief Robert Schaller reviewed the proposed site plan and identified three main areas of concern that could impact police/fire.

1. To the west of the development is the entrance/exit into the shared Town Center drive which is heavily congested at times and will lead to traffic backing up in all directions. Currently at the Chase bank location across the street, the teller lanes often are 5-6 vehicles deep during peak times. Similar queues at the proposed bank location may lead to vehicles stacking up impacting the west entrance/exit and creating an even greater traffic bottleneck.
2. To the east at the restaurant drive through, Chief Schaller agrees with the concerns raised at the November 4th planning commission concept review. If the drive-through restaurant is in any way close to the volume of traffic Chick-fil-A produces, this will no doubt impact the ability to access the car wash.
3. Lastly, the northeast entrance/exit onto Plainfield could present an issue. During peak rush hour times there are occurrences where westbound Plainfield traffic is backed up past the northeast entrance/exit. Regardless, if there is right turn only lane, it is inevitable that traffic will turn left from that lane.

Engineering Comments

As requested by email on January 21, 2021, the Village Engineer has reviewed the PUD Plans and supporting documents. Based on this discussion, this is intended to be the Preliminary PUD submittal and this review is for compliance with Section 9-13-5.B of the Village Code.

1. On the Preliminary Plat of Subdivision, all of the certificates required on the Final Plat will be removed.
2. The Preliminary Plat will provide clarification that Public Utility and Drainage Easements will be provided as required. It remains to be determined if that will be specific easements for each utility, or a blanket easement covering all of the property except for building envelopes.
3. Section 9-13-5.B.4 provides that a statement of ownership be included listing the existing ownership and certification that there are no delinquent taxes on the property. We presume that this has been provided separately or will be added to the Preliminary PUD.
4. Section 9-13-5.B.7 provides for covenants providing various information be submitted. We presume that this will be reviewed by others.
5. Section 9-13-5.B.12 provides for preliminary landscape plans. We will defer review of these drawings to the Village Planner.
6. Section 9-13-5.B.13 provides for preliminary engineering information to be shown. These comments are provided in the next section of this letter.



7. Sections 9-13-5.B.14 – 17 provides for various studies to be provided. We presume these are being reviewed by others.

Preliminary Engineering Plans

Cover Sheet – C0.0

8. Under Utility and Governing Agencies, change the Sanitary Sewer Service to Du Page County Public Works.

Utility Plan – C4.0

9. The alignment for the proposed watermain has been discussed with the design engineer and will be revised. The connection to the existing main in the town center will be made at the driveway connection between the two sites, rather than south near the proposed car wash building. Also, the proposed watermain will be an 8" main. This will result in less new public watermain and is expected to be a lower cost for the applicant. A 20 foot wide utility easement would be required over this main.
10. Each building service shall have an 8" x 6" Tee with valve. This will allow the public main to be constructed, and then each building service can be installed at the time of building construction without interrupting the public main.
11. The last segment of storm sewer west of the car wash, where the south end of the site connects to the off-site sewer, is shown to be 18". We presume this will be revised down to 12" on the final plans.
12. We presume the intent is for the sanitary sewer serving the bank and restaurant to be a public sewer. This will be owned by Du Page County Public Works and will be reviewed by that agency. We presume they will require a 20 foot wide easement over the main.

General Comments

13. A permit will be required from Du Page County Division of Transportation for work within the Plainfield Road right of way.
14. A permit will be required from Du Page County Public Works for the proposed sanitary sewer and connections.
15. The Construction Schedule and Engineer's Opinion of Cost will be reviewed with the Final PUD Plat.
16. The fire truck turning templates indicate that presumed truck can maneuver the site. We presume that prior to final approval the plans will be provided to the fire district for comment on both the geometrics, but also the location of hydrants and fire department connections on each building.

Traffic & Circulation Comments

The Village Traffic Engineer (Gewalt Hamilton Associates, Inc.) offers the following comments that must be addressed before final consideration before the Village Board:

1. GHA has found the documentation and/or responses provided, in general, to adequately address the comments.
2. The applicant should provide documentation of the DuDOT approval of TIS and Plainfield Road access, upon receipt.
3. The response letter adequately addressed the comment pertaining to the trip rates assumed in the traffic study for the proposed carwash component of the development during the weekday morning peak hour; however, the concern raised pertaining to the Saturday morning timeframe was not addressed. Please provide response and/or supporting documentation for use of the lower trip generation estimates based on the tunnel independent variable versus the square-footage of the use.



4. The updated study appropriately provides a queuing exhibit that illustrates the stacking capability of each of the proposed uses, as well as additional supporting documentation. However, the supporting carwash queue data provided is insufficient to verify the projected queue lengths. It references a study that was performed in Minnesota in 2012, where 5 of the 6 carwash locations surveyed were at gasoline station facilities, as well as the referenced study did not include average daily traffic for a measure of comparison. While we understand it may be difficult to obtain peak observations at similar facilities, as carwash operations tend to peak in the wintertime on a sunny day following a snowfall, please provide supporting operational characteristics (e.g., transaction information), as available, pertaining to peak operations and/or supporting operations / documentation from similar Chicagoland area existing sites during average, typical conditions.

In addition, the quick service restaurant use has been identified as Guzman Y Gomez. There is an existing drive-through location in Naperville. Again, please provide either data from observations and/or transaction data to support the capacity provided is adequate to support the peak queue.

The Village Traffic Engineer has indicated that the comments noted above will materially impact the findings and/or recommendations of the Traffic Impact Study.

Certain Relief/Waivers Requested

To maintain compliance with the Village Zoning Code, the proposed development will require certain relief, exceptions, and waivers. Staff recommends that the Applicant work with planning staff to minimize the number of waivers and variations, as feasible.

Zoning Ordinance

Pursuant to Section 9-13-6 of the Village Code, the following waivers from the provisions of the Zoning Ordinance include but are not limited to:

Overall Site

1. That Section 9-3-3(B), Division of Lots, be varied to permit the division of the SUBJECT REALTY into more than two (2) lots that do not conform with all the applicable bulk regulations of the B-2 zoning district.
2. That Section 9-3-3(C), Multiple Uses on a Lot, be varied to permit a reduction in the minimum lot area for two (2) or more special uses from five acres (5 AC) to two and 79/100ths acres (2.79 AC).
3. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required front yard parking area setback from twenty-five feet (25') to twenty-four feet (24') along Plainfield Road.
4. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required height of a landscaped earth berm along the Plainfield Road frontage from three feet (3') to two feet (2'). **STAFF RECOMMENDS REVISIONS TO THE LANDSCAPE PLAN TO ELIMINATE THIS WAIVER.**
5. That Section 9-10-5(L)2(d), Driveway Location On Lots For All Uses Other Than Single-Family Residential, be varied to permit a reduction in the minimum separation requirement between an access driveway entrance and an adjoining lot line from seventy feet (70') to fifty-five point four feet (55.4').
6. That Section 9-10-5(L)2(e), Spacing Between Separate Driveway Entrances On All Lots Other Than In Single-Family Attached Districts, be varied to permit a reduction in the minimum access driveway spacing for separate driveways from four hundred feet (400') to one hundred fifty-three and 8/10ths feet (153.8').
7. That Section 9-10-5(L)2(g), Access Driveways From Arterial Streets For All Uses Other Than Single-Family Residential, be varied to permit a reduction in the driveway storage area from one hundred feet (100') to thirty-eight feet (38').



LOT 1 (Automobile Washing and Cleaning Facility)

8. That Section 9-6-1(E)2, General Conditions, Building Façade Materials, be varied to permit metal siding and metal panels as building façade materials, as shown on the architectural elevations.
9. That Section 9-6B-3(D), Minimum Lot Depth, be varied to permit a reduction in the minimum required lot depth from two hundred feet (200') to one hundred eighty and three tenths feet (180.3').
10. That Section 9-6B-3(E)1, Required **Setbacks**, Front Yard, be varied to permit a reduction in the minimum required front yard setback from sixty feet (60') to thirteen and five tenths feet (13.5') for the detached canopy along the north side of the proposed car wash building.
11. That Section 9-6B-3(E)4, Required Setbacks, Rear Yard, be varied to permit a reduction in the minimum required rear yard setback from forty feet (40') to eight tenths feet (0.8') for the detached canopy along the south side of the proposed car wash building.
12. That Section 9-6B-3(G), Maximum Height, be varied to permit an increase in the maximum building height from thirty feet (30') to forty feet (40') to the top of the proposed water tank.
13. That Section 9-10-4(H) Off-Street Loading, Required Berths, be varied to permit a reduction of the minimum number of required loading berths from one (1) loading berth to zero (0) loading berths.
14. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required interior side and rear yard parking area setbacks from ten feet (10') to four and four tenths feet (4.4') along the west lot line, to five feet (5') along the east and south lot lines.
15. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required front yard parking area setback from fifteen feet (15') to zero feet along the north lot line.
16. That Section 9-10-5(K), Off-Street Parking, Required Spaces, be varied to permit a reduction in the minimum number of stacking spaces for automobile laundries from fifty (50) spaces to thirty (30) spaces for the automobile washing and cleaning facility.
17. That Section 9-10-5(L)1, Off-Street Parking, Minimum Parking Space And Aisle Dimensions, be varied to permit five (5) parallel parking spaces for employee parking along the southern building façade.
18. That Section 9-11-4(C), Prohibited Signs, Roof Signs, be varied to permit the roof signs, as shown in the elevations.
19. That Section 9-11-4(K), Prohibited Signs, Painted Signs, be varied to permit painted signs on the east and west building facades.
20. That Section 9-11-4(R), Prohibited Signs, Multiple Message, Digital, Dynamic and/or Video Display Signs, be varied to permit the digital displays on the ground sign located near the shared access driveway with the Willowbrook Town Center, as shown in the elevations.
21. That Section 9-11-12(B)1(a), Sign Surface Area, Number and Height Limitations, Business Districts, be varied to permit an increase in the maximum total sign surface area from three hundred thirty-five square feet (335 SF) to [???] for the one (1) freestanding sign proposed at the northwest corner of the lot, for each wall sign proposed on the north, east and west building facades for a total of three (3) wall signs, and for the three (3) roof signs proposed on each of the three detached canopies, as shown in the sign package. **APPLICANT TO PROVIDE SIGN SURFACE AREA**
22. That Section 9-11-11(B)2 "Sign Location", be varied to allow building mounted sign to extend higher than one foot (1') below the top line of the face of the building and exceed a height of twenty feet (20') for the signage on the north elevation of the automobile washing and cleaning facility.
23. That Section 9-12-2, Permitted Accessory Buildings, Structures and Uses, Awning/Canopies/Marquees, be varied to permit an increase in the permitted encroachment from 1/3 bulk district standard to allow the detached canopy located to the south of the building to be set back eight tenths feet (0.8') from the south lot line, and to allow the northernmost detached canopy located to the north of the building to be set back thirteen and five tenths feet (13.5') from the north lot line.
24. That Section 9-12-4(C)3, Bulk Regulations, Detached Accessory Structures, be varied to permit a reduction in the minimum clearance above grade from fourteen feet (14') to twelve feet (12') for the detached pay canopy located to the south of the building and the detached canopies over the vacuum stations.



25. That Section 9-14-2(D)2(c)4, Border Plantings and Foundation Plantings, be varied to permit a reduction in the minimum foundation landscape area width from seven feet (7') to zero feet along the north, east and west building facades.

LOT 2 (Fast Food Establishment and Drive-Through)

26. That Section 9-3-3(C), Multiple Uses on a Lot, be varied to permit a reduction in the minimum lot area for two (2) or more special uses from two acres (2 AC) to seven hundred ninety-five thousandths acre (0.795 AC).
27. That Section 9-6-1(A), Minimum Lot Area; Two Or More Uses On A Lot, be varied to permit reduction in the minimum lot area for two (2) or more special uses (fast food establishment and drive-through) from two acres (2 AC) to seven hundred ninety-five thousandths acre (0.795 AC).
28. That Section 9-6B-3(A), Minimum Lot Area, be varied to permit a reduction in the minimum lot area for the fast food establishment and drive-through from two acres (2 AC) to seven hundred ninety-five thousandths acre (0.795 AC).
29. That Section 9-6B-3(C)1, Minimum Lot Width, be varied to permit a reduction in the minimum lot width for restaurants from two hundred feet (200') to one hundred twenty-two and seven tenths feet (122.7').
30. That Section 9-6B-3(E)2, Minimum Interior Side Yard Setback, be varied to permit a reduction in the minimum interior side yard setback along the east lot line from thirty feet (30') to fifteen and five tenths feet (15.5') for the proposed fast food establishment, to eleven and four tenths feet (11.4') for the proposed attached canopies, to sixteen and one tenths feet (16.1') and to twenty-six and two tenths feet (26.2') for the menu/order boards closest to the east lot line.
31. That Section 9-6B-3(E)4, Required Setbacks, Rear Yard, be varied to permit a reduction in the minimum required rear yard setback from forty feet (40') to twenty-two and nine tenths feet (22.9') and twenty-three feet (23') for the menu/order boards located closest to the south lot line.
32. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required interior side and rear yard parking area setbacks from ten feet (10') to two and four tenths feet (2.4') along the east lot line and to zero feet along the west and south lot lines.
33. That Section 9-10-5(G)3 Off-Street Parking, Interior Parking Lot Landscaping, be varied to permit a reduction in the minimum end parking island width from nine feet (9') to five feet (5') and six and eight tenths feet (6.8') for two end parking islands.
34. That Section 9-10-5(G)3 Off-Street Parking, Interior Parking Lot Landscaping, be varied to eliminate the requirement for one (1) shade tree and nine (9) dwarf variety shrubs required within the three parking islands.
35. That Section 9-11-6(L), Signs, Exemptions, be varied to permit in increase in the permitted sign surface area for private traffic direction signs from four (4) square feet to eight and one tenths square feet (8.1 SF) for each of the three private traffic direction signs and fifteen and six tenths square feet (15.6 SF) for the private traffic direction sign located underneath the freestanding sign.
36. That Section 9-11-12(B)1, Sign Surface Area, Number and Height Limitations, Business Districts, be varied to permit an increase in the total sign surface area from one hundred thirty-eight and four tenths square feet (138.4 SF) to three hundred eighty-three and four tenths square feet (383.4 SF), to permit an increase in the maximum number of wall signs from three (3) wall signs to six (6) wall signs, to permit building mounted signage to exceed a height of twenty feet (20') for the wall signs on the north and west building elevations, and to permit a reduction in the minimum separation distance between wall signs on the north and west building facades to less than 20% of the linear dimension of the business site frontage, in accordance with the building elevations.
37. That Section 9-12-2, Permitted Accessory Buildings, Structures and Uses, Awning/Canopies/Marquees, be varied to permit an increase in the permitted encroachment from 1/3 bulk district standard to eleven and four tenths feet (11.4') from the east lot line for the two attached canopies located along the east building façade.



38. That Section 9-14-2(D)2(c)4, Border Plantings and Foundation Plantings, be varied to permit a reduction in the minimum foundation landscape area width from seven feet (7') to zero feet along the north, west and south building facades, and to less than seven feet (7') along the east building façade.

LOT 3 (Financial Institution and Drive-Through)

39. That Section 9-3-3(C), Multiple Uses on a Lot, be varied to permit a reduction in the minimum lot area for two (2) or more special uses from two acres (2 AC) to seven hundred ninety-two thousandths acre (0.792 AC).
40. That Section 9-3-7(A)4, Specific Setbacks, Plainfield Road, be varied to permit a reduction in the minimum special setback from for one hundred feet (100') to seventy-one and 8/10ths feet (71.8') for the proposed building.
41. That Section 9-6-1(A), Minimum Lot Area; Two Or More Uses On A Lot, be varied to permit reduction in the minimum lot area for two (2) or more special uses (financial institution and drive-through) from two acres (2 AC) to seven hundred ninety-two thousandths acre (0.792 AC).
42. That Section 9-6-1(E)2, General Conditions, Building Façade Materials, be varied to allow aluminum composite panels as building façade materials, as shown on the architectural elevations.
43. That Section 9-6B-3(A), Minimum Lot Area, be varied to permit a reduction in the minimum lot area for the financial institution and drive-through from two acres (2 AC) to seven hundred ninety-two thousandths acre (0.792 AC).
44. That Section 9-6B-3(E)2, Minimum Interior Side Yard Setback, be varied to permit a reduction in the minimum interior side yard setback along the east lot line from thirty feet (30') to seven feet (7') for the proposed building.
45. That Section 9-6B-3(E)4, Required Setbacks, Rear Yard, be varied to permit a reduction in the minimum required rear yard setback from forty feet (40') to thirty-six and two tenths feet (36.2') for the attached canopy along the south side of the building.
46. That Section 9-10-4(H) Off-Street Loading, Required Berths, be varied to permit a reduction of the minimum number of required loading berths from one (1) loading berth to zero (0) loading berths.
47. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required interior side and rear yard parking area setbacks from ten feet (10') to eight and five tenths feet (8.5') along the west lot line and to zero feet along the east and south lot lines.
48. That Section 9-11-6(L), Signs, Exemptions, be varied to permit in increase in the permitted sign surface area for private traffic direction signs from four (4) square feet to four and six tenths square feet (4.6 SF) for the private traffic direction sign located to the southwest of the building.
49. That Section 9-11-5(E)2, Illuminated Window Signs, be varied to permit an increase in the maximum sign surface area for an illuminated window sign for the Chase ATM located in the vestibule of the west building facade from four square feet (4 SF) to eight and thirty-four hundredths square feet (8.34 SF).
50. That Section 9-11-6(M), Exemptions, ATM Signs, be varied to permit an increase in the maximum sign surface area from four square feet (4 SF) to eight and thirty-four hundredths square feet (8.34 SF).
51. That Section 9-11-12(B)1(a), Sign Surface Area, Number and Height Limitations, Business Districts, be varied to permit an increase in the maximum total sign surface area from one hundred ninety-seven and eighty-five hundredths square feet (197.85 SF) to three hundred forty-four square feet (344 SF) for the one (1) freestanding sign proposed at the northwest corner of the lot and for each wall sign proposed on the north, east and west building facades for a total of three wall signs, as shown in the sign package.
52. That Section 9-11-12(B), Sign Surface Area, Number and Height Limitations, Business Districts, be varied to permit the signs for the financial institution in accordance with the building elevations.
53. That Section 9-12-2, Permitted Accessory Buildings, Structures and Uses, Awning/Canopies/Marquees, be varied to permit an increase in the permitted encroachment from 1/3 bulk district standard to allow the attached canopy located to the south of the building to be set back eleven and five tenths feet (11.5') from the east lot line.



54. That Section 9-14-2(D)2(c)4, Border Plantings and Foundation Plantings, be varied to permit a reduction in the minimum foundation landscape area width from seven feet (7') to zero feet along the north, east, west and south building facades.

PUD Standards

Pursuant to Section 9-13-6 of the Village Code, the following variations from the provisions of the Planned Unit Development Regulations include but are not limited to:

55. That Section 9-13-6(B) PUD Standards, Size and Ownership, be varied to waive the requirement that the subject realty be under single ownership and/or unified control.
56. That Section 9-13-6(F) PUD Standards, Yards, be varied to waive the requirement that the required yards or setbacks along the periphery of the planned unit development be at least equal in width or depth to that of the applicable required yard within the adjacent zoning district.
57. That Section 9-13-6(L) PUD Standards, Other Standards, be varied to waive the requirement that the planned unit development must comply with the minimum standards set forth in subsections (A), (B), (C), (D), (H), (I) and (K) of this section.

Subdivision Ordinance

Pursuant to Section 10-8-7 of the Village Code, the following variations from the provisions of the Subdivision Regulations be and the same are requested:

58. That Section 10-4-3(A)2, Lots, Sizes and Shapes, be varied to waive the requirement that the lot areas and lot widths conform to at least the minimum requirements of the zoning ordinance for the district in which the subdivision is proposed for Lots 2 and 3.
59. That Section 10-4-3(A)3, Lots, Sizes and Shapes, be varied to waive the requirement that building setback lines conform to at least the minimum requirements of the zoning ordinance for the district.
60. That Section 10-4-3(B), Lots, Arrangement, be varied to waive the requirement that Lot 1 front on a public street.

Applicable Findings

Section 9-14-5(B) of the Willowbrook Zoning Ordinance establishes seven (7) standards for a Special Use Permit that must be evaluated by the Plan Commission and Village Board. The applicant must meet all standards and draft responses to these standards if they wish to proceed with a petition for special use approval.

Additionally, the Plan Commission and Village Board shall not recommend or grant variations from the regulations of the Village's Zoning Ordinance unless affirmative findings of fact are made as to all of the standards set forth in Section 9-14-4(E) of the Willowbrook Zoning Ordinance. The applicant is also applying for a Planned Unit Development, and therefore the applicant must also draft responses to each of the Standards for Planned Unit Developments.

Summary

The Plan Commission last saw concepts for a Planned Unit Development (PUD) proposal at 735 Plainfield Road at the February 5, 2020 meeting and then again at the November 4, 2020 meeting. The previous proposal included a ninety (90) room hotel and a coffee shop with drive-through. Due to the economic downturn caused by the Covid-19 pandemic, the hotel operator has since withdrawn, and the Applicant has since replaced the hotel use with a car wash and has found a fast food/drive-through operator for the



previous coffee shop use. These two uses are anticipated to bring additional foot traffic to the area and will support surrounding commercial uses.

While there are some minor changes that will still be required prior to forwarding to the Village Board for formal consideration, the bigger issues needing feedback from the Plan Commission before a recommendation should be considered is a review of a Market Study and Tax Impact Study as well as finalizing signage. Other questions or concerns may come out of the public hearing process and can also be incorporated into any final direction to the applicant to be used in finalizing their plans for a formal vote at a future meeting date that can be determined by the Plan Commission at the meeting. If the Plan Commission chooses to continue the public hearing, any motion to continue the hearing should include the date and location of the continued meeting.

Staff is generally supportive of the proposed permitted and special uses and the proposed design layout on the site. For a number of reasons, the subject property has been slow to develop over the years. Efforts so far have focused on making this product work physically on this site, and there are modifications in that regard that are required. Additional efforts, however, are required to ensure the intended uses on and within the property are fully understood and to finalize details on signage or identity as discussed in this report. This is the last remaining commercial lot within the Village in a very visible and well-traveled part of the community, and additional efforts from the Village and the developer are required to solidify details to ensure the end product will be successful for the end use while remaining compatible with the commercial environment it will share with its neighbors. Staff therefore recommends that the Applicant continue to work with the Village to address the outstanding comments provided in the review letters (Tri-State, Police, Engineering, Traffic and Planning) and to provide the required studies, prior to forwarding a recommendation to the Village Board for consideration.

If the Plan Commission wishes to continue the public hearing, staff recommends approval of the following sample motion:

Sample Motion to Continue the Public Hearing

Based on the submitted petition, the testimony provided by the Applicant, and the staff report for PC 21-03 at the February 3, 2021 Plan Commission meeting, I move that the Plan Commission continue the public hearing to March 3, 2021 (or to another date to be determined) to allow Village Staff and the Applicant time to address the various issues raised by the Plan Commission and Village staff.

If the Plan Commission wishes to support the project, staff recommends approval of the following sample motion:

Sample Motion to Recommend Approval

Based on the submitted petition, the testimony provided by the applicant, and the staff report prepared for PC 21-03 at the February 3, 2021 Plan Commission meeting, I move that the Plan Commission recommend and forward to the Village Board the Findings of Fact presented and discussed by the Plan Commission at the February 3, 2021 meeting, and further recommend that the Village Board approve the following:

1. A special use for a planned unit development associated with PC 21-03, including the "proposed waivers" outlined in the staff report.
2. Special uses for one 5,582 square foot automobile washing and cleaning facility, one 2,830 square foot fast food establishment with drive-through, and one 6,660 square foot financial institution with drive-through.



3. Approval of the Preliminary Plat of Subdivision and Preliminary Plat of PUD for "735 Plainfield Road Subdivision", except for revisions required by the Village Engineer to be revised prior to forwarding to the Village Board for consideration.

Subject to the following conditions:

Recommended Conditions of Approval

1. All plans and documents shall be revised and resubmitted as required by Village staff and the Plan Commission as indicated in the staff report or as discussed during the February 3, 2021 meeting, and approved by staff prior to being forwarded to the Village Board for final consideration.
2. Approval of Preliminary Plat of PUD is expressly conditioned on the approval of an amendment of the Town Center PUD with respect to changes in the ingress, egress and incorporation of a dedicated left-turn lane as shown on 735 Plainfield Road Plat of PUD. An exclusive southbound left-turn lane shall be provided at the proposed full movement access drive in order to keep the southbound through lane clear and minimize the queueing back toward Plainfield Road.
3. That as part of the approval of a Preliminary Plat of Subdivision and Preliminary Plat of PUD, the applicant shall submit a Market Study and Tax Impact Study or related studies to the satisfaction of the Village, prior to Village Board consideration.
4. That as part of the approval of a Preliminary Plat of Subdivision and Preliminary Plat of PUD, the applicant shall submit a Cross Access Agreement and indicate the required easements on their preliminary plats.
5. That as part of the approval of a Preliminary Plat of Subdivision and Preliminary Plat of PUD, a parking agreement shall be executed to allow three of the parking spaces located on Lot 3 to satisfy the parking requirement on Lot 2.
6. That as part of the Final PUD and Final Plat of Subdivision processes, the Applicant shall investigate the feasibility of proposing an Ingress and Egress Easement on the northeast end of the subject property to allow vehicular access between the subject realty and the TCF Bank property.
7. Prior to approval of a Final Plat of Subdivision and Final Plat of PUD, the applicant shall submit all required executed Traffic Regulation, Improvement and Redevelopment Agreements in a form acceptable to the Village Board, approved by Village staff and subject to review by the Village attorney.
8. That as part of the Final PUD and Final Plat of Subdivision processes, the applicant shall submit the Declaration of Covenants, which is subject to the approval by the Village.
9. The Applicant shall provide documentation of the DuDOT approval of Traffic Impact Study and Plainfield Road access, upon receipt.
10. The completion of all County of DuPage and Village traffic improvements shall be made prior to the issuance of the first permanent occupancy permit for the subject realty.
11. Outdoor dining and restaurant seating shall not be allowed on the subject realty except where identified as "Outdoor Dining" on the fast food establishment architectural plans.
12. Off-site improvements shall include a sidewalk to be constructed in accordance with the PUD plans. The sidewalk shall be installed along Plainfield Road and is subject to the County of DuPage permitting, inspection and approval.
13. That the digital signs on the SUBJECT REALTY shall be at all times subject to the following requirements:
 - a. Operational Limitations: Display shall contain static messages only, and shall not have movement of any kind, or the appearance or optical illusion of movement, of any part of the sign.
 - b. Minimum Display Time: Each message on the sign must be displayed for a minimum of 8 seconds or such longer duration as is hereafter enacted in the Village Sign Ordinance for comparable signs.



- c. Message Change Sequence: The change between static messages must be accomplished immediately, with no use of any transitions.
 - d. Illumination: The sign must include light sensors and dimmer controls that automatically adjust to outdoor lighting levels so that illumination levels are dimmer at night and on cloudy days than during sunny days; but in no instance shall illumination and lighting not be in compliance with Section 9-11-13 of the Willowbrook Zoning Ordinance.
 - e. Only one freestanding or ground sign shall be constructed or erected on Lot 1.
 - f. The sign shall not contain any other advertising other than the identity of the car wash, the address; and the promotion of related business products for Lot 1.
 - g. A separate sign permit shall be obtained pursuant to Village Code.
14. All freestanding or ground signs shall not contain any advertisement other than the identity of the business located therein; therefore, the freestanding sign proposed for each business shall be located on its own lot.
 15. The multi-tenant sign located north of the shared access drive with the Town Center shall be removed from all plans prior to consideration before the Village Board.
 16. No deliveries or other loading and unloading activities shall be allowed on the subject realty between the hours of 7:00 pm to 10:00 am.
 17. No trucks shall be permitted to sit idling on the subject realty.
 18. No outside loudspeakers shall be permitted other than businesses with approved drive-through windows and then only for the operation of the drive-through service.
 19. Outside refuse compactors shall only be operated between the hours of 9:00 a.m. and 6:00 p.m. and shall be screened by a masonry wall large enough to visually large enough to visually screen the compactor dumpster and buffer any noise created by the compactor unit.
 20. The earthen berms located along the Plainfield Road frontage shall be constructed prior to the issuance of the first temporary or permanent occupancy permit for the subject realty, or such earlier time as is reasonably practical. All exiting movements from the drive-throughs shall be under stop sign control.
 21. Exiting movements from the bank drive-through shall be under stop sign control.
 22. "Do Not Enter" signs shall be placed at the exit of each drive-through lane to deter opposing traffic from entering the drive-throughs from the one-way exit direction.
 23. A permit will shall be required from Du Page County Division of Transportation for work within the Plainfield Road right of way.
 24. A permit shall be required from Du Page County Public Works for the proposed sanitary sewer and connections.
 25. Prior to final approval, the plans will be provided to the fire district for comment on both the geometrics and the location of hydrants and fire department connections on each building.

Next Steps...

The Plan Commission and the Applicant were advised that the next steps include the Applicant's revision to the plans and submission of the required studies to evaluate the market and tax impacts, after which staff will complete reviews and inform the Applicant of additional modifications (if any) before the item is schedule for consideration by the Village Board for a formal vote.



Attachment 1
Legal Description

LOT 1 IN WILLOWBROOK CENTER UNIT NO. 1, OF PART OF LOT 7 IN OWNER'S SUBDIVISION IN SECTIONS 23 AND 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED OCTOBER 16, 1963, AS DOCUMENT R63-37895, IN DUPAGE COUNTY, ILLINOIS.

PIN: 09-23-406-003

ADDRESS: 735 Plainfield Road, Willowbrook IL 60527.



Attachment 2
Submitted Plans

ALTA/NSPS LAND TITLE AND TOPOGRAPHIC SURVEY

GRAPHIC SCALE



SITE LOCATION

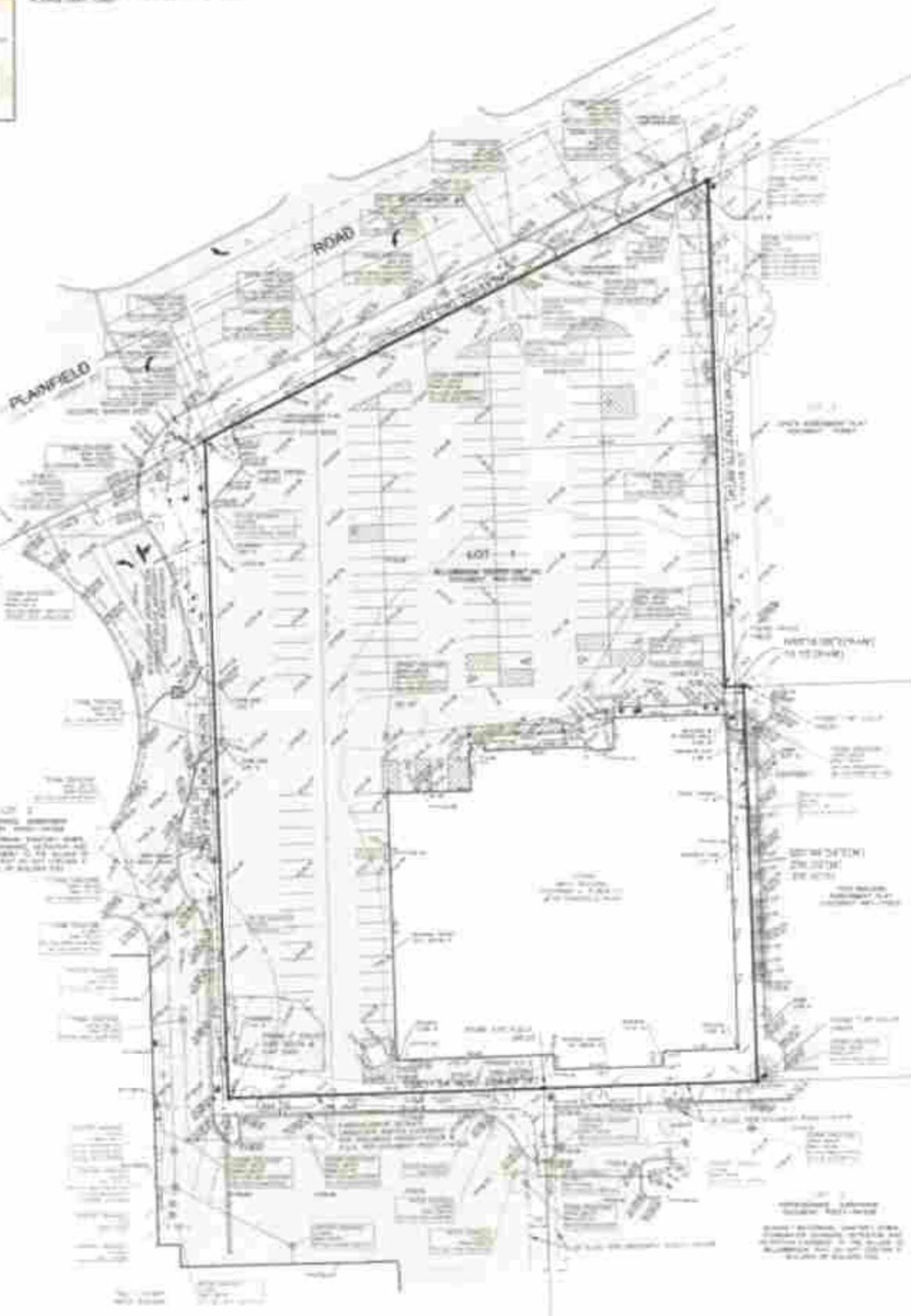
LEGAL DESCRIPTIONS

LEGAL DESCRIPTIONS

PROPERTY MAP

SUBJECT DESCRIPTIONS

SUBJECT DESCRIPTIONS



NOTES

LEGEND

- Survey Station
- Survey Point
- Survey Marker
- Survey Boundary
- Survey Easement
- Survey Right-of-Way
- Survey Encroachment
- Survey Obstruction
- Survey Boundary
- Survey Easement
- Survey Right-of-Way
- Survey Encroachment
- Survey Obstruction

ABBREVIATIONS

- Survey Station
- Survey Point
- Survey Marker
- Survey Boundary
- Survey Easement
- Survey Right-of-Way
- Survey Encroachment
- Survey Obstruction

SYMBOLS

- Survey Station
- Survey Point
- Survey Marker
- Survey Boundary
- Survey Easement
- Survey Right-of-Way
- Survey Encroachment
- Survey Obstruction

LIST OF CONTACTS

- Survey Station
- Survey Point
- Survey Marker
- Survey Boundary
- Survey Easement
- Survey Right-of-Way
- Survey Encroachment
- Survey Obstruction

PROFESSIONAL SEAL

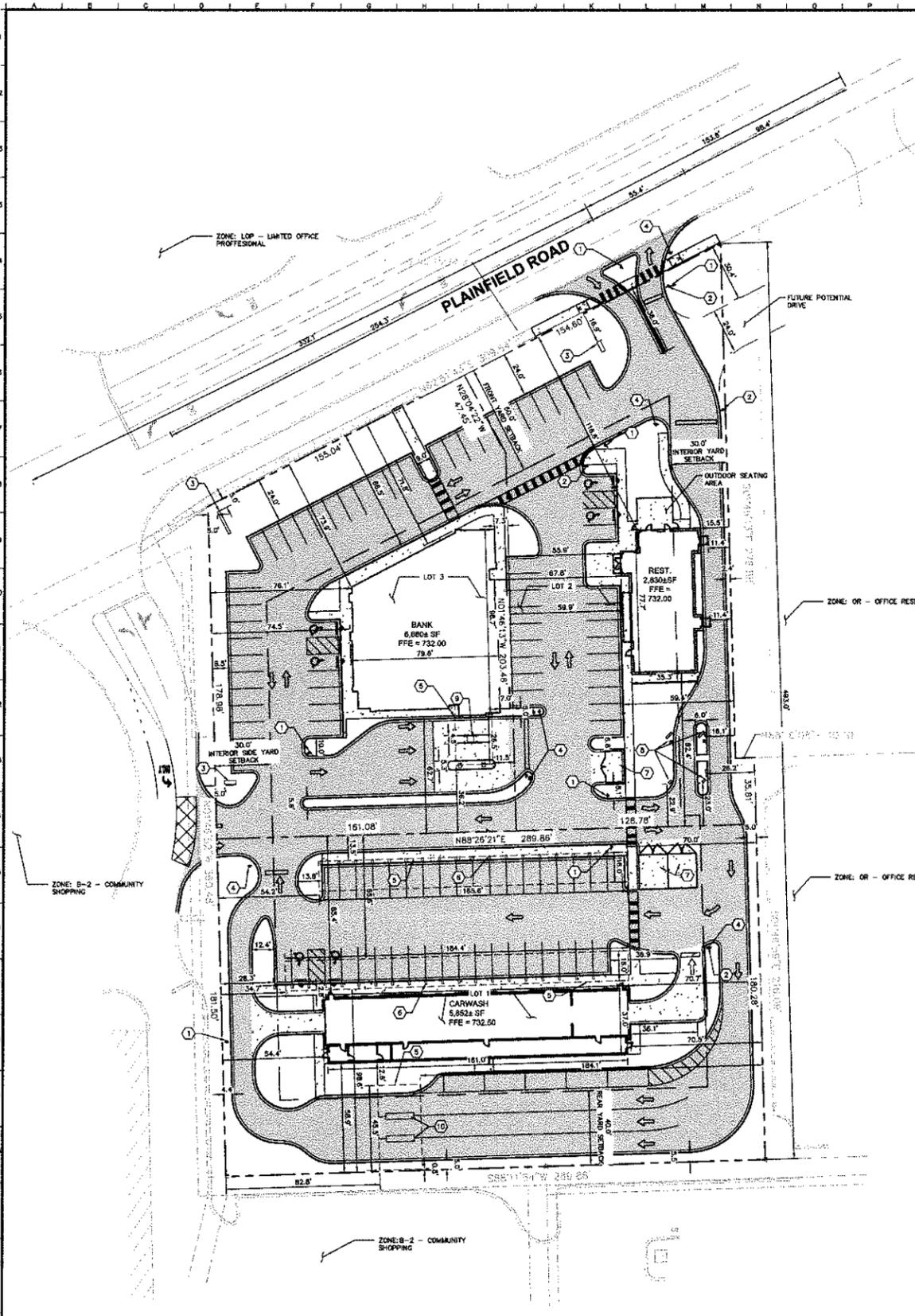
PROFESSIONAL SEAL

SURVEYOR'S CERTIFICATION

SURVEYOR'S CERTIFICATION



Drawing Name: K:\WORK\188726006_001_PUD\188726006_001_PUD_Plan\188726006_001_PUD_Plan.dwg
 Date: 12/15/2020
 Project: 188726006
 Title: PRELIMINARY PUD PLANS
 Author: J. B. BISHOP
 Plot Date: 12/15/2020
 Plot Time: 10:00 AM
 Plot Scale: 1/8" = 1'-0"
 Plot Orientation: Landscape
 Plot Size: 24" x 36"



PROPOSED LOT INFORMATION

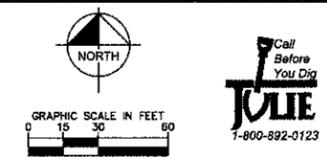
	LOT 1	LOT 2	LOT 3
LOT AREA (AC)	1.20	0.80	0.79
LOT COVERAGE (AC)	14.30%	9.20%	20.80%
MAXIMUM BUILDING HEIGHT (FT)	25.00	20.25	21.50
FAR	0.112	0.077	0.192

EXISTING LOT INFORMATION

	LOT 1
LOT AREA (AC)	2.79
LOT WIDTH (LF)	285
LOT DEPTH (LF)	423
LOT COVERAGE (AC)	28.20%
MAXIMUM HEIGHT	NA
FAR	0.287

BASIC BUILDING HEIGHTS

LOT 1	
- TOP OF WATER TANK HEIGHT:	40'
- TOP OF BUILDING HEIGHT:	25'
LOT 2	
- TOP OF BUILDING HEIGHT:	20.25'
- TOP OF TOWER HEIGHT:	28'
- T/O WINDOWS:	12'
- T/O STOREFRONT:	10.25'
LOT 3	
- TOP OF BUILDING HEIGHT:	21.5'
- MAX FREESTANDING SIGN HEIGHT:	8.5'



Call Before You Dig
1-800-892-0123

- ### GENERAL NOTES
- ALL DIMENSIONS REFER TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 - BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
 - REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY ALL BUILDING DIMENSIONS.
 - RADIUS ADJACENT TO PARKING STALL AND NOT DIMENSIONED ON THIS PLAN SHALL BE 3'-FEET, TYPICAL.
 - REFER TO ARCHITECTURAL PLANS FOR FREESTANDING SIGN DETAILS. SEE MEP PLANS FOR SITE ELECTRICAL DRAWINGS.
 - ALL PROPOSED ON-SITE STRIPING SHALL BE PAINTED UNLESS OTHERWISE NOTED.
 - SIDEWALK AT THE PLAINFIELD ROAD ENTRANCE WILL HAVE ADA COMPLIANT CURB RAMPS.

- ### KEY NOTES
- DIRECTIONAL SIGN (SEE DETAILS)
 - STOP SIGN (SEE DETAILS)
 - FREESTANDING SIGNS (SEE ARCHITECTURAL PLANS FOR DETAILS)
 - DO NOT ENTER SIGN (SEE DETAILS)
 - CANOPY, TYP. (SEE DETAILS)
 - VACUUM, TYP. (SEE DETAILS)
 - TRASH COMPACTOR AND ENCLOSURE (SEE ARCHITECTURAL PLANS FOR DETAILS)
 - MENU / ORDER BOARDS (SEE DETAILS)
 - CHASE BANK ATM (SEE DETAILS)
 - CAR WASH PAY STATION, TYP. (SEE DETAILS)

PARKING SUMMARY

PARKING SPACES REQUIRED	
BANK (1 SPACE/300SF)	= 23 SPACES
DRIVE-THRU RESTAURANT (1 SPACE/100SF)	= 29 SPACES
CAR LAUNDRY (1/1.5 EMPLOYEES)	= 2 SPACES
TOTAL PARKING SPACES REQUIRED	= 54 SPACES
STANDARD PARKING SPACES PROVIDED	
BANK	= 28 SPACES
DRIVE-THRU RESTAURANT	= 29 SPACES
*3 PARKING SPOTS FROM LOT 3 DONATED TO LOT 2)	= 3 SPACES
CAR LAUNDRY	= 40 SPACES
*3 PARALLEL PARKING EMPLOYEE SPOTS PROVIDED)	= 3 SPACES
ACCESSIBLE PARKING SPACES REQUIRED	= 5 SPACES
ACCESSIBLE PARKING SPACES PROVIDED	= 6 SPACES
BANK	= 2 SPACES
DRIVE-THRU RESTAURANT	= 2 SPACES
CAR LAUNDRY	= 2 SPACES
TOTAL PARKING SPACES PROVIDED	= 103 SPACES
MAXIMUM CAR STACKING (SEE CAR STACKING EXHIBIT)	
CARWASH	= 30 AUTOMOBILES
BANK	= 7 AUTOMOBILES
DRIVE-THRU RESTAURANT	= 11 AUTOMOBILES

<p>Kimley-Horn</p> <p>© 2020 KIMLEY-HORN AND ASSOCIATES, INC. 1000 WILSON AVENUE, SUITE 200 WILLOW BROOK, IL 60097 WWW.KIMLEY-HORN.COM</p>	<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NO.</th> <th>REVISIONS</th> <th>DATE</th> <th>BY</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>	NO.	REVISIONS	DATE	BY				
NO.	REVISIONS	DATE	BY						
<p>PRELIMINARY PUD</p>									
<p>GW PROPERTIES PUD PLANS</p> <p>729 PLAINFIELD ROAD WILLOW BROOK, IL 60097</p>									
<p>ORIGINAL ISSUE: 12/15/2020 KHA PROJECT NO. 188726006 SHEET NUMBER C1.0</p>									

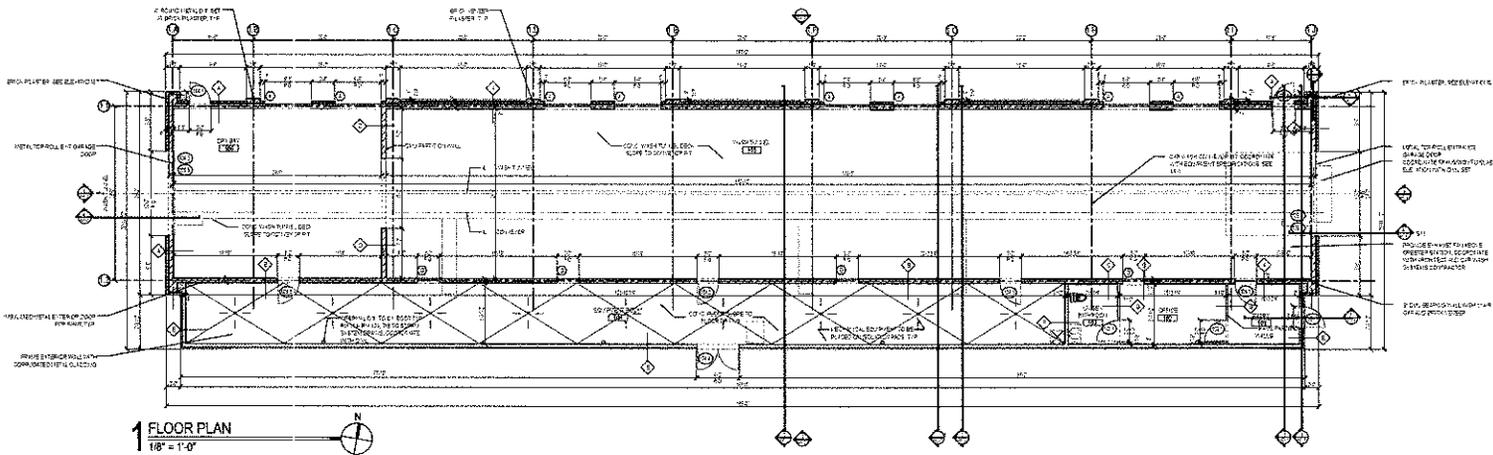




MIKE SHIVELY
ARCHITECTURE

225 S. GARDNER AVENUE
DURHAM, NC 27603
TEL: 919.487.1100
WWW.MIKESHIVELY.COM

PROJECT: ENCORE CAR WASH
DATE: 12.01.2009
DRAWING NO: A2.1



Encore Car Wash
Architectural Floor Plan
12.01.2009
12.01.2009

A2.1

Car Wash First
Floor Plan

Revisions	
1	Automatic Design 12.01.2009
2	
3	
4	
5	
6	
7	
8	
9	
10	

Plot Date: 01.12.2009



MIKE SHIVELY
ARCHITECTS

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Encore Car Wash
1201 20th St
Bismarck, ND 58501

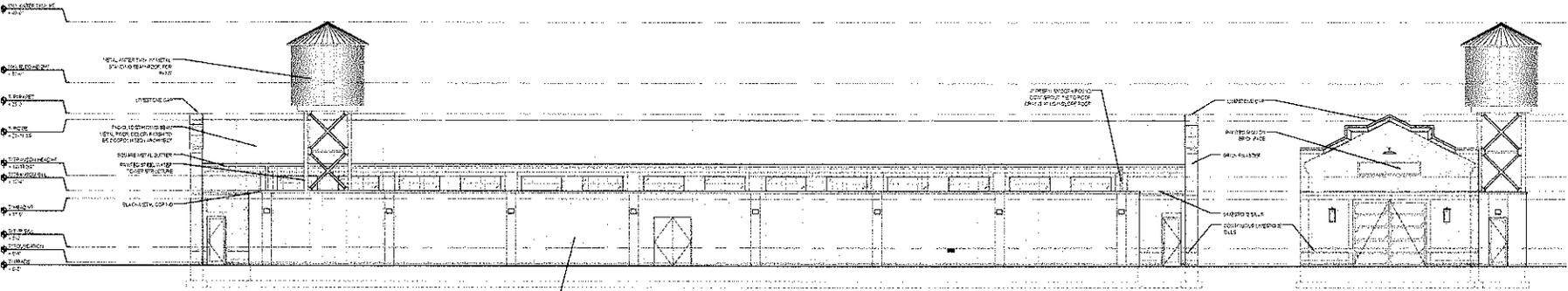
12/01/2018
12/01/2018

A4.0

Car Wash Exterior Elevations

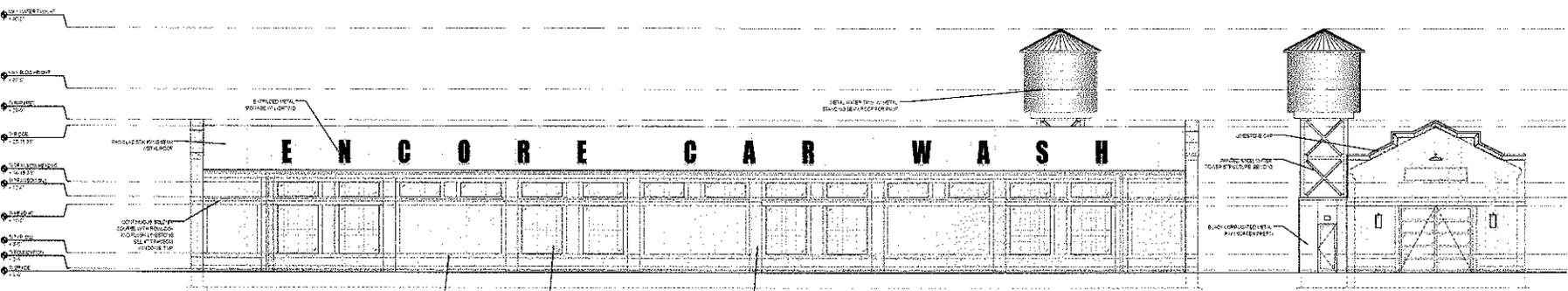
Revision	Description	Date
1	Issued for Design	12/01/2018
2		
3		
4		
5		
6		
7		
8		
9		
10		

Rev Date 12/01/2018



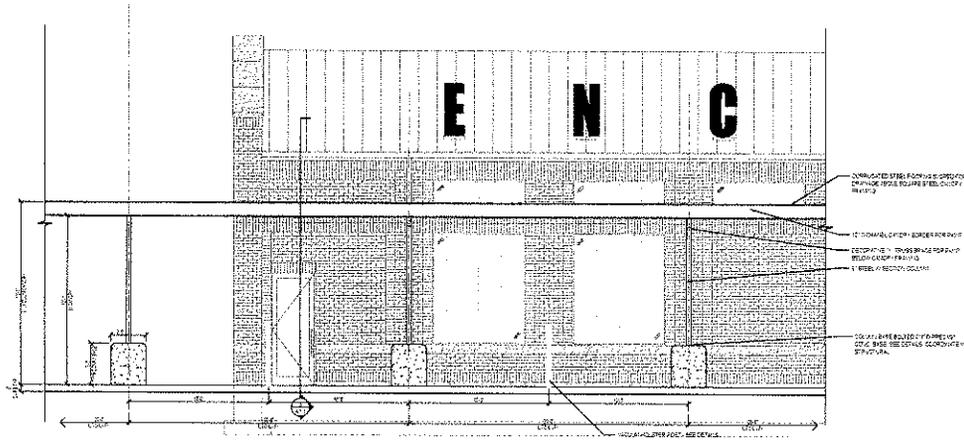
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1/8" = 1'-0"

2 WEST ELEVATION (ENTRY)
1/8" = 1'-0"

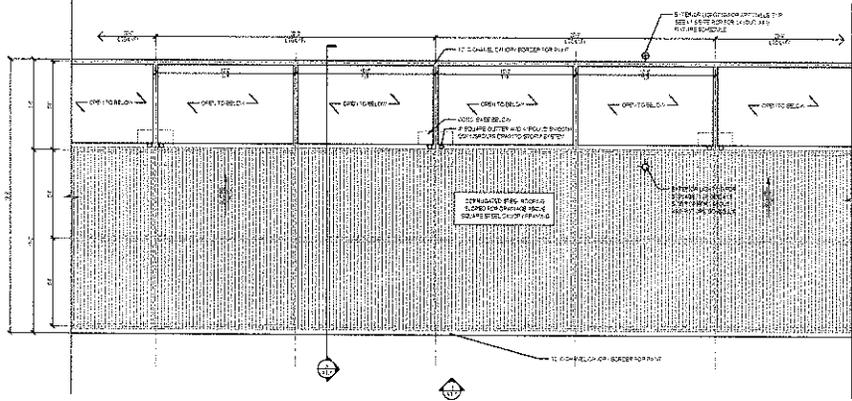


3 NORTH ELEVATION
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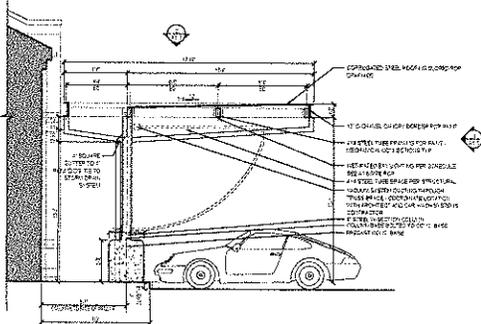
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1/8" = 1'-0"



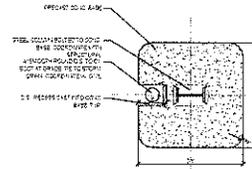
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1/4" = 1'-0"



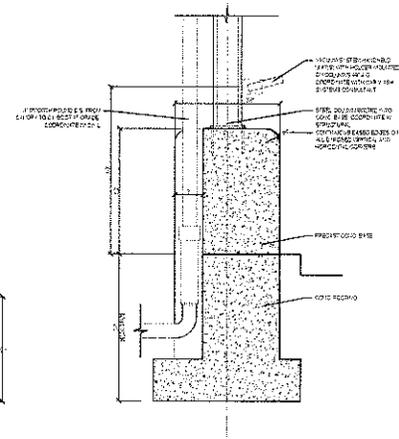
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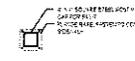
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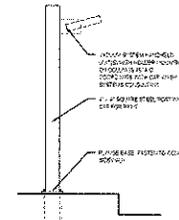
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5 COLUMN BASE SECTION DTL
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6 VACUUM HOLSTER POST PLAN
3/4" = 1'-0"



7 VACUUM HOLSTER POST ELEVATION
3/4" = 1'-0"



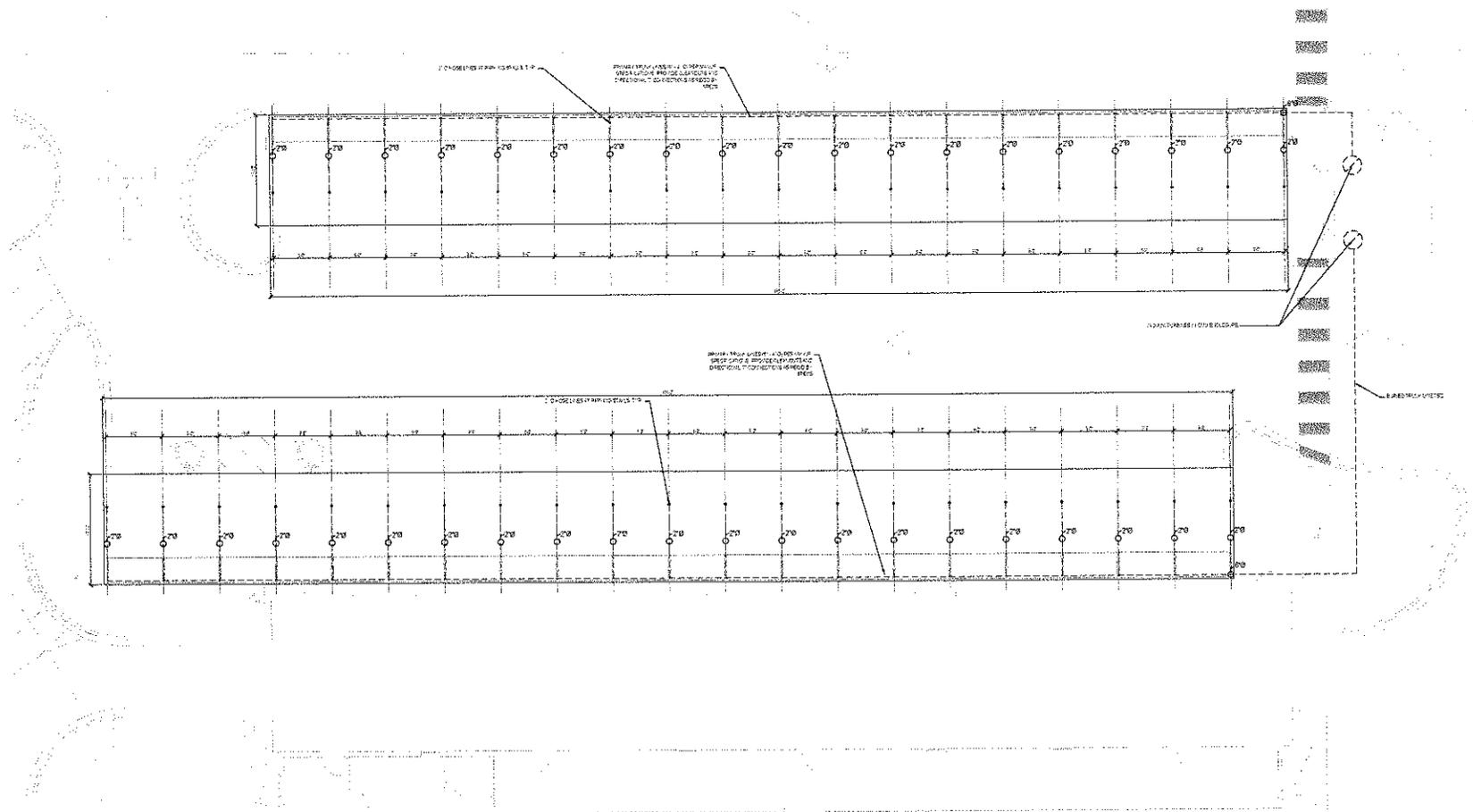
MIKE SHIVELY ARCHITECTS
12011 Highway 104
Overland Park, KS 66213
913-241-1100
www.mikeshively.com

Encore Car Wash
11800 Southline Blvd
Overland Park, KS 66213
913-241-1100

A1.1
Vacuum Canopy
Plans, Elevations,
Sections

Revision	By	Date
1	Shively Design	12/01/2010
2		
3		
4		
5		
6		
7		
8		
9		
10		

PKA Date: 01.25.2011



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ARCHITECTURE

2700 Gateway Center
Atlanta, Georgia 30326
404.525.4500
www.mikeshively.com

Architectural Services
Mechanical, Electrical, Plumbing
and Fire Protection Engineering
Civil Engineering
Surveying

10000 Peachtree Dunwoody Road, Suite 100
Atlanta, Georgia 30338

Encore Car Wash
10000 Peachtree Dunwoody Road, Suite 100
Atlanta, Georgia 30338

Mike Shively
Architect

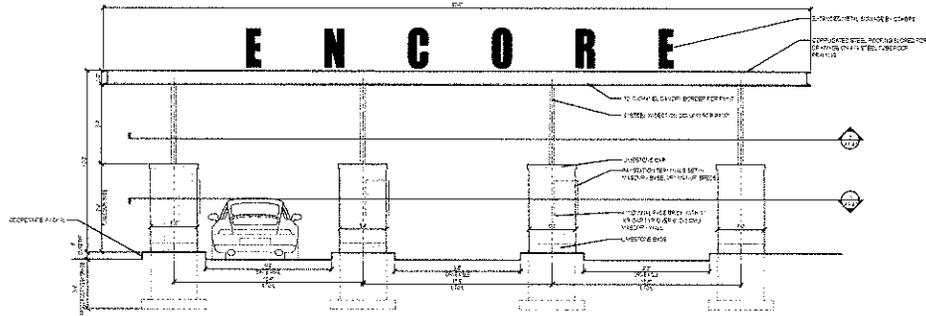
A1.2
Vacuum Canopy
Duct plan diagram

1 VACUUM CANOPY PLAN DIAGRAM
1/8" = 1'-0"

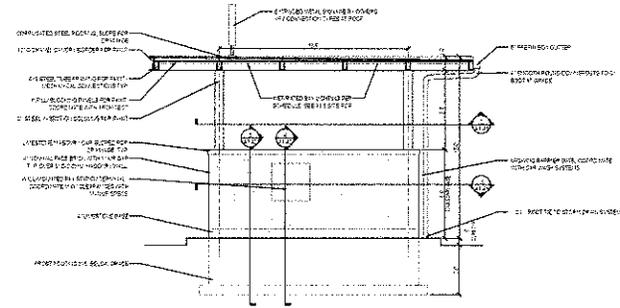


Revisions	By	Date
1	Spreckels Design	12-01-2020
2		
3		
4		
5		
6		
7		
8		
9		
10		

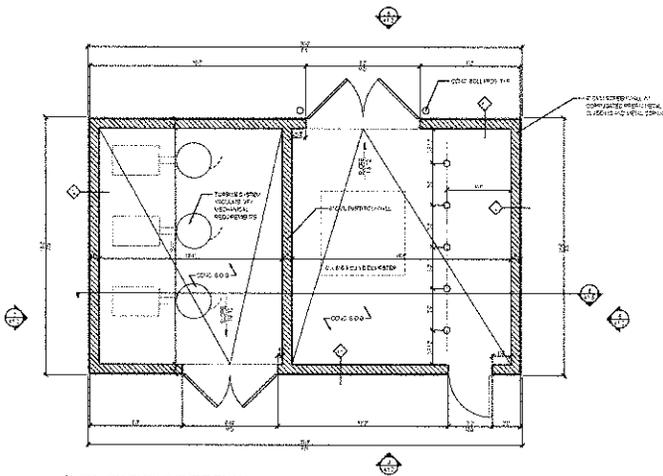
Prep Date: 01-26-2021



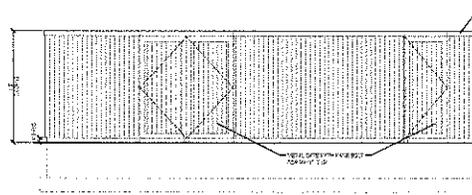
1 PAY CANOPY FRONT ELEVATION
1/4" = 1'-0"



2 PAY CANOPY LATERAL SECTION
1/4" = 1'-0"



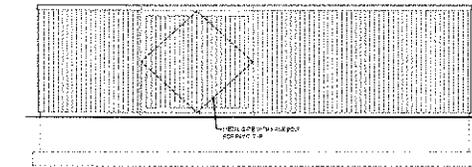
3 TRASH ENCLOSURE PLAN
1/4" = 1'-0"



4 REFUSE ENCLOSURE REAR ELEVATION
1/4" = 1'-0"



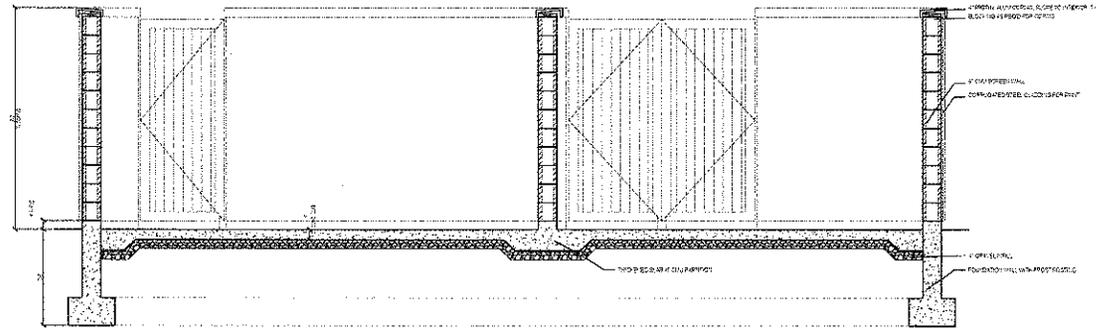
5 REFUSE ENCLOSURE RIGHT ELEVATION
1/4" = 1'-0"



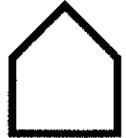
6 REFUSE ENCLOSURE FRONT ELEVATION
1/4" = 1'-0"



7 REFUSE ENCLOSURE LEFT ELEVATION
1/4" = 1'-0"



8 REFUSE ENCLOSURE SECTION
1/2" = 1'-0"



MIKE SHIVELY
ARCHITECTURE

2001 Greenway
Chicago, IL 60601
312.252.1234

Project: Enclosure Design for the
garage, the refuse enclosure
and the trash enclosure
Chicago, IL

Owner: Enclosure
Chicago, IL

Scale: 1/4" = 1'-0"

Encore Car Wash
and Enclosure Design (Rev. 04)

Sheet 004 of 9

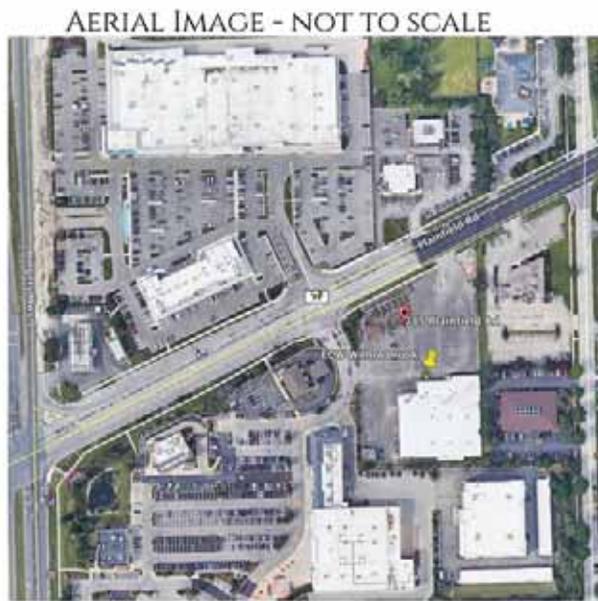
720 Riverside Blvd
Westbrook, IL 60091

A1.2
Pay Station
Elevations, Trash
Enclosure

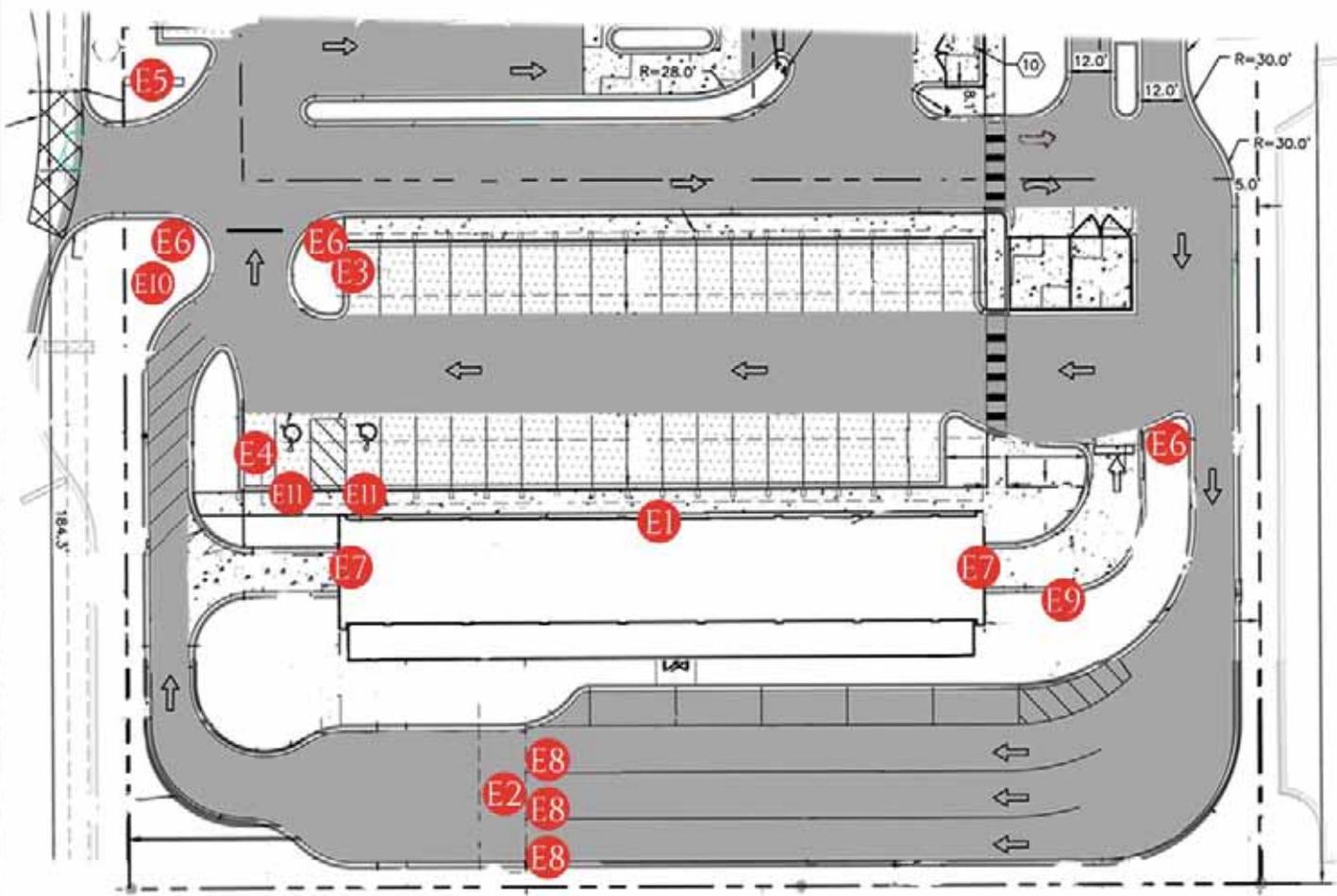
Revisions	
1	Schematic Design 12.01.2009
2	
3	
4	
5	
6	
7	
8	
9	
10	

Proj Date: 01.14.2009

EXTERIOR SIGN LEGEND			
Elevation	Sign No.	Description	Sq. Ft.
North	E.1	Illuminated Channel Letters	202.45
East	E.2	Illuminated Channel Letters	47.69
West	E.3	Illuminated Channel Letters	20.58
West	E.4	Illuminated Channel Letters	15.08
North	E.5	Free Standing Sign	47.5
	E.6	Non-Illuminated Directional Sign	
	E.7	Non-Illuminated Painted Sign on Brick	
East	E.8	Illuminated Menu Sign (24sqft per qty 3- 1 per pay lane)	72
West	E.9	Illuminated Exit Indicator Sign	12.5
	E.10	Temporary Construction Sign - Skid Mount	
	E.11	Post Mounted Handicap Stall Sign	
			417.8



735 PLAINFIELD RD
 WILLOWBROOK, IL
 LOT 1



SITE PLAN FOR LOCATION REFERENCE - NOT TO SCALE

Computer generated colors are not a true match to any PMS, vinyl or paint.
 Rendering shown is for concept only. Actual scale and placement shown are approximate.

1100 Route 34 Aurora, Illinois 60503 JC 630 898 5900 office 630 898 6091 fax	Prepared For: ENCORE CAR WASH	Address: 735 PLAINFIELD RD	Draw: 210073	Sheet Map	Design Date: 1/14/2021	LANDLORD APPROVAL SIGNATURE	TITLE:
	Location Name:	City/State: WILLOWBROOK, IL	Rev 1:			PRINT	DATE:

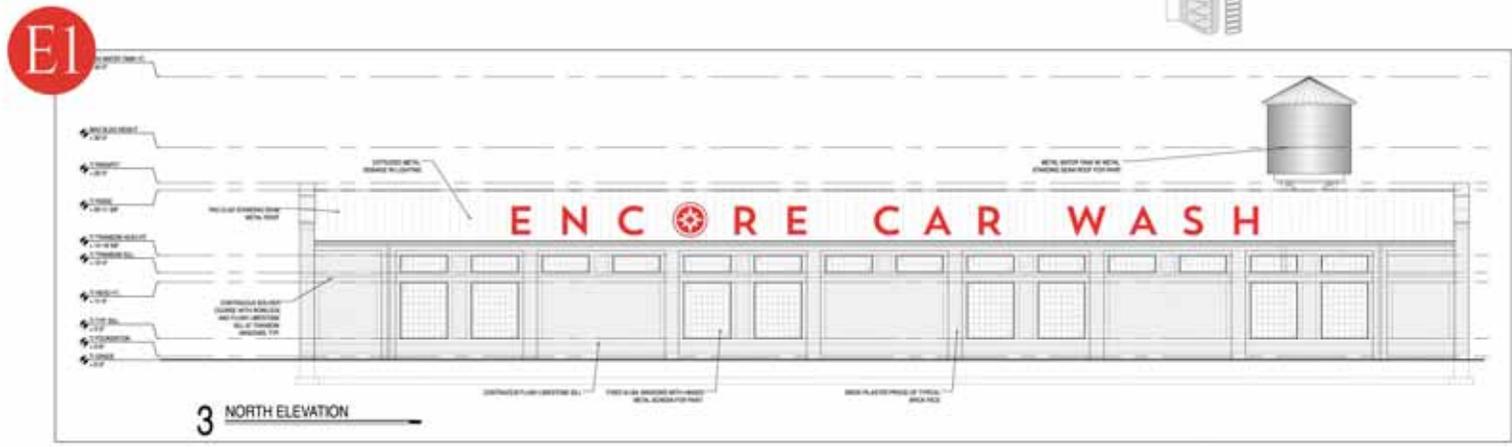
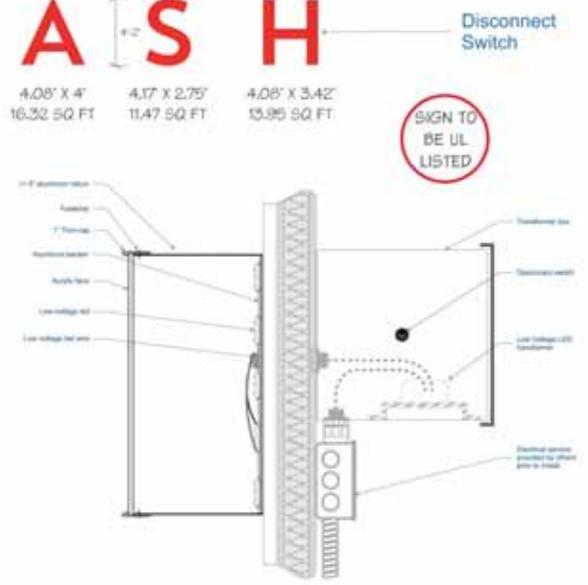
All Signs Shall Be Installed in Accordance With N.E.C. Article 600
 Engineering Specifications: All Signs Fabricated as per
 A.S.A. Specifications & 2014 I.B.C. Electrical Specifications:
 All Signs Fabricated as per
 2015 N.E.C. Specifications



202.45 SQ FT

ILLUMINATED CHANNEL LETTERS FLUSH MOUNTED TO WALL
 FABRICATE AND INSTALL CHANNEL LETTERS OF ALUMINUM AND ACRYLIC.
 LETTER FACES TO BE ACRYLIC WITH VINYL APPLIED FIRST SURFACE.
 ALL EXPOSED METAL SURFACES TO BE COATED WITH ACRYLIC POLYURETHANE.
 INTERNAL ILLUMINATION TO BE WHITE LEDS.
 SCALE 1/8"=1'

INSTALLATION INSTRUCTIONS
 CENTER SIGN LEFT/RIGHT, TOP/BOTTOM ON WALL AS SHOWN.
 CONNECT TO ELECTRICAL SERVICE TO BE PROVIDED BY OTHERS PRIOR TO INSTALL.



Computer generated colors are not a true match to any PMS, vinyl or paint.
 Rendering shown is for concept only. Actual scale and placement shown are approximate.

1100 Route 34 Aurora, Illinois 60503 JC 630 898 5900 office 630 898 6091 fax	Prepared For: ENCORE CAR WASH	Address: 735 PLAINFIELD RD	Draw: 210073	Sheet: 2	Design Date: 1/14/2021	LANDLORD APPROVAL SIGNATURE	TITLE:
	Location Name:	City/State: WILLOWBROOK, IL	Rev 1:	Rev 2:	PRINT		DATE:

NOTE: THIS DRAWING IS THE PROPERTY OF AURORA SIGN CO. IT IS NOT TO BE REPRODUCED, COPIED, OR EXHIBITED IN ANY FASHION WITHOUT WRITTEN CONSENT FROM AURORA SIGN CO. CHARGES OF UP TO \$2000.00 WILL BE ASSESSED FOR ANY MIS-USE OF THESE DRAWINGS.

SIGN TO
 BE UL
 LISTED



ILLUMINATED CHANNEL LETTERS

FABRICATE AND INSTALL CHANNEL LETTERS OF ALUMINUM AND ACRYLIC.
 LETTER FACES TO BE ACRYLIC WITH VINYL APPLIED FIRST SURFACE.
 ALL EXPOSED METAL SURFACES TO BE COATED WITH ACRYLIC POLYURETHANE.
 TRIM-CAP AND RETURNS TO BE BLACK.
 INTERNAL ILLUMINATION TO BE WHITE LEDs.
 SCALE 1/4"=1'

47.69 SQ FT

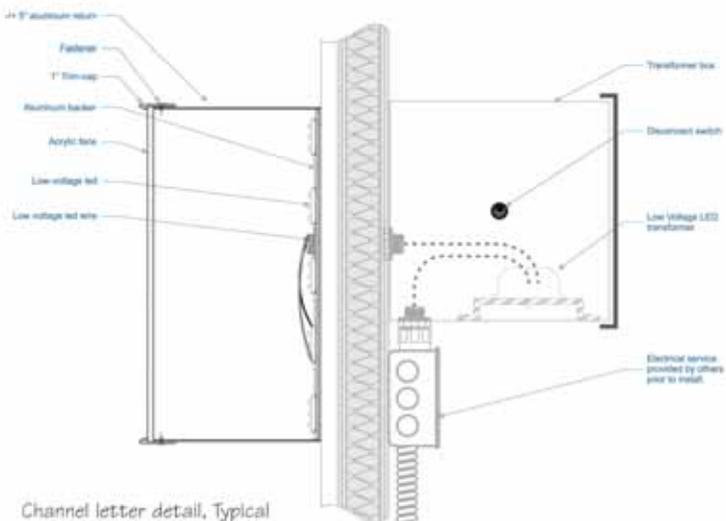
INSTALLATION INSTRUCTIONS

INSTALL SIGN ON AREA SHOWN.

WEST ELEVATION



Computer generated colors are not a true match to any PMS, vinyl or paint.
 Rendering shown is for concept only. Actual scale and placement shown are approximate.



1100 Route 34
 Aurora, Illinois 60503
 JC 630 898 5900 office
 630 898 6091 fax

Prepared For: ENCORE CAR WASH

Address: 735 PLAINFIELD RD

Draw: 210073 Sheet: 4 Design Date: 1/14/2021

LANDLORD APPROVAL SIGNATURE

TITLE:

Location Name:

City/State: WILLOWBROOK, IL

Rev 1:

PRINT

DATE:

Rev 2:



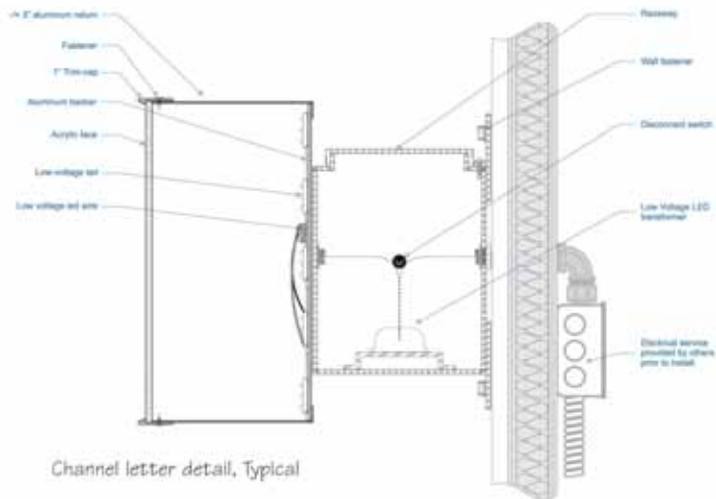
Disconnect Switch

SIGN TO BE UL LISTED

ILLUMINATED CHANNEL LETTERS ON A RACEWAY
 FABRICATE AND INSTALL CHANNEL LETTERS OF ALUMINUM AND ACRYLIC.
 LETTER FACES TO BE ACRYLIC WITH VINYL APPLIED FIRST SURFACE.
 ALL EXPOSED METAL SURFACES TO BE COATED WITH ACRYLIC POLYURETHANE.
 TRIM-CAP AND RETURNS TO BE BLACK.
 INTERNAL ILLUMINATION TO BE WHITE LEDS.
 RACEWAY PAINTED TO MATCH CANOPY,
 SCALE 3/4"=1'

20.58 SQ FT

INSTALLATION INSTRUCTIONS
 INSTALL SIGN ON AREA SHOWN.
 CONNECT TO ELECTRICAL SERVICE TO BE PROVIDED BY OTHERS PRIOR TO INSTALL.



E3



Computer generated colors are not a true match to any PMS, vinyl or paint.
 Rendering shown is for concept only. Actual scale and placement shown are approximate.

1100 Route 34 Aurora, Illinois 60503 JC 630 898 5900 office 630 898 6091 fax	Prepared For: ENCORE CAR WASH	Address: 735 PLAINFIELD RD	Draw: 210073	Sheet: 5	Design Date: 1/14/2021	LANDLORD APPROVAL SIGNATURE	TITLE:
	Location Name:	City/State: WILLOWBROOK, IL	Rev 1:			PRINT	DATE:

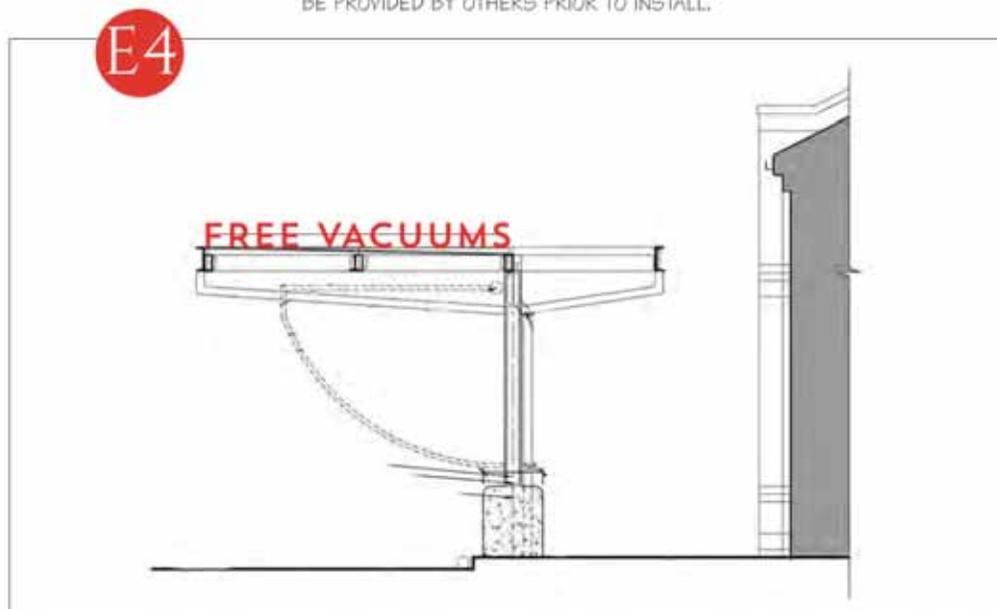
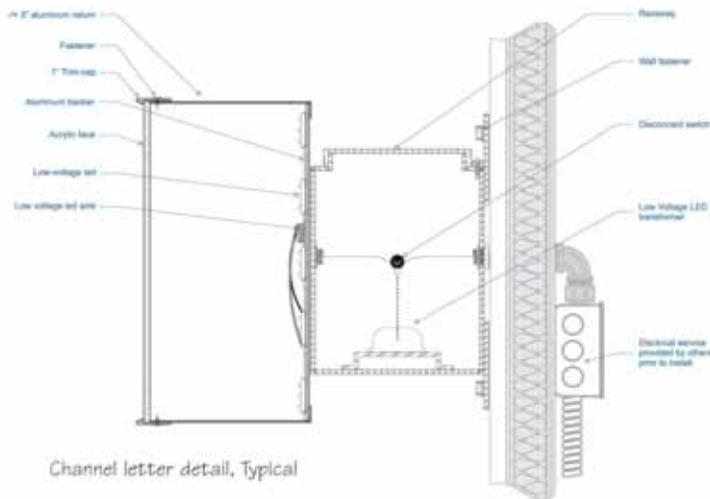


SIGN TO BE UL LISTED

15.08 SQ FT

ILLUMINATED CHANNEL LETTERS ON A RACEWAY
 FABRICATE AND INSTALL CHANNEL LETTERS OF ALUMINUM AND ACRYLIC.
 LETTER FACES TO BE ACRYLIC WITH VINYL APPLIED FIRST SURFACE.
 ALL EXPOSED METAL SURFACES TO BE COATED WITH ACRYLIC POLYURETHANE.
 TRIM-CAP AND RETURNS TO BE BLACK.
 INTERNAL ILLUMINATION TO BE WHITE LEDs.
 RACEWAY PAINTED TO MATCH CANOPY,
 SCALE 3/4"=1'

INSTALLATION INSTRUCTIONS
 INSTALL SIGN ON AREA SHOWN.
 CONNECT TO ELECTRICAL SERVICE TO
 BE PROVIDED BY OTHERS PRIOR TO INSTALL.

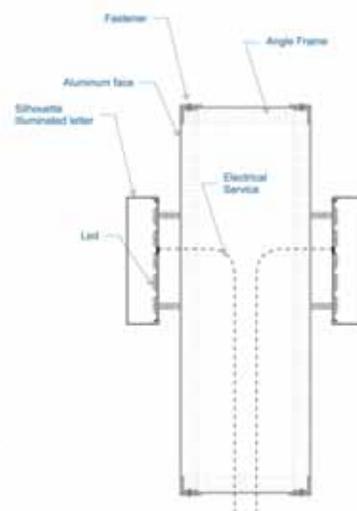
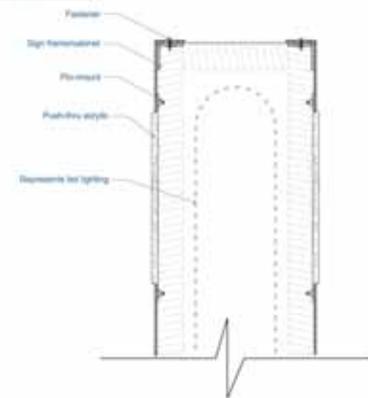


Computer generated colors are not a true match to any PMS, vinyl or paint.
 Rendering shown is for concept only. Actual scale and placement shown are approximate.

1100 Route 34 Aurora, Illinois 60503 JC 630 898 5900 office 630 898 6091 fax	Prepared For: ENCORE CAR WASH	Address: 735 PLAINFIELD RD	Draw: 210073	Sheet: 6	Design Date: 1/14/2021	LANDLORD APPROVAL SIGNATURE	TITLE:
	Location Name:	City/State: WILLOWBROOK, IL	Rev 1:				PRINT



SIGN TO BE UL LISTED



INSTALLATION INSTRUCTIONS

SET 4" STEEL PIPE IN CONCRETE FOOTING TO BE 2' X 3' BY 4' DEEP. CONNECT TO ELECTRICAL SERVICE TO BE PROVIDED BY OTHERS PRIOR TO INSTALL.

SEE SURVEY FOR SIGN LOCATION

DOUBLE FACE ILLUMINATED SIGN

FABRICATE AND INSTALL SIGN OF ALUMINUM AND ACRYLIC.
 ALL EXPOSED METAL SURFACES TO BE COATED WITH ACRYLIC POLYURETHANE.
 INTERNAL ILLUMINATION TO BE WHITE LEDs.
 SEE SCHEDULE A FOR MESSAGE UNIT
 SCALE 1/2"=1'

SCHEDULE A

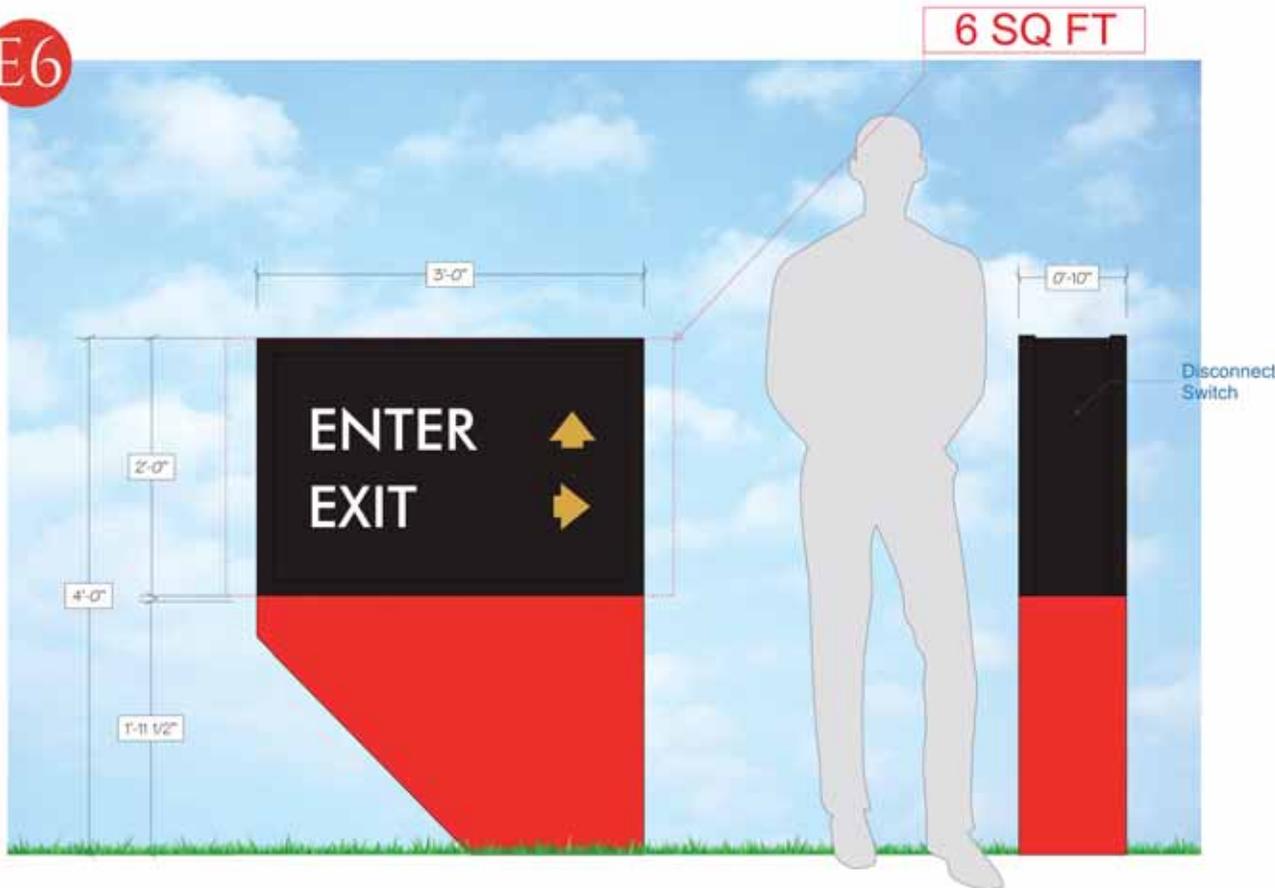
W16MM COLOR
 PIXEL MATRIX 36X126
 CABINET SIZE 41" H X 6' 3" L
 VIEWING ARE 36" H X 6' L
 DOUBLE FACE
 CHARACTER SIZE 4 LINES
 /25 CHARACTERS AT A 4" TYPE

Computer generated colors are not a true match to any PMS, vinyl or paint.
 Rendering shown is for concept only. Actual scale and placement shown are approximate.

1100 Route 34 Aurora, Illinois 60503 JC 630 898 5900 office 630 898 6091 fax	Prepared For: ENCORE CAR WASH	Address: 735 PLAINFIELD RD	Draw: 210073	Sheet: 7	Design Date: 1/14/2021	LANDLORD APPROVAL SIGNATURE	TITLE:
	Location Name:	City/State: WILLOWBROOK, IL	Rev 1:				PRINT

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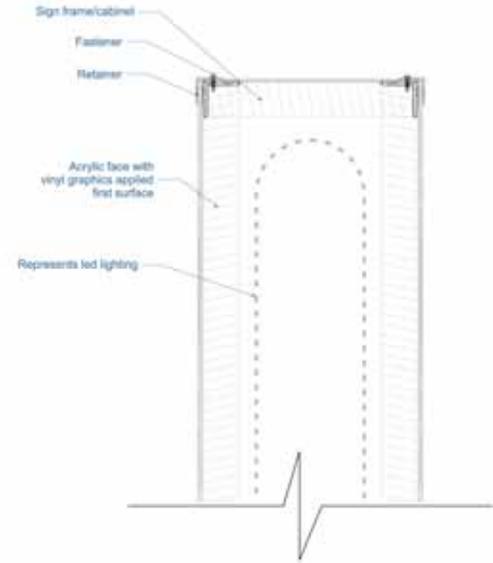
E6



SIGN TO BE UL LISTED

AURORA SIGN CO.

All Signs Shall Be Installed in Accordance With N.E.C. Article 600
 Engineering Specifications All Signs Fabricated as per A.S. & Specifications A-2014 U.S.C.
 Electrical Specifications All Signs Fabricated as per 2015 N.E.C. Specifications



INSTALLATION INSTRUCTIONS

SET 2" STEEL PIPE IN CONCRETE FOOTING TO BE 10" ROUND AT 4' DEEP. CONNECT TO ELECTRICAL SERVICE TO BE PROVIDED BY OTHERS PRIOR TO INSTALL.

DOUBLE FACE ILLUMINATED DIRECTIONAL SIGN

FABRICATE AND INSTALL SIGN OF ALUMINUM AND ACRYLIC.
 SIGN FACE TO BE WHITE ACRYLIC WITH VINYL APPLIED FIRST SURFACE.
 ALL EXPOSED METAL SURFACES TO BE COATED WITH ACRYLIC POLYURETHANE.
 INTERNAL ILLUMINATION TO BE WHITE LEDS.
 SCALE 1"=1'

Computer generated colors are not a true match to any PMS, vinyl or paint. Rendering shown is for concept only. Actual scale and placement shown are approximate.

1100 Route 34 Aurora, Illinois 60503 JC 630 898 5900 office 630 898 6091 fax	Prepared For: ENCORE CAR WASH	Address: 735 PLAINFIELD RD	Draw: 210073	Sheet: 8	Design Date: 1/14/2021	LANDLORD APPROVAL SIGNATURE	TITLE:
	Location Name:	City/State: WILLOWBROOK, IL	Rev 1:				PRINT

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FREE STANDING SIGN X 1



WAY FINDING SIGN

ENCORE CAR WASH BUILDING SIGN X1

FREE VACUUMS VAC (SOUTH) CANOPLY SIGN X 1

PAINTED 'CAR WASH' - PER ARCHITECTUAL PLANS -BY OTHERS X 2 @ 12 SQFT EACH

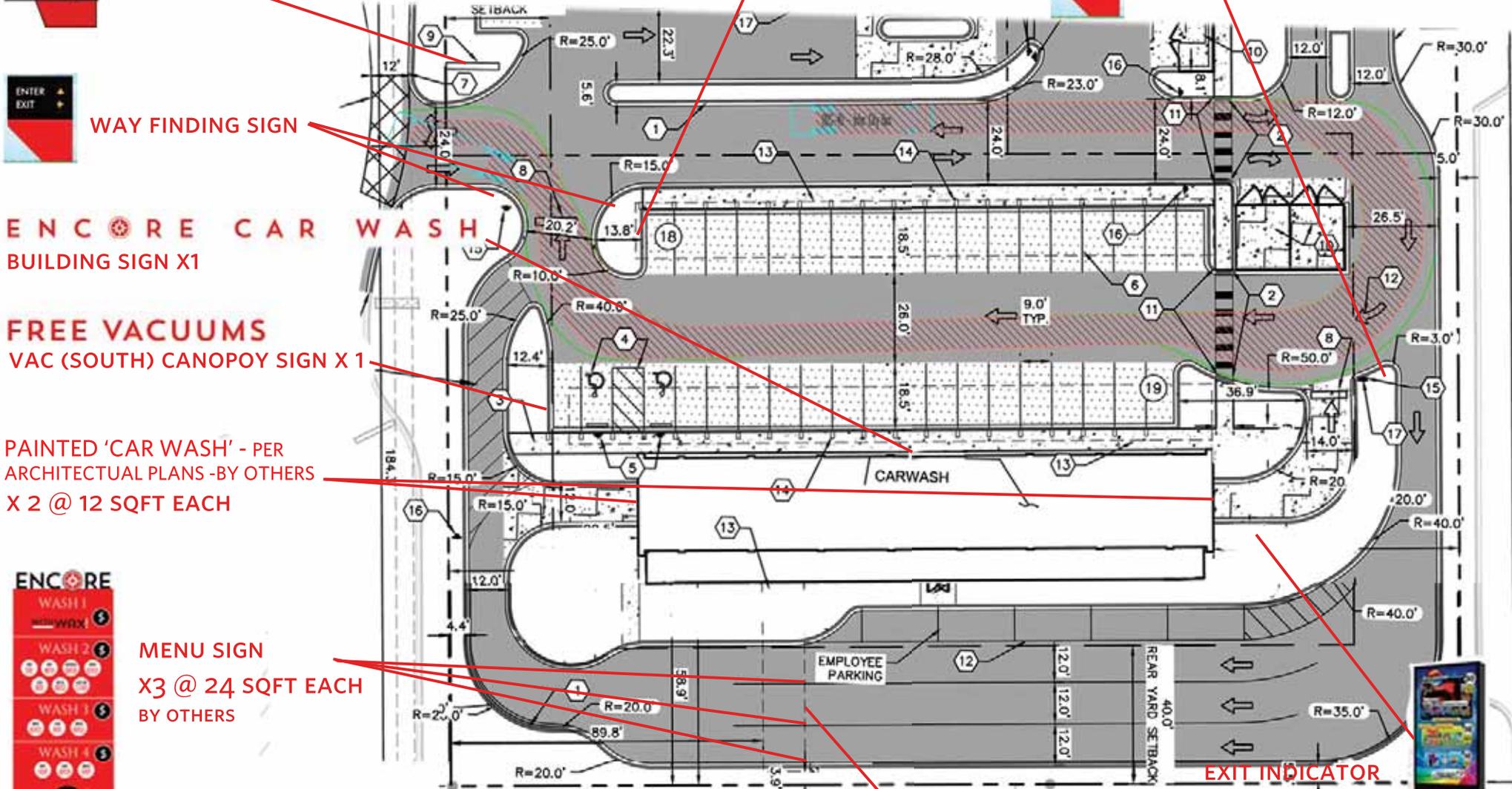


MENU SIGN X3 @ 24 SQFT EACH BY OTHERS

CAR WASH VAC (NORTH) CANOPLY SIGN X 1



WAY FINDING SIGN



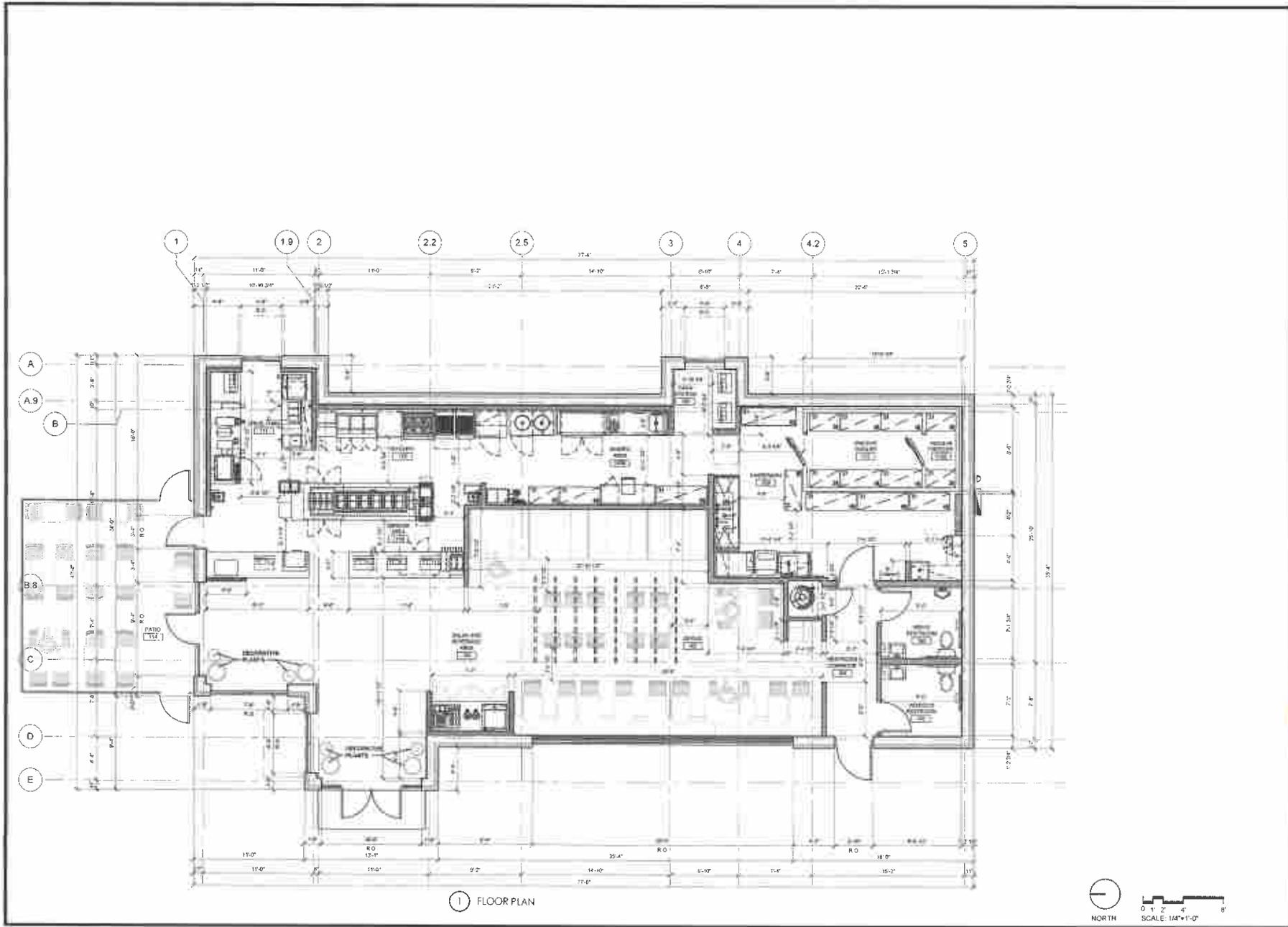
ENCORE PAY CANOPY SIGN X 1

EXIT INDICATOR SIGN BY OTHERS X1 @ 12.5 SQFT



Computer generated colors are not a true match to any PMS, vinyl or paint. Rendering shown is for concept only. Actual scale and placement shown are approximate.

1100 Route 34 Aurora, Illinois 60503 JC 630 898 5900 office 630 898 6091 fax	Prepared For: ENCORE CAR WASH	Address: 735 PLAINFIELD RD	Draw: 210073	Sheet: Map	Design Date: 1/14/2021	LANDLORD APPROVAL SIGNATURE	TITLE:
Location Name:	City/State: WILLOWBROOK, IL	Rev 1:	Rev 2:	PRINT	DATE:		



INTERPLANE

ARCHITECTURE
ENGINEERING
INTERIOR DESIGN
PROJECT MANAGEMENT

DESIGN TEAM REGISTRATION#
#15402311-0054

300 SOUTH 780 SUMMIT AVE. SUITE D
DANBROOK TERRACE & 451ST
PH 435.932.2236
FX 435.532.2339

SEAL:

THIS DOCUMENT IS NOT
FOR REGULATORY
APPROVAL, PERMITTING,
OR CONSTRUCTION.

09/20/19 15:04:19
REVISED



**GUZMAN
Y GOMEZ**

735 FLAMFIELD ROAD
WILLOWBROOK, IL 60577

PROJECT NO: 2008019
DATE: 01/20/21

PFP
REGISTERED PROFESSIONAL
FLOOR PLAN

CHECKED: MF DRAWING: DC

Sign A UL Listed channel Letters

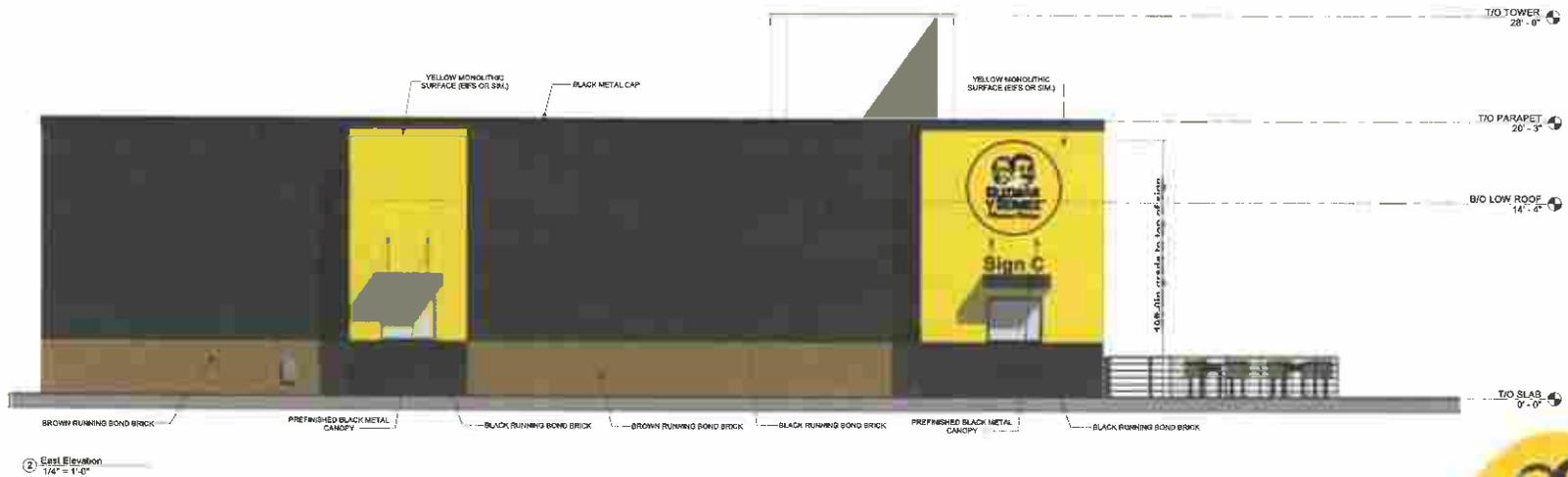


5in projection from wall

Illuminated LOGO
plex face/LED internal illumination
UL Listed

5in projection

Sign B & C



735 PLAINFIELD RD.
WILLOWBROOK, IL 60527
01.20.21



Sign D UL Listed channel Letters



5in projection from wall

Sign E

Illuminated LOGO
plex face/LED internal illumination
UL Listed
5in projection



① South Elevation
1/4" = 1'-0"



② West Elevation
1/4" = 1'-0"

Sign F



UL Listed channel Letters

5in projection from wall

735 PLAINFIELD RD.
WILLOWBROOK, IL 60527
01.20.21



735 PLAINFIELD RD.
WILLOWBROOK, IL 60527

R4

Sign G



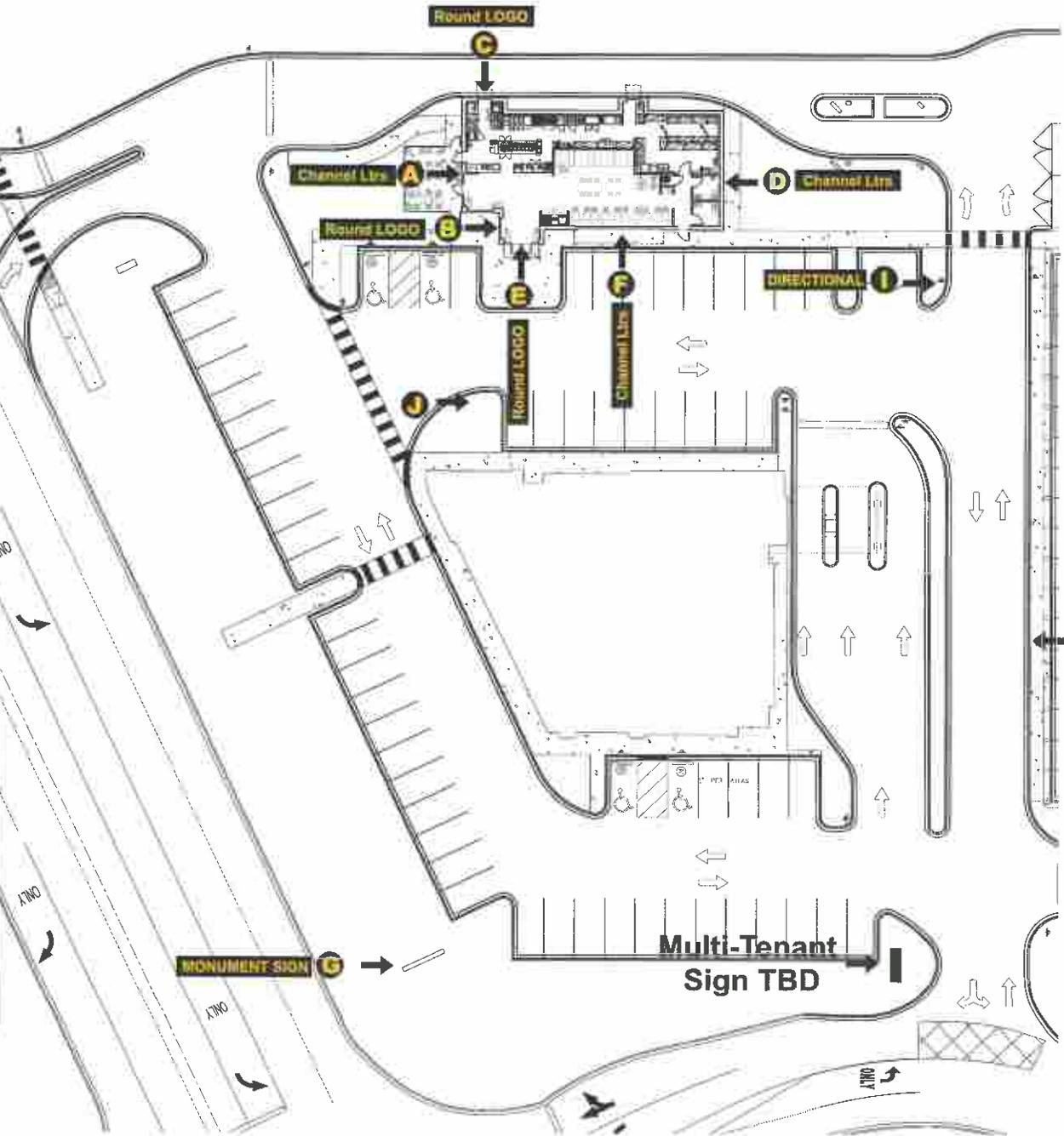
DF Monument Sign

Building Frontage
43ft-4in X 77ft-8in

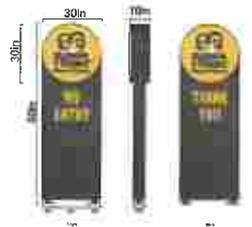
Max wall sign SSA allowed per code 300 sqft
Wall sign SSA proposed 245.2 sqft

GyG Willowbrook SSA Chart

Sign	SSA sqft	Height	Width
Sign A	46.6	18ft-6in	18ft-6in
Sign B	49	26ft-6in	19ft-0in
Sign C	49	19ft-0in	19ft-0in
Sign D	46.6	18ft-6in	18ft-6in
Sign E	49	26ft-6in	19ft-0in
Sign F	75	19ft-0in	19ft-0in
315.2 total wall sign SSA			
Monument Sign	39.8	monument sign SSA	
H, I, J Directionals: Directional (I) lettering 14in X 18in=1.8 sqft Logo 30in X 30in=6.3 sqft Directional Sign 8.1 sqft SSA 4.3=24.3 sqft			
Total Proposed SSA = 379.20sqft			



Signs H, I & J
Directionals



lettering 14in X 18in=1.8 sqft
Logo 30in X 30in=6.3 sqft
Directional Sign 8.1 sqft SSA





① Northwest Corner



② Southwest Corner



③ Southeast Corner



④ Northeast Corner





WILLOWBROOK RELO
 730 Plainfield Rd.
 Willowbrook, IL 60527

FLOOR PLAN
 01 22 2021

Architect/Designer
 The Architects Partnership
 200 South Michigan Avenue
 Chicago, IL 60604
 T: 312.563.9903
 F: 312.563.9999
 TAP Project Number: 17073





CAST STONE



DARK GREY GLAZED CERAMIC TILE (36\"/>



DARK GREY INLAY CERAMIC TILE (36\"/>



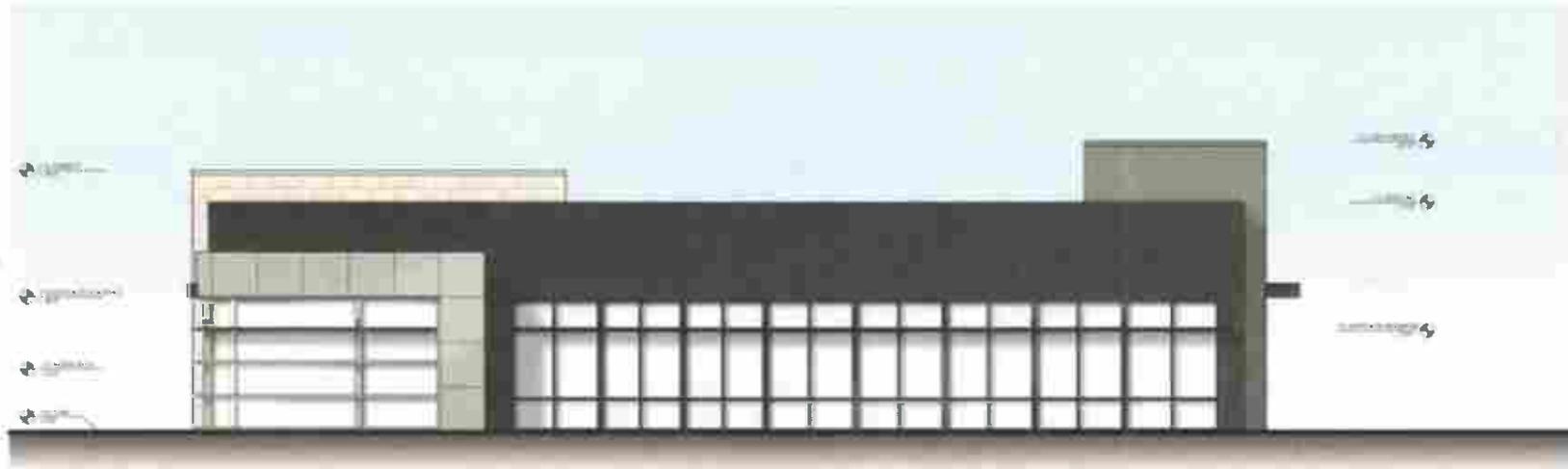
DARK GREY GLAZED CERAMIC TILE (36\"/>



ACM DEEP CLEAN ANODIZED ALUM

GLASS

SKYLINE RENDERING



NORTH ELEVATION



WEST ELEVATION



CHASE
WILLOWBROOK RELO
730 Plainfield Road
Willowbrook, IL 60527

ARCHITECTURAL ELEVATIONS
01.18.2021

Architect/Designer:
The Architects Partnership
200 South Michigan Avenue
Chicago, IL 60604
T: 312.583.8800
F: 312.583.8890
TAP Project Number: 19100





LIGHT STONE



DARK CHARCOAL EXTERIOR WALL



BRICK FOR CAR WASH STRUCTURE



STONE ENTRY COLUMN WITH GLASS PANELS



ASH COLOR CLEAR ANODIZED ALUMINUM

PEARL SILVER

YELLOW BRICKWORK



SOUTH ELEVATION



EAST ELEVATION



EXTERIOR SIGN LEGEND - ALLOWED

Elevation	Sign No.	Sign Type	Description	Sq. Ft.
	E.1	CUSTOM M-50-RE	Illuminated Monument Sign w/ Custom Stone-Clad Base	228.8
West	E.2	LIF-WBO-24	24" White Channel Letters and Logo	36.9
	E.3	LIF-WBO-24	24" White Channel Letters and Logo	36.9
East	E.4	LIF-WBO-24	24" White Channel Letters and Logo	36.9
	E.5	NONE	REMOVED FROM SCOPE OF WORK	
	E.6	NONE	REMOVED FROM SCOPE OF WORK	
	E.7	NONE	REMOVED FROM SCOPE OF WORK	
	E.8	D-2-RE	Non-Illuminated Directional Sign	
	E.9	TC-P-ADA-IL-V-RE	Post Mount Handicap Stall Sign - Van Accessible	
	E.10	TC-P-ADA-IL-RE	Post Mount Handicap Stall Sign	
	E.11	TC-W-H-RE	Wall Mount Do Not Enter Sign	
	E.12	TC-W-H-RE	Wall Mount Do Not Enter Sign	
Northwest	E.13	NONE	REMOVED FROM SCOPE OF WORK	
	E.14	DU-C	ATM Lane Designator Sign	
	E.15	DU-C	ATM Lane Designator Sign	
	E.16	TC-CL-W	Clearance Sign	
	E.17	COMING SOON	Temporary Construction Site Sign - Skid Mount Structure	
	E.18-20	TC-W-A-RE	Wall Mount Bank Parking Only Sign	
	E.21-28	TC-P-A-RE	Post Mount Bank Parking Only Sign	

Total Proposed Sq Ft 339.5



SITE PLAN SCALE: 1" = 50' (1:600)

Signtech
 4444 Federal Blvd. San Diego CA 92102
 Phone: (619) 527-6100 / Fax: (619) 527-6111
 signtech.com



JP Morgan Chase Bank #730

Willowbrook
 730 Plainfield Rd.
 Willowbrook, IL 60527

Initial Date: 09/14/18
 Salesperson: Arthur Kayano
 Coordinator: Tracey Pichieri
 Designer: DDDES
 Scale: AS NOTED

CUSTOMER APPROVAL

Customer Signature _____ Date _____
COPY, COLORS & SIZES

Signtech does NOT provide primary electrical to sign location - **RESPONSIBILITY OF OTHERS!**

Customer Signature _____ Date _____

This drawing is the exclusive property of Signtech and shall not be reproduced or used in any way without prior written approval.

Drawing Number: 18-01613

Project ID: CHASE_730_1

Revision: R13 - 01 19.21

Sign Legend / Site Plan

INTERIOR SIGN LEGEND

Elevation	Sign No.	Sign Type	Description	Sq.Ft.
	E.29	SUR-TTW-U-4-TP	Universal Thin-Profile ATM Surround	
	I.30	SUR-TTW-U-4-TP	Universal Thin-Profile ATM Surround	32.95
	E.31	ADA-EP	ADA Accessible Entrance Plaque	
	E.32	ADA-EP	ADA Accessible Entrance Plaque	
	I.33	ADA-EX	ADA Exit Plaque	
	I.34	ADA-EX	ADA Exit Plaque	
	I.35	ADA-EX	ADA Exit Plaque	
	I.36	ADA-EX	ADA Exit Plaque	
	I.37	ADA-TW	ADA Accessible Teller Window Plaque	
	I.38	OCT-21-INT	Illuminated Bitro-Blue Interior Octagon	
	I.39	NONE	REMOVED FROM SCOPE OF WORK	
	I.40	NONE	REMOVED FROM SCOPE OF WORK	
	I.41	NONE	REMOVED FROM SCOPE OF WORK	
	I.42	ADA-RRW-A-G	ADA Accessible Women's Restroom Plaque	
	I.43	ADA-RRM-A-G	ADA Accessible Men's Restroom Plaque	
	I.44	ADA-EEX	ADA Emergency Exit Plaque	
	I.45	CUSTOM-OCT-24-INT	Illuminated Interior Blue Bitro Octagon - Ceiling-Hung	



FLOOR PLAN

SCALE: 1/16" = 1'-0"

DATE	DESIGNER
10/21/19	DG

DESIGN STANDARDS	
CE 2020	
HARDNESS ZONE	7B
OVERHEAD DOOR ACCEPTABLE	



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JP Morgan Chase Bank #730

Willowbrook
730 Plainfield Rd.
Willowbrook, IL 60527

Initial Date: 09/14/18
Salesperson: Arthur Navarro
Coordinator: Tracey Picham
Designer: hntb
Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
COPY, COLORS & SIZES

Signtech does NOT provide primary electrical to sign location - **RESPONSIBILITY OF OTHERS!**

Customer Signature _____ Date _____
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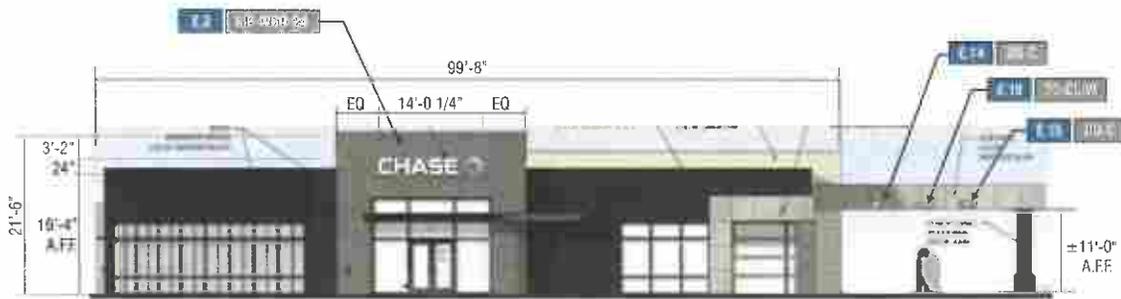
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Project ID: CHASE_730_1

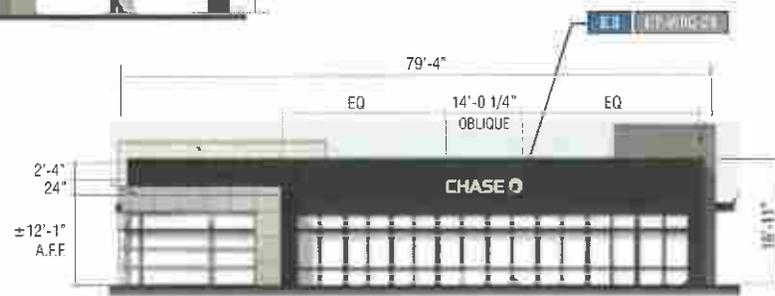
Revision: R13 - 01.19.21

Sign Legend / Floor Plan

SIGNAGE OVERVIEW - ALLOWED



WEST ELEVATION SCALE: 1/16" = 1'-0"



NORTH ELEVATION SCALE: 1/16" = 1'-0"



EAST ELEVATION SCALE: 1/16" = 1'-0"



SOUTH ELEVATION SCALE: 1/16" = 1'-0"



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JP Morgan Chase Bank #730

Willowbrook
730 Plainfield Rd.
Willowbrook, IL 60527

Initial Date: 09/14/18
Salesperson: Arthur Navarro
Coordinator: Tanya Pichon
Designer: bjonas
Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____

COPY COLORS & SIZES

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Customer Signature _____ Date _____

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Drawing Number: 18-01613

Project ID: CHASE_730_1

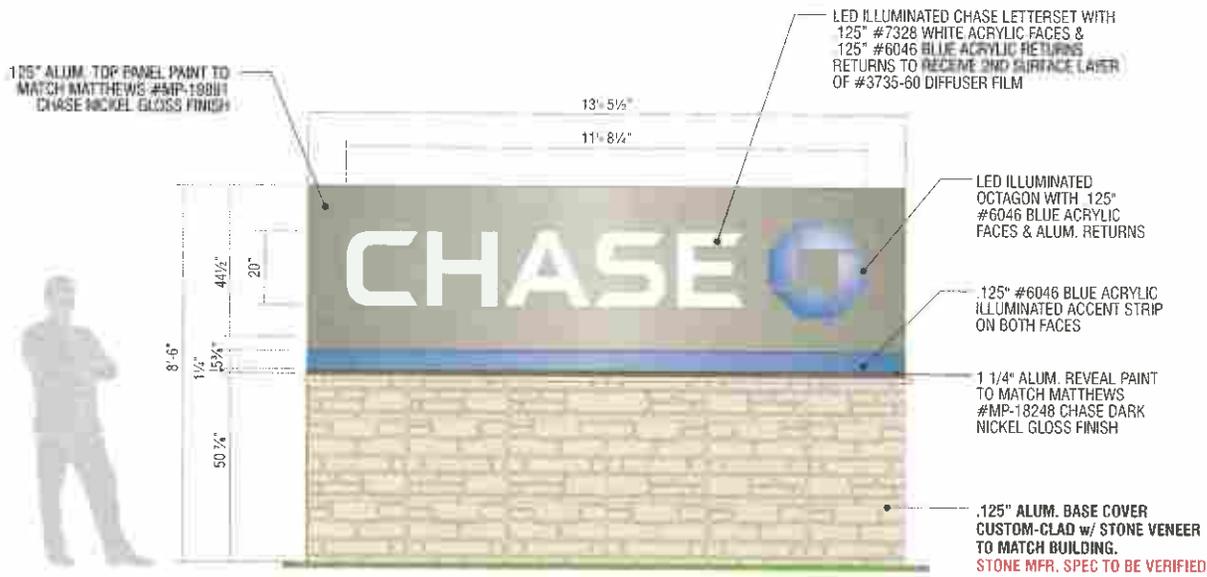
Revision: R13 - 01.19.21

Exterior Elevations

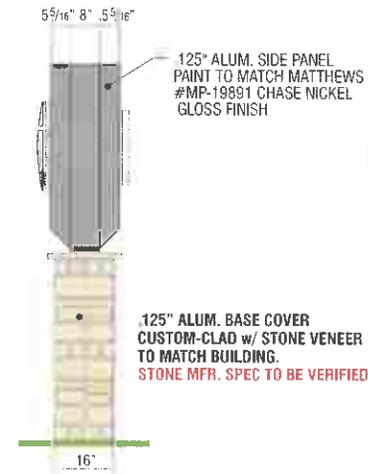
SIGNAGE OVERVIEW



TOP VIEW SCALE: 3/8" = 1'-0"



FRONT VIEW SCALE: 3/8" = 1'-0"



END VIEW

E.1 SIGN TYPE **FELESTONE-CLAD BASE TO MATCH BLDG. DIRECT BURIAL**
 MANUFACTURE AND INSTALL ONE (1) INTERNALLY ILLUMINATED D/F MONUMENT SIGN



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JP Morgan Chase Bank #730

Willowbrook
 730 Plainfield Rd.
 Willowbrook, IL 60527

Initial Date: 09/14/18
 Salesperson: Alvin Navarro
 Coordinator: Tracy Dickert
 Designer: Sobota
 Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
 COPY, COLORS & SIZES

Signtech does NOT provide primary electrical to sign location - RESPONSIBILITY OF OTHERS!

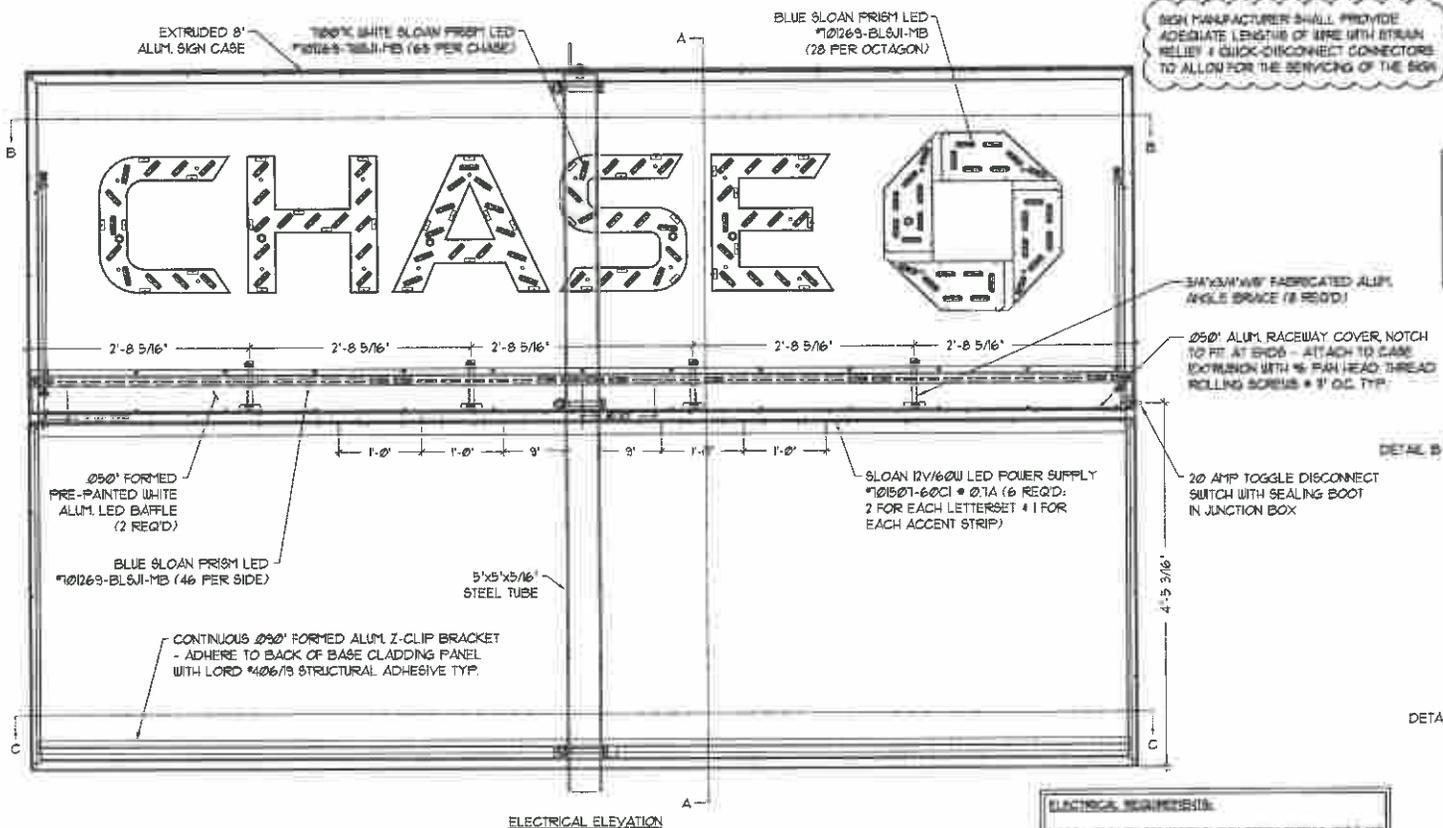
Customer Signature _____ Date _____

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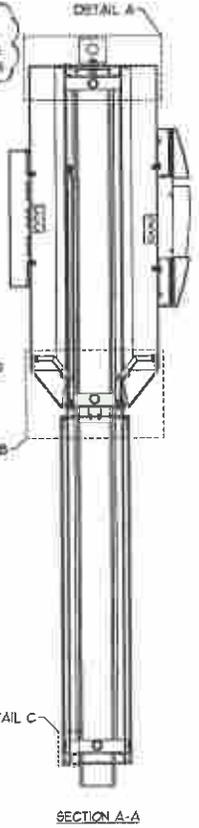
Drawing Number: 18-01613

Project ID: CHASE_730_1

Revision: R13 - 01 19.21



SIGN MANUFACTURER SHALL PROVIDE ADEQUATE LENGTH OF WIRE WITH STRAIN RELIEF + QUICK-DISCONNECT CONNECTORS TO ALLOW FOR THE SERVICING OF THE SIGN



ELECTRICAL REQUIREMENTS:
 1.005 (10A) 100% WHITE SLOAN PRISM LED 120269-BL6J1-MB
 (46) BLUE SLOAN PRISM LED 120269-BL6J1-MB
 POWER SUPPLY 12V/60W LED POWER SUPPLY # 101501-60C1 # 0.1A (6 REQ'D)
 TOTAL LOAD: 438 WATT
 CIRCUIT: 10 AMP REQ'D.



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JP Morgan Chase Bank #730

Willowbrook
 730 Plainfield Rd.
 Willowbrook, IL 60527

Initial Date: 09/14/18
 Salesperson: Arthur Navarro
 Coordinator: Tracey Pichiani
 Designer: Diones
 Signer: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
 COPY, COLORS & SIZES

Signtech does NOT provide primary electrical to sign location - **RESPONSIBILITY OF OTHERS!**

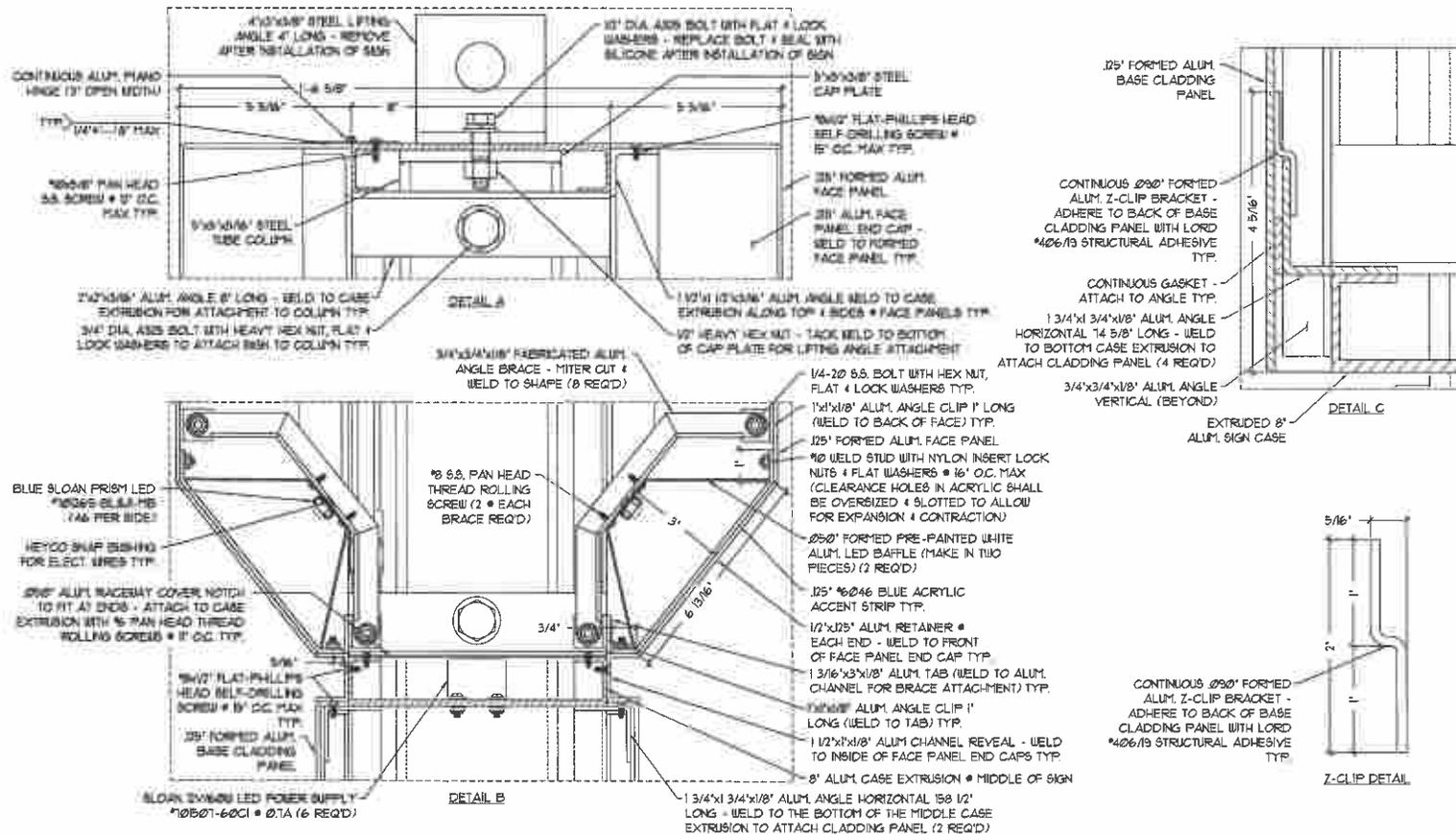
Customer Signature _____ Date _____

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Drawing Number: 18-01613

Project ID: CHASE_730_1

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JP Morgan Chase Bank #730

Willowbrook
730 Plainfield Rd.
Willowbrook, IL 60527

Initial Date: 09/14/18
Salesperson: Arthur Navarro
Coordinator: Tracey Pichieri
Designer: Nichol
Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
COPY, COLORS & SIZES

Signtech does NOT provide primary electrical to sign location - **RESPONSIBILITY OF OTHERS!**

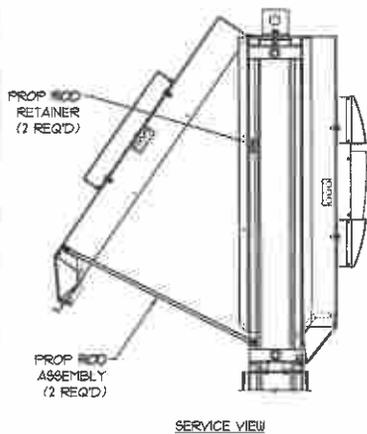
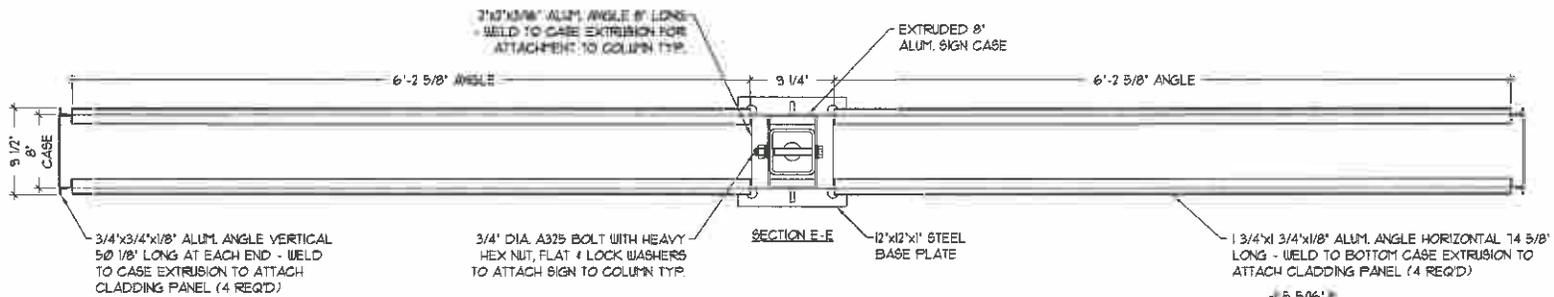
Customer Signature _____ Date _____

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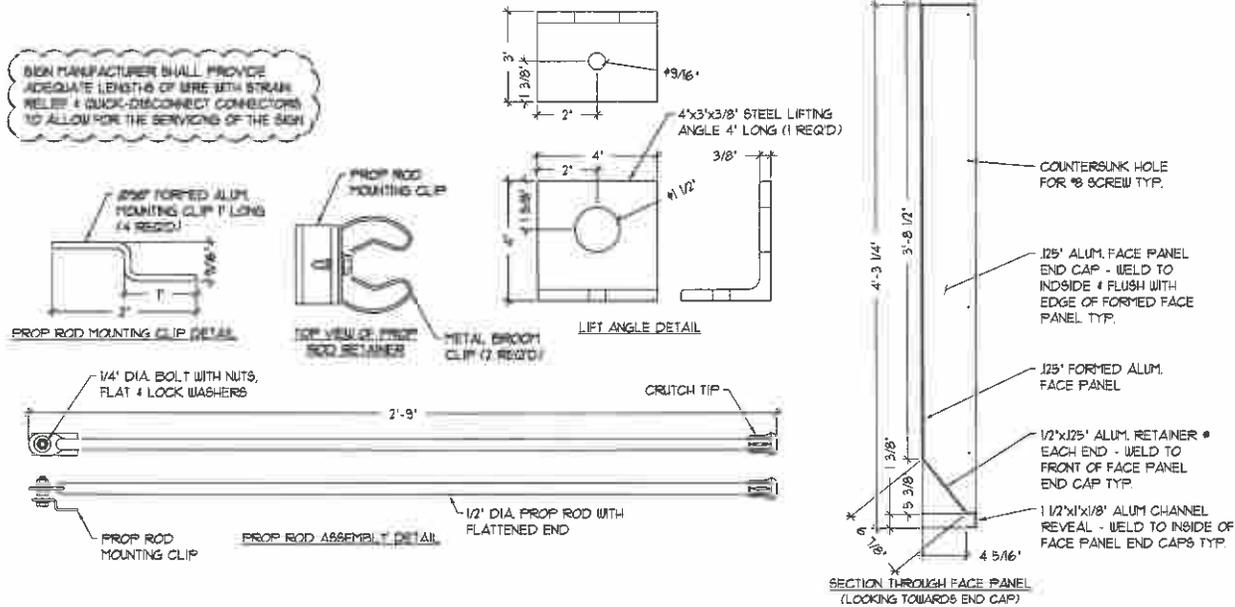
Drawing Number: 18-01613

Project ID: CHASE_730_1

Revision: R13 - 01.19.21



SIGN MANUFACTURER SHALL PROVIDE ADEQUATE LENGTHS OF WIRE WITH STRAIN RELIEF & QUICK-DISCONNECT CONNECTORS TO ALLOW FOR THE SERVICING OF THE SIGN.



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JP Morgan Chase Bank #730

Willowbrook
730 Plainfield Rd.
Willowbrook, IL 60527

Initial Date: 09/14/18
Salesperson: AUSTIN FAVARO
Coordinator: TRACY PICKETT
Designer: COOPER
Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
COPY, COLORS & SIZES

Signtech does NOT provide primary electrical to sign location - **RESPONSIBILITY OF OTHERS!**

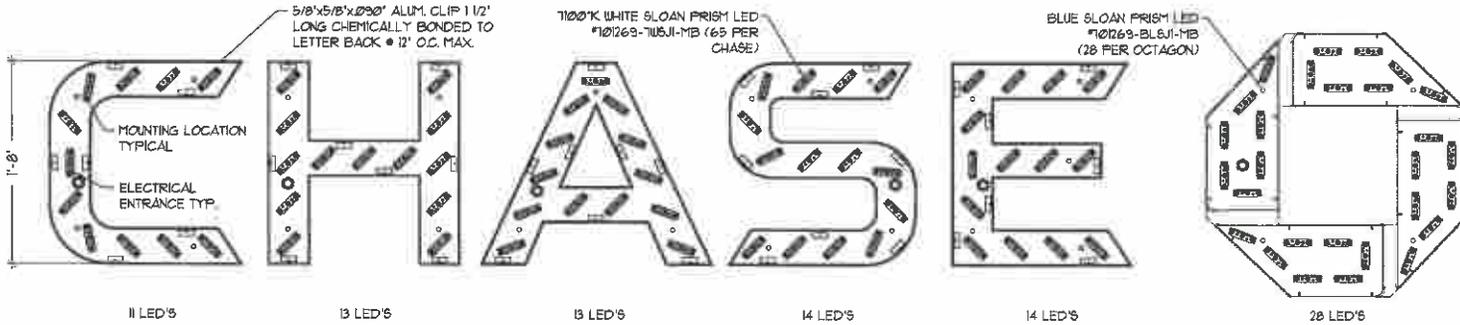
Customer Signature _____ Date _____

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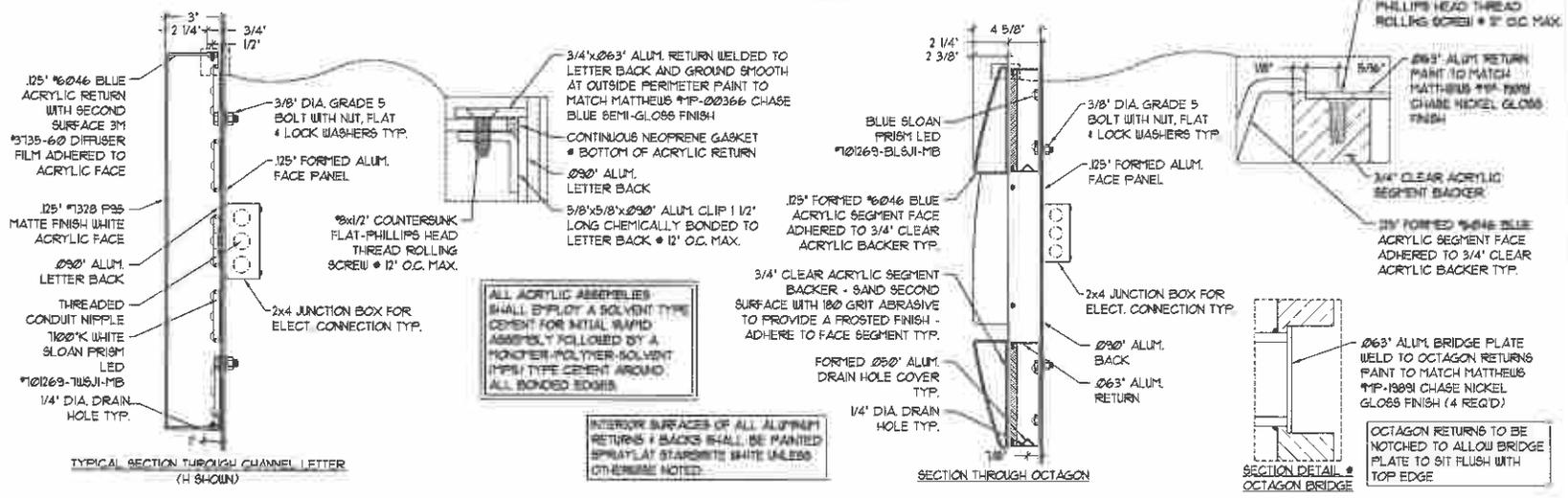
Drawing Number: 18-01613

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LETTERSSET LED LAYOUT



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JP Morgan Chase Bank #730

Willowbrook
 730 Plainfield Rd.
 Willowbrook, IL 69527

Initial Date: 09/14/18
 Salesperson: Arthur Navarro
 Coordinator: Tommy Plummer
 Designer: DJD/ML
 Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
COPY COLORS & SIZES

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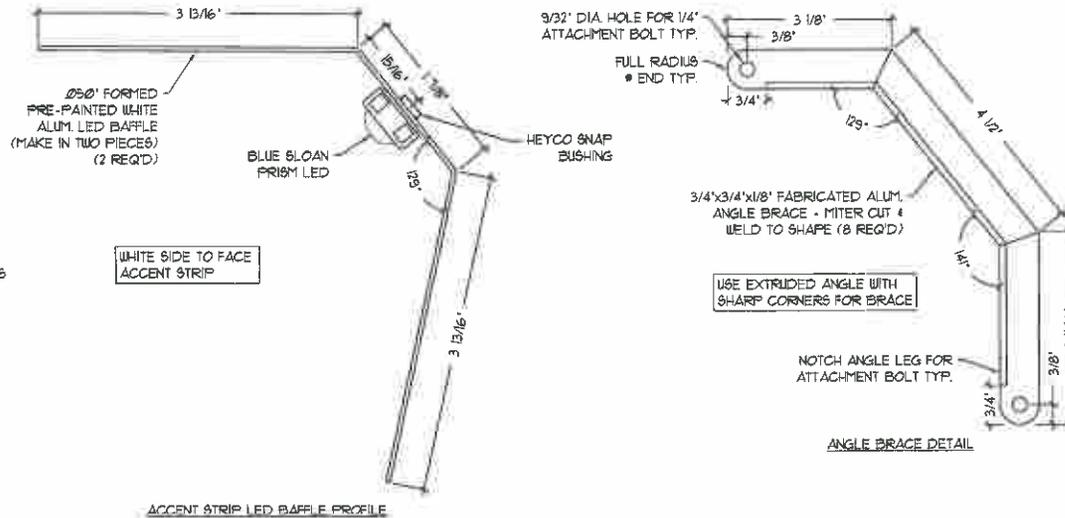
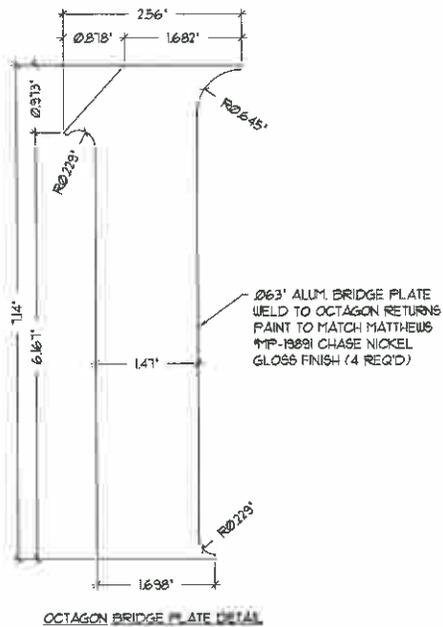
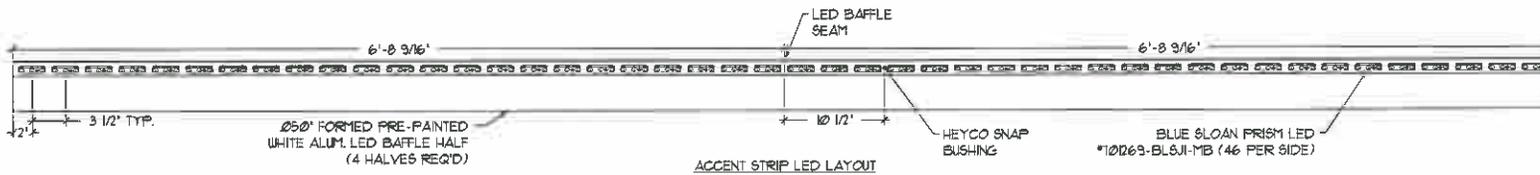
Customer Signature _____ Date _____

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JP Morgan Chase Bank #730

Willowbrook
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 Willowbrook, IL 60527

Initial Date: 09/14/18
 Salesperson: Arthur Navarro
 Coordinator: Tracey Pichierri
 Designer: Rigdon
 Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
 COPY COLORS & SIZES

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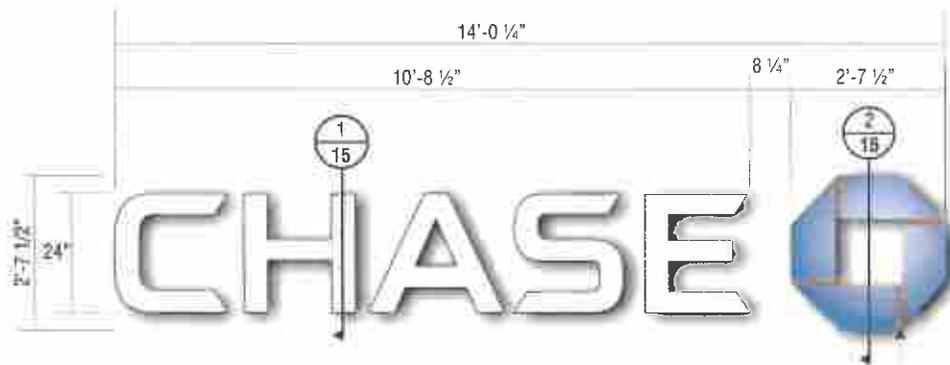
Customer Signature _____ Date _____

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FRONT VIEW SCALE : 1/2" = 1'-0"

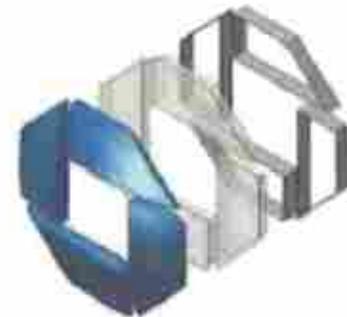
PAINT OCTAGON BRIDGE
TO MATCH WALL COLOR
COLOR TBV



**END VIEW
OF LETTER**
SCALE : 1/2" = 1'-0"



**END VIEW
OF LOGO**
SCALE : 1/2" = 1'-0"



3D VIEW OF OCTAGON

**ALL EXPOSED FASTENER HEADS
SHALL BE PAINTED TO MATCH
THE EXTERIOR SURFACE FINISH**

ELECTRICAL REQUIREMENTS FOR LED ILLUMINATION
As required by standards #12101, Signcoast Electrical, etc., exclusively uses secondary Ground fault-Interrupted (GFI) transformers.
Each Sign MUST have:
- A dedicated branch circuit
- Three wires - Line, Ground, and Neutral.
- Wire Size: Min 12 GA THHN Copper Wire.
NOTE: The gauge of the wire is dictated by the height of the run & ampacity as per NEC Article 310.
- The ground wire must be continuous & go from the sign to the panelboard ground bus.

E.2 E.3 E.4
SIGN TYPE LIF-WBO-24

MANUFACTURE AND INSTALL THREE (3) SET OF ILLUMINATED LETTERS & LOGO w/PLEX FACES & RETURNS

- LETTERS:**
FACE: .177" ARISTECH #7328 P-95 WHITE PLEX (MATTE SIDE TO BE 1ST SURFACE) CHEM WELDED TO RETURNS
RETURNS: 0.118" x 4" DEEP ARISTECH #6046 BLUE WITH 3635-70 DIFFUSER VINYL ON 2ND (INTERIOR) SURFACE.
BACKS: .080" ALUM BACK WITH 3/4" x .080" OUTSIDE ALUM RETURN AND 1/2" x 1/8" ALUM ANGLE CLIPS ON INTERIOR OF LETTER FOR FACE ATTACHMENT
PAINT: EXTERIOR PAINTED MATTHEWS MP-00366 CHASE BLUE SEMI-GLOSS FINISH (60-70 UNITS) AND INTERIOR PAINTED SPRAY-LAT STAR BRITE WHITE LIGHT ENHANCEMENT PAINT.
ILLUMINATION: SLOAN PRISM #701269-7WSJ1-MB WHITE LEDs AND SLOAN POWER SUPPLY REQUIRED FOR PROPER EVEN ILLUMINATION.
- LOGO:**
BACKS: .080" ALUM BACK WITH STACK WELDED 4" x .080" ALUM RETURNS.
LOGO CAN FACE: .080" ALUM BRIDGE ELEMENTS WELDED TO INSIDE & OUTSIDE RETURNS
PLEX FACE: .118" THERMO FORMED ARISTECH #6046 BLUE LOGO ELEMENTS CHEM WELDED TO 3/4" CLEAR PLEX 2ND SURFACE FACES.
PAINT: EXTERIOR PAINT MATTHEWS "CHASE NICKEL" #MP-19891 GLOSS FINISH AND INTERIOR PAINTED SPRAY-LAT STAR BRITE WHITE LIGHT ENHANCEMENT PAINT.
ILLUMINATION: SLOAN PRISM #701269-BLSJ1-MB BLUE LEDs AND SLOAN POWER SUPPLY AS REQUIRED FOR PROPER EVEN ILLUMINATION

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WBA MEMBER OF THE ASSOCIATION OF SIGN CONTRACTORS

JP Morgan Chase Bank #730

Willowbrook
 730 Plainfield Rd.
 Willowbrook, IL 60527

Initial Date: 09/14/18
 Salesperson: Arthur Navarro
 Coordinator: Tracey Pichierri
 Designer: ibondit
 Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
COPY, COLORS & SIZES

Signtech does NOT provide primary electrical to sign location - **RESPONSIBILITY OF OTHERS!**

Customer Signature _____ Date _____

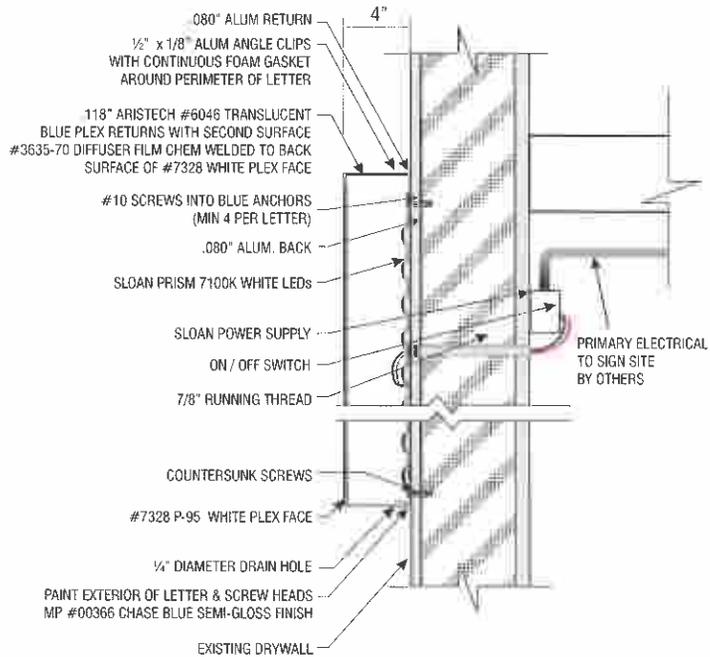
Your signature is the exclusive property of Signtech and shall not be reproduced or altered in any way without prior written approval.

Drawing Number: 18-01613

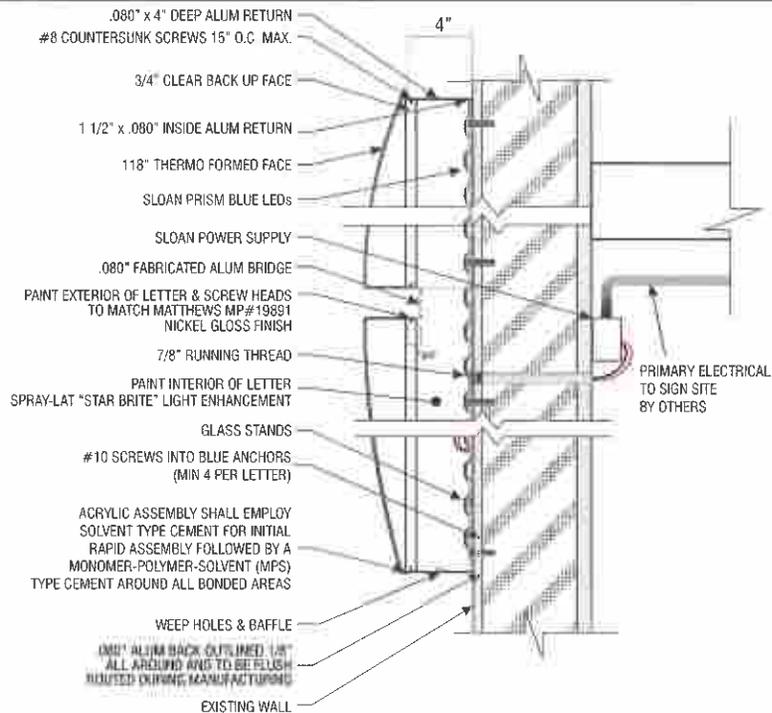
Project ID: CHASE_730_1

Revision: R13 - 01.19.21

ATTACHMENT METHOD TO BE VERIFIED BY INSTALLER AT THE FIELD DEPENDING ON WALL TYPE AND ACCESS.



1
15 PLEX LETTER SECTION DETAIL SCALE: 1 1/2" = 1'-0"



2
15 LOGO SECTION DETAIL SCALE: 1 1/2" = 1'-0"



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JP Morgan Chase Bank #730

Willowbrook
730 Plainfield Rd.
Willowbrook, IL 60527

Initial Date: 09/14/18
Salesperson: Arthur Navarro
Coordinator: Tracey Pichierri
Designer: blanes
Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
COPY, COLORS & SIZES

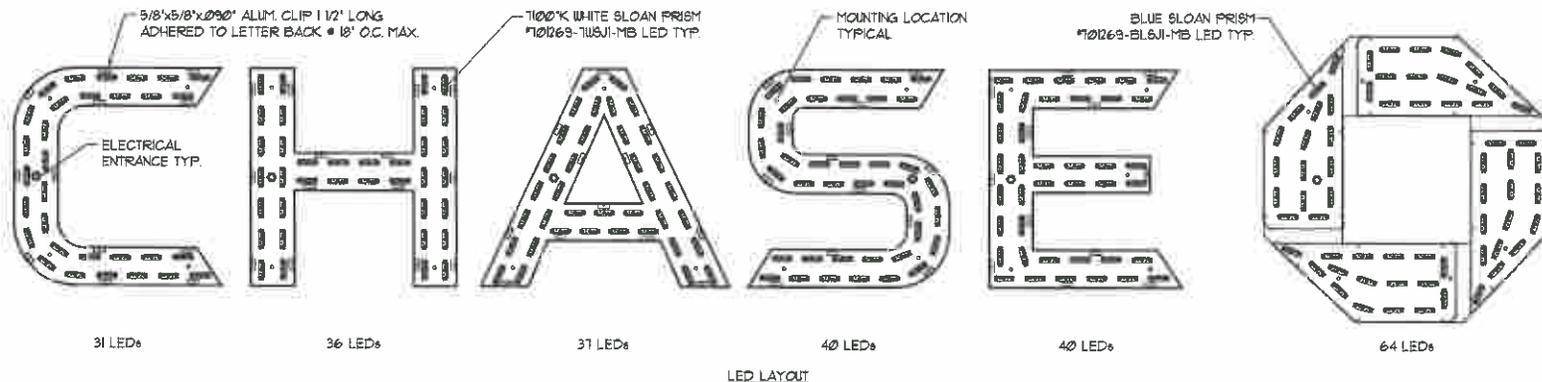
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Customer Signature _____ Date _____
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Drawing Number: 18-01613

Project ID: CHASE_730_1

Revision: R13 - 01.19.21

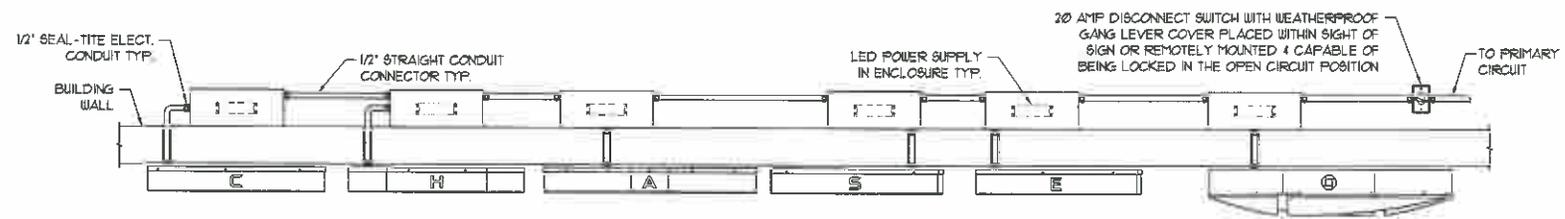


ELECTRICAL REQUIREMENTS:

LED_s: (124) 1100°K WHITE SLOAN PRISM #101269-T1W5J1-MB
 (64) BLUE SLOAN PRISM #101269-BL6J1-MB
 POWER SUPPLY: (6) SLOAN 6001 60W #101501-60C1 • 0.71A
 TOTAL LOAD: 420A • 120VAC
 CIRCUITS: (1) 20 AMP REQUIRED

ALL PRIMARY & SECONDARY WIRING MUST BE GROUNDED PER ALL APPLICABLE UL AND NEC GUIDELINES.

SEE SHEET 1 OF 1 FOR PREFERRED DISCONNECT SWITCH LOCATION FOR REMOTE, NON-PARAPET WALL INSTALLATION



WIRING DIAGRAM FOR REMOTE, NON-PARAPET WALL INSTALLATION



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JP Morgan Chase Bank #730

Willowbrook
 730 Plainfield Rd.
 Willowbrook, IL 60527

Initial Date: 09/14/18
 Salesperson: Arthur Navarro
 Coordinator: Tracey Robinson
 Designer: Ingrid
 Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
 COPY, COLORS & SIZES

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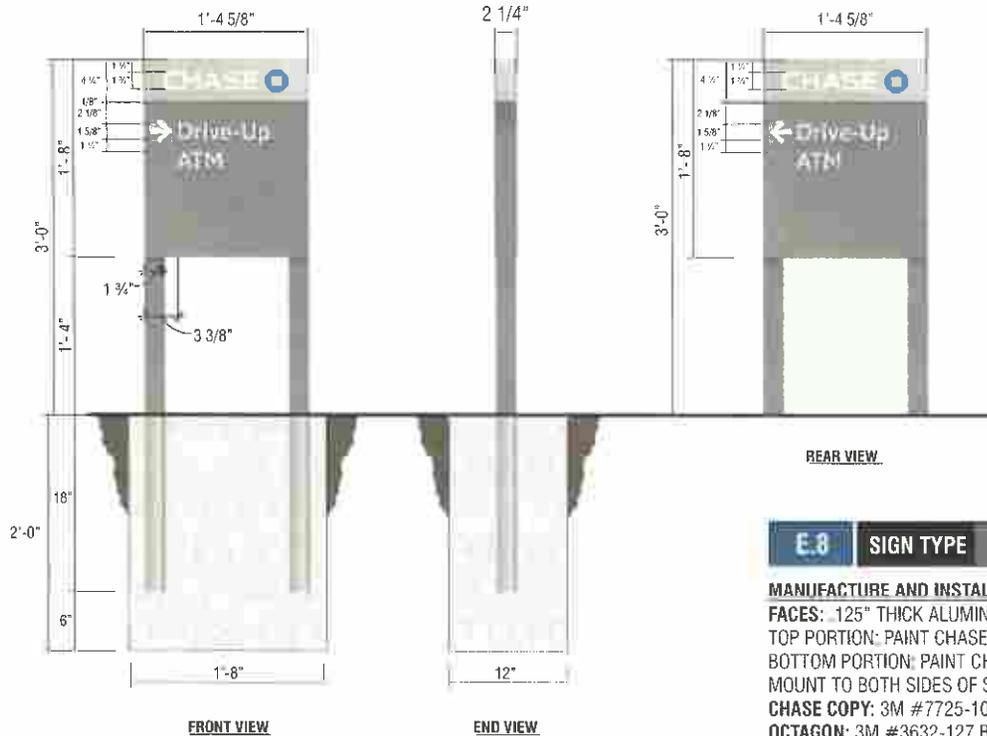
Drawing Number: 18-01613

Project ID: CHASE_730_1

Revision: R13 - 01.19.21



TOP VIEW SCALE: 1"=1'-0"



E-8 SIGN TYPE D-2-RE

MANUFACTURE AND INSTALL ONE (1) NON-ILLUMINATED DIRECTIONAL SIGN
FACES: 125" THICK ALUMINUM FACES, QUARTER ROUND EDGES.
TOP PORTION: PAINT CHASE NICKEL #19891, GLOSS FINISH.
BOTTOM PORTION: PAINT CHASE DARK NICKEL #18248, GLOSS FINISH.
MOUNT TO BOTH SIDES OF SQUARE TUBE POSTS.
CHASE COPY: 3M #7725-10 WHITE VINYL.
OCTAGON: 3M #3632-127 BLUE OVER #7725-10 WHITE.
SECONDARY COPY: TO BE 3M SCOTCHLITE #680-10 REFLECTIVE WHITE VINYL.
POSTS: 2" X 2" X .125" ALUMINUM RECTANGLE TUBE POSTS,
TOP PORTION: CONTINUOUS BAND TO BE PAINTED NICKEL #19891, GLOSS FINISH.
BOTTOM PORTION: PAINT CHASE DARK NICKEL #18248, GLOSS FINISH.
TOP CAP: .125" ALUMINUM PAINTED CHASE NICKEL #19891, GLOSS FINISH.
BASE: 125" SHEARED 1/4 ROUNDED ALL EDGES WELDED TO POST, PAINTED CHASE DARK NICKEL #18248 GLOSS FINISH.



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JP Morgan Chase Bank #730

Willowbrook
 730 Plainfield Rd.
 Willowbrook, IL 60527

Initial Date: 09/14/18
 Designer: Arthur Rowland
 Coordinator: Tracey Pichieri
 Designer: DODDS
 Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
 COPY, COLORS & SIZES

Signtech does NOT provide primary electrical to sign location. RESPONSIBILITY OF OTHERS!

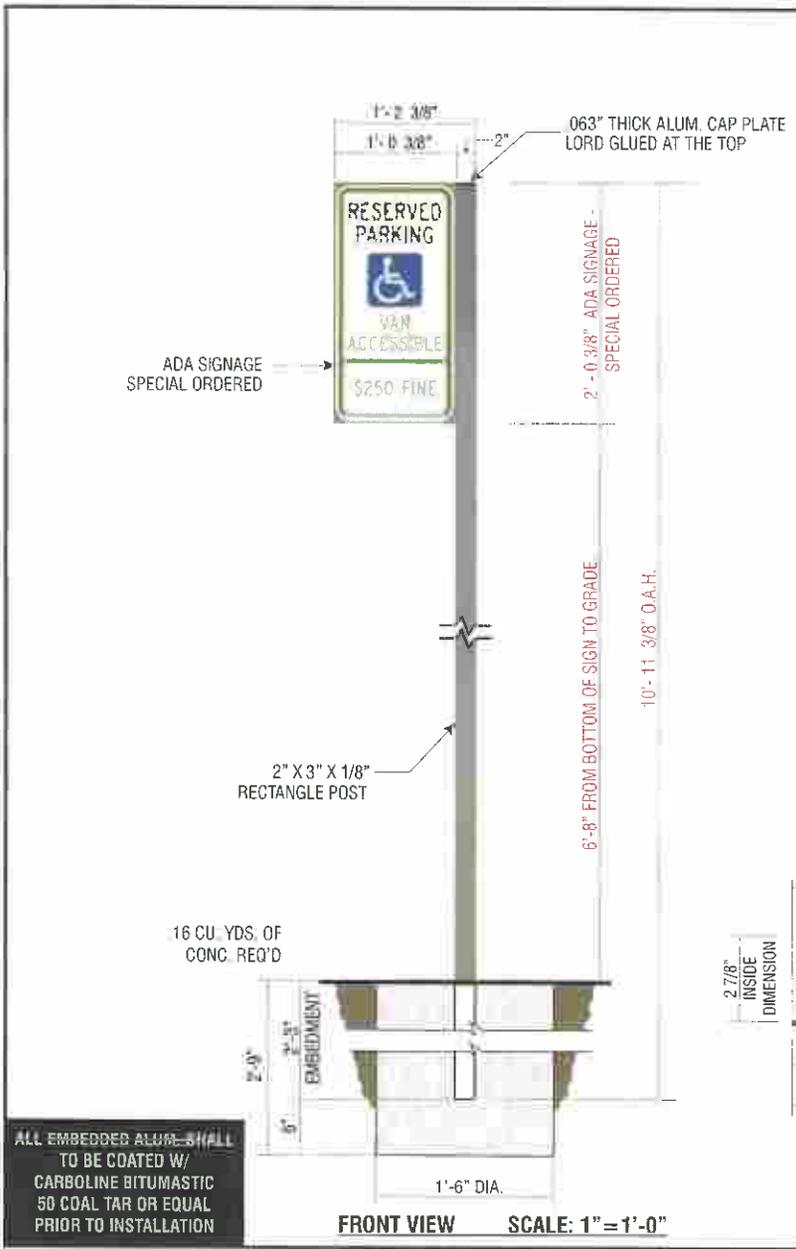
Customer Signature _____ Date _____

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Drawing Number: 18-01613

Project ID: CHASE_730_1

Revision: R13 - 01 19.21

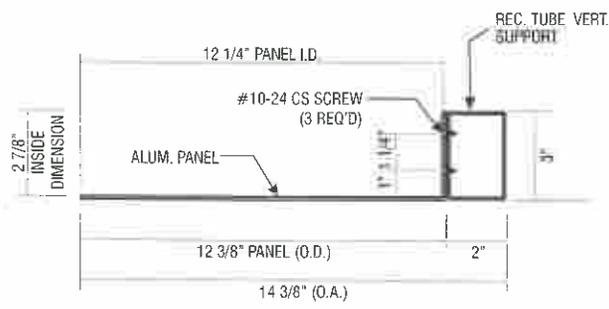


E.9

SIGN TYPE TC-P-ADA-IL-11-R6

MANUFACTURE AND INSTALL ONE (1) N-1 POLE MOUNTED PARKING SIGN

1. 125" BRAKE FORMED ALUMINUM SIGN FACE.
PAINT FINISH ALL EXPOSED SURFACES DARK NICKEL
MATTHEWS MP # 18248, GLOSS FINISH (85-90 UNITS).
FORM AS SHOWN.
2. SIGN FACE TO BE ORDERED BY "COMPLIANCE SIGNS" 800.578.1245
FACE TO BE LORDS GLUE TO 125" BRAKE FORM ALUM. SIGN FACE
3. 2" X 3" X 1/8" X 10'-11 3/8" LONG EXTRUDED ALUM. RECTANGULAR POST
W/ 063" THICK ALUM. CAP PLATE @ TOP.
PAINT FINISH ALL EXPOSED SURFACES TO MATCH
DARK NICKEL MATTHEWS MP # 18248, GLOSS FINISH (60-70 UNITS).
SET SUPPORT INTO CONCRETE FOUNDATION.
NO DIRECT EARTH BURIAL ALLOWED.



A HORIZONTAL SECTION
SCALE: 3" = 1'-0"



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JP Morgan Chase Bank #730

Willowbrook
730 Plainfield Rd.
Willowbrook, IL 60527

Initial Date: 09/14/18
Salesperson: Arthur Navarro
Coordinator: Tracey Pichlerri
Designer: HMM
Scale: As noted

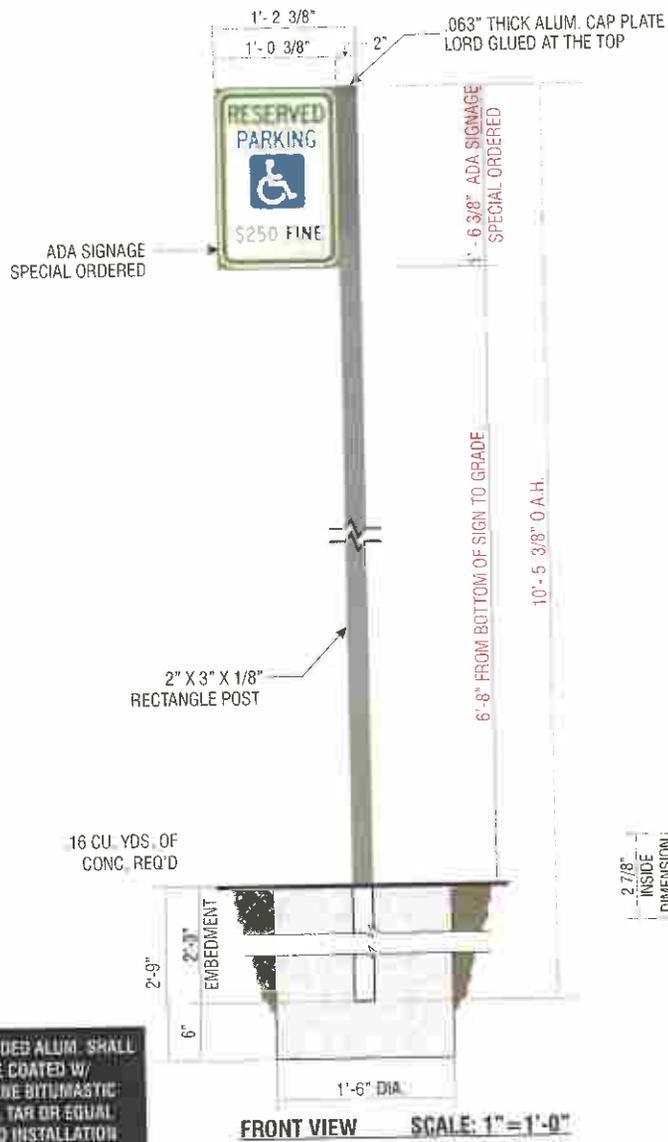
CUSTOMER APPROVAL

Customer Signature _____ Date _____
COPY, COLORS & SIZES

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electrical to sign location -
RESPONSIBILITY OF OTHERS!

Customer Signature _____ Date _____
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Drawing Number: 18-01613
Project ID: CHASE_730_1
Revision: R13 - 01 19 21



E.10

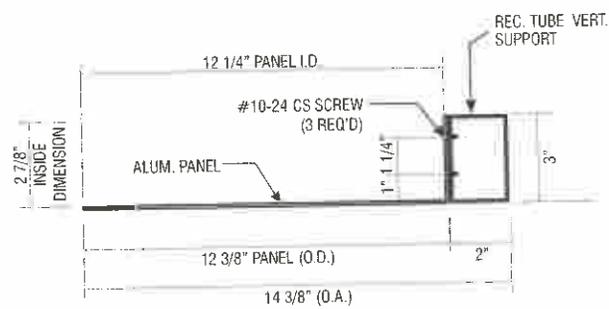
SIGN TYPE TC-P-ADA-IL-RE

MANUFACTURE AND INSTALL ONE (1) N-1 POLE MOUNTED PARKING SIGN

1. 125" BRAKE FORMED ALUMINUM SIGN FACE. PAINT FINISH ALL EXPOSED SURFACES DARK NICKEL MATTHEWS MP #18248, GLOSS FINISH (85-90 UNITS) FORM AS SHOWN.
2. SIGN FACE TO BE ORDERED BY "COMPLIANCE SIGNS" 800.578.1245 FACE TO BE LORDS GLUE TO .125" BRAKE FORM ALUM. SIGN FACE
3. 2" X 3" X 1/8" X 10'-5 3/8" LONG EXTRUDED ALUM. TRIANGULAR POST W/ .063" THICK ALUM. CAP PLATE @ TOP. PAINT FINISH ALL EXPOSED SURFACES TO MATCH DARK NICKEL MATTHEWS MP #18248, GLOSS FINISH (60-70 UNITS). SET SUPPORT INTO CONCRETE FOUNDATION. NO DIRECT EARTH BURIAL ALLOWED.



ALL FASTENERS SHALL BE STAINLESS STEEL OR OTHERWISE COATED TO PREVENT CORROSION.



A HORIZONTAL SECTION
SCALE: 3" = 1'-0"

ALL EMBEDDED ALUM. SHALL TO BE COATED W/ CARBOLINE BITUMASTIC 50 COAL TAR OR EQUAL PRIOR TO INSTALLATION



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Phone: (619) 527-6100 / Fax: (619) 527-6111
signtech.com



JP Morgan Chase Bank #730

Willowbrook
730 Plainfield Rd.
Willowbrook, IL 60527

Initial Date: 09/14/18
Sponsor: Arthur Novatis
Coordinator: Tracey Picchetti
Designer: biondes
Scale: As noted

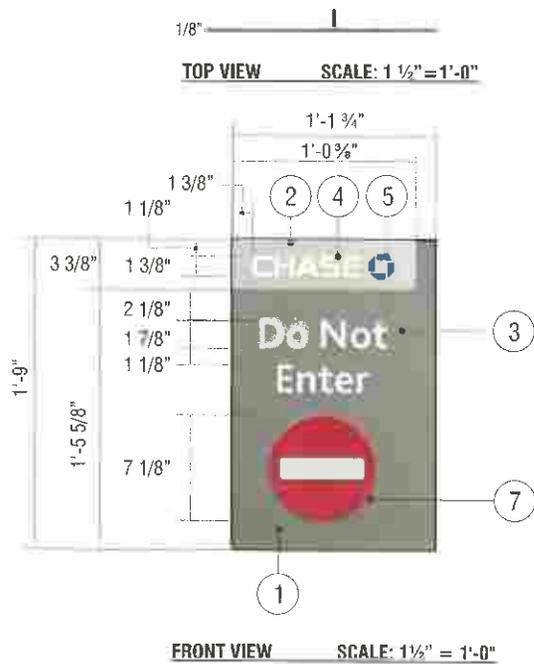
CUSTOMER APPROVAL

Customer Signature _____ Date _____
COPY, COLORS & SIZES

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Customer Signature _____ Date _____
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Drawing Number: 18-01613
Project ID: CHASE_730_1
Revision: R13 - 01.19.21



E.11 E.12

SIGN TYPE TC-W-H-RE

MANUFACTURE AND INSTALL TWO (2) NON-ILLUMINATED PARKING SIGNS

1. 125" ALUMINUM SIGN FACE. PAINT FINISH ALL EXPOSED SURFACES DK NICKEL. MATTHEWS MP#18248, GLOSS FINISH. FORM AS SHOWN.
2. TOP BAND TO BE FIRST SURFACE APPLIED. 3M #180C-220 LIGHT SILVER METALLIC VINYL.
3. SUPPORT COPY TO BE 3M SCOTCHLITE #680-10 WHITE.
4. "CHASE" TO BE 3M FILM #7725-10 WHITE.
5. OCTAGON TO BE 3M FILM #3632-127 BLUE OVER 3M FILM #7725-10 WHITE.
6. PROVIDE NELSON STUDS OFF BACK SIDE OF ALUM. TO ANCHOR TO WALL SURFACE
7. FIRST SURFACE DECORATE "DO NOT ENTER" SYMBOL WITH 3M SCOTCHLITE REFLECTIVE FILM #680-72 RED. HORIZONTAL BAR TO BE 3M SCOTCHLITE FILM #680-10 WHITE.



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JP Morgan Chase Bank #730

Willowbrook
 730 Plainfield Rd.
 Willowbrook, IL 60527

Initial Date: 09/14/18
 Salesperson: Arthur Navarro
 Coordinator: Tracey Mulholland
 Designer: bjones
 Scale: As noted

CUSTOMER APPROVAL

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COPY, COLORS & SIZES

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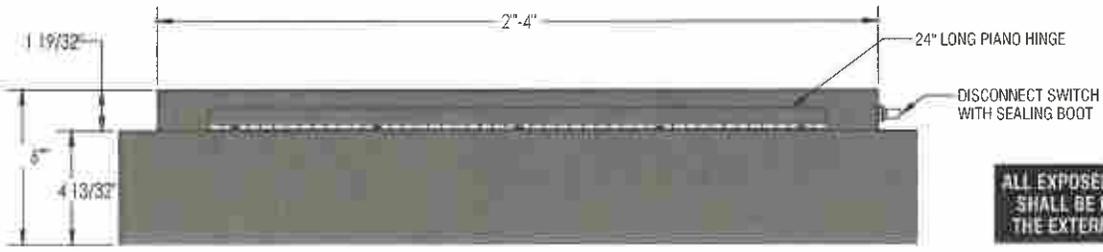
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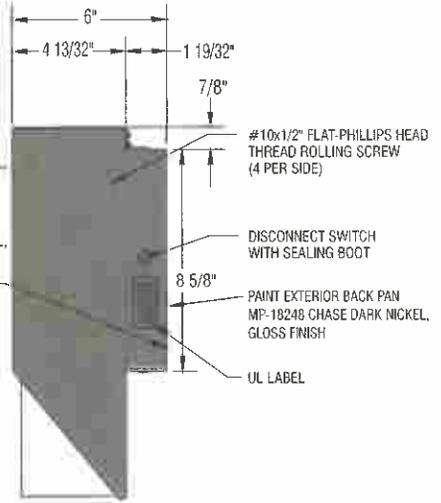
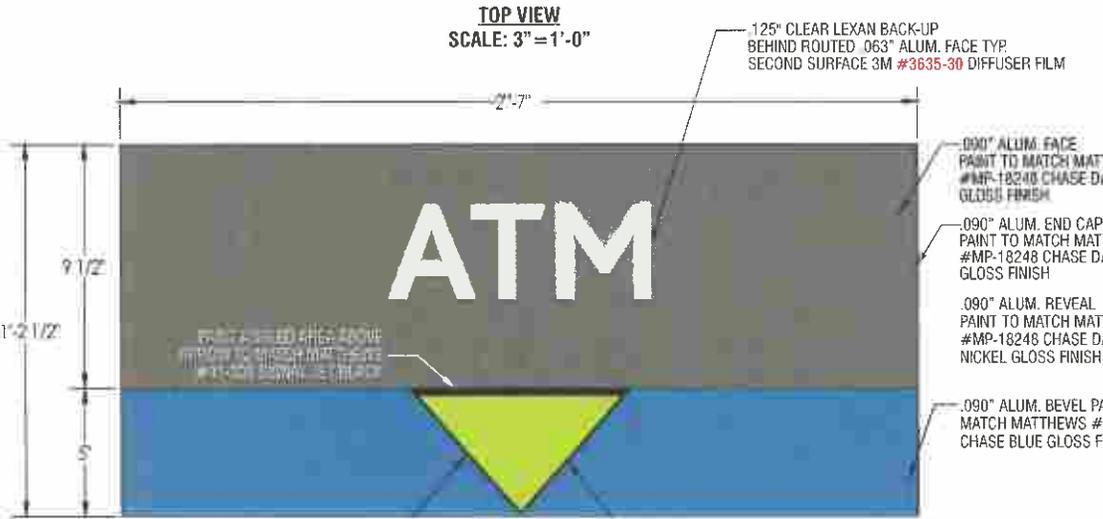
Revision: R13 - 01 19.21

ADD WEEP HOLES TO THE SIGN



LANE INDICATOR ILLUMINATION GUIDELINES
 WHITE COPY (ATM)
 ILLUMINATES 24 HOURS A DAY, 7 DAYS A WEEK
 ARROW
 ILLUMINATES 24 HOURS A DAY, 5 DAYS A WEEK,
 BRANCH TO SWITCH BETWEEN RED & GREEN
 AS NECESSARY

ALL EXPOSED FASTENERS HEADS SHALL BE PAINTED TO MATCH THE EXTERIOR CABINET FINISH
INTERIOR SURFACES SHALL BE PAINTED SPRAYLAT STARBRITE WHITE UNLESS OTHERWISE NOTED



ALL EXPOSED PAINTED SURFACES SHALL BE COATED WITH MATTHEWS #282-208SP VOC GLOSS CLEAR, WITH MINIMUM 2 MILS DRY FILM THICKNESS (DFT) PER MATTHEWS APPLICATION SPECIFICATIONS.

END VIEW SCALE: 3" = 1'-0"

FRONT VIEW SCALE: 3" = 1'-0"

- NOTES:**
- Design is based on 90 mph 3 sec gust design wind speed per IBC 2006, Exposure C. Refer to structural drawings.
 - Aluminum shapes be extruded from 6061-T6 alloy. Aluminum sheet shall be 3003-H14 alloy. Aluminum plate shall be 5052-H34 alloy.
 - Structural bolts shall be zinc coated A325 unless otherwise noted. All other fasteners shall be stainless steel or otherwise coated to prevent corrosion.
 - Welds shall be made a 4000 series for aluminum by persons qualified in accordance with AWS standards within the past two years.
 - This design is prototypical and should not be used for site specific applications unless deemed suitable by a competent Professional Engineer.

E.14 E.15 SIGN TYPE DU-C

MANUFACTURE & INSTALL TWO (2) INTERNALLY-ILLUMINATED WALL SIGNS w/ ROUT-OUT FACES

10/22/18 TRR K



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 Coordinator: Tracey Pichierri
 Designer: NCOOM
 Scale: As noted

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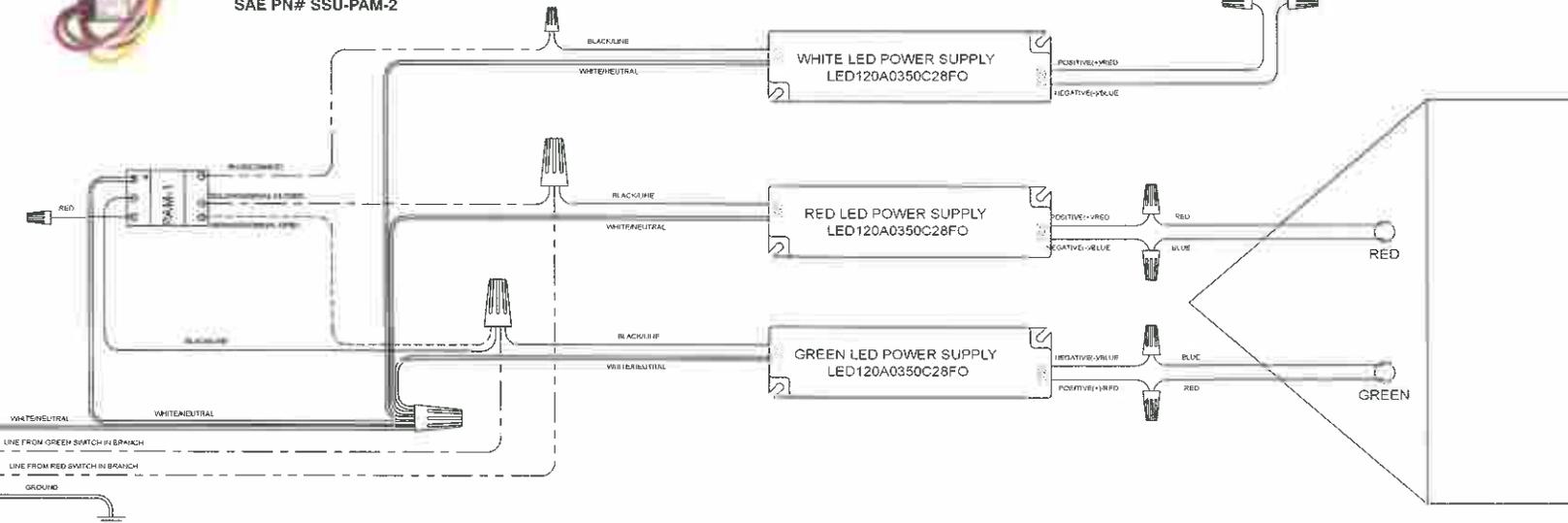
Revision: R13 = 01.19.21

LANE DESIGNATOR WIRING DIAGRAM

Q = 21505
L = 249



SSU-PAM-2 RELAY PROVIDES 7.0 AMP FORM C CONTACTS. THE RELAY MAY BE ENERGIZED BY ONE OF TWO INPUT VOLTAGES: 12VDC OR 24VDC. THE INPUT VOLTAGES ARE POLARITY SENSITIVE AND DIODE PROTECTED. A RED LED IS PROVIDED WHICH, WHEN ILLUMINATED, INDICATES THE RELAY COIL IS ENERGIZED.
SAE PN# SSU-PAM-2



ACILIGHT **SLIMLINE 15W 12V Power Supply**



Product information
Part Number: PS12-SL-15W-12V-277V

ELECTRICAL REQUIREMENTS

- LEDS COPY:** (1) AGILIGHT LED PANEL PRE-POPULATED WITH WHITE LEDES FOR "ATM", "BUSINESS", & "DRIVE-UP"
- LED ARROW:** (1) AGILIGHT LED PANEL PRE-POPULATED WITH RED & GREEN LEDES
- POWER SUPPLY:** (3) AGILIGHT SLIM-LINE PS12-SL-15W-120-277V @ 0.20A
- RELAY:** (1) PAM-1 RELAY @ 0.015 AMPS PER POSITION
- SWITCH:** (1) TOGGLE SWITCH
- TOTAL LOAD:** 2.1 AMPS
- CIRCUITS:** (1) 20 AMP REQ'D.

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CUSTOMER APPROVAL
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COPY COLORS & SIZES

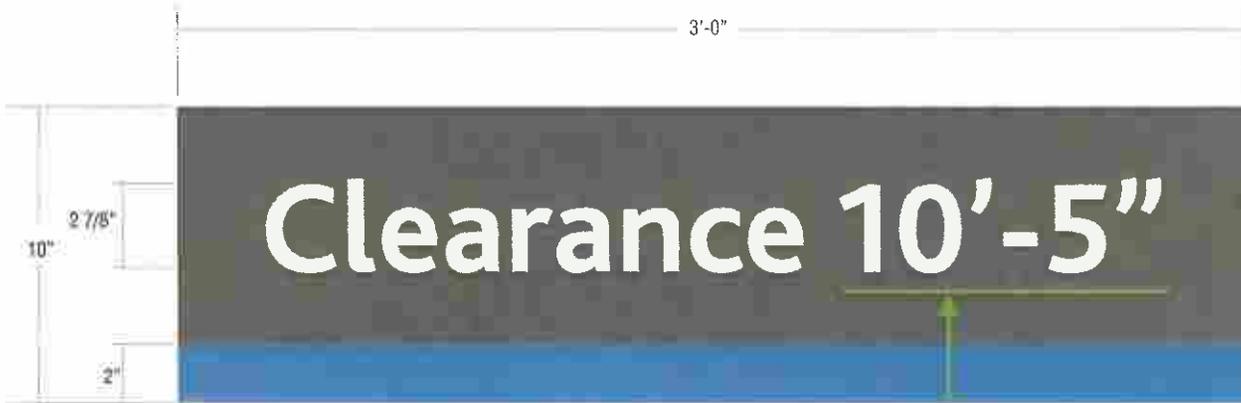
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Revision: R13 - 01.19.21



TOP VIEW SCALE: 3" = 1'-0"



FRONT VIEW SCALE: 3" = 1'-0"



END VIEW SCALE: 3" = 1'-0"

VERIFY HEIGHT DIMENSION
PER SITE SURVEY

E.16

SIGN TYPE TC-CL-W

MANUFACTURE AND INSTALL ONE (1) NON-ILLUMINATED S/E CLEARANCE SIGN
FACE: 125" ALUMINUM PAINTED CHASE DARK NICKEL MATTHEWS MP#18248, GLOSS FINISH.
 PAINT ACCENT BAND ACROSS BOTTOM CHASE BLUE MP#00366 BLUE (OR PMS #300 BLUE).
"CLEARANCE": COPY TO BE 3M SCOTCHLITE #680-10 WHITE.
INSTALL: PROVIDE NELSON STUDS OFF BACK SIDE OF ALUM. TO ANCHOR TO WALL SURFACE



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 Designer: Hirono
 Scale: As noted

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 COPY, COLORS & SIZES

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E.17

SIGN TYPE CHA-TEMP-CS-48X96-SKID

MANUFACTURE ONE (1) D/F CONSTRUCTION / TEMPORARY SITE SIGN

FACE: 4' x 8' x 1/2" MDO PLYWOOD, PRIMED AND PAINTED CHASE DARK NICKEL #MP18248 OVERLAY WITH DIGITALLY PRINTED GRAPHIC.

FRAME: 2" x 4" D.F. WOOD FRAMEWORK WITH 3/8" DEEP 9/16" DADO GROOVES FOR FACE TO LOCK INTO. SEE DRAWING ON FOLLOWING PAGES FOR WHICH PARTS. PAINT CHASE DARK NICKEL #MP18248

FRAME ATTACHMENT: USE 3/8" DIA. S.S. MACHINE BOLT FOR BASE ATTACHMENT USE LAG SCREWS TO ATTACH FRAMING TOGETHER.

SAND BAGS: (4) ± 40LB SAND BAGS AT EACH BASE CORNER AS SHOWN SIGN INSTALLER TO SUPPLY SAND BAGS.

ARTWORK PATH: P:\Design\2K20\Chase Bank\Typicals\COMING SOON - WE'RE OPEN Construction Sign\Final CSS 8x4.cdr



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Designer: JIMMIE
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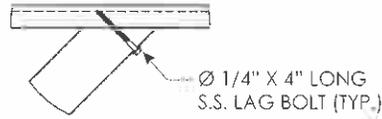
Revision: R13 - 01 19.21

ITEM NO.	QTY.	DESCRIPTION	LENGTH
1	4	2x4	84
2	2	2x4	84
3	2	2x4	95.25
4	2	2x4 R	42.78
5	4	2x4 R	47.73

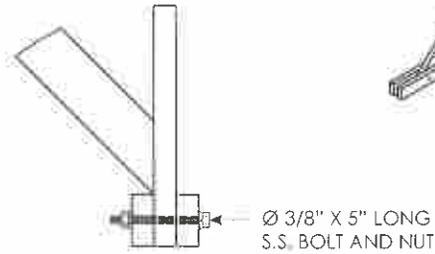
SIGN TYPE COMING SOON TEMP SIGN



SECTION A-A

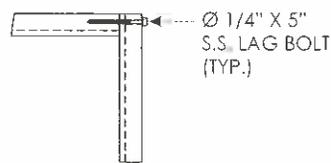
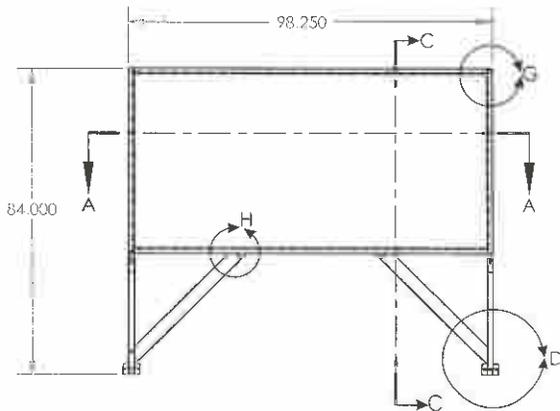
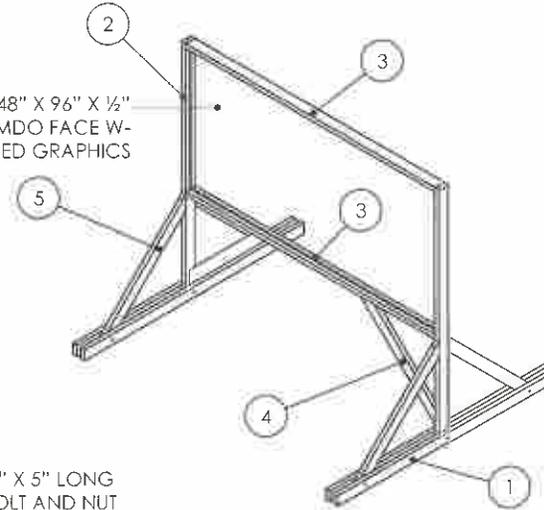


DETAIL H
SCALE 1 : 8



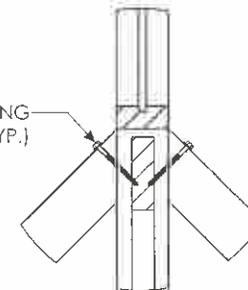
DETAIL D
SCALE 1 : 8

48" X 96" X 1/2"
MDO FACE W-
PRINTED GRAPHICS

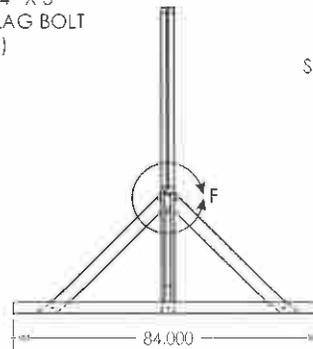


DETAIL G
SCALE 1 : 8

Ø 1/4" X 4" LONG
S.S. LAG BOLT (TYP.)



DETAIL F
SCALE 1 : 8



SECTION C-C



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**JP Morgan Chase Bank
#730**

Willowbrook
730 Plainfield Rd.
Willowbrook, IL 60527

Initial Date: 09/14/18
Salesperson: Arthur Navarro
Coordinator: Tracie Pichem
Designer: [redacted]
Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
COPY, COLORS & SIZES

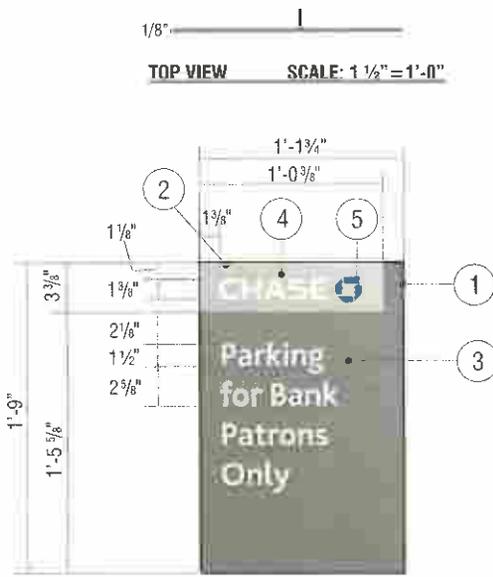
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1/8" **TOP VIEW SCALE: 1 1/2" = 1'-0"**

FRONT VIEW SCALE: 1 1/2" = 1'-0"



END VIEW SCALE: 3" = 1'-0"

E.18 E.19 E.20

SIGN TYPE TC-W-A-RE PARKING FOR BANK PATRONS ONLY

MANUFACTURE AND INSTALL THREE (3) NON-ILLUMINATED CUSTOM MESSAGE SIGNS

1. 125" ALUMINUM SIGN FACE.
PAINT FINISH ALL EXPOSED SURFACES MATTHEWS MP#18248, DARK NICKEL GLOSS FINISH.
2. TOP BAND TO BE FIRST SURFACE APPLIED
3M #180C-220 LIGHT SILVER METALLIC VINYL.
3. SUPPORT COPY TO BE 3M SCOTCHLITE #680-10 WHITE.
4. "CHASE" TO BE 3M FILM #7725-10 WHITE.
5. OCTAGON TO BE 3M FILM #3632-127 BLUE OVER
3M FILM #7725-10 WHITE
6. PROVIDE NELSON STUDS OFF BACK SIDE OF ALUM. TO ANCHOR TO COLUMN.



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JP Morgan Chase Bank #730

Willowbrook
730 Plainfield Rd.
Willowbrook, IL 60527

Initial Date: 09/14/18
Salesperson: Arthur Navarro
Coordinator: Tracy Eckhart
Designer: Gordon
Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
COPY COLORS & SIZES

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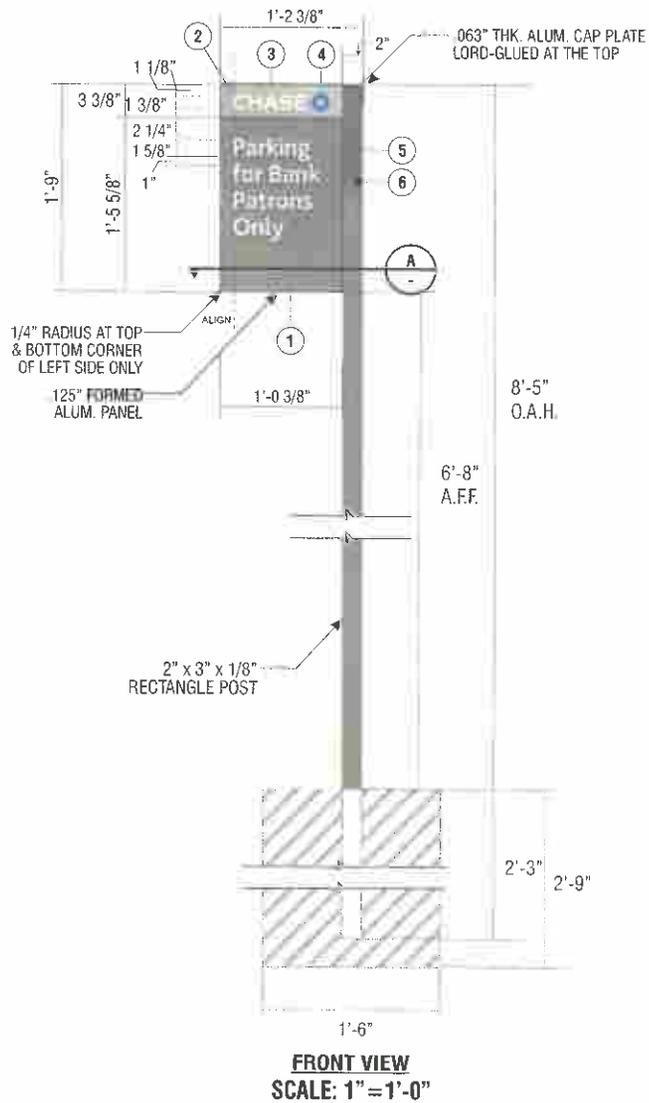
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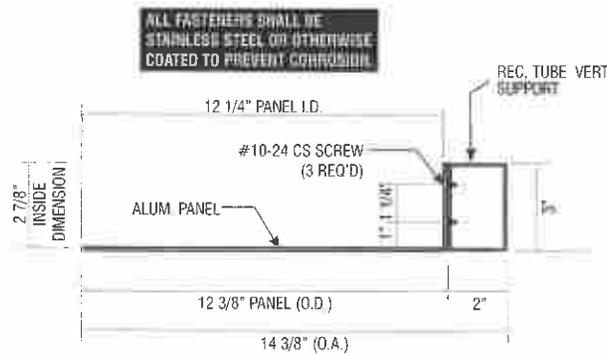


E.21	E.22	E.23	E.24
E.25	E.26	E.27	E.28

SIGN TYPE TC-P-A-RE

MANUFACTURE AND INSTALL EIGHT (8) NON-ILLUMINATED PARKING SIGNS

1. 125" BREAK-FORMED ALUMINUM SIGN FACE.
PAINT FINISH ALL EXPOSED SURFACES DARK NICKEL MATTHEWS MP #18248, GLOSS FINISH FORM AS SHOWN.
2. TOP BAND - FIRST SURFACE APPLIED - 3M #180C-220 LIGHT SILVER METALLIC VINYL.
3. "CHASE" - 3M FILM #7725-10 WHITE VINYL.
4. OCTAGON - 3M FILM #3632-127 CHASE BLUE VINYL.
5. SUPPORT COPY TO BE 3M SCOTCHLITE #680-10 REFLECTIVE WHITE
6. 2" X 3" X 1/8" X 128" LONG RECTANGLE ALUM. POST
W/ .063" THICK ALUM. CAP PLATE @ TOP.



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Initial Date: 09/14/18
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Designer: HERR
Scale: As noted

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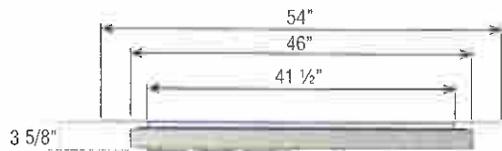
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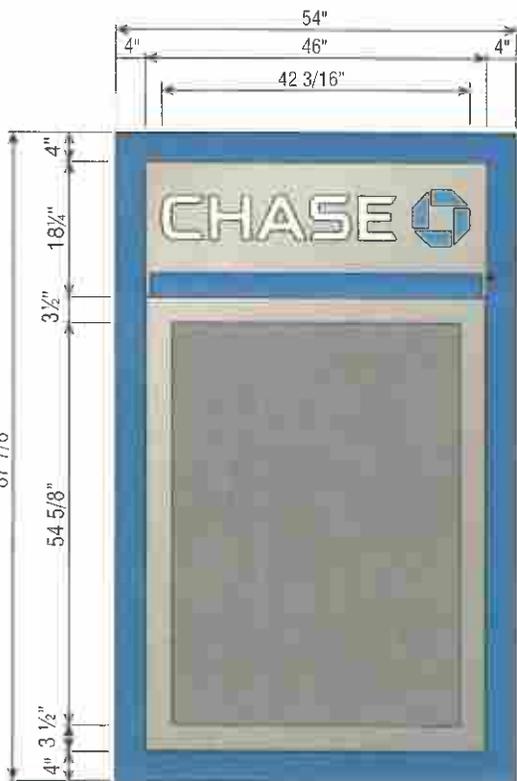
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TOP VIEW SUR-TTW-U-4
SCALE: 3/4" = 1'-0"



FRONT VIEW SCALE: 3/4" = 1'-0"

STIPPLE FINISH PAINT PROCESSES FOR MATTHEWS JPM-7501 CHASE MODEL:
BASE COAT: 3-4 MEDIUM COATS OF MAF 5000077 FOR BASE COAT.
ALLOW TO AIR DRY 12-24 HRS ON FRESH SUR AT 500 DEG. F FOR 1 HRS.
STIPPLE FINISH COAT: MAF 5000078 WITH CATALYST BUT. TWO REDUCED.
GET PRESSURE POT AT 18-20 PSI WITH PRESSURE AND
20-25 PSI ATOMIZING AIR AT THE PRINT GUN.
USE 1/8 TO 1/2 FLAT NOZZLE.
APPLY 3-4 INCHES OF A FINE TIGHT STIPPLE.

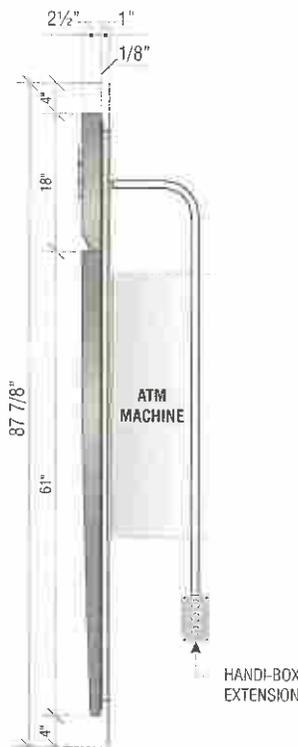
STIPPLE FINISH PAINT PROCESSES FOR MATTHEWS JPM-7501 CHASE BANK NETWORK:
BASE COAT: 3-4 MEDIUM COATS OF MAF 5000077 FOR BASE COAT.
ALLOW TO AIR DRY 12-24 HRS ON FRESH SUR AT 500 DEG. F FOR 1 HRS.
STIPPLE FINISH COAT: MAF 5000078 WITH CATALYST BUT. TWO REDUCED.
GET PRESSURE POT AT 18-20 PSI WITH PRESSURE AND
20-25 PSI ATOMIZING AIR AT THE PRINT GUN.
USE 1/8 TO 1/2 FLAT NOZZLE.
APPLY 3-4 INCHES OF A FINE TIGHT STIPPLE.

NOTE: PUSH-THRU ACRYLIC SHALL HAVE
A MILL FINISH ON ALL ROUNDED EDGES

ALL EXPOSED FASTENER HEADS
SHALL BE PAINTED TO MATCH THE
EXTERIOR CABINET FINISH

SLOAN BLUE LED STRIPE AROUND
PERIMETER TO PROVIDE HALO-LIT
EFFECT AGAINST BACK PANEL

GRAPHICS DEPICTED ARE FOR
ILLUSTRATIVE PURPOSES ONLY
USE ONLY APPROVED ARTWORK
FOR PRODUCTION



SIDE VIEW



PERSPECTIVE VIEW

E.29 I.30 SIGN TYPE SUR-TTW-U-4-TP

MANUFACTURE AND INSTALL TWO (2) INTERNALLY ILLUMINATED THIN PROFILE ATM SURROUNDS



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ColorLINE

The Ultimate LED Border Tube



- Super Bright
- Profile Similar to Neon
- Lit, Mitered Corners
- Fully Compatible with All SloanLED 12 VDC Products

COLORLINE SIZES AND CUT LENGTHS:

- (3) EA. #701800-COLOR-8 (96 1/2" LONG)
- MAKE VERTICAL TUBES FROM (2) 96 1/2" PARTS.
- MAKE HORIZONTAL TUBES FROM (1) 96 1/2" PART.
- EACH PART MUST HAVE A FACTORY RUBBER END.
- ONCE PLASTIC CAP IS ATTACHED TO THE CUT SIDE, THE TUBE WILL FINISH AT **77 5/32"** FOR VERTICAL IN LENGTH INCLUDING CAP
- THE TUBE WILL FINISH AT **45 3/16"** FOR HORIZONTAL IN LENGTH INCLUDING CAP

Item Description	Part # (Tube)	Actual Tube Length	Cut Increments	Power per Foot (meter)	LEDs per Foot (meter)	Limited Warranty
2' Tube	701800-Color-8	84-1/2" (215cm)	2-1/4" (6cm)	2.20W (7.5W)	20 (66)	Warm Colors: 5 Year
4' Tube	701800-Color-4	42-1/4" (108cm)	2-1/4" (6cm)	2.20W (7.5W)	20 (66)	
6' Tube	701800-Color-2	21-1/4" (54cm)	2-1/4" (6cm)	2.20W (7.5W)	20 (66)	
S' Tube	701800-Color-0	96 1/2" (245cm)	2-1/4" (6cm)	2.20W (7.5W)	20 (66)	Cool Colors: 3 Year

~~Warm Colors: Red, Orange, Yellow~~ ~~Cool Colors: Blue, Green, White~~

Feet (Meters) per 12VDC Power Supply

SloanLED Power Supply	Self Contained	Modular	Quad
	20	60	240
ColorLINE 2 Feet (Meters)	9 (2.7)	24 (7.5)	96 (30) 24 (7.5) per leg



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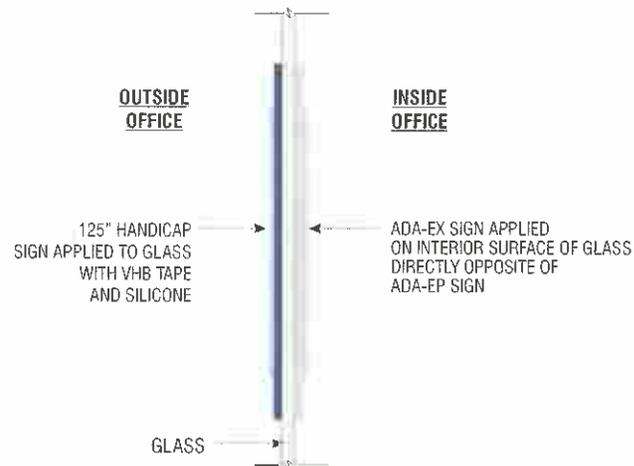
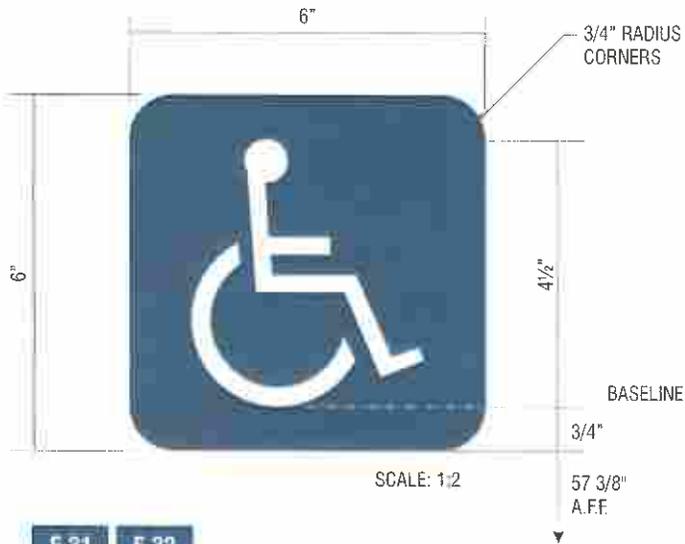
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E.31 E.32

SIGN TYPE ADA-EP ACCESSIBLE BUILDING ENTRANCE - A02

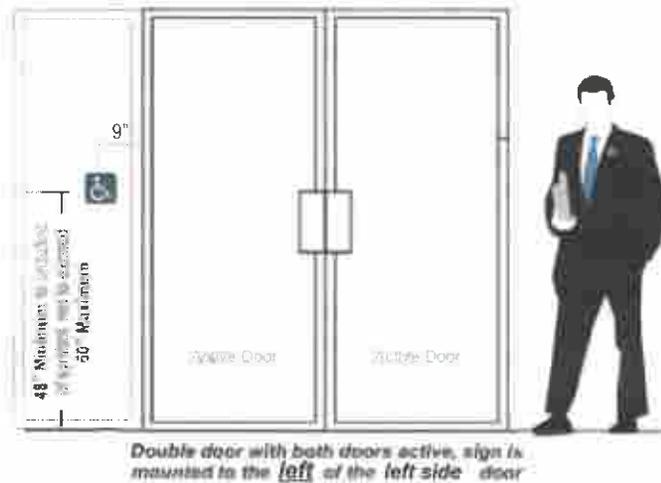
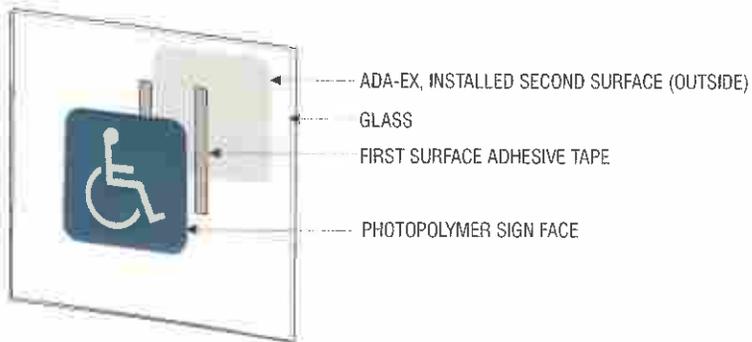
MANUFACTURE AND INSTALL TWO (2) ADA STANDARD ENTRANCE PLAQUES

FACE: 1/8" BLUE GRAVOTAC #341-501V, SATIN NON-GLARE FINISH.

ISA SYMBOL: 1/32" RAISED, BRIGHT WHITE GRAVOTAC.

BRILLE: 1/32" RAISED CLEAR BEAD, MAXIMUM 1/16" DIA.

INSTALL: MOUNT TO LATCH SIDE OF DOOR WITH VHB TAPE AND SILICONE.



Signtech

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 Phone: (619) 527-6100 / Fax: (619) 527-6111
 signtech.com



JP Morgan Chase Bank #730

Willowbrook
 730 Plainfield Rd.
 Willowbrook, IL 60527

Initial Date: 09/14/18
 Salesperson: Arthur Favaris
 Coordinator: Tracy Pichard
 Designer: Moses
 Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
COPY, COLORS & SIZES

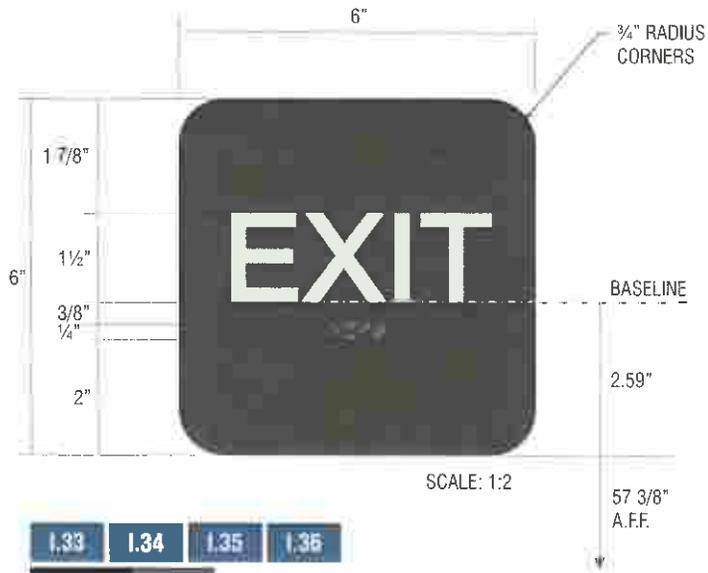
Signtech does NOT provide primary electrical to sign location - **RESPONSIBILITY OF OTHERS!**

Customer Signature _____ Date _____
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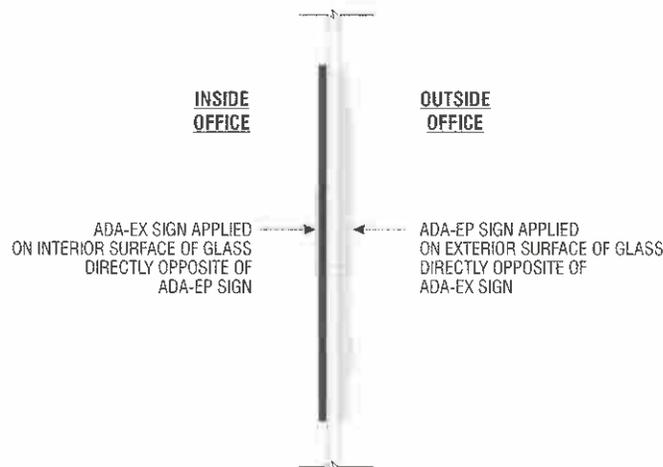
Drawing Number: 18-01613

Project ID: CHASE_730_1

Revision: R13 - 01 19.21



SCALE: 1:2

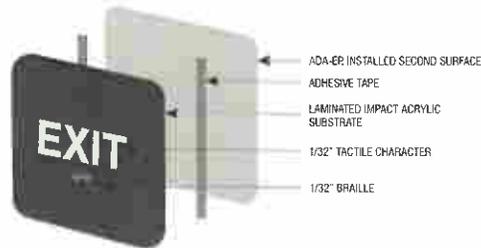


1.33 1.34 1.35 1.36

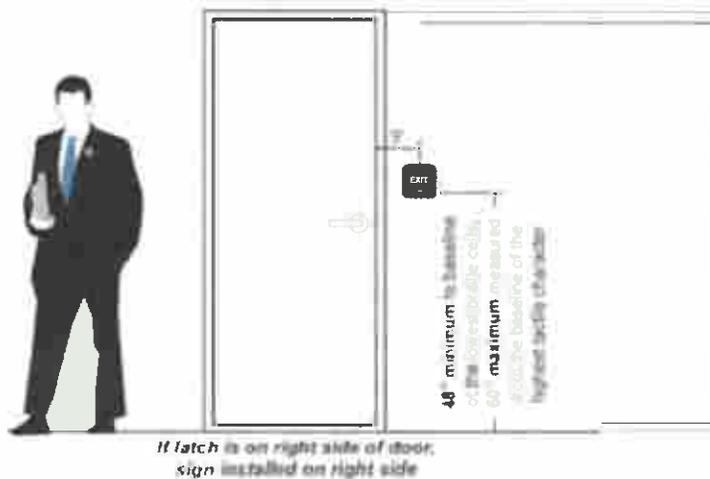
SIGN TYPE ADA-EX EXIT DOOR SIGNAGE - A13

MANUFACTURE AND INSTALL FOUR (4) ADA STANDARD EXIT PLACARDS

FACE: 1/8" GRAVOTAC CHARCOAL GREY, SATIN NON-GLARE FINISH.
COPY: 1/32" RAISED, BRIGHT WHITE GRAVOTAC, HELVETICA REGULAR TT FONT.
BRILLE: 1/32" RAISED CLEAR BEAD, MAXIMUM 1/16" DIA.
INSTALL: MOUNT TO LATCH SIDE OF DOOR WITH VHB TAPE AND SILCONE.



TYPICAL FABRICATION DETAIL NTS



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JP Morgan Chase Bank #730

Willowbrook
 730 Plainfield Rd.
 Willowbrook, IL 60527

Initial Date: 09/14/18
 Salesperson: Arthur Navarro
 Coordinator: Tracey Pichierri
 Designer: Jines
 Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
 COPY, COLORS & SIZES

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Customer Signature _____ Date _____
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Drawing Number: 18-01613
 Project ID: CHASE_730_1
 Revision: R13 - 01.19.21



FRONT VIEW

SCALE: 3/8" = 1"



40" MIN.
A.F.F
FLOOR TO
H.C. SYMBOL
BASELINE

TYPICAL INSTALLATION - NO BULLET-RESISTANT GLASS SCALE: 1/2" = 1'-0"

1.37

SIGN TYPE **ADA-TW** ACCESSIBLE TELLER WINDOW - A14

MANUFACTURE AND INSTALL ONE (1) ADA ACCESSIBILITY PLAQUE

FACE: 1/8" BLUE GRAVOTAC #341-501V, SATIN NON-GLARE FINISH.

ISA SYMBOL: 1/32" RAISED, BRIGHT WHITE GRAVOTAC.

INSTALL: MOUNT AT ACCESSIBLE TELLER LINE, AS SHOWN, WITH VHB TAPE AND SILCONE.



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**JP Morgan Chase Bank
#730**

Willowbrook
730 Plainfield Rd.
Willowbrook, IL 60527

Initial Date: 09/14/18
Salesperson: Arthur Navarro
Coordinator: Tracey Pichierri
Designer: [Signature]
Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____

COPY, COLORS & SIZES

Signtech does NOT provide primary
electrical to sign location -
RESPONSIBILITY OF OTHERS!

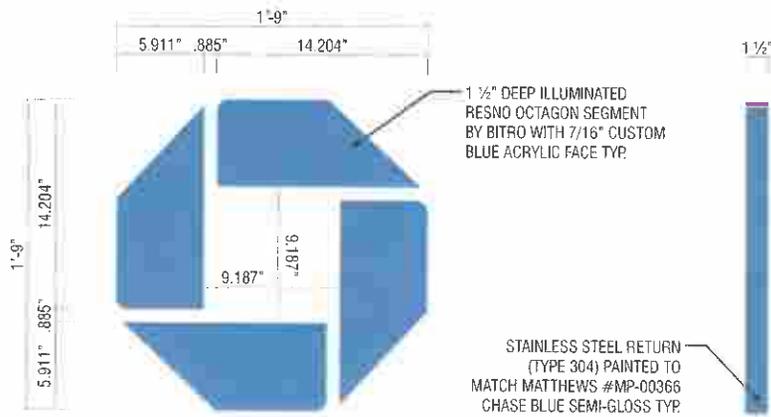
Customer Signature _____ Date _____

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Drawing Number: 18-01613

Project ID: CHASE_730_1

Revision: R13 - 01.19.21



FRONT VIEW SCALE: 1 1/2" = 1'-0"

SIDE VIEW SCALE: 1 1/2" = 1'-0"

I.38

SIGN TYPE OCT-21-INT BLUE BITRO

MANUFACTURE AND INSTALL ONE (1) INTERNALLY-ILLUMINATED OCTAGON

GENERAL NOTES:

- DESIGN IS BASED ON A 5 PSF LATERAL LOAD FOR INTERIOR APPLICATIONS PER IBC 2015 SEISMIC DESIGN CATEGORY D
- THE CUSTOMER'S BUILDING ENGINEER IS TO DETERMINE THE ADEQUACY OF THE SUPPORTING STRUCTURE.
- ALL FASTENERS SHALL BE STAINLESS STEEL OR ZINC COATED TO PREVENT CORROSION.
- J.P. MORGAN CHASE WILL NOT BE RESPONSIBLE FOR THE SAFETY ON THIS JOB SITE BEFORE, DURING OR AFTER INSTALLATION OF THIS STRUCTURE. IT IS THE RESPONSIBILITY OF THE CONTRACTORS AND INSTALLERS TO ENSURE THAT THE INSTALLATION AND ERECTION OF THIS STRUCTURE IS PERFORMED USING METHODS THAT ARE IN FULL COMPLIANCE WITH OSHA REGULATIONS.
- ANY DEVIATION FROM THIS DESIGN OR FROM ANY PART OF THIS DRAWING, INCLUDING THE GENERAL NOTES, WITHOUT PRIOR WRITTEN CONSENT FROM J.P. MORGAN CHASE VOIDS THIS DRAWING IN ITS ENTIRETY.
- THE STRUCTURE DESIGNED ON THIS DRAWING IS PROTOTYPICAL AND SHOULD NOT BE USED FOR SITE SPECIFIC APPLICATIONS UNLESS DEEMED SUITABLE BY A COMPETENT PROFESSIONAL ENGINEER.



TYPICAL INTERIOR ELEVATION NOT TO SCALE



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JP Morgan Chase Bank #730

Willowbrook
730 Plainfield Rd.
Willowbrook, IL 60527

Initial Date: 09/14/18
Salesperson: Arthur Navarro
Coordinator: Tracey Pichierni
Designer: J. J. J. J.
Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
COPY, COLORS & SIZES

Signtech does NOT provide primary electrical to sign location - **RESPONSIBILITY OF OTHERS!**

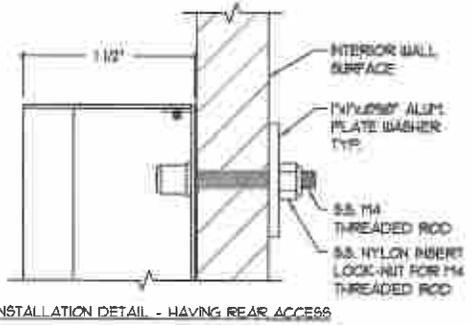
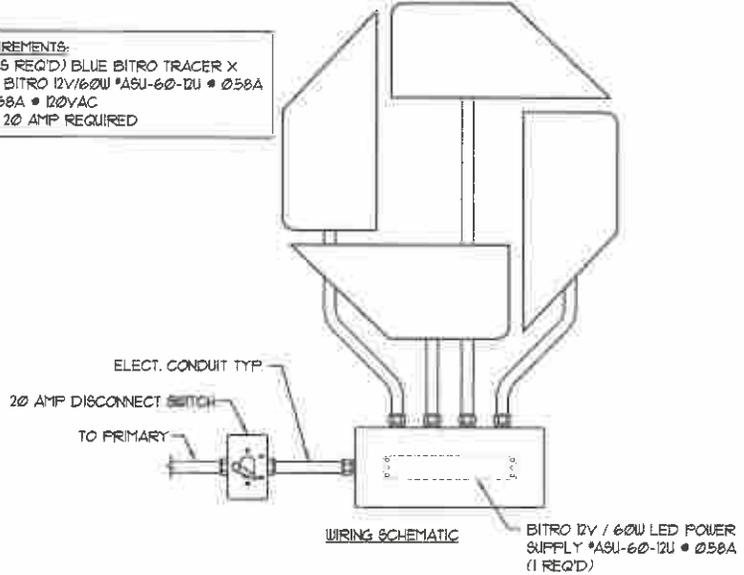
Customer Signature _____ Date _____
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Drawing Number: 18-01613

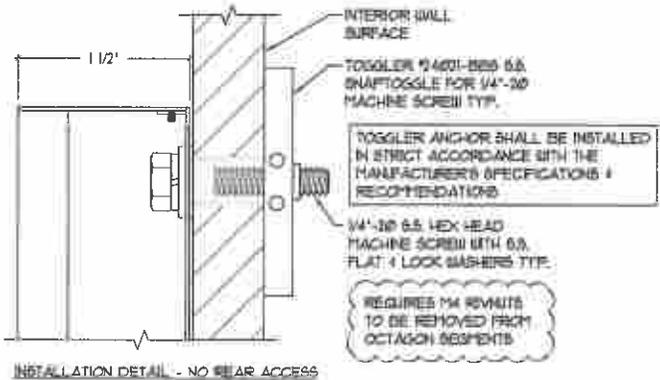
Project ID: CHASE_730_1

Revision: R13 - 01.19.21

ELECTRICAL REQUIREMENTS:
 LED: (AS REQ'D) BLUE BITRO TRACER X
 POWER SUPPLY: (1) BITRO 12V/60W *ASU-60-12U * 0.58A
 TOTAL LOAD: 0.58A @ 120VAC
 CIRCUITS: (1) 20 AMP REQUIRED



INSTALLATION DETAIL - HAVING REAR ACCESS



INSTALLATION DETAIL - NO REAR ACCESS

REQUIRES #14 BOLTS TO BE REMOVED FROM OCTAGON DESIGNS

INTERIOR APPLICATION ONLY

ALL EXPOSED FASTENER HEADS SHALL BE PAINTED TO MATCH THE EXTERIOR CABINET FINISH

OCTAGON SHALL BE WIRED TO A REMOTELY MOUNTED POWER SUPPLY



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JP Morgan Chase Bank #730

Willowbrook
 730 Plainfield Rd.
 Willowbrook, IL 60527

Initial Date: 09/14/18
 Salesperson: Arthur Navarro
 Coordinator: Tracey Picchieri
 Designer: [Signature]
 Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
 COPY, COLORS & SIZES

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Customer Signature _____ Date _____
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Drawing Number: 18-01613
 Project ID: CHASE_730_1
 Revision: R13 - 01.19.21



FRONT VIEW SCALE: 3/8" = 1"

1.42

SIGN TYPE ADA-RRW-A-G **FOR RESTROOMS THAT ARE ACCESSIBLE - A09**

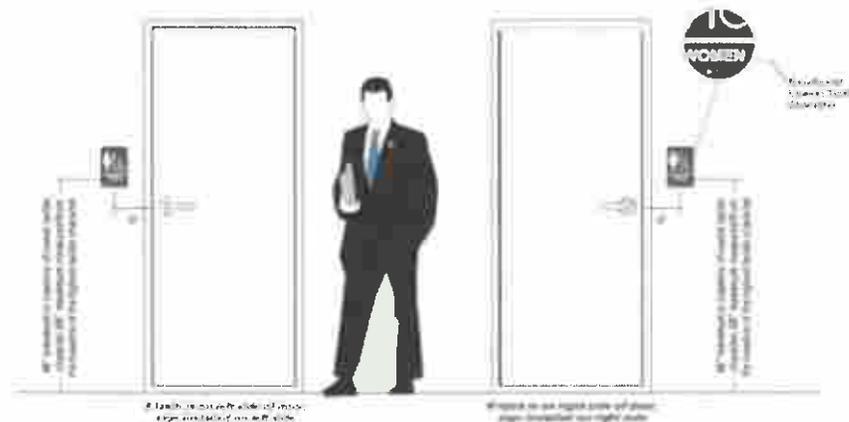
MANUFACTURE AND INSTALL ONE (1) ADA WOMEN RESTROOM SIGN

FACE: 1/8" GRAVOTAC CHARCOAL GREY, SATIN NON-GLARE FINISH.

COPY: 1/32" RAISED, BRIGHT WHITE GRAVOTAC HELVETICA REGULAR TT FONT.

BRAILLE: 1/32" RAISED CLEAR BEAD, MAXIMUM 1/16" DIA.

INSTALL: MOUNT TO LATCH SIDE OF DOOR WITH VHB TAPE AND SILICONE.



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**JP Morgan Chase Bank
#730**

Willowbrook
730 Plainfield Rd.
Willowbrook, IL 60527

Initial Date: 09/14/18
Salesperson: Arthur Navarro
Coordinator: Tracy Pedrini
Designer: GION9S
Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
COPY, COLORS & SIZES

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RESPONSIBILITY OF OTHERS!

Customer Signature _____ Date _____

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Drawing Number: 18-01613

Project ID: CHASE_730_1

Revision: R13 - 01.19.21



FRONT VIEW SCALE: 3/8" = 1"

1.43

SIGN TYPE ADA-RRM-A-G **FOR RESTROOMS THAT ARE ACCESSIBLE - A09**

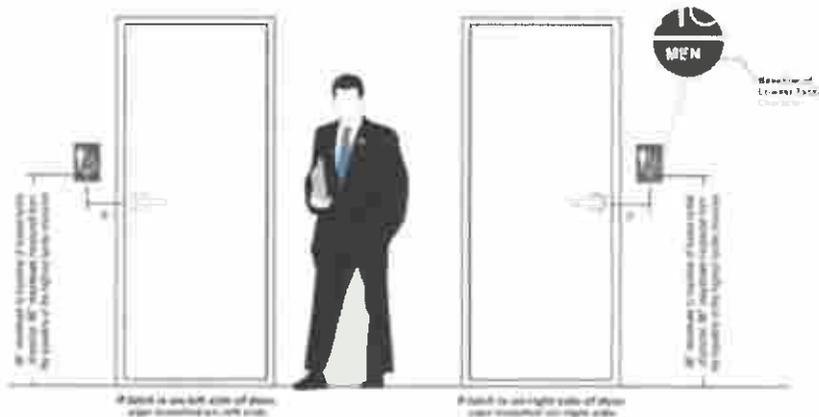
MANUFACTURE AND INSTALL ONE (1) ADA MEN RESTROOM SIGN

FACE: 1/8" GRAVOTAC CHARCOAL GREY, SATIN NON-GLARE FINISH.

COPY: 1/32" RAISED, BRIGHT WHITE GRAVOTAC, HELVETICA REGULAR TT FONT.

BRAILLE: 1/32" RAISED CLEAR BEAD, MAXIMUM 1/16" DIA.

INSTALL: MOUNT TO LATCH SIDE OF DOOR WITH VHB TAPE AND SILCONE.



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**JP Morgan Chase Bank
#730**

Willowbrook
730 Plainfield Rd.
Willowbrook, IL 60527

Initial Date: 09/14/18
Salesperson: Arthur Navarro
Coordinator: Tracey Pichern
Designer: J. Jones
Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
COPY, COLORS & SIZES

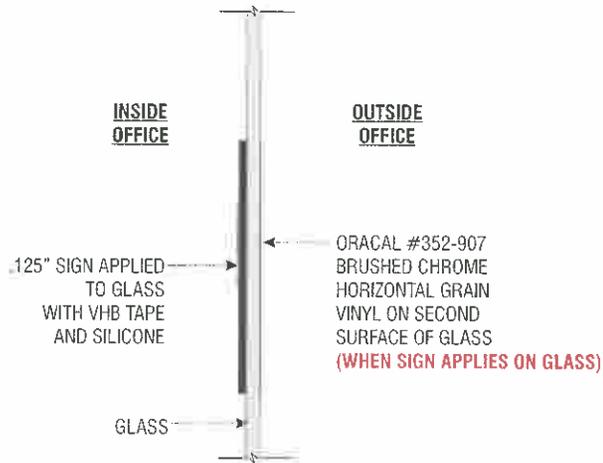
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Drawing Number: 18-01613

Project ID: CHASE_730_1

Revision: R13 - 01.19.21



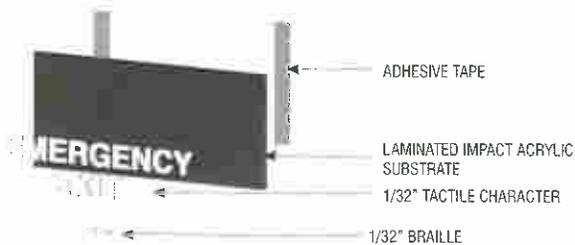
1.44

SIGN TYPE ADA-EEX EMERGENCY EXIT **PERMANENT EMERGENCY EXIT DOOR SIGNAGE - A13E**

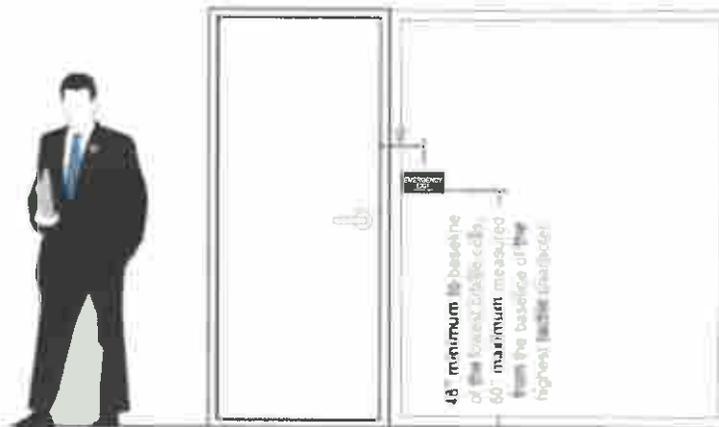
MANUFACTURE AND INSTALL ONE (1) DOOR IDENTIFICATION PLAQUE

FACE: 1/8" GRAVOTAC CHARCOAL GREY, SATIN NON-GLARE FINISH.
COPY: 1/32" RAISED, BRIGHT WHITE GRAVOTAC HELVETICA REGULAR TT FONT.
BRAILLE: GRADE II STANDARD, 1/32" RAISED CLEAR BEAD, MAXIMUM 1/16" DIA.
INSTALL: MOUNT TO LATCH SIDE OF DOOR WITH VHB TAPE AND SILICONE.

WHEN SIGN APPLIES ON GLASS: APPLY LAYER OF 6" X 6" ORACAL #352-907 BRUSHED CHROME HORIZONTAL GRAIN VINYL ON SECOND SURFACE OF GLASS (CUT SHAPE TO MATCH FRONT PLAQUE)



TYPICAL FABRICATION DETAIL NTS



If latch is on right side of door, sign installed on right side.



Signtech

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JP Morgan Chase Bank #730

Willowbrook
 730 Plainfield Rd.
 Willowbrook, IL 60527

Initial Date: 09/14/18
 Salesperson: Arthur Nowam
 Coordinator: Tracy Pichler
 Designer: Joyce
 Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
COPY, COLORS & SIZES

Signtech does NOT provide primary electrical to sign location - **RESPONSIBILITY OF OTHERS!**

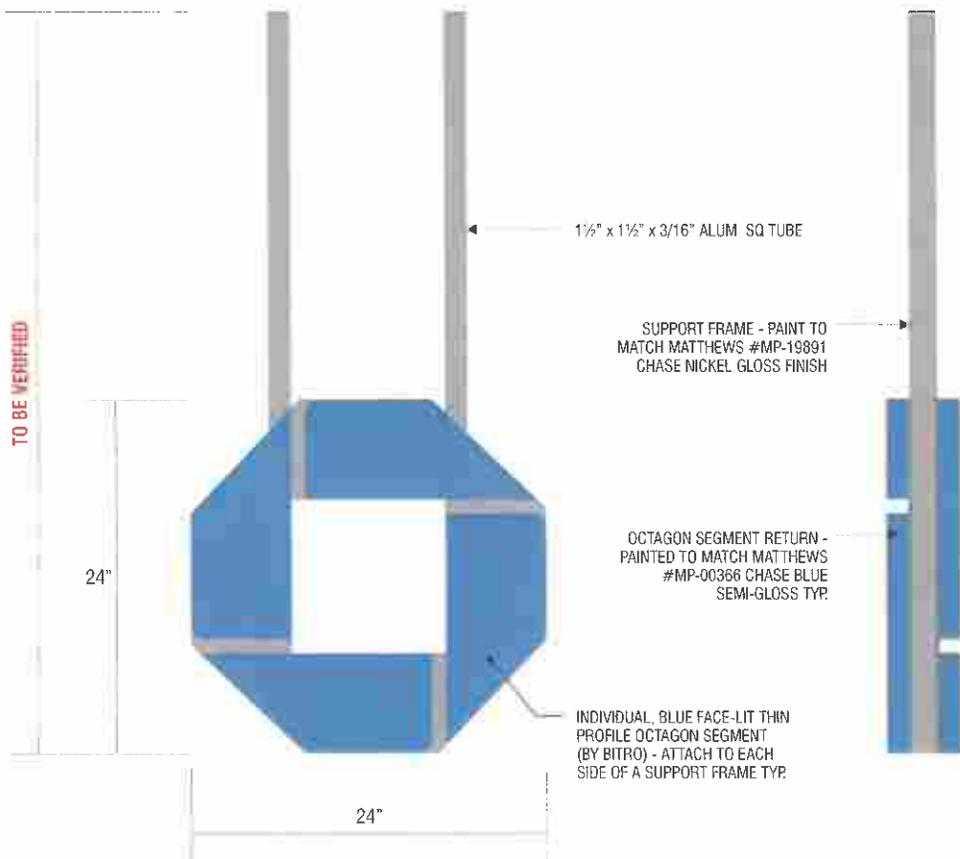
Customer Signature _____ Date _____

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Drawing Number: 18-01613

Project ID: CHASE_730_1

Revision: R13 = 01.19.21



ELEVATION SCALE: 1 1/2" = 1'-0"

General Notes:

1. Design is based on a 5 pcf lateral load for interior applications per IBC 1605, Seismic Design Category D. The contractor's building engineer is to determine the adequacy of the supporting structure.
2. Extruded aluminum shapes shall be 6061-T6 alloy. Aluminum sheet shall be 3003-H14 alloy. Aluminum plate shall be 5052-H34 alloy.
3. All welds shall be made using 5556 filler for aluminum by persons qualified in accordance with AWS standards within the past two years.
4. All fasteners shall be stainless steel or zinc-coated to prevent corrosion.
5. JP Morgan Chase will not be responsible for the safety of the job site before, during or after installation of this structure. It is the responsibility of the contractor and installers to ensure that the installation and erection of this structure is performed using methods that are in full compliance with OSHA regulations.
6. Any deviation from the design or from any part of the drawing, including the General Notes, without prior written consent from JP Morgan Chase voids this drawing in its entirety.
7. The structure designed on this drawing is proprietary and should not be used for site specific applications unless obtained in writing by a Certified Professional Engineer.

INTERIOR APPLICATION ONLY

ALL EXPOSED FASTENER HEADS SHALL BE PAINTED TO MATCH THE EXTERIOR CABINET FINISH

- ILLUMINATED OCTAGON SEGMENTS SHALL BE WIRED TO A REMOTELY MOUNTED 12V / 60W POWER SUPPLY
- 12V / 60W POWER SUPPLY & QUANTITY SHALL BE AS SPECIFIED BY BITRO GROUP

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Phone: (619) 527-6100 / Fax: (619) 527-6111
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JP Morgan Chase Bank #730

Willowbrook
730 Plainfield Rd.
Willowbrook, IL 60527

Initial Date: 09/14/18
Salesperson: Arthur Navarro
Coordinator: Tracey Pichlerri
Designer: None
Scale: As noted

CUSTOMER APPROVAL

Customer Signature _____ Date _____
COPY, COLORS & SIZES

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Customer Signature _____ Date _____

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Drawing Number: 18-01613

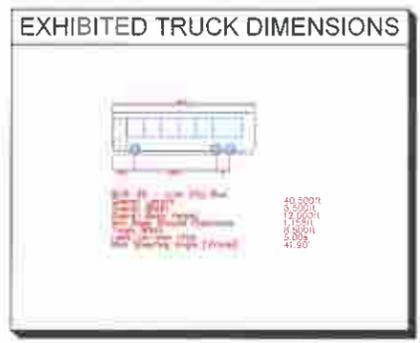
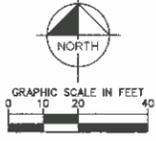
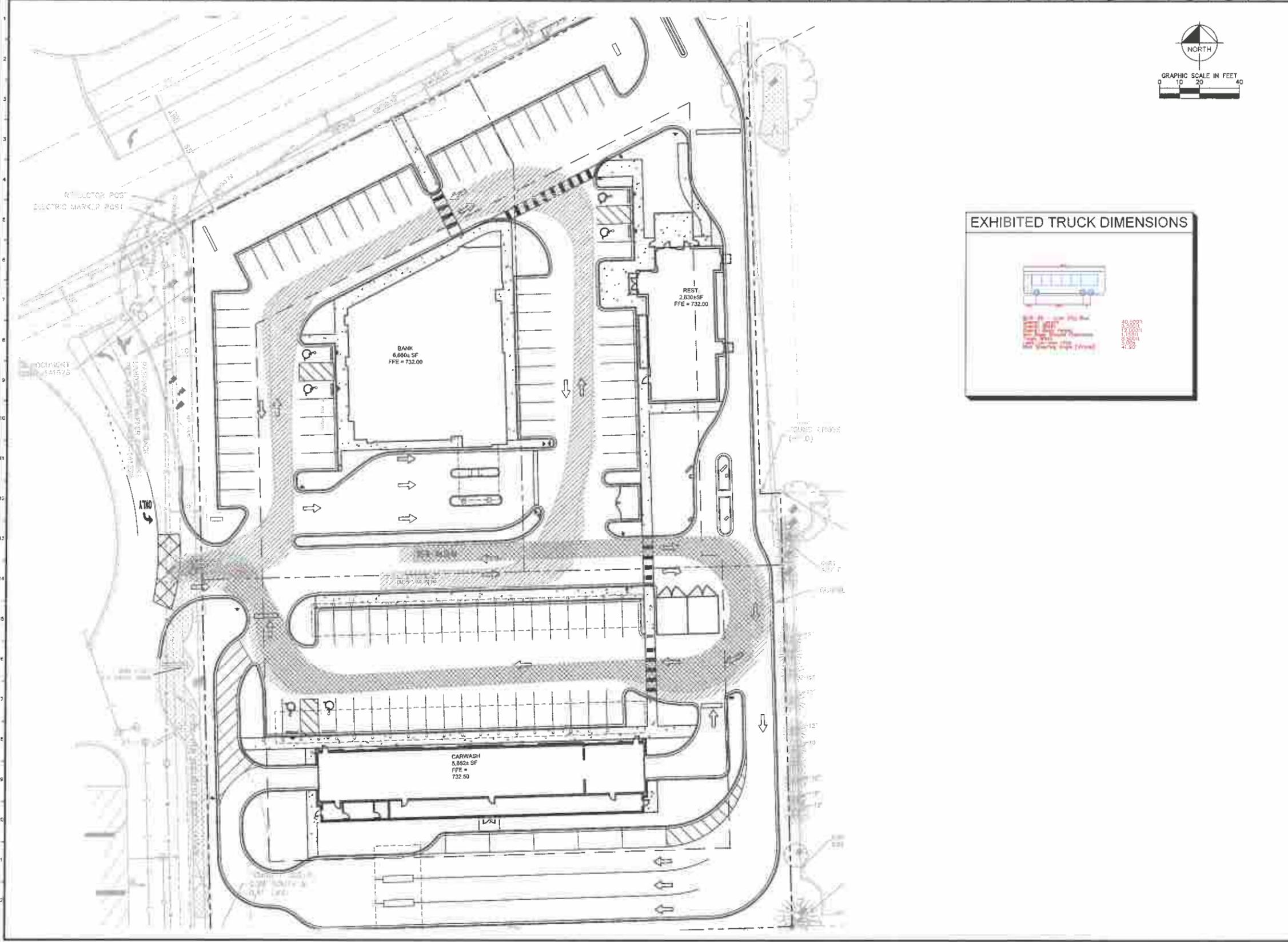
Project ID: CHASE_730_1

Revision: R13 - 01.19.21

1.45 SIGN TYPE OCT-TP-H-24 ATTACHMENTS TO BE VERIFIED

MANUFACTURE AND INSTALL ONE (1) INTERNALLY-ILLUMINATED CEILING-HUNG BLUE BITRO OCTAGON

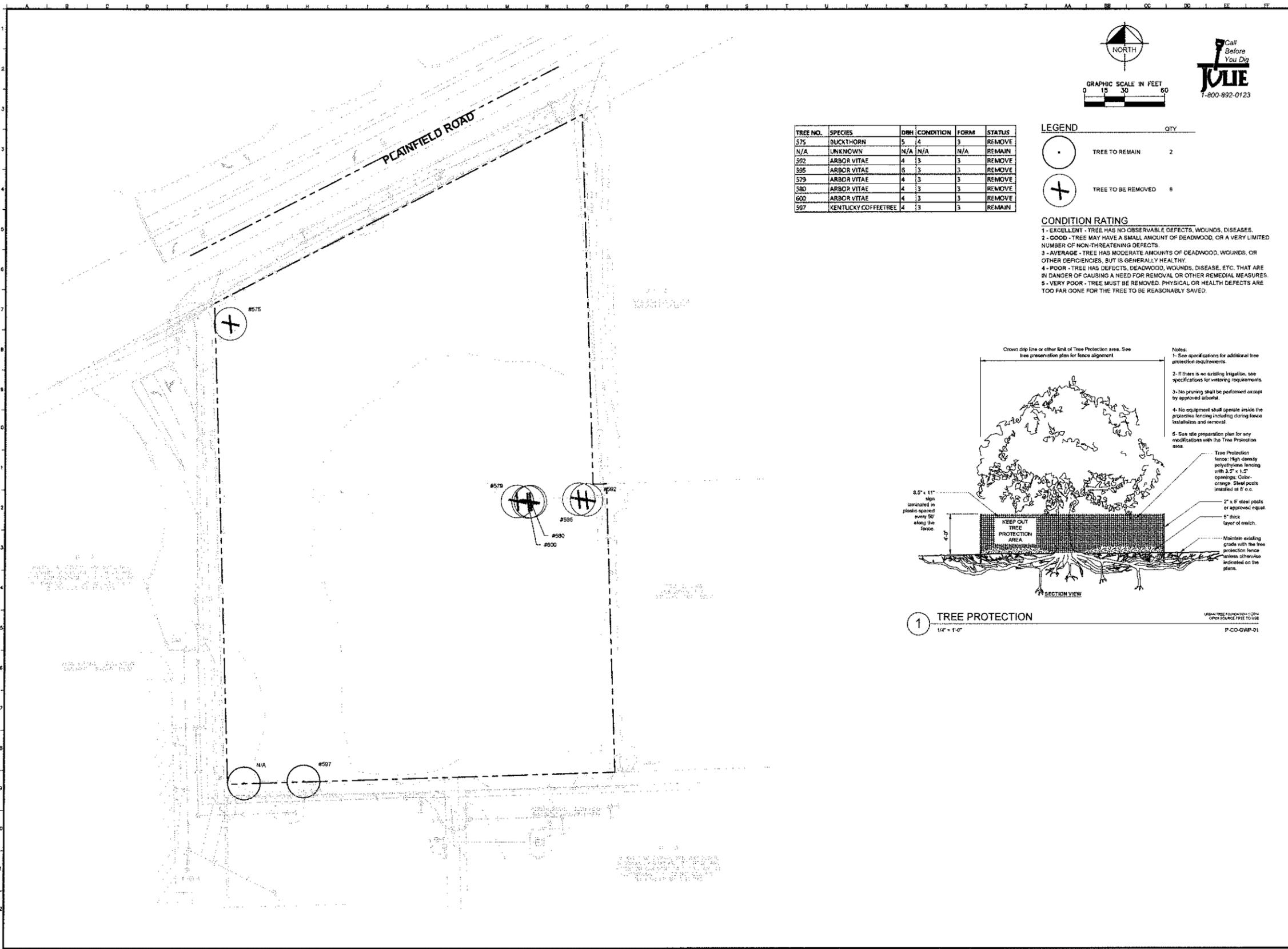
Drawing name: K:\NSA\18726009_204\18726009_204_Plan\18726009_204_Plan.dwg, Plot Date: 07/21/2021 10:00am, by: jns
 This document, together with the contract and any amendments thereto, shall constitute the entire agreement between the parties. It is intended that the drawings and specifications shall be read in conjunction with the contract and shall be construed to be subject to the contract. No part of this drawing shall be used for any other purpose without the written consent of Kimley-Horn and Associates, Inc.



GW PROPERTIES PUD PLANS 735 PLAINFIELD ROAD WILLOWBROOK, IL 60527	FIRE TRUCK TURN EXHIBIT	Kimley-Horn © 2007 by Kimley-Horn and Associates, Inc. 4301 WHEELER ROAD, SUITE 600 PLAINFIELD, IL 62554 PHONE: 618-497-2500 WWW.KIMLEY-HORN.COM	REVISIONS NO. DATE BY
		SCALE: AS NOTED DESIGNED BY: NS DRAWN BY: NS CHECKED BY: EJT	REVISED PER VILLAGE COMMENTS 07/21/21 JNS DATE DATE BY

ORIGINAL ISSUE:
 12/15/2020
 KHA PROJECT NO.
 188726009
 SHEET NUMBER
EXH.

Drawing name: K:\GIS\JULIE\168726009_09_PUDPlans\168726009_PUDPlans\168726009_TREE_PRESERVATION_PLAN.dwg, L1.0, 12/15/2020, 11:08am, by: Chandra.Banzal
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TREE NO.	SPECIES	DBH	CONDITION	FORM	STATUS
575	BUCKTHORN	5	4	3	REMOVE
N/A	UNKNOWN	N/A	N/A	N/A	REMAIN
592	ARBOR VITAE	4	3	3	REMOVE
595	ARBOR VITAE	6	3	3	REMOVE
579	ARBOR VITAE	4	3	3	REMOVE
580	ARBOR VITAE	4	3	3	REMOVE
600	ARBOR VITAE	4	3	3	REMOVE
597	KENTUCKY COFFEE TREE	4	3	3	REMAIN

LEGEND

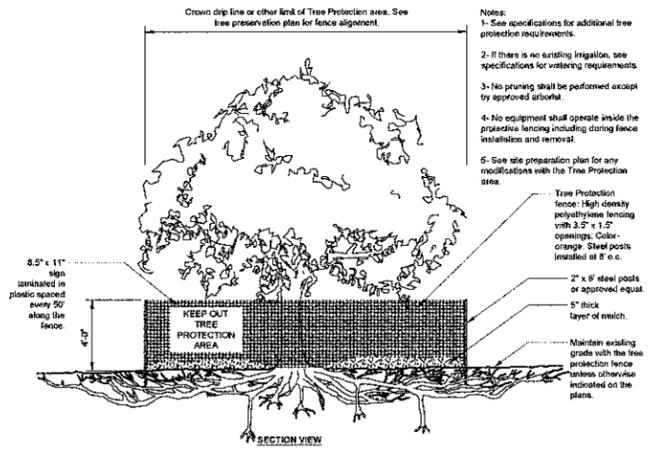
○ TREE TO REMAIN 2

⊕ TREE TO BE REMOVED 6

QTY

CONDITION RATING

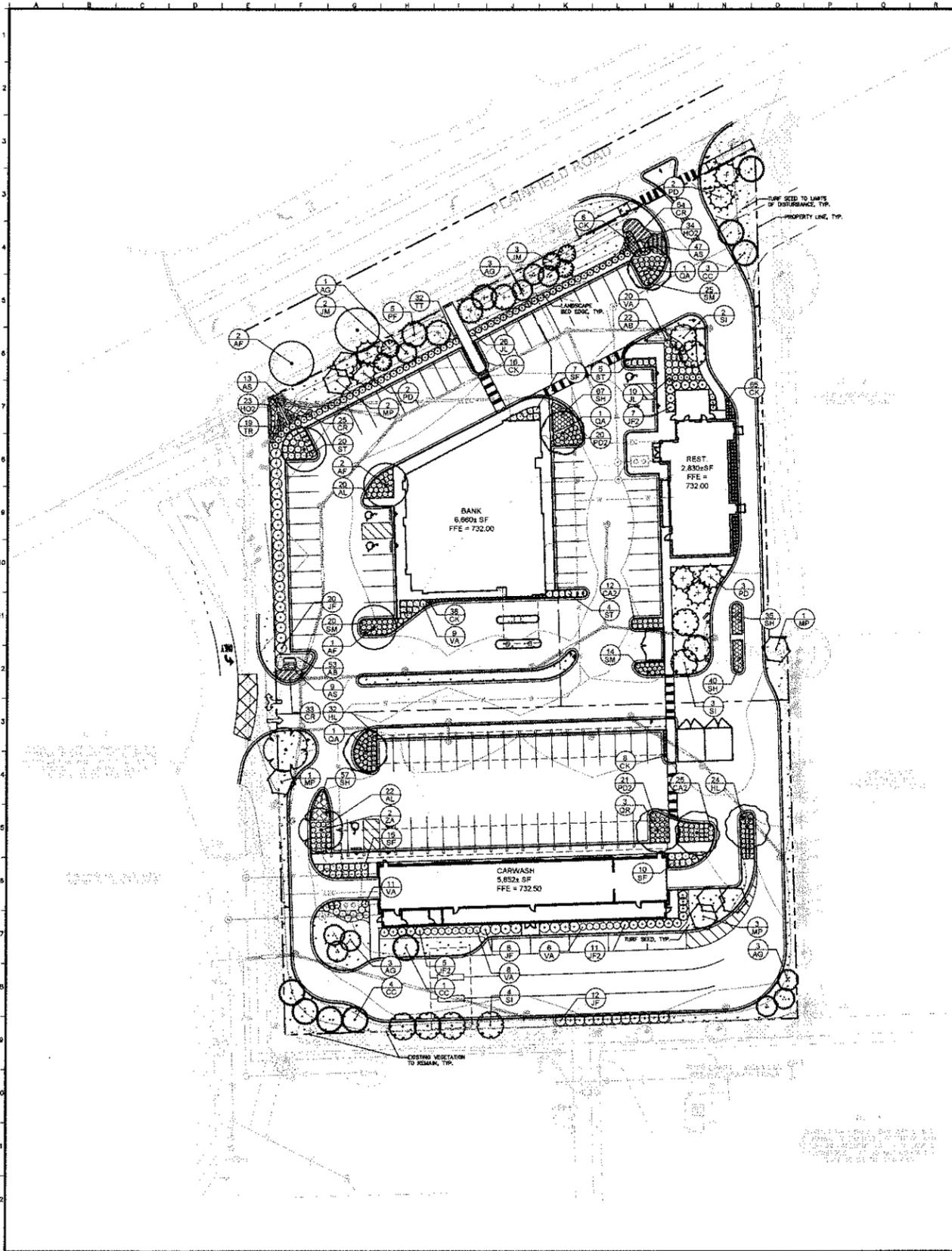
1 - EXCELLENT - TREE HAS NO OBSERVABLE DEFECTS, WOUNDS, DISEASES.
 2 - GOOD - TREE MAY HAVE A SMALL AMOUNT OF DEADWOOD, OR A VERY LIMITED NUMBER OF NON-THREATENING DEFECTS.
 3 - AVERAGE - TREE HAS MODERATE AMOUNTS OF DEADWOOD, WOUNDS, OR OTHER DEFICIENCIES, BUT IS GENERALLY HEALTHY.
 4 - POOR - TREE HAS DEFECTS, DEADWOOD, WOUNDS, DISEASE, ETC. THAT ARE IN DANGER OF CAUSING A NEED FOR REMOVAL OR OTHER REMEDIAL MEASURES.
 5 - VERY POOR - TREE MUST BE REMOVED. PHYSICAL OR HEALTH DEFECTS ARE TOO FAR GONE FOR THE TREE TO BE REASONABLY SAVED.



1 TREE PROTECTION
 1/4" = 1'-0"

<p>Kimley-Horn</p> <p>6320 CALVERT ROAD AND ASSOCIATES, INC. 1000 WILLOW LANE, SUITE 600 WILLOW BROOK, ILLINOIS 60092 PHONE: 630-497-8000 WWW.KHAPROFES.COM</p>	<p>DESIGNED BY: MS</p> <p>DRAWN BY: MS</p> <p>CHECKED BY: LUT</p>	<p>REVISIONS</p> <table border="1"> <thead> <tr> <th>NO.</th> <th>DATE</th> <th>BY</th> <th>REVISIONS</th> </tr> </thead> <tbody> <tr> <td>1</td> <td>01/21/21</td> <td>INS</td> <td>REVISED PER VILLAGE COMMENTS</td> </tr> </tbody> </table>	NO.	DATE	BY	REVISIONS	1	01/21/21	INS	REVISED PER VILLAGE COMMENTS
NO.	DATE	BY	REVISIONS							
1	01/21/21	INS	REVISED PER VILLAGE COMMENTS							
<p>TREE PRESERVATION PLAN</p>										
<p>GW PROPERTIES PUD PLANS</p> <p>705 PLAINFIELD ROAD WILLOWBROOK, IL 60527</p>										
<p>ORIGINAL ISSUE: 12/15/2020</p> <p>KHA PROJECT NO. 168726009</p> <p>SHEET NUMBER</p> <p style="font-size: 24pt; font-weight: bold;">L1.0</p>										

Drawing name: K:\NORTH\18728009_001_Proposed_L2.0_PUD\DWG\LANDSCAPE_PLAN.dwg L2.0 LANDSCAPE PLAN Date: 12/15/2020 11:00am
 Designer: J. Kimley-Horn and Associates, Inc. 1500 Willowbrook Road, Suite 100, Willowbrook, IL 60527
 This document, together with the contract and design professional fees, is the property of Kimley-Horn and Associates, Inc. and shall be used only for the specific purpose for which it was prepared. Release of any information contained herein to any other party without the written consent of Kimley-Horn and Associates, Inc. is prohibited.




 GRAPHIC SCALE IN FEET
 0 15 30 60

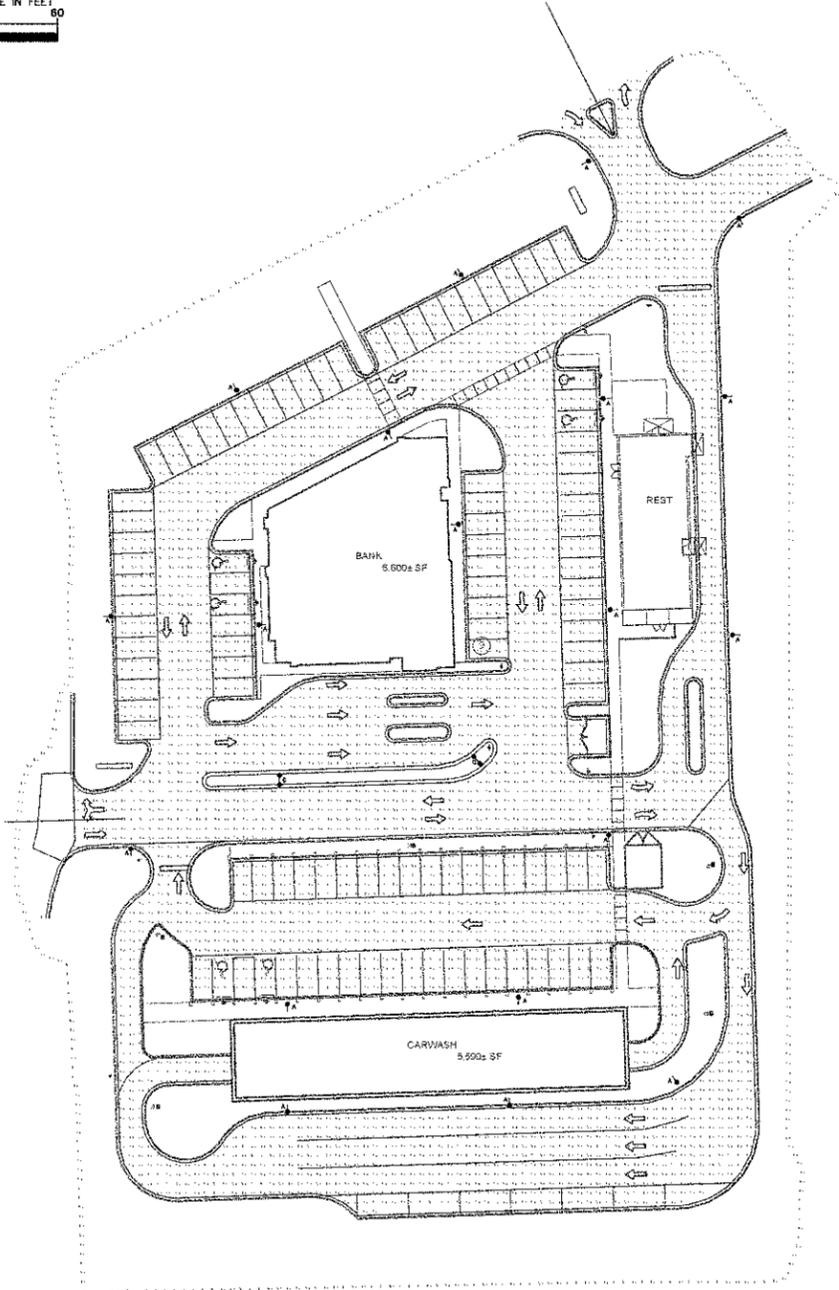
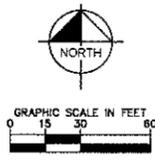
 Call Before You Dig
 1-800-892-0123

PLANT SCHEDULE

CONIFER TREES	CODE	QTY	BOTANICAL / COMMON NAME	CONT.	CAL.	SIZE
	JM	5	JUNIPERUS CHINENSIS 'MOUNTBATTEN' / CHINESE JUNIPER	B & B		5' HT MIN
	PD	7	PSEUDOTSUGA MENZIESII / DOUGLAS FIR	B & B		5' HT MIN
	PF	5	PINUS FLEXILIS 'VANDERWOLF'S PYRAMID' / VANDERWOLF'S PYRAMID LAMBER PINE	B & B		5' HT MIN
ORNAMENTAL TREES	CODE	QTY	BOTANICAL / COMMON NAME	CONT.	CAL.	SIZE
	AG	10	AMELANCHIER X GRANDIFLORA 'AUTUMN BRILLIANCE' / AUTUMN BRILLIANCE APPLE SERVICEBERRY	B & B	1.5"	MULTISTEM
	CC	8	CERCIS CANADENSIS / EASTERN REDBUD	B & B	1.5"	SINGLE STEM
	MP	7	MALUS X PRAIRIFIRE / PRAIRIFIRE CRABAPPLE	B & B	1.5"	SINGLE STEM
	SI	9	SYRINGA RETICULATA 'IVORY SILK' / IVORY SILK JAPANESE TREE LILAC	B & B	1.5"	SINGLE STEM
SHADE TREES	CODE	QTY	BOTANICAL / COMMON NAME	CONT.	CAL.	SIZE
	AF	5	ACER FREEMANII 'JEFFERSRED' TM / AUTUMN BLAZE MAPLE	B & B	2.5"	CAL MIN
	QA	3	QUERCUS ALBA / WHITE OAK	B & B	2.5"	CAL MIN
	QR	3	QUERCUS RUBRA / RED OAK	B & B	2.5"	CAL MIN
	ZA	2	ZELKOVA SERRATA 'AUTUMN GLOW' / AUTUMN GLOW JAPANESE ZELKOVA	B & B	2.5"	CAL MIN
SHRUBS	CODE	QTY	BOTANICAL / COMMON NAME	CONT.	SPACING	SIZE
	AL	42	ARONIA MELANOCARPA 'UCONNAM185' TM / LOW SCAPE MOUND BLACK CHOKEBERRY	2 GAL	SEE PLAN	18" HT MIN
	CA2	37	CEANOTHUS AMERICANUS / NEW JERSEY TEA	3 GAL	SEE PLAN	24" HT MIN
	HL	66	HYDRANGEA PANICULATA 'LITTLE LIME' / LITTLE LIME HYDRANGEA	3 GAL	SEE PLAN	24" HT MIN
	PO2	41	POTENTILLA FRUTICOSA 'GOLD DROP' / GOLD DROP POTENTILLA	3 GAL	SEE PLAN	24" HT MIN
	SF	32	SPIRAEA JAPONICA 'NEON FLASH' / NEON FLASH SPIREA	3 GAL	SEE PLAN	24" HT MIN
	SM	59	SPIRAEA JAPONICA 'WALBURA' / MAGIC CARPET JAPANESE SPIREA	3 GAL	SEE PLAN	18" HT MIN
	ST	29	SPIRAEA BETULIFOLIA 'TOR' / TOR BIRCHLEAF SPIREA	3 GAL	SEE PLAN	24" HT MIN
	VA	54	VIBURNUM DENTATUM 'CHRISTOM' / BLUE MUFFIN VIBURNUM		SEE PLAN	48" HT MIN
EVERGREEN SHRUBS	CODE	QTY	BOTANICAL / COMMON NAME	CONT.	SPACING	SIZE
	JF	40	JUNIPERUS CHINENSIS 'SEA GREEN' / SEA GREEN JUNIPER	1 GAL	SEE PLAN	36" HT MIN
	JF2	23	JUNIPERUS CHINENSIS 'FAIRVIEW' / FAIRVIEW JUNIPER	B & B	SEE PLAN	6' HT MIN
	JL	36	JUNIPERUS CHINENSIS 'GOLD LACE' / GOLD LACE JUNIPER	5 GAL	SEE PLAN	24" HT MIN
	TR	19	THUJA OCCIDENTALIS 'ANNA'S MAGIC BALL' / ANNA'S MAGIC BALL ARBORVITAE	1 GAL	SEE PLAN	12" HT MIN
	TT	32	TAXUS X MEDIA 'TAUNTONI' / TAUNTON'S VIEW	5 GAL	SEE PLAN	24" HT MIN
GRASSES	CODE	QTY	BOTANICAL / COMMON NAME	CONT.	SPACING	SIZE
	CK	133	CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER' / FEATHER REED GRASS	1 GAL	SEE PLAN	
GRASSES	CODE	QTY	BOTANICAL / COMMON NAME	CONT.	SPACING	SIZE
	SH	199	SPOROBOLUS HETEROLEPIS / PRAIRIE DROPS EED	1 GAL		
PERENNIALS	CODE	QTY	BOTANICAL / COMMON NAME	CONT.	SPACING	SIZE
	AB	22	AMSONIA X 'BLUE ICE' / BLUE ICE BLUESTAR	1 GAL		
	AS	69	ALLIUM X 'SUMMER BEAUTY' / SUMMER BEAUTY ORNAMENTAL ONION	1 GAL		
	CR	112	COREOPSIS X 'RED SATIN' / RED SATIN THREADLEAF COREOPSIS	1 GAL		
	HO2	110	HEMEROCALLIS X 'STELLA DE ORO' / STELLA DE ORO DAYLILY	1 GAL		
GROUND COVERS	CODE	QTY	BOTANICAL / COMMON NAME	CONT.	SPACING	SIZE
			KENTUCKY BLUE GRASS SEED			

<p style="text-align: center;">Kimley-Horn</p> <p style="font-size: small;"> 1500 WILLOWBROOK ROAD, SUITE 100, WILLOWBROOK, IL 60527 PHONE: 630-471-5500 WWW.KIMLEY-HORN.COM </p>	<p style="text-align: center;"> ORIGINAL ISSUE: 12/15/2020 KHA PROJECT NO. 188728009 SHEET NUMBER L2.0 </p>
<p style="text-align: center;"> LANDSCAPE PLAN </p>	<p style="text-align: center;"> 788 PLAINFIELD ROAD WILLOWBROOK, IL 60527 </p>
<p style="font-size: x-small;"> SCALE: AS NOTED DESIGNED BY: INS DRAWN BY: INS CHECKED BY: EJT </p>	<p style="font-size: x-small;"> REVISIONS NO. DATE BY 1 07/21/21 INS 2 07/21/21 INS </p>

Drawing name: K:\Projects\168726009_GW Properties_PUD Plans\168726009_Photometrics\168726009_Photometrics.dwg
 Date: 12/15/2020 10:02am
 User: J. Kimley-Horn and Associates, Inc.
 This document, together with the drawings and design presented herein, is intended only for the specific project and site for which it was prepared. Reuse of this document, together with the drawings and design presented herein, for any other project without the written authorization and approval of Kimley-Horn and Associates, Inc. is strictly prohibited.



Revisions	NO.	DATE	BY	REVISIONS
1	12/15/2020	J. Kimley-Horn and Associates, Inc.		Original Issue

NOTE: LIGHT POLES SHALL BE 20 FEET MOUNTED ON 3 FOOT EXPOSED FOUNDATION FOR A LIGHT FIXTURE MOUNTING HEIGHT OF 23 FEET.

Item	Description	Quantity	Unit	Notes
1	Light Pole	10	Each	20' height, 3' base
2	Light Fixture	10	Each	23' mounting height
3	Foundation	10	Sq. Ft.	3' x 3'

AL-ED11150WZ

Beam Spread	Height	Footcandle
15°	23'	1.5
30°	23'	0.4
45°	23'	0.2

Technical Specifications:

- Model: AL-ED11150WZ
- Height: 23 feet
- Beam Spread: 15 degrees
- Mounting: 3 feet exposed foundation

AL-ED11150WZ

Technical Specifications:

- Model: AL-ED11150WZ
- Height: 23 feet
- Beam Spread: 15 degrees
- Mounting: 3 feet exposed foundation

AL-ED11150W

Beam Spread	Height	Footcandle
15°	23'	1.5
30°	23'	0.4
45°	23'	0.2

Technical Specifications:

- Model: AL-ED11150W
- Height: 23 feet
- Beam Spread: 15 degrees
- Mounting: 3 feet exposed foundation

AL-ED11150W

Technical Specifications:

- Model: AL-ED11150W
- Height: 23 feet
- Beam Spread: 15 degrees
- Mounting: 3 feet exposed foundation

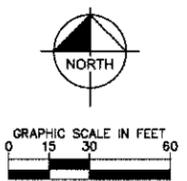
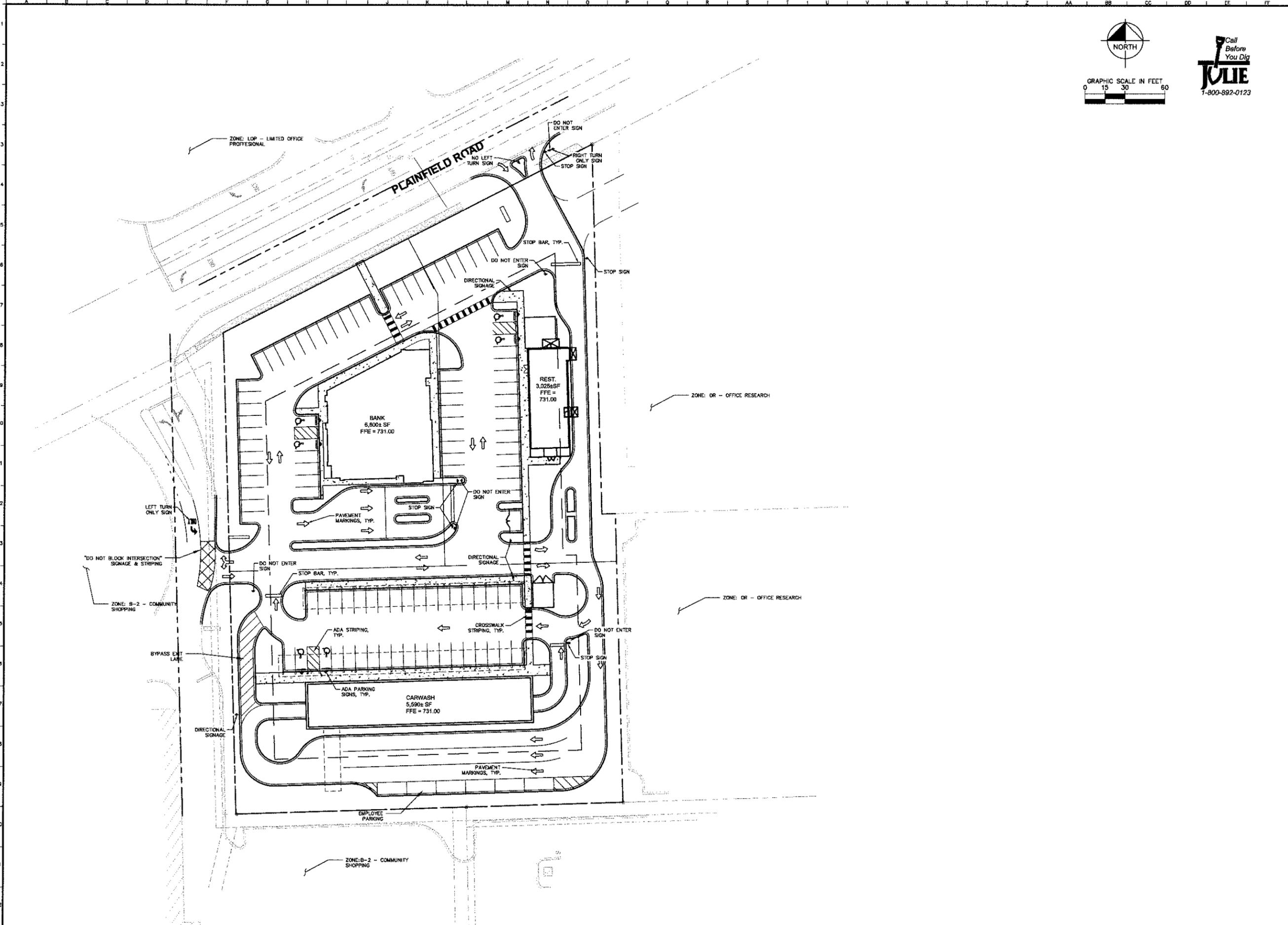
PHOTOMETRICS PLAN

GW PROPERTIES PUD PLANS
 785 PLAINFIELD ROAD
 WILLOWBROOK, IL 60227

ORIGINAL ISSUE:
 12/15/2020
 KHA PROJECT NO.
 168726009
 SHEET NUMBER
E1.0

Kimley-Horn
 A KIMLEY-HORN COMPANY
 200 WEST WASHINGTON STREET, SUITE 600
 WILLOWBROOK, IL 60227
 PHONE: 630-487-2000
 WWW.KIMLEY-HORN.COM

File name: K:\GIS_LDP\168726008_DW Properties_735_Planfield_Midwest\11_V Design\CAD\KSHA\2020_1215 Signage Plan.dwg 02.10 Dec 16, 2020 8:18am by: Ian Spence
 document, together with the concepts and design presented herein, is intended only for the specific purpose and client for which it was prepared. Review of and reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



Kimley-Horn							
© 2020 KIMLEY-HORN AND ASSOCIATES, INC. 4021 WINDFIELD ROAD, SUITE 600 MANNING, MD 21053 WWW.KIMLEY-HORN.COM							
SCALE:	AS NOTED						
DESIGNED BY:	INS						
DRAWN BY:	INS						
CHECKED BY:	EJT						
TRAFFIC CONTROL PLAN							
GW PROPERTIES PUD PLANS							
735 PLAINFIELD ROAD WILLOWBROOK, IL 60527							
ORIGINAL ISSUE: 12/15/2020							
KHA PROJECT NO. 168726009							
SHEET NUMBER							
1 OF 1							

PRELIMINARY PLAT OF SUBDIVISION 735 PLAINFIELD ROAD SUBDIVISION A PLANNED UNIT DEVELOPMENT

OWNER'S CERTIFICATE

STATE OF _____)
COUNTY OF _____) SS

THIS IS TO CERTIFY THAT _____, LIMITED LIABILITY COMPANY, IS THE OWNER OF THE PROPERTY DESCRIBED AND SHOWN HEREON AND AS SUCH OWNER, HAS CAUSED THE PROPERTY TO BE SURVEYED AND SUBDIVIDED AS SHOWN HEREON, FOR THE USES AND PURPOSES THEREIN SET FORTH AND AS ALLOWED AND PROVIDED BY STATUTES, AND SAID LIMITED LIABILITY COMPANY DOES HEREBY ACKNOWLEDGE AND ADOPT THE SAME UNDER THE STYLE AND TITLE AFORESAID.

ALSO, THIS IS TO CERTIFY THAT THE PROPERTY BEING SUBDIVIDED AFORESAID AND, TO THE BEST OF OWNER'S KNOWLEDGE AND BELIEF, SAID SUBDIVISION LIES ENTIRELY WITHIN THE LIMITS OF SCHOOL DISTRICT _____.

DATED AT _____ THIS _____ DAY OF _____ A.D., 20____

OWNER NAME: _____
ADDRESS: _____

BY: _____
SIGNATURE
TITLE: _____
PRINT TITLE

NOTARY'S CERTIFICATE

STATE OF _____)
COUNTY OF _____) SS

I, _____, A NOTARY PUBLIC IN AND FOR THE SAID COUNTY IN THE STATE AFORESAID, DO HEREBY CERTIFY THAT _____ (PRINT NAME), (TITLE) OF SAID LIMITED LIABILITY COMPANY, WHO ARE PERSONALLY KNOWN TO ME TO BE THE SAME PERSON WHOSE NAME IS SUBSCRIBED TO THE FOREGOING INSTRUMENT AS SUCH,

APPEARED BEFORE ME THIS DAY IN PERSON AND ACKNOWLEDGED THAT THEY SIGNED AND DELIVERED THE SAID INSTRUMENT AS THEIR OWN FREE AND VOLUNTARY ACT AND AS THE FREE AND VOLUNTARY ACT OF SAID LIMITED LIABILITY COMPANY, FOR THE USES AND PURPOSES THEREIN SET FORTH.

GIVEN UNDER MY HAND AND NOTARIAL SEAL THIS _____ DAY OF _____ A.D., 20____

NOTARY PUBLIC SIGNATURE _____
(PRINT NAME)

MORTGAGEE'S CERTIFICATE

STATE OF _____)
COUNTY OF _____) SS

MORTGAGEE UNDER _____, AS PROVISIONS OF A CERTAIN MORTGAGE DATED _____ AND RECORDED IN THE RECORDER'S OFFICE OF _____ COUNTY, ILLINOIS, AS DOCUMENT NUMBER _____, HEREBY CONSENTS TO RECORDING OF THE PLAT HEREIN SHOWN.

DATED AT _____ THIS _____ DAY OF _____ A.D., 20____

BY: _____ BY: _____
TITLE: _____ TITLE: _____

NOTARY'S CERTIFICATE

STATE OF _____)
COUNTY OF _____) SS

I, _____, A NOTARY PUBLIC IN THE COUNTY AND STATE AFORESAID, DO HEREBY CERTIFY THAT _____ (TITLE) AND _____ (TITLE) OF _____ (COMPANY), WHO ARE PERSONALLY KNOWN TO ME TO BE THE SAME PERSONS WHO ARE SUBSCRIBED TO THE FOREGOING MORTGAGEE'S CERTIFICATE APPEARED BEFORE ME THIS DAY IN PERSON AND ACKNOWLEDGED THE EXECUTION OF THIS INSTRUMENT IN THEIR CAPACITY FOR THE FOR THE USES AND PURPOSES THEREIN SET FORTH AS THE FREE AND VOLUNTARY ACT OF SAID CORPORATION.

GIVEN UNDER MY HAND AND NOTARIAL SEAL THIS _____ DAY OF _____ A.D., 20____

BY: _____
NOTARY PUBLIC

PLANNING COMMISSION CERTIFICATE

STATE OF ILLINOIS)
COUNTY OF DUPAGE) SS

I, _____, THE UNDERSIGNED, AS CHAIRMAN OF THE PLANNING COMMISSION OF THE VILLAGE OF WILLOWBROOK, DUPAGE COUNTY, ILLINOIS, DO HEREBY CERTIFY THAT THIS DOCUMENT HAS BEEN APPROVED BY SAID PLANNING COMMISSION.

THIS _____ DAY OF _____ A.D., 2020.

PLANNING COMMISSION, VILLAGE OF WILLOWBROOK

CHAIRMAN _____

VILLAGE ENGINEER'S CERTIFICATE

STATE OF ILLINOIS)
COUNTY OF DUPAGE) SS

THIS IS TO CERTIFY THAT I, _____, THE VILLAGE ENGINEER FOR THE VILLAGE OF WILLOWBROOK, ILLINOIS, HAVE REVIEWED AND APPROVED THIS PLAT.

DATED THIS _____ DAY OF _____, 2020 A.D.

VILLAGE ENGINEER _____

HEALTH OFFICER CERTIFICATE

STATE OF ILLINOIS)
COUNTY OF DUPAGE) SS

ACCEPTED AND APPROVED BY THE COUNTY HEALTH OFFICER, OF DUPAGE COUNTY, ILLINOIS THIS _____ DAY OF _____ A.D. 2017

COUNTY HEALTH OFFICER _____

VILLAGE BOARD CERTIFICATE

STATE OF ILLINOIS)
COUNTY OF DUPAGE) SS

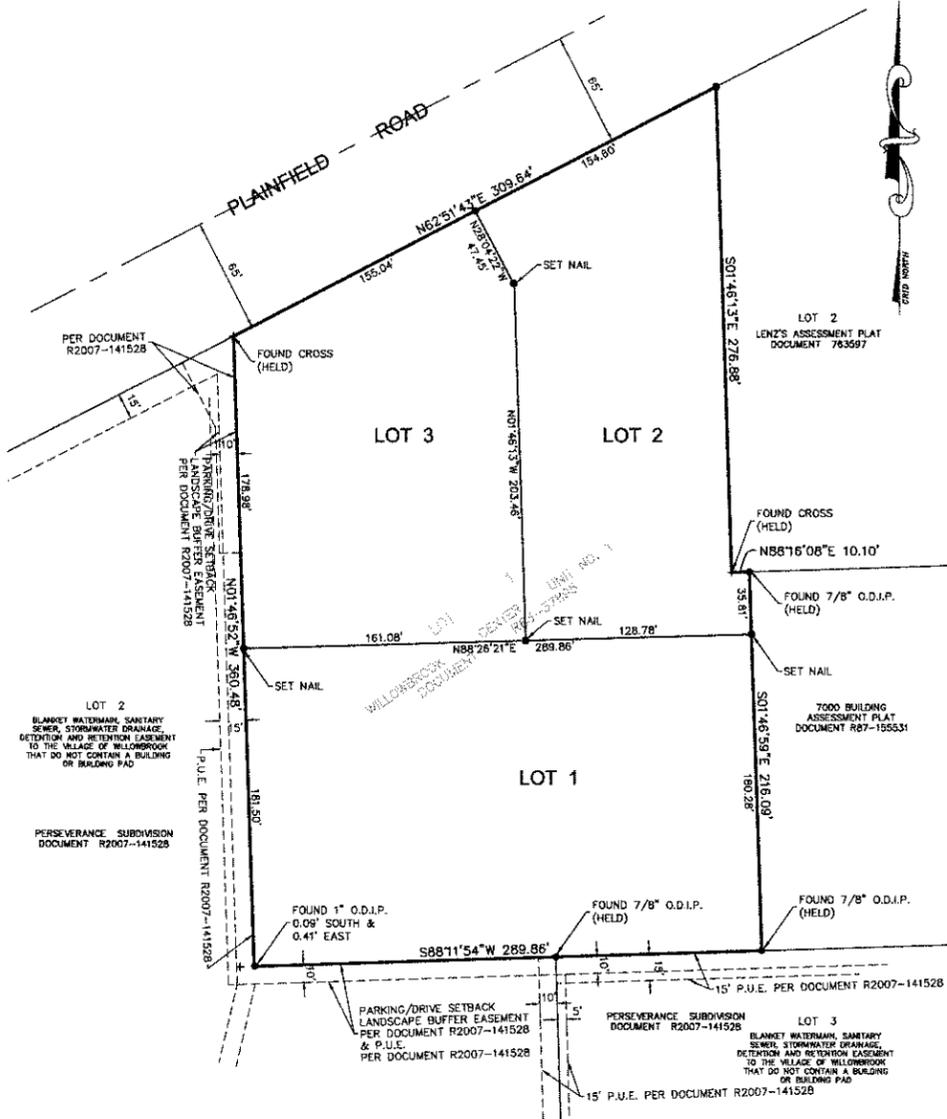
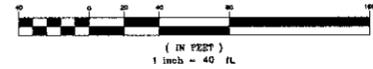
APPROVED BY THE VILLAGE PRESIDENT AND THE VILLAGE BOARD OF TRUSTEES OF THE VILLAGE OF WILLOWBROOK, DUPAGE COUNTY, ILLINOIS, THIS _____ DAY OF _____, 2020 A.D.

PRESIDENT _____

ATTEST: _____
VILLAGE CLERK

LOT 1 IN WILLOWBROOK CENTER UNIT NO. 1, OF PART OF LOT 7 IN OWNER'S SUBDIVISION IN SECTIONS 23 AND 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED OCTOBER 18, 1963, AS DOCUMENT R63-37895, IN DUPAGE COUNTY, ILLINOIS.

GRAPHIC SCALE



LEGEND

- SET 7/8" O.D.I.P. UNLESS OTHERWISE NOTED
- SET CONCRETE MONUMENT UNLESS OTHERWISE NOTED
- + SET CROSS IN CONCRETE UNLESS OTHERWISE NOTED

LINE LEGEND

- SUBDIVISION BOUNDARY LINE
- ADJACENT LAND PARCEL LINE
- LOT LINE
- EASEMENT LINE
- CENTERLINE
- BUILDING SETBACK LINE
- SECTION LINE

ABBREVIATIONS

- O.D.I.P. = OUTSIDE DIAMETER IRON PIPE
- N = NORTH
- S = SOUTH
- E = EAST
- W = WEST
- (R) = RECORD BEARING OR DISTANCE
- (M) = MEASURED BEARING OR DISTANCE
- (C) = CALCULATED BEARING OR DISTANCE
- (D) = DEED BEARING OR DISTANCE
- B.S.L. = BUILDING SETBACK LINE
- U.E. = UTILITY EASEMENT
- D.E. = DRAINAGE EASEMENT
- P.U.E. = PUBLIC UTILITY EASEMENT
- P.O.B. = POINT OF BEGINNING
- P.O.B. & D.E. = PUBLIC UTILITY AND DRAINAGE EASEMENT

SURVEYOR'S AUTHORIZATION TO RECORD

STATE OF ILLINOIS)
COUNTY OF KANE) SS

I HEREBY DESIGNATE _____ AND/OR REPRESENTATIVES THEREOF, TO RECORD THIS PLAT, A TRUE COPY OF WHICH HAS BEEN RETAINED BY ME TO ASSURE NO CHANGES HAVE BEEN MADE TO SAID PLAT.

DATED THIS _____ DAY OF _____, 20____ AT AURORA, KANE COUNTY, ILLINOIS.

COMPASS SURVEYING LTD
PROFESSIONAL DESIGN FIRM
LAND SURVEYOR CORPORATION NO. 184-002778
LICENSE EXPIRES 4/30/2023

BY: _____
SCOTT C. KREBS
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3509
EXPIRES 11/30/2022

SURVEYOR'S CERTIFICATE

STATE OF ILLINOIS)
COUNTY OF KANE) SS

I, SCOTT C. KREBS, ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3509, HAVE SURVEYED AND SUBDIVIDED THE FOLLOWING PROPERTY:

LOT 1 IN WILLOWBROOK CENTER UNIT NO. 1 IN SECTION 23, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT RECORDED OCTOBER 16, 1963 AS DOCUMENT R63-37895, IN DUPAGE COUNTY, ILLINOIS.

AS SHOWN BY THE ANNEXED PLAT WHICH IS A CORRECT REPRESENTATION OF SAID SURVEY AND SUBDIVISION. ALL DISTANCES ARE SHOWN IN FEET AND DECIMALS THEREOF. THIS SUBDIVISION IS WITHIN THE VILLAGE OF WILLOWBROOK WHICH HAS ADOPTED AN OFFICIAL COMPREHENSIVE PLAN AND IS EXERCISING THE SPECIAL POWERS AUTHORIZED BY THE STATE OF ILLINOIS ACCORDING TO 65 ILCS 5/11-12-6 AS HERETOFORE AND HEREAFTER AMENDED, AND THIS SITE FALLS WITHIN "OTHER AREAS ZONE X" (AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN) AS DEFINED BY THE FLOOD INSURANCE RATE MAP COMMUNITY PANEL NUMBER 170220 0189 J, MAP NUMBER 1704300189J HAVING A REVISED DATE OF AUGUST 1, 2019.

GIVEN UNDER MY HAND AND SEAL AT AURORA, ILLINOIS THIS _____ DAY OF _____, 20____

COMPASS SURVEYING LTD
PROFESSIONAL DESIGN FIRM
LAND SURVEYOR CORPORATION NO. 184-002778
LICENSE EXPIRES 4/30/2023

BY: _____
SCOTT C. KREBS
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3509
LICENSE EXPIRES 11/30/2022

AREA SUMMARY

LOT 1	52,432 SQUARE FEET	OR	1.204 ACRES
LOT 2	34,652 SQUARE FEET	OR	0.795 ACRES
LOT 3	34,483 SQUARE FEET	OR	0.792 ACRES
R.O.W. DEDICATION	0 SQUARE FEET	OR	0 ACRES
NET AREA 121,567 SQUARE FEET OR 2.791 ACRES (TO HEAVY LINES) (BASED ON MEASURED VALUES)			

COUNTY CLERK'S CERTIFICATE

STATE OF ILLINOIS)
COUNTY OF DUPAGE) SS

I, _____, COUNTY CLERK OF DUPAGE COUNTY, ILLINOIS, DO HEREBY CERTIFY THAT THERE ARE NO DELINQUENT GENERAL TAXES, UNPAID CURRENT GENERAL TAXES, DELINQUENT SPECIAL ASSESSMENTS OR UNPAID CURRENT SPECIAL ASSESSMENTS AGAINST ANY OF THE LAND INCLUDED IN THE DESCRIBED PROPERTY. I FURTHER CERTIFY THAT I HAVE RECEIVED ALL STATUTORY FEES IN CONNECTION WITH THE PLAT.

GIVEN UNDER MY HAND AND SEAL OF THE COUNTY CLERK OF DUPAGE COUNTY, ILLINOIS THIS _____ DAY OF _____, A.D. 2020.

DUPAGE COUNTY CLERK _____

PROJECT 735 PLAINFIELD ROAD SUBDIVISION 735 Plainfield Road Willowbrook, IL	CLIENT Kimley-Horn © 2017 KIMLEY-HORN AND ASSOCIATES, INC. 1001 WARRENVILLE ROAD, SUITE 350, LYONS, IL 60522 PHONE: 630-497-5500 WWW.KIMLEY-HORN.COM	DATE: 1/31/20	PG: N/A	DRAWN BY: MRA	CHECKED BY: SK	BOOK: N/A	PG: N/A	
		REVISIONS 1. REVISION LOT LINES 2. PFA CLIENT COMMENTS 3. PER IN HOUSE REVIEW 4. ADD CERTS.						

COMPASS SURVEYING LTD
ALTA SURVEYS • TOPOGRAPHY • CONSTRUCTION STAKING
2631 GINGER WOODS PARKWAY, STE 100
AURORA, IL 60202
PHONE: (630) 820-9100 FAX: (630) 820-9190 EMAIL: ADMIN@CLSURVEYING.COM

SCALE: 1" = 40'
1 OF 1
2020.006

Traffic Impact Study Proposed Retail Development Willowbrook, Illinois



Prepared For:



January 20, 2021

1. Introduction

This report summarizes the methodologies, results, and findings of a traffic impact study conducted by Kenig, Lindgren, O'Hara, Aboona, Inc. (KLOA, Inc.) for a proposed retail development to be located in Willowbrook, Illinois. The site, which is currently mostly vacant, is located in the southeast quadrant of the signalized intersection of Plainfield Road with the access drive serving Willowbrook Town Center. As proposed, the site will be developed with an approximate 2,800 square-foot quick service restaurant with a drive-through window, an approximate 6,600 square-foot drive-in bank, an automatic car wash tunnel, and approximately 107 parking spaces. Access to the proposed development will be provided via a right-in/right-out access drive on Plainfield Road and via an internal access drive off the Willowbrook Town Center access drive.

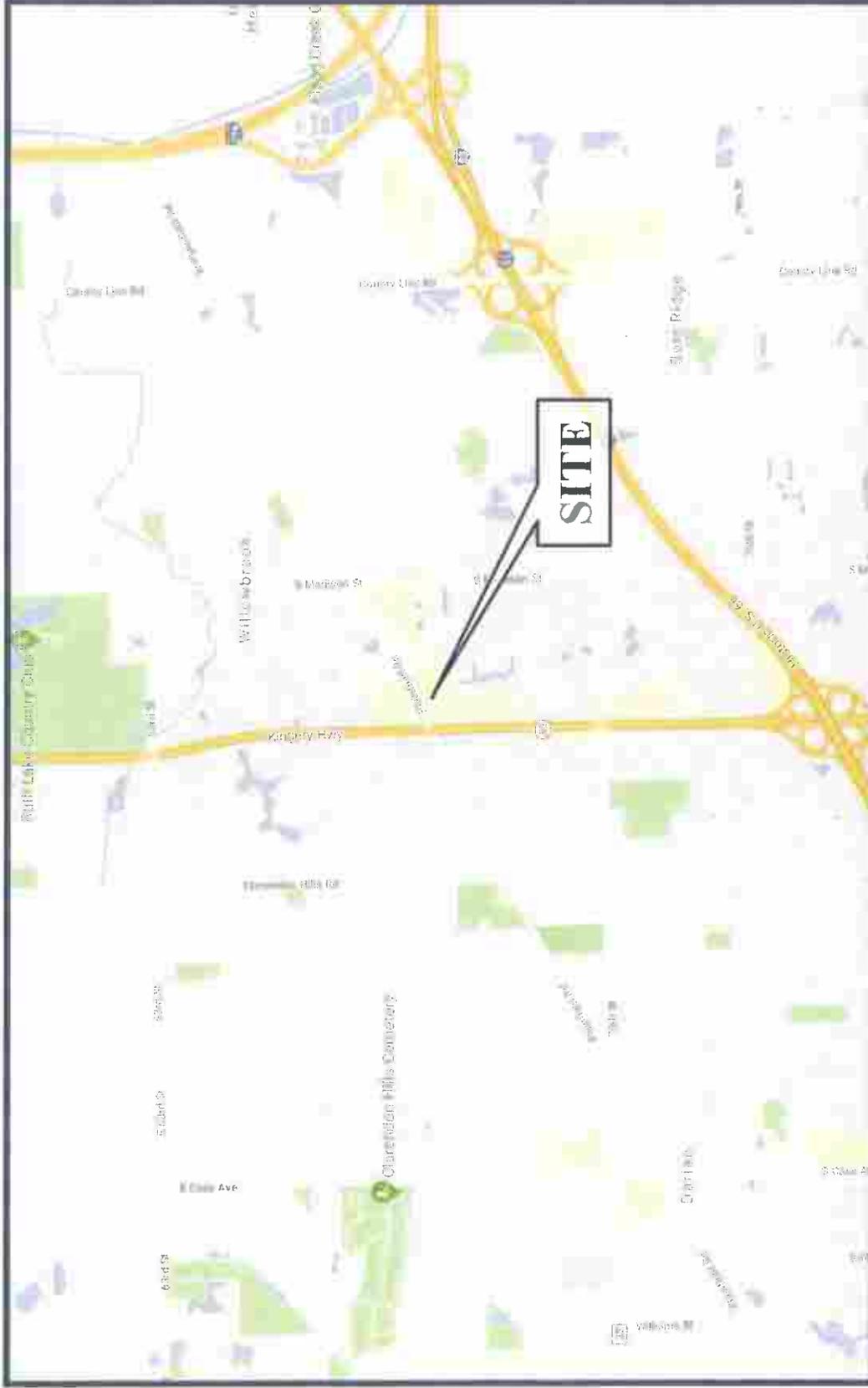
The purpose of this study was to examine background traffic conditions, assess the impact that the proposed development will have on traffic conditions in the area, and determine if any roadway or access improvements are necessary to accommodate the traffic generated by the proposed development.

Figure 1 shows the location of the site in relation to the area roadway system. **Figure 2** shows an aerial view of the site. The sections of this report present the following:

- Existing roadway conditions
- A description of the proposed development
- Directional distribution of the development traffic
- Vehicle trip generation for the development
- Future traffic conditions including access to the development
- Traffic analyses for the weekday morning, weekday evening, and Saturday midday peak hours
- Recommendations with respect to adequacy of the site access and adjacent roadway system

Traffic capacity analyses were conducted for the weekday morning, weekday evening, and Saturday midday peak hours for the following conditions:

1. Existing Conditions – Analyzes the capacity of the existing roadway system using existing peak hour traffic volumes in the surrounding area.
2. No-Build Conditions – Analyzes the capacity of the existing roadway system using existing peak hour traffic volumes including ambient traffic growth.
3. Projected Conditions – Analyzes the capacity of the future roadway system using the projected traffic volumes that include the existing traffic volumes, ambient traffic growth, and the traffic estimated to be generated by the full buildout of the proposed development.



Site Location

Figure 1

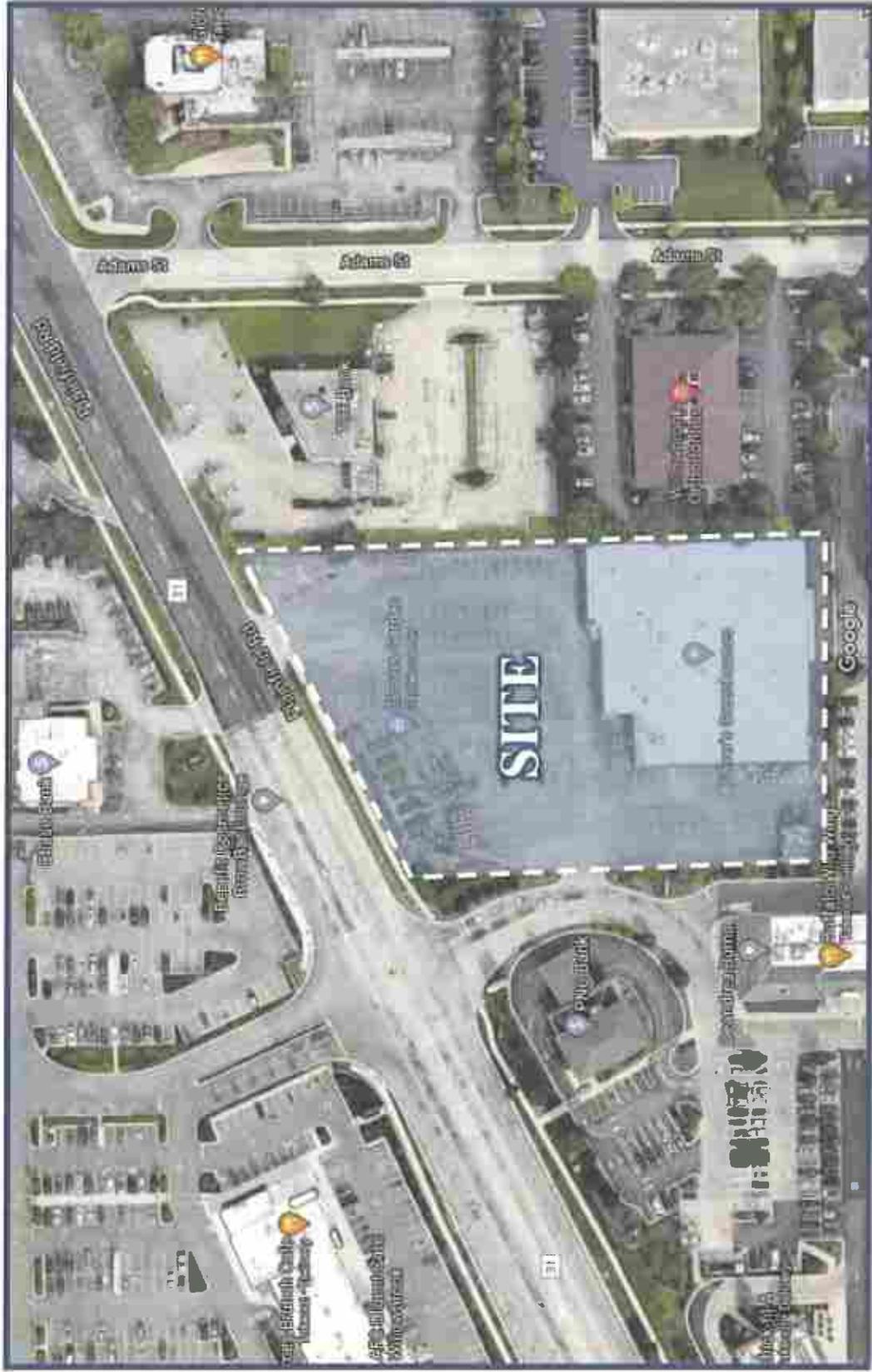


Figure 2

Aerial View of Site

2. Existing Conditions

The following provides a detailed description of the physical characteristics of the roadways including geometry and traffic control, adjacent land uses, and peak hour traffic flows along area roadways.

Site Location

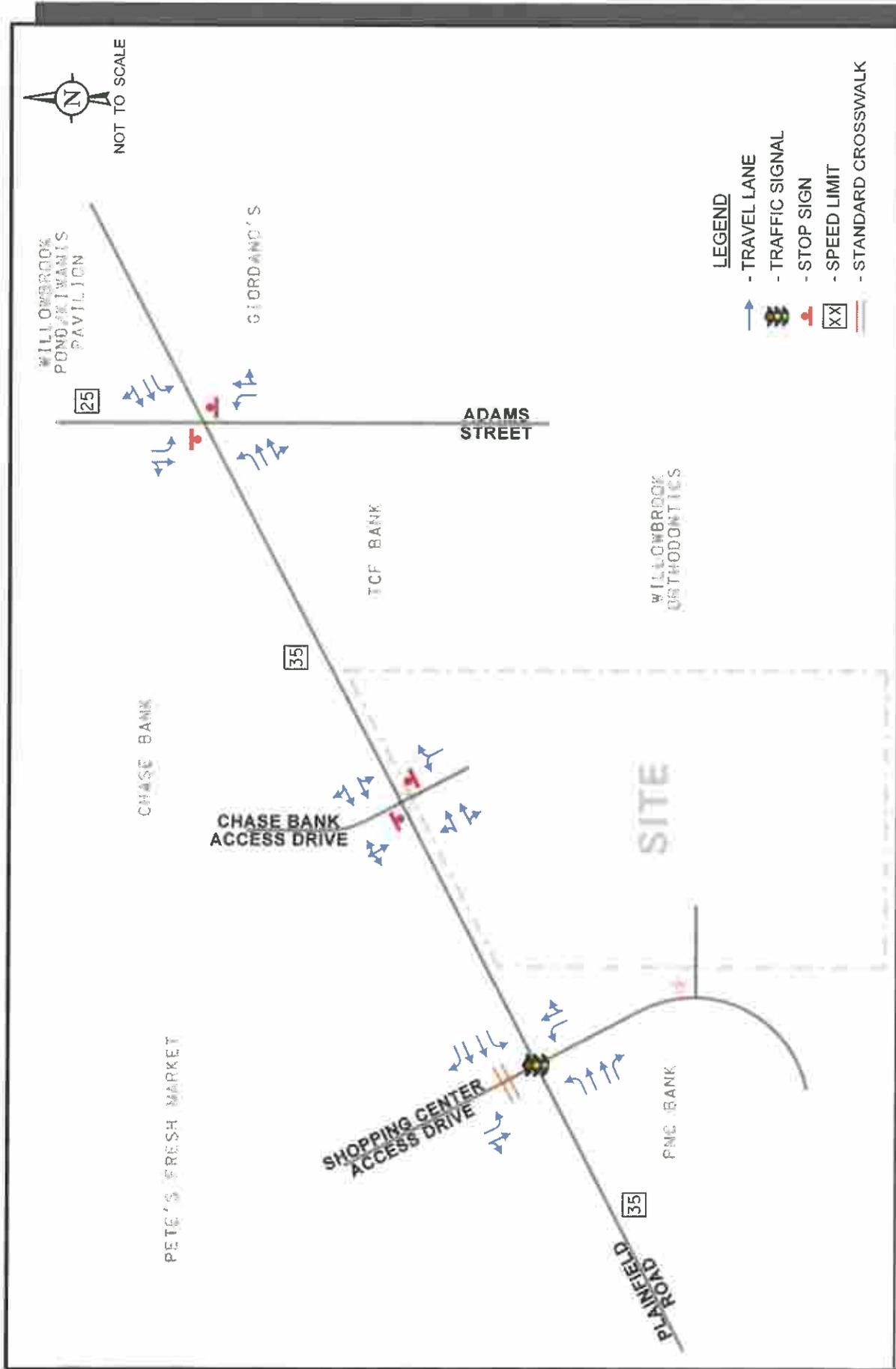
The site, which is currently mostly vacant, is bounded by Plainfield Road to the north, TCF bank and Willowbrook Orthodontics to the east, Marshall's department store and The Lock Up Self Storage to the south, and the Willowbrook Town Center access drive to the west. Land uses in the vicinity of the site are primarily commercial.

Existing Roadway System Characteristics

The characteristics of the existing roadways that surround the proposed development are illustrated in **Figure 3** and described below.

Plainfield Road is an east-west minor arterial roadway that generally provides two lanes in each direction in the vicinity of the site. At its signalized intersection with the access drive serving the Willowbrook Town Center, Plainfield Road provides an exclusive left-turn lane, two through lanes and an exclusive right-turn lane on both approaches. At its unsignalized intersection with Adams Street, Plainfield Road provides an exclusive left-turn lane, a through lane and a combined through/right-turn lane on both approaches. At its unsignalized intersection with the access serving Chase Bank, Plainfield Road provides a combined through/left-turn lane and a combined through/right-turn lane on both approaches. Plainfield Road is under the jurisdiction of DuPage County Division of Transportation (DuDOT), is not designated as a Strategic Regional Arterial (SRA) and carries an Annual Average Daily Traffic (AADT) volume of 21,700 vehicles (IDOT 2016). In addition, Plainfield Road has a posted speed limit of 35 miles per hour.

Adams Street is a north-south local roadway that generally provides one lane in each direction in the vicinity of the site. At its unsignalized intersection with Plainfield Road, Adams Street provides an exclusive left-turn lane and a combined through/right-turn lane on both approaches under stop sign control. Adams Street is under the jurisdiction of the Village of Willowbrook and has a posted speed limit of 25 miles per hour.



Existing Traffic Volumes

In order to determine current traffic conditions in the vicinity of the site, KLOA, Inc. conducted peak period vehicle, pedestrian, and bicycle movement traffic counts on Thursday, March 5, 2020 during the weekday morning (6:00 to 9:00 A.M.) and evening (3:00 to 6:00 P.M.) peak periods and on Saturday, February 29, 2020 during the midday (11:00 A.M. to 2:00 P.M.) peak period at the following intersections:

- Plainfield Road with Willowbrook Town Center Access Drive
- Plainfield Road with Chase Bank Access Drive
- Plainfield Road with Adams Street
- Willowbrook Town Center Access Drive with Internal Driveway

It should be noted that the traffic counts were conducted before the COVID-19 lockdown and, as such, the counts represent normal traffic conditions. The results of the traffic counts showed that the weekday morning peak hour of traffic occurs from 7:45 A.M. to 8:45 A.M., the weekday evening peak hour of traffic occurs from 4:45 P.M. to 5:45 P.M., and the Saturday midday peak hour occurs from 1:00 P.M. to 2:00 P.M.

Figure 4 illustrates the existing traffic volumes. Copies of the traffic count summary sheets are included in the Appendix.

Crash Data Analysis

KLOA, Inc. obtained crash data¹ for the past five years (2014 to 2018) for the intersections of Plainfield Road with Willowbrook Town Center access drive, the Chase Bank access drive, and Adams Street. **Tables 1** through **3** summarize the crash data for these intersections. A review of the crash data indicated that no fatalities were reported at any of these intersections between 2014 and 2018.

¹ IDOT DISCLAIMER: The motor vehicle crash data referenced herein was provided by the Illinois Department of Transportation. Any conclusions drawn from analysis of the aforementioned data are the sole responsibility of the data recipient(s). Additionally, for coding years 2015 to present, the Bureau of Data Collection uses the exact latitude/longitude supplied by the investigating law enforcement agency to locate crashes. Therefore, location data may vary in previous years since data prior to 2015 was physically located by bureau personnel.

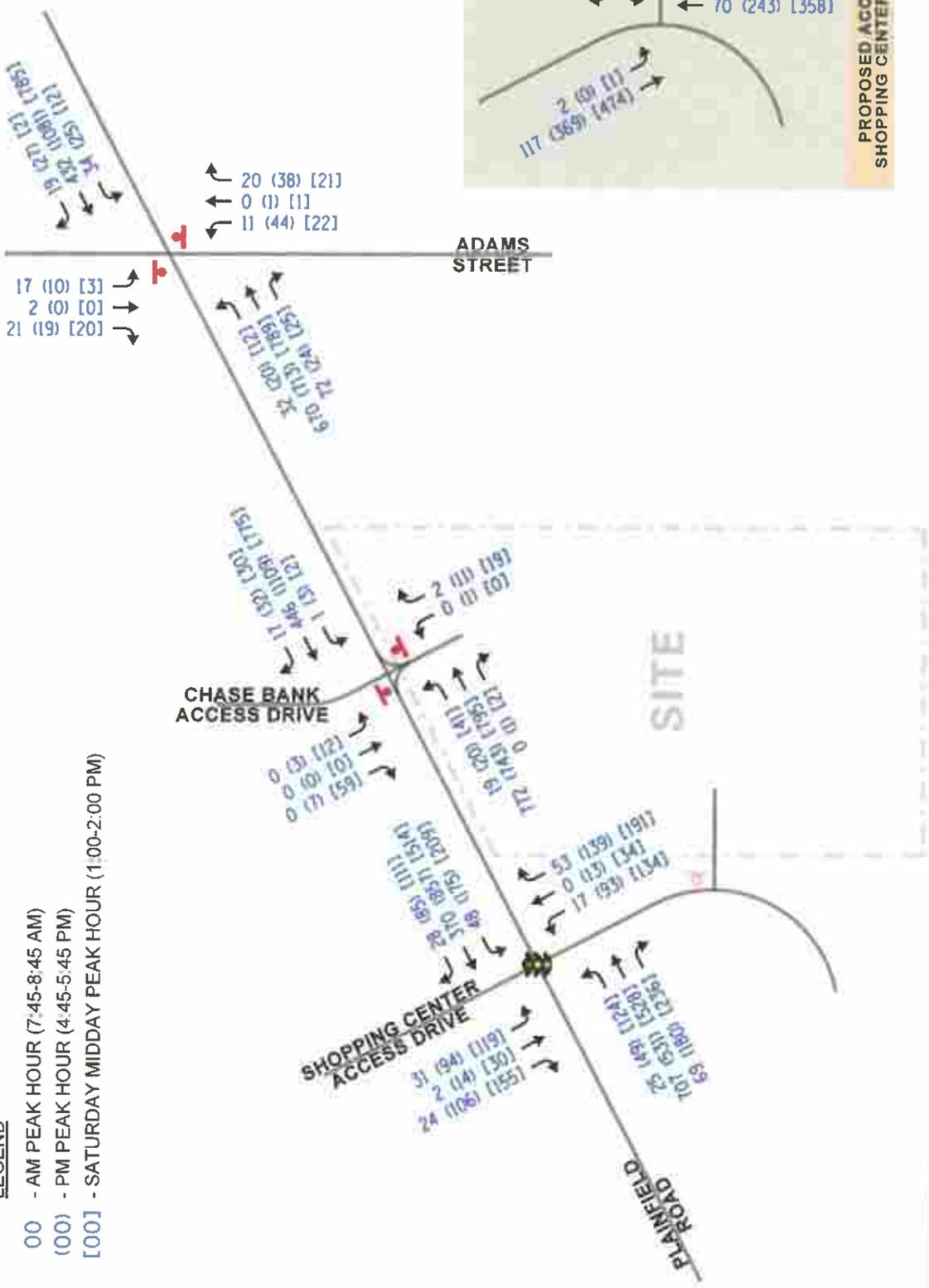
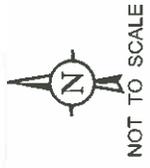


Table 1
 PLAINFIELD ROAD WITH WILLOWBROOK TOWN CENTER ACCESS DRIVE – CRASH
 SUMMARY

Type of Crash Frequency								
Year	Angle	Head On	Object	Rear End	Sideswipe	Turning	Other	Total
2014	0	0	0	0	0	0	0	0
2015	0	0	0	1	0	5	0	6
2016	0	0	0	0	0	0	0	0
2017	0	0	0	0	0	0	0	0
2018	<u>1</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>3</u>	<u>0</u>	<u>4</u>
Total	1	0	0	1	0	8	0	10
Average	<1.0	0	0	<1.0	0	1.6	0	2.0

Table 2
 PLAINFIELD ROAD WITH CHASE BANK ACCESS DRIVE – CRASH SUMMARY

Type of Crash Frequency								
Year	Angle	Head On	Object	Rear End	Sideswipe	Turning	Other	Total
2014	0	0	0	0	0	0	0	0
2015	1	0	0	0	0	0	0	1
2016	0	0	0	1	0	2	0	3
2017	0	0	0	0	0	1	0	1
2018	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>5</u>	<u>0</u>	<u>5</u>
Total	1	0	0	1	0	8	0	10
Average	<1.0	0	0	<1.0	0	1.6	0	2.0

Table 3
 PLAINFIELD ROAD WITH ADAMS STREET – CRASH SUMMARY

Year	Type of Crash Frequency							Total
	Angle	Head On	Object	Rear End	Sideswipe	Turning	Other	
2014	0	0	0	1	0	2	0	3
2015	0	0	0	0	0	3	0	3
2016	0	0	0	0	0	0	0	0
2017	0	0	0	0	0	2	1	3
2018	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>1</u>	<u>0</u>	<u>0</u>	<u>1</u>
Total	0	0	0	1	1	7	1	10
Average	0	0	0	<1.0	<1.0	1.4	<1.0	2.0

3. Traffic Characteristics of the Proposed Development

To evaluate the impact of the subject development on the area roadway system, it was necessary to quantify the number of vehicle trips the site will generate during the respective three peak hours and then determine the directions from which the proposed traffic will approach and depart the site.

Proposed Site and Development Plan

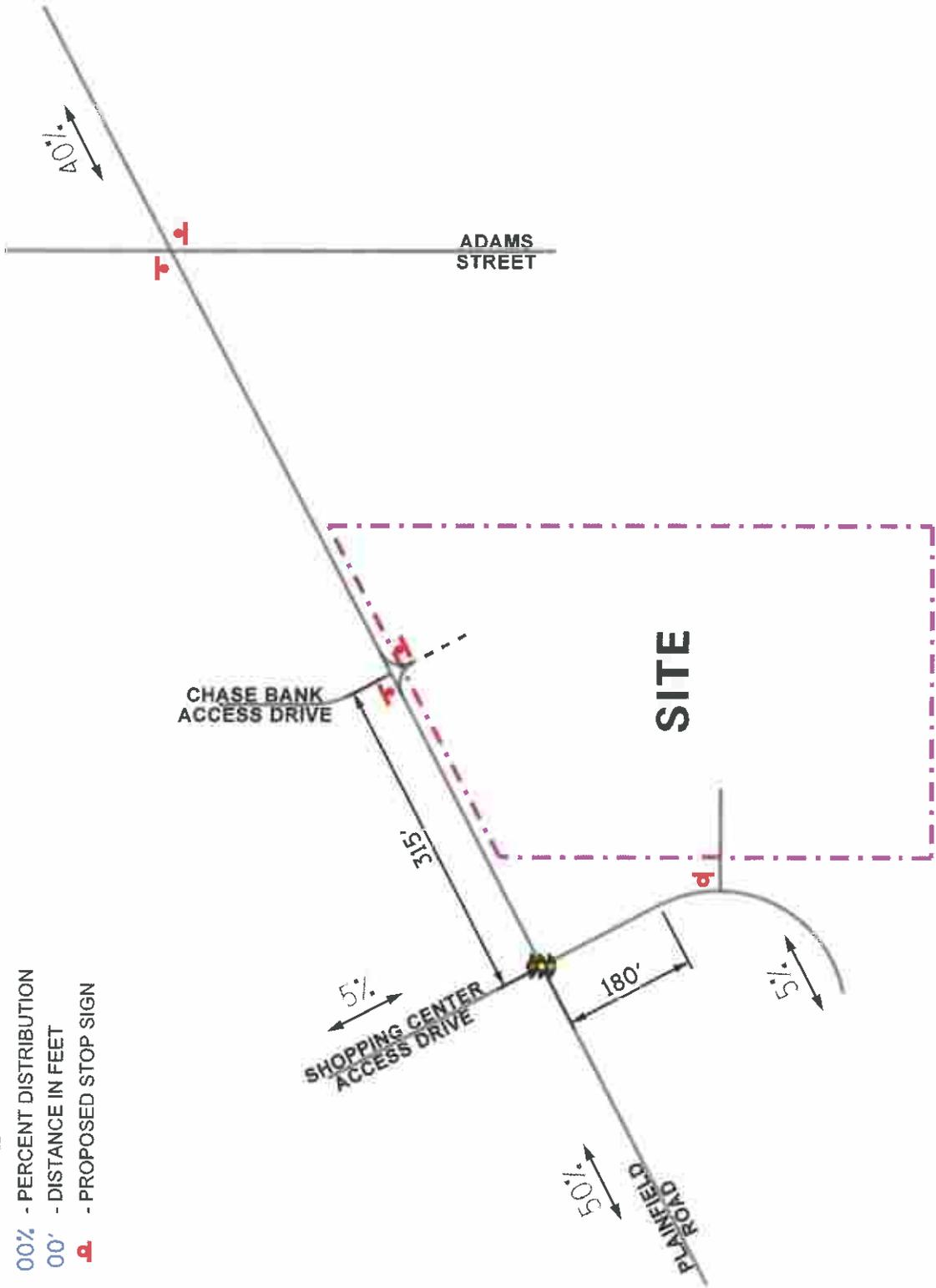
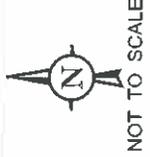
As proposed, the site will be developed with an approximate 2,800 square-foot quick serve restaurant with a drive-through window, an approximate 6,600 square-foot drive-in bank, an automatic car wash tunnel, and approximately 107 parking spaces. Access to the proposed development will be provided via the following:

- A proposed right-in/right-out access drive off Plainfield Road (which is currently a full movement access drive that serves Clovers Garden) located approximately 315 feet east of the main access drive serving the Willowbrook Town Center. This access drive will provide one inbound lane and one outbound lane with turning movements physically restricted to right turns only via a mountable triangular median. Outbound movements should be under stop sign control.
- An existing full movement access drive (which currently serves Clovers Garden) off the main access drive serving the Willowbrook Town Center located approximately 180 feet south of Plainfield Road. This access drive provides one inbound lane and one outbound lane with outbound movements under stop sign control. It should be noted that this intersection will be analyzed as follows:
 - With a southbound combined through/left-turn lane
 - With a southbound through lane and a proposed southbound exclusive left-turn lane serving the site of approximately 29 feet of storage and 37 feet of taper

A copy of the site plan is included in the Appendix.

Directional Distribution of Development Traffic

The directional distribution of how traffic will approach and depart the site was estimated based on the general travel patterns through the study area derived from the peak hour traffic volumes. **Figure 5** shows the established directional distribution for this development.



LEGEND

- 00% - PERCENT DISTRIBUTION
- 00' - DISTANCE IN FEET
- PROPOSED STOP SIGN

Estimated Directional Distribution

Retail Development
Willowbrook, Illinois

Development Traffic Generation

The estimate of vehicle traffic to be generated by the proposed development is based upon the proposed land use types and sizes. The vehicle trip generation for the overall development was calculated using data published in the Institute of Transportation Engineers (ITE) *Trip Generation Manual*, 10th Edition.

It is important to note that surveys conducted by ITE have shown that approximately 50 and 30 percent of trips made to fast-food restaurant uses, and drive-in bank uses, respectively, are diverted from the existing traffic on the roadway system. This is particularly true during the weekday morning and evening peak hours when traffic is diverted from the home-to-work and work-to-home trips. Such diverted trips are referred to as pass-by traffic. As such, these pass-by percentages were applied to the trips estimated to be generated by these uses. It should be noted that while it is likely internal interaction will occur between the proposed land uses, which will further reduce the estimated trips, it was not applied in order to present a conservative analysis.

Table 4 shows the estimated vehicle trip generation for the weekday morning, weekday evening, and Saturday midday peak hours and daily trips. It should be noted that ITE does not have daily trip data for car washes. The ITE trip generation summary sheets are included in the Appendix.

Table 4
ESTIMATED PEAK HOUR VEHICLE TRIP GENERATION FOR PROPOSED RETAIL DEVELOPMENT

ITE Land-Use Code	Type/Size	Weekday Morning Peak Hour			Weekday Evening Peak Hour			Saturday Midday Peak Hour			Daily
		In	Out	Total	In	Out	Total	In	Out	Total	
934	Quick Serve Restaurant with Drive Through Window (2,800 square feet)	58	55	113	47	44	91	79	75	154	1,319
	<i>50 Percent Pass-By Reduction</i>	-28	-28	-56	-23	-23	-46	-38	-38	-76	-660
932	Drive-In Bank (6,600 square feet)	37	26	63	68	67	135	89	85	174	660
	<i>30 Percent Pass-By Reduction</i>	-9	-9	-18	-20	-20	-40	-26	-26	-52	-198
948	Automatic Car Wash (one tunnel)	10	10	20	39	39	78	19	22	41	NA
	Total Development Trips	105	91	196	154	150	304	187	182	369	NA
	Total Pass-By Trips	37	37	74	43	43	86	64	64	128	858
	Total New Trips	68	54	122	111	107	218	123	118	241	NA



4. Projected Traffic Conditions

The total projected traffic volumes take into consideration the existing traffic volumes, increase in background traffic due to growth, and the traffic estimated to be generated by the proposed subject development.

Development Traffic Assignment

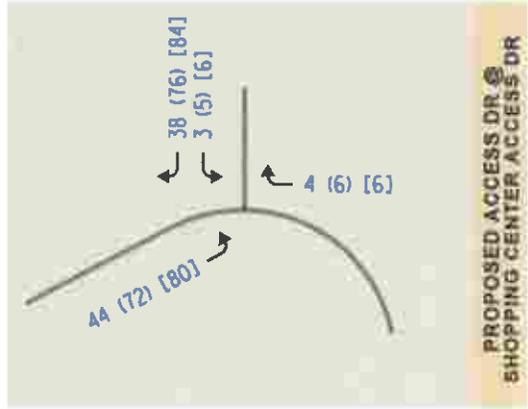
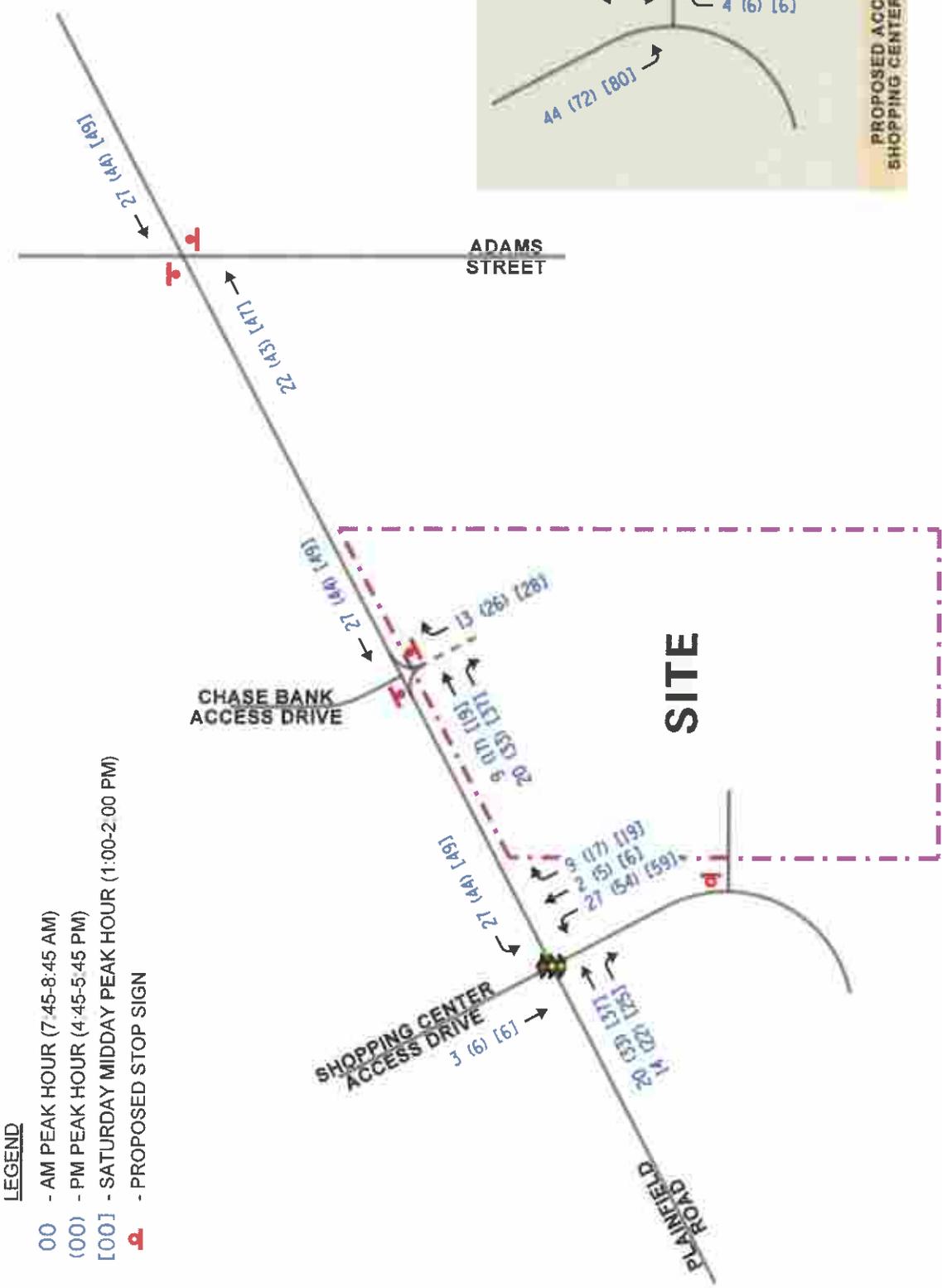
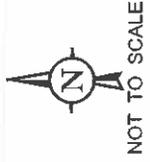
The estimated weekday morning, weekday evening, and Saturday midday peak hour traffic volumes that will be generated by the proposed development were assigned to the roadway system in accordance with the previously described directional distribution (Figure 5). **Figure 7** illustrates the traffic assignment of the new passenger vehicle trips and **Figure 8** illustrates the traffic assignment of the pass-by passenger vehicle trips.

Background Traffic Conditions

The existing traffic volumes (Figure 4) were increased by a regional growth factor to account for the increase in existing traffic related to regional growth in the area (i.e., not attributable to any particular planned development). Based on 2050 Average Daily Traffic (ADT) projections provided by the Chicago Metropolitan Agency for Planning (CMAP) in a letter dated December 2, 2020, the existing traffic volumes were increased by an annually compounded growth rate for six years (one-year buildout plus five years) totaling 2.4 percent to represent Year 2026 no-build conditions. **Figure 9** shows the Year 2026 no-build traffic conditions. A copy of the CMAP 2050 projections letter is included in the Appendix.

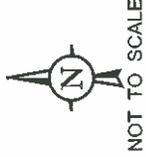
Year 2026 Total Projected Traffic Conditions

The new and pass-by development-generated traffic (Figures 7 and 8) was added to the no-build traffic volumes (Figure 9) to determine the Year 2026 total projected traffic volumes, which are illustrated in **Figure 10**.



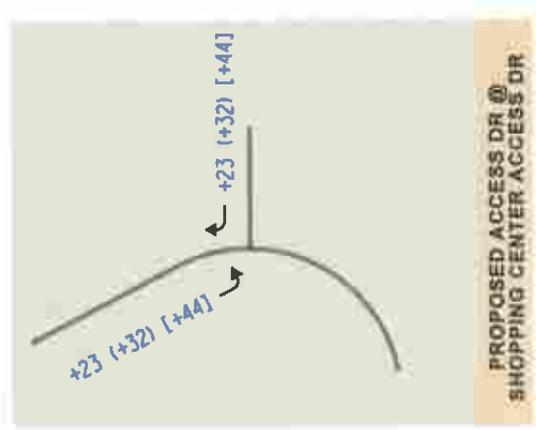
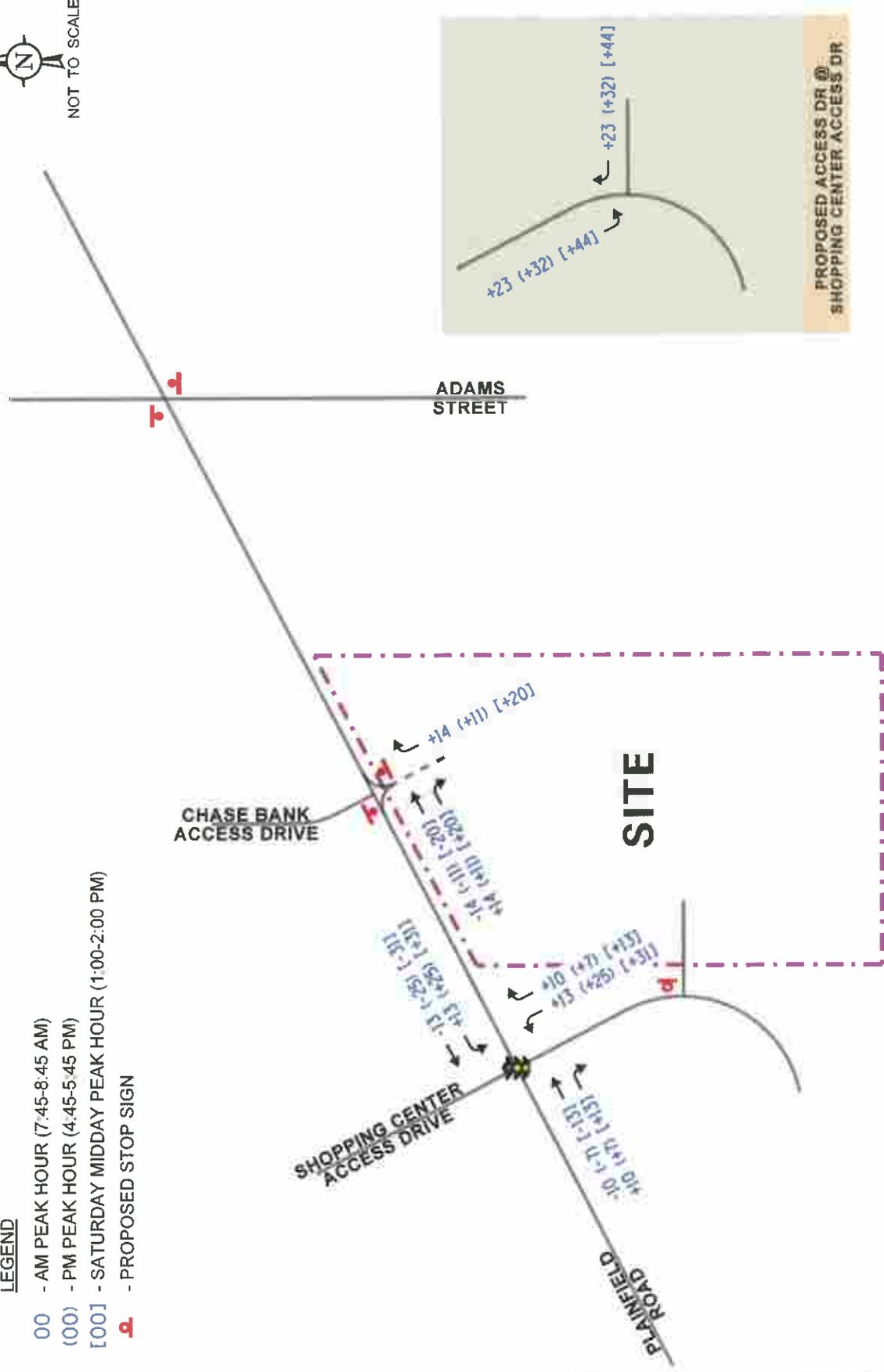
Estimated New Site-Generated Traffic Volumes

Retail Development
 Willowbrook, Illinois



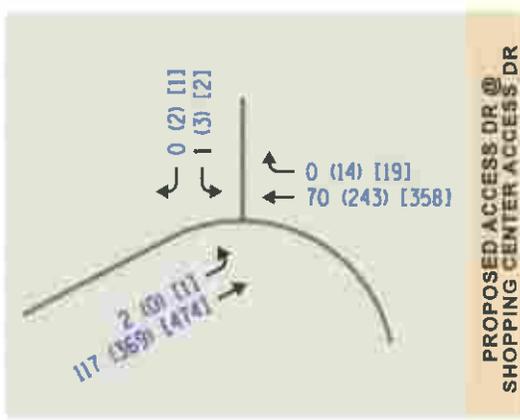
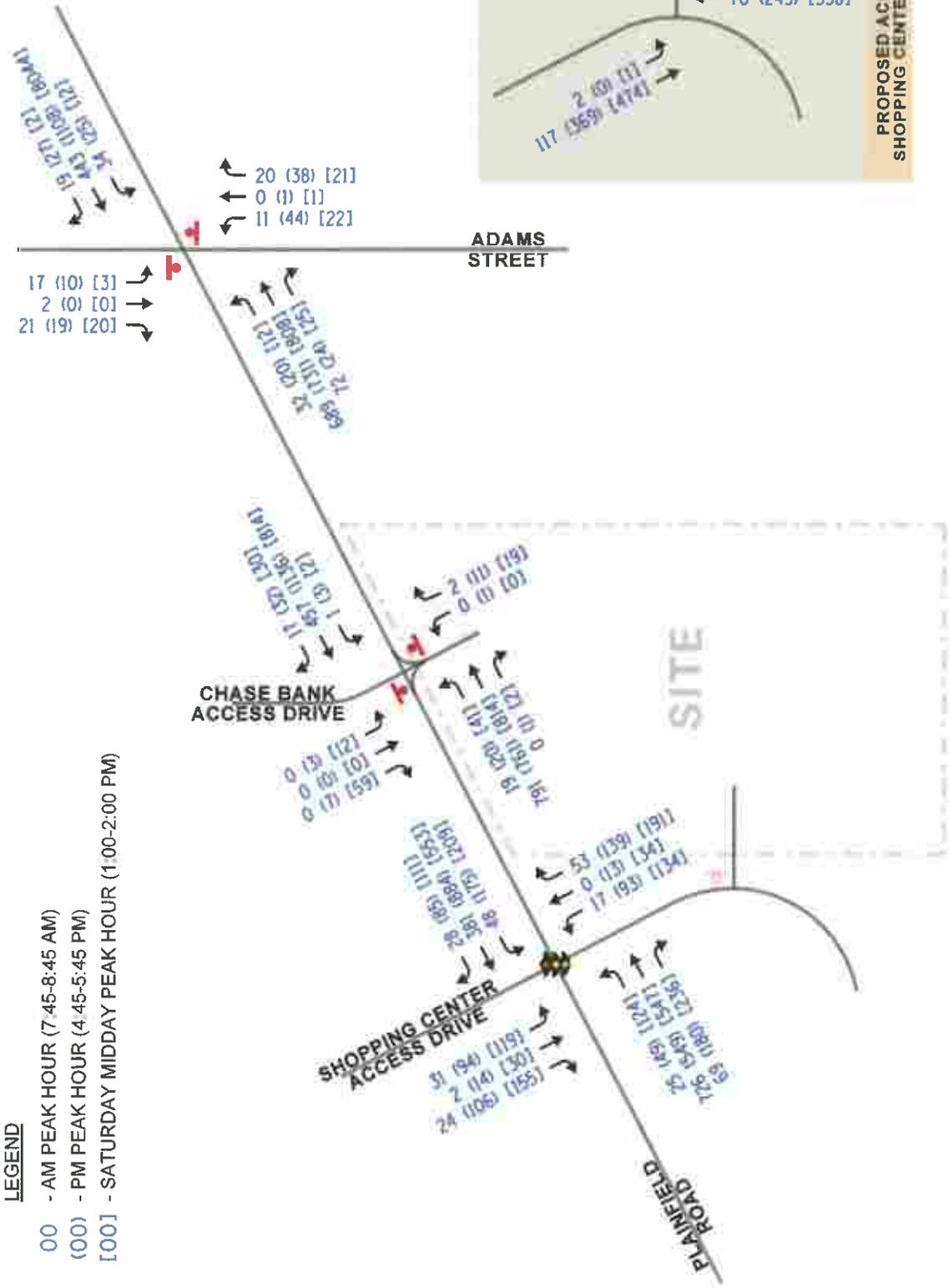
LEGEND

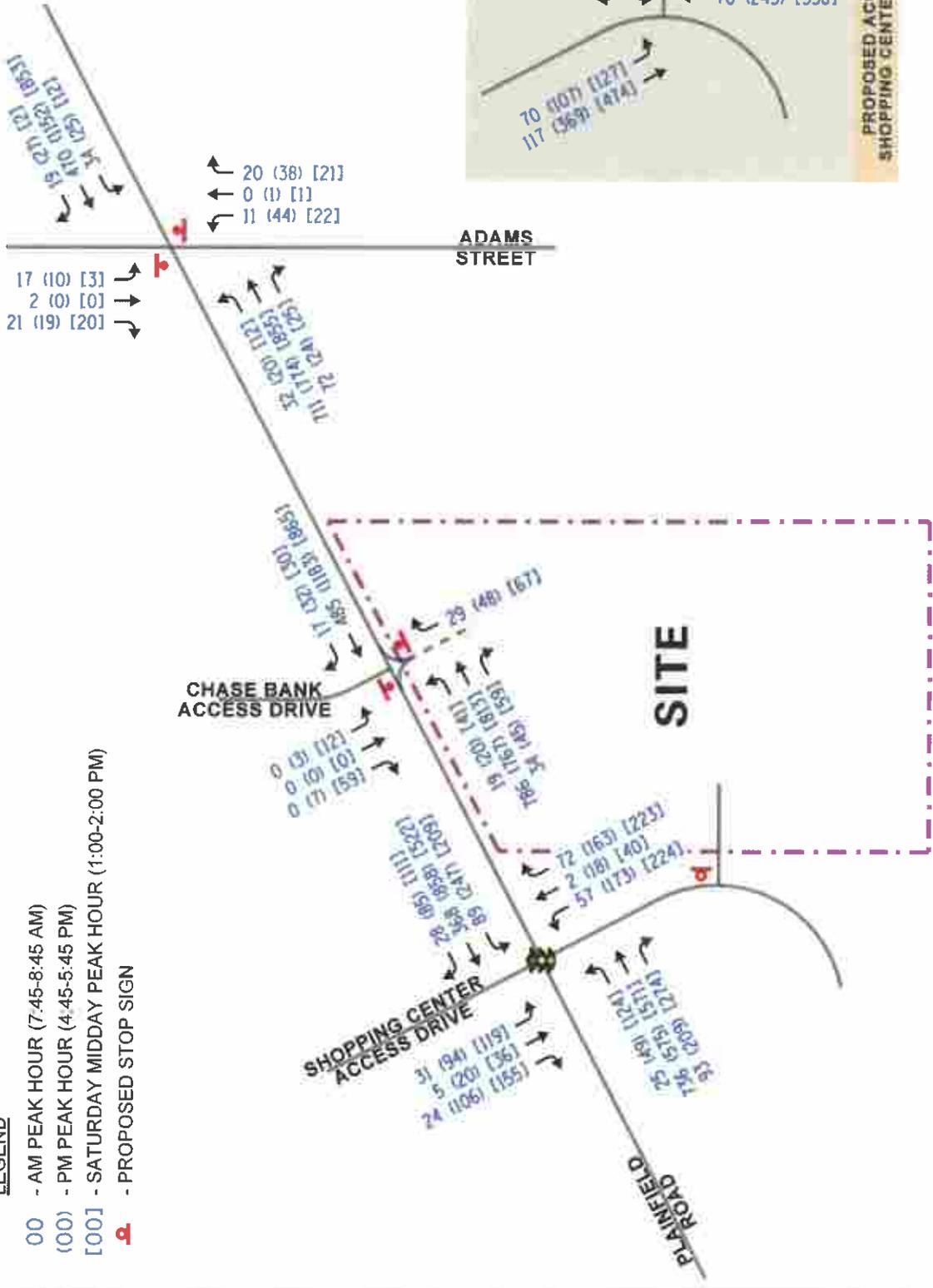
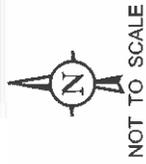
- 00 - AM PEAK HOUR (7:45-8:45 AM)
- (00) - PM PEAK HOUR (4:45-5:45 PM)
- [00] - SATURDAY MIDDAY PEAK HOUR (1:00-2:00 PM)
- ⏹ - PROPOSED STOP SIGN



Retail Development
Willowbrook, Illinois

Pass-By Traffic Volumes





Year 2026 Total Projected Traffic Volumes

Retail Development
 Willowbrook, Illinois

5. Traffic Analysis and Recommendations

The following provides an evaluation conducted for the weekday morning, weekday evening, and Saturday midday peak hours. The analysis includes conducting capacity analyses to determine how well the roadway system and access drives are projected to operate and whether any road improvements or modifications are required.

Traffic Analyses

Roadway and adjacent or nearby intersection analyses were performed for the weekday morning, weekday evening, and Saturday midday peak hours for the existing (Year 2020), Year 2026 no-build, and Year 2026 total projected traffic volumes.

The traffic analyses were performed using the methodologies outlined in the Transportation Research Board's *Highway Capacity Manual (HCM)*, 6th Edition and analyzed using Synchro/SimTraffic 10 software. The analysis for the traffic-signal controlled intersections were accomplished using actual cycle lengths, phasings and offsets to determine the average overall vehicle delay and levels of service.

The analyses for the unsignalized intersections determine the average control delay to vehicles at an intersection. Control delay is the elapsed time from a vehicle joining the queue at a stop sign (includes the time required to decelerate to a stop) until its departure from the stop sign and resumption of free flow speed. The methodology analyzes each intersection approach controlled by a stop sign and considers traffic volumes on all approaches and lane characteristics.

The ability of an intersection to accommodate traffic flow is expressed in terms of level of service, which is assigned a letter from A to F based on the average control delay experienced by vehicles passing through the intersection. The *Highway Capacity Manual* definitions for levels of service and the corresponding control delay for signalized intersections and unsignalized intersections are included in the Appendix of this report.

Summaries of the traffic analysis results showing the level of service and overall intersection delay (measured in seconds) for the existing and total projected conditions are presented in **Tables 5 through 8**. A discussion of each intersection follows. Summary sheets for the capacity analyses are included in the Appendix.

Table 5
CAPACITY ANALYSIS RESULTS
PLAINFIELD ROAD WITH WILLOWBROOK TOWN CENTER ACCESS DRIVE-- SIGNALIZED

Year 2020 Conditions	Peak Hour	Eastbound			Westbound			Northbound			Southbound			Overall
		L	T	R	L	T	R	L	T/R	L	T/R	L	T/R	
Year 2020 Conditions	Weekday Morning Peak Hour	A	A	A	A	A	A	D	E	D	E	D	E	B 13.4
		5.4	9.6	4.6	5.5	8.0	4.4	46.1	71.8	49.3	62.4	49.3	62.4	
		A - 9.1			A - 7.5			E - 65.4			D - 55.3			
Year 2020 Conditions	Weekday Evening Peak Hour	A	B	A	A	B	A	D	F	D	F	D	E	C 23.9
		8.6	15.7	7.9	9.5	15.5	6.0	44.8	87.2	45.8	72.0	45.8	72.0	
		B - 13.4			B - 13.9			E - 69.9			E - 60.5			
Year 2026 No-Build Conditions	Saturday Midday Peak Hour	B	B	A	B	B	A	D	F	D	F	D	E	C 28.8
		10.7	19.1	9.7	12.4	18.5	8.3	36.5	87.2	37.1	70.9	37.1	70.9	
		B - 15.4			B - 15.6			E - 68.3			E - 57.7			
Year 2026 No-Build Conditions	Weekday Morning Peak Hour	A	A	A	A	A	A	D	E	D	E	D	E	B 13.3
		5.4	9.7	4.6	5.5	8.0	4.4	46.1	71.8	49.3	62.4	49.3	62.4	
		A - 9.1			A - 7.5			E - 65.4			D - 55.3			
Year 2026 Total Projected Conditions	Weekday Evening Peak Hour	A	B	A	A	B	A	D	F	D	F	D	E	C 23.8
		8.6	15.8	7.9	9.6	15.7	6.0	44.8	85.2	45.8	72.0	45.8	72.0	
		B - 13.5			B - 14.1			E - 69.9			E - 60.5			
Year 2026 Total Projected Conditions	Saturday Midday Peak Hour	B	B	A	B	B	A	D	F	D	F	D	E	C 28.7
		10.8	19.3	9.7	12.5	18.7	8.3	36.5	87.2	37.1	70.9	37.1	70.9	
		B - 15.6			B - 15.9			E - 68.3			E - 57.7			
Year 2026 Total Projected Conditions	Weekday Morning Peak Hour	A	A	A	A	A	A	D	E	D	E	D	E	B 16.1
		5.7	11.4	4.6	6.1	8.6	4.6	49.2	75.7	48.8	65.1	48.8	65.1	
		B - 10.5			A - 7.9			E - 64.1			E - 56.7			
Year 2026 Total Projected Conditions	Weekday Evening Peak Hour	A	B	A	B	B	A	D	F	D	F	D	F	C 26.6
		8.7	16.8	7.6	11.8	16.0	6.1	52.6	91.8	46.0	80.3	46.0	80.3	
		B - 14.0			B - 14.5			E - 72.7			E - 65.7			
Year 2026 Total Projected Conditions	Saturday Midday Peak Hour	B	B	A	B	B	A	D	F	D	F	D	F	D 35.8
		10.8	19.6	9.2	16.2	18.5	8.3	49.0	99+	39.5	96.9	39.5	96.9	
		B - 15.6			B - 16.6			F - 88.0			E - 74.9			

Letter denotes Level of Service; Delay is measured in seconds. L - Left Turns T - Through R - Right Turns



Table 6
 CAPACITY ANALYSIS RESULTS
 UNSIGNALIZED INTERSECTIONS – EXISTING CONDITIONS

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour		Saturday Midday Peak Hour	
	LOS	Delay	LOS	Delay	LOS	Delay
Plainfield Road with Chase Bank Access Drive						
• Northbound Approach	B	11.1	B	14.2	B	11.4
• Eastbound Left Turn	A	8.4	B	11.3	A	9.8
• Westbound Left Turn	A	9.5	B	10.5	A	9.6
• Southbound Approach	A	0.1	D	28.7	C	20.4
Plainfield Road with Adams Street						
• Northbound Approach	B	14.8	C	21.5	C	17.9
• Eastbound Left Turn	A	8.6	B	11.2	A	9.5
• Westbound Left Turn	A	9.7	A	9.7	A	9.7
• Southbound Approach	B	13.8	C	19.7	B	12.8
Willowbrook Town Center with Internal Access Drive						
• Westbound Approach	A	9.6	B	12.0	B	14.8
• Southbound Left Turn	A	7.3	A	0.1	A	8.1
LOS = Level of Service Delay is measured in seconds.						

Table 7
 CAPACITY ANALYSIS RESULTS
 UNSIGNALIZED INTERSECTIONS – YEAR 2026 NO-BUILD CONDITIONS

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour		Saturday Midday Peak Hour	
	LOS	Delay	LOS	Delay	LOS	Delay
Plainfield Road with Chase Bank Access Drive						
• Northbound Approach	B	11.2	B	14.5	B	11.5
• Eastbound Left Turn	A	8.5	B	11.4	B	10.0
• Westbound Left Turn	A	9.6	B	10.6	A	9.6
• Southbound Approach	A	0.1	D	29.7	C	21.8
Plainfield Road with Adams Street						
• Northbound Approach	C	15.0	C	22.1	C	18.3
• Eastbound Left Turn	A	8.6	B	11.4	A	9.7
• Westbound Left Turn	A	9.8	A	9.8	A	9.8
• Southbound Approach	B	14.0	C	20.2	B	13.1
Willowbrook Town Center with Internal Access Drive						
• Westbound Approach	A	9.6	B	12.0	B	14.8
• Southbound Left Turn	A	7.3	A	0.1	A	8.1
LOS = Level of Service Delay is measured in seconds.						

Table 8
 CAPACITY ANALYSIS RESULTS
 UNSIGNALIZED INTERSECTIONS – YEAR 2026 TOTAL PROJECTED CONDITIONS

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour		Saturday Midday Peak Hour	
	LOS	Delay	LOS	Delay	LOS	Delay
Plainfield Road with Chase Bank Access Drive/Proposed Right-In/Right-Out Access Drive						
• Northbound Approach	B	11.5	B	11.8	B	12.2
• Eastbound Left Turns	A	8.6	B	11.7	B	10.3
• Southbound Approach	A	0.1	D	33.0	C	24.8
Plainfield Road with Adams Street						
Northbound Approach	C	15.3	C	23.5	C	19.4
• Eastbound Left Turn	A	8.7	B	11.7	A	9.9
• Westbound Left Turn	A	9.9	B	10.0	B	10.0
• Southbound Approach	B	14.4	C	21.1	B	13.5
Willowbrook Town Center with Internal Access Drive						
• Westbound Approach	A (A)	9.1 (9.1)	B (B)	11.4 (11.4)	B (B)	13.7 (13.6)
• Southbound Left Turn	A (A)	7.5 (7.5)	A (A)	8.1 (8.1)	A (A)	8.6 (8.6)
LOS = Level of Service Delay is measured in seconds. XX(XX) – With a southbound combined through/left-turn lane (With a southbound exclusive left turn lane and a southbound through lane)						

Discussion and Recommendations

The following is an evaluation of the analyzed intersections based on the projected traffic volumes and the capacity analyses performed.

Plainfield Road with Willowbrook Town Center Access Drive

The results of the capacity analysis indicate that overall this intersection currently operates at Level of Service (LOS) B during the weekday morning peak hour and LOS C during the weekday evening and Saturday midday peak hours. The southbound approach currently operates at LOS D during the weekday morning peak hour and LOS E during the weekday evening and Saturday midday peak hours, and the northbound approach is operating at LOS E during the three peak hours. In addition, the eastbound and westbound approaches currently operate at LOS B or better during all three peak hours.

Under Year 2026 no-build conditions, overall this intersection will continue to operate at the same existing levels of service during the weekday morning, weekday evening, and Saturday midday peak hours with increases in delay of less than one second. All approaches will continue operating at the same existing levels of service during all three peak hours with increases in delay of approximately one second.

Under Year 2026 total projected conditions, overall this intersection will continue to operate at LOS C or better during all three peak hours with increases in delay of approximately three seconds during the weekday morning and evening peak hours and seven seconds during the Saturday midday peak hour, over no-build conditions. The eastbound and westbound approaches will continue to operate at LOS B or better during the weekday morning, weekday evening, and Saturday midday peak hours with increases in delay of approximately one second. In addition, the northbound approach will operate at LOS E during the weekday morning and evening peak hours and LOS F during the Saturday midday peak hour, and the southbound approach will operate at LOS E during all three peak hours. It should be noted that these analyses are conservative, since they do not take into consideration right-turn on red movements. Based on the results of the simulation, the northbound queues will experience 95th percentile queues of approximately 90 feet during the weekday morning peak hour, 167 feet during the weekday evening peak hour, and 164 feet during the Saturday midday peak hour and, as such, will extend to and beyond the proposed full movement access drive by one to two vehicle lengths during the weekday evening and Saturday midday peak hours. However, it is important to note that the queues will generally clear with every green phase during all three peak hours. As such, this intersection has sufficient reserve capacity to accommodate the traffic estimated to be generated by the proposed development and no roadway or traffic control improvements will be required.

Plainfield Road with Chase Bank Access Drive and the Proposed Right-In/Right-Out Access Drive

The results of the capacity analysis indicate that the southbound approach is operating at LOS A during the weekday morning peak hour, LOS D during the weekday evening peak hour, and LOS C during the Saturday midday peak hour. All other movements will operate at LOS B or better during all three peak hours.

Under Year 2026 no-build conditions, all movements will operate at the same existing levels of service with increases in delay of approximately one second during all three peak hours.

As previously indicated, the turning movements at the access drive serving the site will be physically restricted to right turns only via a mountable triangular median. Under Year 2026 total projected conditions, the northbound right-turn movement will operate at LOS B during the weekday morning, weekday evening, and Saturday midday peak hours with 95th percentile queues of one to two vehicles. In addition, the eastbound left-turn movement and the southbound approach will continue to operate at an acceptable LOS D or better during all three peak hours. It should also be noted that the restriction of left-turn inbound and outbound movements at this access drive will reduce the impact of the site-generated traffic on the westbound through movements. As such, this access drive will be adequate in accommodating the traffic estimated to be generated by the proposed development, will ensure efficient and flexible access is provided, and will reduce the traffic load experienced at the signalized intersection of Plainfield Road with the access drive serving Willowbrook Town Center.

Plainfield Road with Adams Street

The results of the capacity analysis indicate that the northbound and southbound approaches are operating at LOS C or better during the weekday morning, weekday evening, and Saturday midday peak hours. In addition, the eastbound and westbound left-turn movements currently operate at LOS B or better during all three peak hours.

Under Year 2026 no-build conditions, all movements will continue to operate at LOS C or better during the weekday morning, weekday evening, and Saturday midday peak hours with increases in delay of less than one second.

Under Year 2026 total projected conditions, all movements will continue to operate at LOS C or better during all three peak hours with increases in delay of approximately one second over no-build conditions. As such, this intersection has sufficient reserve capacity to accommodate the traffic estimated to be generated by the proposed development and no roadway or traffic control improvements will be required.

Willowbrook Town Center with Internal Access Drive

The results of the capacity analysis indicate that the outbound movements will operate at LOS A during the weekday morning peak hour and LOS B during the weekday evening and Saturday midday peak hours with 95th percentile queues of one to two vehicles, with or without the provision of an exclusive southbound left-turn lane. In addition, the southbound left-turn movement will operate at LOS A during the weekday morning, weekday evening, and Saturday midday peak hours, with or without the provision of an exclusive southbound left-turn lane.

When a southbound exclusive left-turn lane is not provided at the access drive, the results of the simulation indicate that vehicles are queuing back toward Plainfield Road, especially during the weekday evening and Saturday midday peak hours.

Therefore, the provision of an exclusive southbound left-turn lane is needed in order to keep the southbound through lane clear and minimize the queuing back toward Plainfield Road. In addition, “Do Not Block Intersection” signage should be provided to prevent vehicles from blocking the access drive. It is also important to note that the provision of a right-in/right-out access drive on Plainfield Road will reduce the impacts of the proposed development-generated traffic at this intersection. As such, this access drive will be adequate in accommodating the traffic estimated to be generated by the proposed development and will ensure efficient and flexible access is provided.

Parking Evaluation

As previously stated, the proposed development calls for a retail development with an approximate 2,800 square-foot quick service restaurant with a drive-through window, an approximate 6,600 square-foot drive-in bank, an automatic car wash tunnel, and approximately 107 parking spaces. From our understanding, the site will contain approximately 43 parking spaces for the use of vacuums and for employee parking of the car wash, with the remaining 64 spaces for the use of the drive-in bank and the quick-service restaurant with drive-through window. In order to determine the parking of the bank and the restaurant, the parking demand was estimated based on the Village of Willowbrook Zoning Code and the rates published in the Institute of Transportation Engineers’ (ITE) Parking Generation Manual, 5th Edition. Based on the two methodologies, the parking demand for the bank and the restaurant uses is as follows:

Village of Willowbrook Zoning Ordinance

- Bank: 22 parking spaces (ratio of one space per 300 square feet)
- Restaurant: 28 parking spaces (ratio of one space per 100 square feet)

ITE Parking Generation Manual

- Drive-In Bank: 25 parking spaces (ratio of 3.72 spaces per 1,000 square feet)
- Fast-Food Restaurant with Drive-Through Window: 24 parking spaces (ratio of 8.66 spaces per 1,000 square feet)

Based on the two above methodologies, the total parking demand for the proposed bank and restaurant will be 50 spaces and 49 spaces, respectively. Therefore, the proposed parking supply of 64 parking spaces will be adequate in accommodating the parking needs of the proposed drive-in bank and the quick-serve restaurant with drive-through window.

On-Site Circulation and Drive-Through Stacking

Car Wash Stacking and Circulation

The single-lane automatic car wash tunnel will have a clockwise circulation. The entrance to the car wash circulation drive will be located in the southeast corner of the site off the two-way drive aisle that connects the site access drive. The circulation drive will provide three approach lanes and each lane will be individually gated with a separate automatic pay station. The gates will meter the traffic flow proceeding to the car wash tunnel entrance and will open in sequence based on the order of vehicle arrival. Once the gate is lifted for the respective lane, the individual vehicle will proceed west and north to the entrance to the tunnel. At the entrance to the car wash tunnel, the driver will remain in the vehicle and the car wash will automatically pull the vehicle through the tunnel. After exiting the tunnel, the vehicles will proceed to turn left to continue to the two-day drive aisle. According to the site plan, there is stacking for approximately 30 vehicles before extending onto the internal circulation drives. Based on KLOA, Inc.'s experience with similar car washes, the site plan provides for efficient circulation and adequate stacking for the proposed car wash. In addition, based on surveys conducted by Spack Consulting at six car washes located in Minnesota, the average maximum queue at a car wash was five vehicles. Wayfinding signage should be posted to guide vehicles to the circulation drive to minimize vehicle turning movements within the internal site circulation area. In addition, "Do Not Enter" signs should be posted at the exit of the car wash tunnel to deter opposing traffic from entering the car wash tunnel or circulation drive from the one-way exit direction.

Quick-Service Restaurant Drive-Through Stacking and Circulation

The drive-through facility for the proposed quick-serve restaurant will extend along the west side of the building. As proposed, vehicles will access the drive-through lane at the southeast corner of the building and exit at the northeast corner of the building. A review of the site plan indicated that approximately eleven vehicles will be able to be accommodated within the drive-through lane without blocking the access drives or internal circulations. This stacking area will be sufficient to accommodate the peak demand of the proposed QSR. Wayfinding signage should be provided directing vehicles to the entrance of the drive-through. Additionally, exiting movements from the drive-through should be under stop sign control.

Bank Drive-Through Stacking and Circulation

The three drive-through lanes for the proposed bank will extend along the south side of the building. As proposed, vehicles will access the drive-through lane at the southwest corner of the building and exit at the southeast corner of the building. A review of the site plan indicated that approximately three to four vehicles will be able to be accommodated within each of the drive-through lanes without blocking the access drives or internal circulation. This should be sufficient to accommodate the peak demand of the proposed bank. Wayfinding signage should be provided directing vehicles to the entrance of the drive-through. Additionally, exiting movements from the drive-through should be under stop sign control.

Based on the above, the following is recommended:

- The site plan provides for efficient circulation and adequate stacking for the proposed uses.
- Appropriate wayfinding signs and striping should be provided within the site directing customers to and from the entrance of the drive-throughs.
- “Do Not Enter” signs should be placed at the exit of each drive-through lane to deter opposing traffic from entering the drive-throughs from the one-way exit direction.
- Exiting movements from the drive-throughs should be under stop sign control.

6. Conclusion

Based on existing conditions and the traffic capacity analyses for the full buildout of the development, the findings and recommendations of this study are outlined below:

- The volume of traffic estimated to be generated by the proposed retail development will be reduced due to pass-by trips and internal capture.
- The results of the capacity analysis indicate that the proposed development traffic will not have a significant impact on the area roadways.
- The proposed access drives will be adequate in accommodating the traffic projected to be generated by the proposed development and will ensure that a flexible access system is provided.
- The provision of a right-in/right out access drive on Plainfield Road will reduce the volume of site-generated traffic traversing the signalized intersection of Plainfield Road with the Willowbrook Town Center access drive.
- The restriction of left-turn inbound and outbound movements at the Plainfield Road access drive will reduce the impact of the site-generated traffic on the westbound through movements.
- Based on a review of the simulation, an exclusive southbound left-turn lane should be provided at the proposed full movement access drive in order to keep the southbound through lane clear and minimize the queueing back toward Plainfield Road.
- The proposed exclusive southbound left-turn lane serving the site will provide approximately 29 feet of storage and 37 feet of taper.
- The site plan provides for efficient circulation and adequate stacking for the proposed uses.
- Appropriate wayfinding signs and striping should be provided within the site directing customers to and from the entrance of the drive-throughs.
- “Do Not Enter” signs should be placed at the exit of each drive-through lane to deter opposing traffic from entering the drive-throughs from the one-way exit direction.
- Exiting movements from the drive-throughs should be under stop sign control.
- Based on a review of the Village of Willowbrook Zoning Code and the rates published in the Institute of Transportation Engineers’ (ITE) *Parking Generation Manual*, 5th Edition, the proposed parking supply of 64 parking spaces will be adequate in accommodating the parking needs of the proposed drive-in bank and the quick-service restaurant with drive-through window.

Return Recorded Document
to:

Brian Pabst
Village Administrator
Village of Willowbrook
835 Midway Drive
Willowbrook, IL 60527

TRAFFIC REGULATION AGREEMENT

THIS TRAFFIC REGULATION AND ENFORCEMENT AGREEMENT (the “Agreement”) is made as of this 16th day of December, 2020, by and between GW Property Group LLC (or its designee), a limited liability company authorized to conduct business in the State of Illinois (the “Owner”), and the Village of Willowbrook, an Illinois home-rule municipal corporation (the “Village”), within which the “Complex” (as defined in Section 1.1 below) is located, for the regulation of traffic and other matters within the Complex and for the enforcement of said regulations by the assigned traffic law enforcement personnel of the Village.

WITNESSETH:

ARTICLE 1. DEFINITIONS: As used in this Agreement, the following definitions apply:

- 1.1 Complex: The land, buildings and other improvements commonly known as 735 Plainfield Road, situated in the Village of Willowbrook, DuPage County, Illinois, and legally described in the attached Exhibit “A”.
- 1.2 Permanent Index Numbers (PINs): 09-23-406-003

- 1.3 Manager: Those persons or entity employed or retained by Owner from time to time with authority to administer, manage and operate the Complex for the purposes of this Agreement.
- 1.4 Owner: GW Property Group, LLC (or its designee).
- 1.5 Village: Village of Willowbrook, DuPage County, Illinois.

ARTICLE 2. RECITAL OF FACTS: The following recitals of fact are an integral part of this Agreement.

- 2.1 Owner holds record title to the Complex.
- 2.2 The Complex is located within the corporate jurisdiction of the Village.
- 2.3 It is the mutual desire of the parties hereto that the Village shall have the authority, but not the obligation, to regulate the parking of vehicles, traffic, roller skating, bicycle riding, skateboarding and/or other recreational activities within the Complex, and to enforce said regulations by the assigned traffic law enforcement personnel of the Village.
- 2.4 The Illinois Vehicle Code (625 ILCS 5/11-209), the Illinois Municipal Code (65 ILCS 5/1-1-7) and the Village's home rule authority provide authority for such agreement between the Village and the Owner and said statutory authorization enumerated on those matters which may be included in such agreement. Further, additional matters may be included in such agreements pursuant to Article VII, Section 6 and Section 10 of the Illinois Constitution.
- 2.5 Manager, in its capacity with Owner, is empowered to enter into this Agreement.

ARTICLE 3. COVENANTS: In consideration of the foregoing premises and of the covenants and conditions hereinafter contained, the adequacy and sufficiency of which the parties hereto hereby stipulate, each of the respective parties hereto covenants and agrees as follows:

- 3.1 The Owner will cause the Manager or its designated representative to cooperate with the Chief of Police of the Village or his designated representative in inspecting the parking area of the Complex to determine what, if any, stop signs, yield signs, person with disabilities parking area signs, or any other traffic markers are to be erected, and to determine what, if any, areas are to be marked as stop intersections, yield intersections, person with disabilities parking areas or pedestrian crossings, in order to provide for the safe and efficient trafficking of the parking area of the Complex. Further, such determination may include the regulation and/or prohibition of roller skating, the riding of bicycles, the riding or

operation of skateboards and/or other recreational activities in and upon the Complex, and the posting of the signs with the respect thereto.

- 3.2 If it be determined, pursuant to Section 3.1 of this Agreement, that stop signs, yield signs, person with disabilities parking area signs, or any other markers are to be erected or that specified intersections are to be marked as stop intersections, yield intersections, or pedestrian crossings, the Owner agrees to erect such signs and markers at the indicated places on the Complex, in accordance with all applicable regulations and specifications promulgated by the State of Illinois. The Owner shall bear the costs and expenses of obtaining, erecting and maintaining any and all such signs and markers necessary for enforcement of the regulations agreed to by the parties herein.
- 3.3 The Owner shall cause the Manager to mark such fire lanes as the local Fire Protection District Chief or his designated representatives shall recommend as necessary for effective movement of Fire Department and other emergency vehicles.
- 3.4 Signs or other devices providing for the regulation of traffic and parking, or the regulation or prohibition of roller skating, bicycle riding, skateboarding and/or other recreational activities, within the Complex, as well as a designation of the exact regulations to be imposed thereon, shall be installed as shown in the Traffic Control Plan for GW Properties Willowbrook as prepared by Kimley-Horn, 4201 Winfield Road, Warrenville IL, 60555, referenced as Project No. 168726009, consisting of one sheet, dated December 15th, a copy of which is attached hereto as Exhibit "B" and by this reference incorporated herein.
- 3.5 The Village has the authority, but not the obligation, to enforce all regulations in the parking areas of the Complex by use of assigned traffic enforcement personnel of the Village; to issue citations to any and all violators of such regulations; and to adopt and enforce any additional reasonable rules and regulations with respect to traffic and parking in the parking area as local conditions may require or the safety and convenience of the public or the users of the parking area.
- 3.6 The Owner hereby agrees to provide for the removal of vehicles that are abandoned or parked in areas where stopping, standing or parking is prohibited. Such removal shall be done pursuant to a towing agreement and in accordance with the requirements of 625 ILCS 5/4-203, including, but not limited to, posting of the notice required therein.
- 3.7 Neither the Owner nor the Manager shall permit any person to park a vehicle at any location in the Complex for the purpose of displaying such vehicle for sale, lease or ride-share. The owner of any such vehicle may be ticketed by the Village.

- 3.8 The Owner has named the Village as an additional insured on a primary non-contributory basis on its Comprehensive General Liability Insurance Policy and on their Excess Liability Insurance Policy and a copy of said Certificate of Insurance is attached hereto as Exhibit "C" and incorporated herein. Such liability insurance shall provide that the Village, its officers, agents, agencies, employees, and departments shall be additional insured under such insurance. Said insurance shall be in the minimum amount of one million dollars (\$1,000,000) combined single limit or in such amounts acceptable to the Village and shall be in such form and with such Company as shall be approved by the corporate authorities.

The Owner hereby agrees to keep said policies in full force and effect throughout the terms of this Agreement. A mandatory written notice must be provided upon the cancellation of any policy as outlined under the Certificate of Insurance evidencing the coverage provided for herein.

- 3.9 In the event the Owner changes Managers, the Owner shall notify in writing the Village within ten (10) days of such change.

ARTICLE 4. TERM:

- 4.1 This Agreement shall be in full force and effect from and after the date of its execution for a period of twenty (20) years of the date thereof, and may, by further agreement of the parties, be continued for additional periods of like duration.

Notwithstanding any provision contained herein to the contrary, after this Agreement has been in effect for a term of one (1) year, this Agreement may be canceled upon the giving of thirty (30) days prior written notice by either party hereto, except to the extent that the Owner may be required to maintain this Agreement pursuant to any zoning relief granted by the Village.

- 4.2 The sole remedy available to the Owner, upon any breach of this Agreement by the Village, shall be the cancellation of the Agreement under its terms. It is of the essence of this Agreement that the Village shall not be liable in money damages for any breach of this Agreement.

ARTICLE 5. SUCCESSORS: This Agreement shall be binding upon and inure to benefit the respective assigns, successors and personal representatives of each of the parties hereto, and this Agreement shall run with title to the Complex and the obligations of the Owner shall be and are binding on future owners of any portion of the Complex.

ARTICLE 6. GOVERNING LAW: This Agreement shall be governed by the laws of the State of Illinois.

ARTICLE 7. EXECUTION AND RECORDING: This Agreement shall be executed by the parties' prior to the commencement of operations of the Complex. A fully executed copy of the Agreement shall be recorded in the Office of the Recorder of Deeds of the County of DuPage of the State of Illinois against title to the Complex, and it is agreed, pursuant to the statutes set forth above, that no regulation made pursuant to this Agreement shall be effective or enforceable until three (3) days after this Agreement is recorded.

ARTICLE 8. NOTICES: All notices hereunder shall be in writing and sent by Certified Mail, addressed to the Manager at 2211 N Elston, Suite 304, Chicago, IL 60614 and, if to the Village, at the Office of the Village Administrator of Willowbrook, 7760 Quincy Street, Willowbrook, Illinois 60527.

IN WITNESS WHEREOF, the parties hereto have caused this Agreement to be duly executed, as of the date and year first above mentioned.

OWNER

BY:

M. H. [Signature]

(Managing Agent or Owner)

VILLAGE OF WILLOWBROOK

BY.

President

ATTEST:

Village Clerk

EXHIBIT "A"

LEGAL DESCRIPTION OF COMPLEX

EXHIBIT "B"

TRAFFIC REGULATION PLAN

Pursuant to the attached agreement, the Willowbrook Police Department has the authority, but not the obligation, to enforce the following areas as designated on the traffic control plan attached hereto.

1. Traffic control signs including posted speed limit signs, stop signs, yield signs, and one-way signs.
2. No parking within 15 feet of hydrant locations as marked.
3. Handicap parking areas as marked with an official sign.
4. Prohibition of parking along all streets between 2:00 a.m. and 6:00 a.m.
5. Enforcement of yellow curb markings with adjacent "No Parking" signs indicating no parking areas.
6. Enforcement of posted, "No Trespassing" signs.
7. Posted "No Parking" zones.
8. Parking of vehicles for the purpose of being displayed for sale.
9. Enforcement of Village ordinance violations.
10. Prohibition of roller skating, bicycle riding, skateboarding and/or other recreational activities within the complex as posted.

Any future signs, crosswalks, and so forth may be agreed upon at a later date.

Owners' Representative

Chief of Police

[ATTACH PLAN]

EXHIBIT "C"

CERTIFICATE OF INSURANCE

SPACE ABOVE RESERVED FOR RECORDER' S USE

After recording, return to:
Northstone Law LLC
1016 W. Jackson Blvd., Suite 509
Chicago, IL 60607
Attn: Animesh K. Ravani, Esq.

DECLARATION OF OPERATING AND RECIPROCAL EASEMENT AGREEMENT

THIS DECLARATION OF OPERATING AND RECIPROCAL EASEMENT AGREEMENT (“Agreement”), entered into as of the ____ day of _____, 2020 (the “Effective Date”), by **735 PLAINFIELD PROPERTY LLC**, an Illinois limited liability company, **JPMORGAN CHASE BANK, NATIONAL ASSOCIATION**, a national banking association, and **ENCORE CW, LLC**, a Delaware limited liability company.

RECITALS

A. Encore CW, LLC is the fee simple owner of land located in the Village of Willowbrook, DuPage County, Illinois, which parcel of land is hereinafter referred to as the “Lot 1” (the “Lot 1 Owner”). Lot 1 is legally described on **Exhibit A-1** attached hereto and is depicted on **Exhibit B** (the “Site Plan”).

B. 735 Plainfield Property LLC is the fee simple owner of land located in the Village of Willowbrook, DuPage County, Illinois, which parcel of land is hereinafter referred to as the “Lot 2” (the “Lot 2 Owner”). Lot 2 is legally described on **Exhibit A-2** attached hereto and is depicted on the Site Plan.

C. JPMorgan Chase Bank is the fee simple owner of land located in the Village of Willowbrook, DuPage County, Illinois, which parcel of land is hereinafter referred to as the “Lot 3” (the “Lot 3 Owner”). Lot 3 is legally described on **Exhibit A-3** attached hereto and is depicted on the Site Plan.

D. Lot 1, Lot 2, and Lot 3 are sometimes together referred to as the "Overall Development" and individually as a "Lot" or "Parcel" and collectively, as the "Lots" or the "Parcels." The fee simple owner(s) of Lots are sometimes together referred to as the "Lot Owners" or "Parties" or singularly as a "Lot Owner" or "Party." The Overall Development is located south of that public right of way currently known as 735 Plainfield Road, Willowbrook, Illinois (collectively, the "Adjacent Public Rights of Way").

E. Lot 2 Owner intends to develop the Overall Development in accordance with the Site Plan, with common roads, common entry drives and certain common utilities although nothing in this Agreement shall constitute an express or implied obligation on the part of either Owner to make any improvements to their respective Lot, except as expressly provided in this Agreement. The Overall Development shall contain (i) driveways to allow for the ingress and egress of persons and vehicles to and from the Overall Development and _____ and to and from the Overall Development and _____ (collectively, the "Interior Drives"), (ii) designated paved areas for the parking of vehicles (the "Interior Parking Areas"). Together the Interior Drives and the Interior Parking Areas are referred to herein as the "Common Areas." The Common Areas are depicted on the Site Plan.

NOW, THEREFORE, in consideration of the foregoing, subject to the conditions, covenants and agreements set forth herein, each Lot Owner hereby declares, grants and imposes the following easements, rights, obligations and conditions as to the Overall Development:

ARTICLE I EASEMENTS

1.1 Project Signs.

a. Each Lot Owner shall have the right to signage on the common monument signs ("Project Signs") located on the Overall Development, and easement rights associated therewith, including, without limitation, access and maintenance right ("Project Sign Easement Areas"). Each Lot Owner hereby grants to the other Lot Owners, perpetual, nonexclusive easements within the Project Sign Easement Areas for the installation, operation, maintenance, repair and replacement of the Project Signs.

b. The Project Signs shall be operated, controlled and maintained in good condition and repair by the Lot 2 Owner, at the Lot 2 Owner's sole cost and expense, in conformity with all governmental regulations. Each Lot Owner granted an easement to a Project Sign shall pay to the Lot 2 Owner, on an annual basis, its allocable share of the Lot 2 Owner's reasonable and documented maintenance costs for the respective Project Signs. Each Lot Owner's allocable share of such maintenance costs shall be based on the square footage of the Lot Owner's sign panels on a Project Sign, in relation to the total square footage of all the sign panels on the applicable Project Sign (but excluding such portions of the sign used for the identification of the Overall Development). Each request for payment shall be in writing, shall be accompanied by paid receipts and bills and a calculation showing how the respective Lot Owner's allocable share was determined. Each such reimbursement shall be due and payable within twenty (20) days after receipt of such items.

c. Notwithstanding the foregoing, each Lot Owner having panel space on a Project Sign shall, at its sole cost and expense, (i) obtain all permits and approvals required for the installation of its individual sign panels, (ii) fabricate its identification panels, install the panels, and (iii) maintain and/or replace its individual sign panels pursuant to all governmental regulations, and in a safe condition and good state of repair.

d. If a Project Sign is no longer available for freestanding sign purposes because of a condemnation or any governmental requirements, the respective owners of the "Project Sign Easement Areas", shall designate a replacement Project Sign area with comparable visibility as close to the original location as is reasonably possible and all signage rights granted herein shall transfer to such replacement Project Sign.

1.2 Interior Drives; Interior Parking Areas (Common Areas)

a. The Lot 1 Owner hereby grants for the benefit of each Lot, each Lot Owner, and each Lot Owner's respective successors, assigns, tenants, licensees, invitees, customers and employees, a perpetual, reciprocal and non-exclusive easements over and across the (i) Interior Drives, as they exist from time to time, for the ingress and egress of persons and vehicles between the Overall Development and the Adjacent Public Rights of Way; and (ii) the Interior Parking Areas, to the extent located on any Lot as they exist from time to time, for the parking of vehicles in connection with business operations conducted on the Lots.

b. The Lot 2 Owner hereby grants for the benefit of each Lot, each Lot Owner, and each Lot Owner's respective successors, assigns, tenants, licensees, invitees, customers and employees, a perpetual, reciprocal and non-exclusive easements over and across the (i) Interior Drives, as they exist from time to time, for the ingress and egress of persons and vehicles between the Overall Development and the Adjacent Public Rights of Way; and (ii) the Interior Parking Areas, to the extent located on any Lot as they exist from time to time, for the parking of vehicles in connection with business operations conducted on the Lots.

c. The Lot 3 Owner hereby grants for the benefit of each Lot, each Lot Owner, and each Lot Owner's respective successors, assigns, tenants, licensees, invitees, customers and employees, a perpetual, reciprocal and non-exclusive easements over and across the (i) Interior Drives, as they exist from time to time, for the ingress and egress of persons and vehicles between the Overall Development and the Adjacent Public Rights of Way; and (ii) the Interior Parking Areas, to the extent located on any Lot as they exist from time to time, for the parking of vehicles in connection with business operations conducted on the Lots.

1.3 Utilities.

a. The term "Utility Lines" shall mean those facilities and systems providing for the transmission of water, sanitary sewer, storm sewer, gas, electric and telecommunications, excluding public utility lines, existing as of the date of this Agreement. The term "Common Utility Lines" shall mean those Utility Lines, which are installed to provide the applicable service to more than one parcel, excluding public utility lines. The term "Separate Utility Lines" shall mean those Utility Lines, which are installed to provide the applicable service to one

particular Parcel. For the purpose of this Agreement, the portion of a Utility Line extending between a Common Utility Line and a building or the termination of the line shall be considered a Separate Utility Line.

b. Each Lot Owner hereby grants for the benefit of each Lot, each Lot Owner, and each Lot Owner's respective successors and assigns, a perpetual, nonexclusive easement under, through and across such portions of the Overall Development for the operation, flow, passage, use, maintenance, connection, repair, removal and replacement the Utility Lines.

c. The Lot Owners shall maintain, repair, and replace the Separate Utility Lines serving their respective Parcel, at such Lot Owner's sole cost and expense, in good order, appearance and repair in full compliance with the applicable laws and regulations and in such a manner to minimize any interference with utility services. The Lot Owners shall use commercially reasonable efforts to complete any maintenance, repair, or replacement to any Separate Utility Lines that are located on another Party's Parcel, expeditiously, in order to minimize any interference with business operations at the repair site and shall promptly restore such Party's Parcel to the condition immediately prior to completing said maintenance, repair, or replacement.

d. The Utility Lines shall not be removed, reconstructed or reconfigured without first obtaining the prior written approval of the Lot 2 Owner, which the Lot 2 Owner may withhold in its sole discretion.

1.4 Common Areas.

a. The Lot 2 Owner shall maintain the Common Areas, at its initial cost, in good order, appearance and repair in full compliance with the applicable laws at a standard that is at least comparable to the standard of other first-class shopping centers in the metropolitan area where the Overall Development is located. The Lot 2 Owner shall use commercially reasonable efforts to complete any maintenance, repair, or replacement to any Common Areas that are located on another Party's Parcel, expeditiously, in order to minimize any interference with business operations at the repair site and shall promptly restore such Party's Parcel to the condition immediately prior to completing said maintenance, repair, or replacement.

b. The Lot Owners shall pay the Lot 2 Owner, on an annual basis, its allocable share of the Lot 2 Owner's out-of-pocket maintenance costs for the Common Areas. The Lot Owner's allocable share of such maintenance costs shall be based on the acreage of the respective Parcel in relation to the acreage of the Overall Development utilizing the respective Common Areas. Each request for payment shall be in writing, shall be accompanied by paid receipts and bills and a calculation showing how each Party's allocable share was determined. Each such reimbursement shall be due and payable within thirty (30) days after receipt of such items. In the event a Lot Owner fails to pay the Lot 2 Owner within the specified time period, (a) the Lot Owner shall be responsible for interest on such amount computed at the rate of the smaller of (i) ten (10%) percent per annum and (ii) the highest interest rate allowed by law, calculated monthly, from the date of any such claim by the Lot 2 Owner to the date of payment; (b) the Lot 2 Owner shall be entitled to pursue whatever remedies it may have in law or equity; and (c) the Lot 2 Owner shall have the right to file a lien against such Lot in the amount of the claim. Any

such lien shall be considered to be akin to a mortgage lien and the Lot 2 Owner may foreclose upon it in the same manner as a mortgage lien. In no event shall any lien filed under this paragraph be superior to any lien of any construction mortgage utilized to facilitate the initial development of a Lot from its current status as a vacant parcel and recorded in the real property records prior to the date such Lot Owner has completed such initial development. The provisions and requirements of the Illinois Construction Lien Act shall not apply to this lien.

ARTICLE II PARCEL RESTRICTIONS & STANDARDS

2.1 **Restricted Uses.**

a. During the Term, no part of Lot 1 or Lot 2 shall be used, directly or indirectly, for the operation a full service financial institution, including, without limitation, the provision of banking, mortgage lending, insurance and securities services, as well as to operate drive-through banking facilities or exterior automatic teller machines (“ATM”). In addition, there shall be no ATMs from operating within the interior of any improvements located on the Lot 1 or Lot 2 that is visible from the exterior or for which there is signage that is visible from the exterior of the any structure located within the Overall Development which advertises or otherwise indicates the presence of an ATM.

2.2 **Applicable Definitions.** As used in this Article II, the following terms have the meaning provided herein:

a. The term “operation” shall include not only the store building but also, the parking areas, drives, entries, truck docks, or any other improvements used in connection with or to support such a store or to support a shopping center of which such a store is a part.

b. The term “gross building floor area” shall mean the gross interior dimensions of the main floor of the building, excluding shelving, racks, decks, mezzanines, basements, and second story areas.

2.3 **Tenant-Specific Restricted Uses.**

a. Pursuant to the terms and conditioned contained within that certain lease by and between the Lot 2 Owner, as landlord, and Guzman Y Gomez, as tenant (“Guzman”), dated _____, 2020 (the “Guzman Lease”), Lot 1, Lot 2, and Lot 3 shall be subject to the restrictions as described on **Exhibit D** attached hereto (the “Guzman Restrictions”). The Guzman Restrictions shall continue during the term of the Guzman Lease, which shall be confirmed by the Lot 2 Owner pursuant to Section 6.12.

2.4 **Maintenance Standard.** Each Lot Owner shall maintain their respective Parcel, including all landscaping and improvements thereon, in a sightly, safe condition and in a good state of repair at a standard that is at least comparable to the standard of other first-class shopping centers in the metropolitan area where the Overall Development is located.

ARTICLE III
INSURANCE & INDEMNITY

3.1 **Property Insurance.** During the Term of this Agreement and subject to the right to self-insure set forth in Section 3.1(g), each Lot Owner shall, at its sole cost and expense, obtain and keep in force:

a. Commercial general liability insurance, including premises/operations, products/completed operations, blanket contractual liability, broad form property damage, independent contractors and personal/advertising injury coverage, fire and explosion legal liability, explosion/collapse/and underground hazard coverage in an amount not less than Three Million Dollars (\$3,000,000) per occurrence; such policy shall be an occurrence policy and not a claims-made policy.

b. Each Party may satisfy its obligation under this Section 3.1(a) by appropriate endorsement of its blanket insurance policies. Such insurance shall name the other Parties (and their parent and affiliated companies) as additional insureds on an endorsement acceptable to the other Parties. The additional insured endorsement shall extend coverage to the contractual liability and completed operations coverage. All liability policies maintained by a Party shall be written as primary policies, not contributing with and not in excess of coverage that any other Party may carry.

c. Automobile liability insurance including coverage for owned, hired, and non-owned automobiles. The limits of liability shall not be less than \$1,000,000 combined single limit each accident for bodily injury and property damage combined.

d. Workers' Compensation coverage for its employees or contractors with statutory limits.

e. All insurance required in this Agreement and all renewals of it shall be issued by companies authorized to transact business within the state in which the Overall Development is located.

f. Subject to the right to self-insure set forth in Section 3.1(g), each Party shall, upon the written request of another Party, promptly furnish the other Party with one (1) or more certificates of insurance evidencing the existence of the insurance required to be carried pursuant to this Agreement. No Party shall be required during any given 180-day period or term of the policy, whichever is shorter, to honor more than one (1) such request from another Party.

g. The Owners each hereby waive any rights one may have against the other on account of any loss or damage occurring to an individual Owner, or its respective property, either real or personal, arising from any risk generally covered by the Owner's property insurance, from any risk covered by standard forms of special form property insurance policies then in effect and from any risk covered by any other property insurance which is actually carried by said Owner. Said Owners shall use reasonable efforts to obtain, if needed, appropriate endorsements to the Owner's property insurance with respect to the foregoing waiver; provided, however, that failure to obtain such endorsements shall not affect the waiver hereinabove given. In addition, said Owners shall cause the insurance companies issuing the Owner's property

insurance to waive any right of subrogation that said insurance companies may have against the Owners. It is the intent of the Parties that with respect to any loss from a peril required to be covered under a policy of Owner's property insurance, the Parties shall look solely to their respective insurance company for recovery.

h. If a Party elects to self-insure all or a part of the insurance required in Section 3.1(a), then Section 3.1(b), 3.1(e) and 3.1(f) hereof shall not apply to any Party or such Party's successors or assigns so long as such Party, or its parent company, maintains a net worth in excess of One Hundred Million Dollars (\$100,000,000) as certified by an officer of such Party.

3.2 Indemnification. Each Lot Owner, for itself and its successors and assigns (each an "Indemnifying Party"), agrees to indemnify and hold the other Lot Owners (each an "Indemnified Party") harmless from and against any loss or damage of any type or nature to the extent caused by the act or omission of an employee, agent or contractor of an Indemnifying Party while performing any construction, maintenance or repair activities on the Indemnified Party's Parcel for the benefit of the Indemnifying Party's Parcel or the Overall Development, including, without limitation, attorneys' fees and costs; provided, however that such indemnification shall not extend to an Owner's negligence or the negligence of such Owner's tenants or invitees. Subject to the provisions of Section 3.1(g) hereof regarding waiver of subrogation with respect to damage to property, each Owner shall defend, indemnify, protect and hold the other Owners harmless for, from and against any and all claims in connection with the loss of life, personal injury and/or damage to property (i) arising from or out of any occurrence in or upon the indemnifying Owner's Lot, including an Owner's own negligence; (ii) occasioned wholly by any negligent or willful act or omission of the indemnifying Owner, its tenants, its invitees or their respective its agents, contractors, servants or employees; or (iii) in connection with the failure to comply with the provisions of this Agreement. An Owner shall have the right to engage its own attorneys in connection with any of the provisions of this Section 3.2 or any of the provisions of this Agreement, including, but not limited to, any defense of an Owner, notwithstanding any contrary provisions of the laws or court decisions of the state in which the Overall Development is located.

ARTICLE IV TERM AND TERMINATION

4.1 Term and Termination. This Agreement shall be effective as of the date first above written and shall continue in full force and effect until 11:59 p.m. on the date that is forty (40) years after the date that this Agreement is recorded in the county and state where the Overall Development is located (the "Term"); provided, however, that (i) the easements referred to in Article I hereof which are specified as being perpetual or as continuing beyond the Term of this Agreement shall continue in full force and effect as provided herein, (ii) the Guzman Restrictions set forth in Section 2.4 of Article II shall continue for the term described in the Guzman Lease and (iii) other time limits shall be as specifically provided in this Agreement. Except as provided in the preceding sentence, upon the termination of this Agreement, all rights and privileges derived from and all duties and obligations created and imposed by the provisions of this Agreement shall terminate and have no further force or effect; provided, however, that the termination of this Agreement shall not limit or affect any remedy at law or in equity that a Party

may have against any other Party with respect to any liability or obligation arising or to be performed under this Agreement prior to the date of such termination.

ARTICLE V ENFORCEMENT

5.2 **Defaults; Self-Help.** If a Lot Owner shall default in the performance of an obligation of such Lot Owner under this Agreement (a “Defaulting Party”), the other Lot Owners (the “Nondefaulting Party”), in addition to all other remedies provided at law or equity, after thirty (30) days’ prior written notice to the Defaulting Party, shall have the right of “self-help” to perform such obligation on behalf of the Defaulting Party; provided, however, that in the event of an emergency, the Nondefaulting Party may exercise the right of self-help if such failure is not cured within a period reasonable for the nature and circumstances of such emergency. In such event, the Defaulting Party shall promptly reimburse the Nondefaulting Party the reasonable, actual out-of-pocket cost thereof, together with interest thereon from the date of outlay at a rate equal to ten percent (10%) in excess of the prime interest rate published in the money rates section of the Wall Street Journal (the “Interest Rate”). The Parties agree that to the extent that the Nondefaulting Party must enter upon the property of the Defaulting Party in order to exercise such right of “self-help”, the Defaulting Party hereby grants the Nondefaulting Party a license to enter and perform such obligation.

ARTICLE VI TAXES AND ASSESSMENTS

6.1 **Taxes and Assessments.**

a. Lot Owners 1 and 3 shall have the right to pay any taxes due with regard to the Overall Development that includes its Lot if the Lot 2 Owner shall fail to pay such taxes on or prior to the due date, which date may be extended to the extent permitted by law if Lot 2 Owner is contesting such taxes. In the event Lot Owner 1 or Lot Owner 3 pays the taxes for the Overall Development within the specified time period, (a) Lot 2 Owner shall reimburse the Lot Owner 1 or Lot Owner 3 for the amount paid on its behalf, less the amount of such taxes applicable to its Lot within fifteen (15) days after Lot 2 Owner’s receipt of written demand therefor from either Lot Owner 1 or Lot Owner 3; (b) Lot Owner 1 or Lot Owner 3 shall be entitled to pursue whatever remedies it may have in law or equity; and (c) Lot Owner 1 or Lot Owner 3 shall have the right to file a lien against such Lot 2 in the amount of the claim. Any such lien shall be considered to be akin to a mortgage lien and the Lot Owner 1 or Lot Owner 3 may foreclose upon it in the same manner as a mortgage lien. In no event shall any lien filed under this paragraph be superior to any lien of any construction mortgage utilized to facilitate the initial development of Lot 2 from its current status as a vacant parcel and recorded in the real property records prior to the date Lot 2 Owner has completed such initial development. The provisions and requirements of the Illinois Construction Lien Act shall not apply to this lien.

b. Each Owner shall on or before the due date therefor all taxes, assessments, or charges of any type levied or made by any governmental body or agency with respect to its Lot.

ARTICLE VII
GENERAL

7.1 **No Covenant to Build.** This Agreement shall not create or impose any obligation on any Lot Owner to build, open, operate, or continuously operate, a business or any particular business on its respective Parcel(s).

7.2 **Easements and Covenants Running With the Land.**

a. Each of the easements granted by this Agreement shall constitute a servitude on the property of the grantor and an appurtenance to the property of the grantee for the Term of this Agreement (unless otherwise stated to be perpetual), shall survive the total or partial destruction of the subject matter of the easement and/or the servient tenement of such grant, and shall run with the land. The grantee of any such easement may extend the benefits thereof to each of its tenants and/or occupants of the benefitted property. An estate for whose benefit an easement is established hereunder shall for the purposes of this Section 6.2 be deemed to be the grantee of such easement.

b. It is intended that during the Term each and all of the covenants and agreements in this Agreement to be performed by or on the part of a Party, whether affirmative or negative in nature, shall be construed as covenants and not as conditions. To the fullest extent legally possible, all such covenants by a Party shall run with the land to the end that (i) the covenants of a Party undertaking a burden on its Parcel shall be appurtenant to the benefitted Parcels, except as otherwise provided, and shall constitute covenants running with the land as between the respective Parcels, with the benefitted Parcel as the dominant tenement, and the burdened Parcel as the servient tenement.

7.3 **Exhibits.** All exhibits and schedules to this Agreement are by this reference made a part of this Agreement to the same extent as if the same had been written directly within the body of this Agreement.

7.4 **Rights Cumulative.** All rights, powers and privileges conferred hereunder shall be cumulative and not restricted to those given by law.

7.5 **Partial Invalidity.** If any covenant, term or condition of this Agreement or any application thereof shall be invalid or unenforceable, the remainder of this Agreement and any other application of such covenant, term or condition shall not be affected thereby.

7.6 **Governing Law.** This Agreement shall be construed according to, and be governed by, the laws of the state in which the Overall Development is located.

7.7 **Waiver of Performance by Any Party.** One or more waivers of any covenant, term or condition of this Agreement by any Party shall not be construed as a waiver of a subsequent breach of the same or any other covenant, term or condition; nor shall any delay or omission by any Party to seek a remedy for any breach of this Agreement or to exercise a right accruing to such Party by reason of such breach be deemed a waiver by such Party of its remedies or rights with respect to such breach. The consent or approval by any Party to or of any act by another

Party requiring such consent or approval shall not be deemed to waive or render unnecessary consent to or approval of any similar act.

7.8 **Counterparts.** This Agreement may be executed in any number of counterparts, each of which shall be an original, but all of which shall together constitute one and the same instrument.

7.9 **Modification.** There shall be one approving Party representing each Parcel (individually an "Approving Party" and together the "Approving Parties"). Each Approving Party shall have absolute authority to make the decisions and/or give the approvals expressly designated to be made and/or given on behalf of the real estate represented by such position regardless of whether the Approving Party then owns all or less than all of the particular Parcel. As to any Parcel, the holder of the Approving Party position shall have the right to assign such position to any other entity having a fee interest in land within that Parcel. If an Approving Party transfers all of its fee interest in a Parcel, and does not assign its Approving Party position, then such Approving Party position shall automatically be deemed assigned to the entity acquiring fee title to the land owned by the transferring Approving Party. The signatories hereto shall be the initial Approving Party for their respective Parcels as herein identified.

7.10 **Notice.** Any notice, demand, consent, approval, request, statement, document or other communication required or permitted to be given to or served upon either Party hereto pursuant to this Agreement or applicable law shall be in writing and shall be sent by certified mail, or by a recognized national courier service, such as, but not limited to, Federal Express or United Parcel Service, postage prepaid, addressed and shall be deemed to have been given on the day after the date on which the notice was delivered to the overnight courier for delivery, or two days after the date the notice was postmarked if the notice was sent by certified mail. The Parties may, by ten (10) days prior notice to the other Parties as aforesaid, designate a different address or different addresses to which communications intended for it are to be sent. Each Party's initial address for notices pursuant to this Section 6.10 are:

If to The Lot 1 Owner: Encore CW, LLC
 12137 W. 159th Street
 Homer Glen, IL 60491

 With a copy to: Sosin, Arnold & Schoenbeck, Ltd.
 9501 W. 144th Place, Suite 205
 Orland Park, IL 60462
 Attn: George J. Arnold

If to The Lot 2 Owner: 735 Plainfield Property LLC
 2211 N. Elston Avenue, Suite 304
 Chicago, IL 60614
 Attn: Mitchell Goltz

 With a copy to: Northstone Law LLC
 1016 W. Jackson Blvd., Suite 509
 Chicago, IL 60607

Attn: Animesh K. Ravani, Esq.

If to The Lot 3 Owner: JPMorgan Chase Bank, National Association
1111 Polaris Parkway, Suite 2A
Mail Code OH1-0274
Columbus, OH 43240

With copies to: JPMorgan Chase Bank, National Association
Legal Department
1111 Polaris Parkway, Suite 4P
Mail Code OH1-0152
Columbus, OH 43240
Attn: Real Estate Counsel

Freeborn & Peters, LLP
311 South Wacker Drive, Suite 3000
Chicago, IL 60606
Attn: Michael A. Moynihan

7.11 **Time of Essence.** Time is of the essence in the performance of the terms and conditions of this Agreement.

7.12 **Estoppel Certificate.** Each Party severally covenants that within 15 business days after written request of the other Party, it will from time to time, but not more than twice in any twelve (12) month period, issue to another Party, to a prospective purchaser or lender or to any mortgagee of such Party, an estoppel certificate stating: (i) whether to the best knowledge of the Party to whom the request has been directed, any default of the requesting Party exists under this Agreement and, if there are known defaults, specifying the nature thereof; (ii) whether to its knowledge this Agreement has been modified or amended in any way (or if it has, then stating the nature thereof); (iii) that to the Party's knowledge this Agreement is, as of that date, in full force and effect; and (iv) to the extent a parcel is subject to any of the use restrictions set forth in Section 2.4, whether or not the lease creating such use restrictions remain in full force and effect.

7.13 **Rule of Construction.** The Parties and their counsel have reviewed and negotiated this Agreement and that the normal rule of construction to the effect that any ambiguities are to be resolved against the drafting Party shall not be employed in the interpretation of this Agreement, or any portion hereof, or any exhibits or amendments or agreements supplementary hereto.

7.14 **No Joint Venture.** Nothing contained in this Agreement shall be construed to make the Parties hereto partners or joint venturers or to render any Party liable for the debts or obligations of any other Party, except as this Agreement may expressly provide.

7.15 **Agreement for Benefit of Parties Only.** This Agreement is not intended to and shall not be construed to dedicate any rights or easements to the general public, nor to benefit any third parties which are not either Parties hereto nor their successors or assigns.

7.16 **Right to Enjoin.** In the event of any violation or threatened violation by any Party or occupant of any term, restriction, condition or covenant of the terms of this Agreement, the Party not in violation shall have the right, to seek actual damages or to enjoin such violation or threatened violation in a court of competent jurisdiction.

7.17 **Attorneys Fees.** In the event a Party commences or engages in any legal action or proceeding against the other (including, without limitation, litigation or arbitration, whether in contract, tort or both) arising out of or in connection with this Agreement or the Overall Development or any other aspect of the Overall Development or the development on the Overall Development (including, without limitation, (a) the enforcement or interpretation of either Party's rights or obligations under this Agreement or (b) the declaration of any rights or obligations under this Agreement), the prevailing Party shall be entitled to recover from the non-prevailing Party, the prevailing Party's reasonable attorney's fees, together with any costs and expenses, incurred in any such action or proceeding, including any reasonable attorney's fees, costs, and expenses incurred on collection and on appeal.

7.18 **Arbitration.** Except where another dispute resolution procedure has been specifically provided for in this Agreement (unless such dispute is not resolved using such procedure), any and all disputes, claims, or other controversy arising out of or relating to this Agreement, or the breach thereof, shall be settled by arbitration administered by the American Arbitration Association, and judgment on the award rendered by the arbitrator(s) may be entered in any court of competent jurisdiction. Within fifteen (15) days after any of the Parties hereto shall have notified another that it desires arbitration, the Parties hereto shall attempt to agree upon one arbitrator to conduct the arbitration; if the Parties cannot agree upon one arbitrator within said fifteen-day period, each Party shall select one arbitrator, and the two arbitrators so selected shall select a third arbitrator; and if they cannot agree, the procedures set forth in this Section 6.18 with respect to completing the panel of arbitrators shall apply.

a. If any of the Parties shall fail to make a choice within the time herein provided, then the Party not in default in selecting an arbitrator, may upon five days' notice to the other Party request the presiding judge of the District Court of the United States for the district in which the Overall Development is located acting in his private and non-judicial capacity to choose an arbitrator or arbitrators to fill the vacancy or vacancies. Such judge may thereupon appoint an arbitrator or arbitrators, as the case may be, to complete the panel of three arbitrators. If such judge shall fail or refuse to make such appointment, the arbitrator or arbitrators needed to complete the panel shall be named or appointed in accordance with the prevailing laws applicable to this Agreement.

b. Either the one arbitrator, or the three arbitrators, as the case may be, upon being duly appointed shall investigate the facts and hold hearings at which the Parties hereto may present their evidence and arguments. A decision by the one, or a majority vote of the three arbitrators shall be rendered within sixty (60) days after the date upon which the last arbitrator is appointed. Such decision shall be final and binding on the Parties. If the arbitrator(s) shall fail to render a decision within said period of sixty (60) days, then either Party shall have the right to institute such action or proceeding in such court as such Party shall deem appropriate in the circumstances. Judgment upon the award rendered in such arbitration may be entered by any court having jurisdiction thereof. In determining any question, matter or dispute before them, the

arbitrators shall apply the provisions of this Agreement, without varying therefrom in any respect and shall be without power to add to, modify or change any of said provisions.

c. No Party shall be considered in default hereunder during the pending of arbitration proceedings relating to such default.

d. The arbitrator(s) shall determine in what proportion the Parties hereto shall bear the cost of such arbitration, except that each Party thereto shall pay the fees and expenses of the arbitrator appointed by or on behalf of such Party and one-half of the fees and expenses of the third arbitrator, if any.

7.19 **Waiver of Jury Trial**. Each Party hereby waives trial by jury, to the extent permitted by law, in any action, proceeding or counterclaim brought by a Party against any other Party hereto or any matter whatsoever arising out of or in any way connected with this Agreement, the relationship of the Parties, the use or occupancy of the Parties' respective parcels or any person claiming through or under a Party, any claim of injury or damage, and any emergency or other statutory remedy; provided, however, the foregoing waiver shall not apply to any action for personal injury or property damage.

IN WITNESS WHEREOF, the Parties have caused this Agreement to be executed and delivered in multiple counterparts, each counterpart being deemed an original, to be effective on the date signed by the last Party to sign hereunder.

[Signature Pages Follow]

EXHIBIT A-1
To Declaration of Operating and Reciprocal Easement Agreement
(Legal Description of Lot 1)

EXHIBIT A-2
To Declaration of Operating and Reciprocal Easement Agreement
(Legal Description of Lot 2)

EXHIBIT A-3
To Declaration of Operating and Reciprocal Easement Agreement
(Legal Description of Lot 3)

EXHIBIT B

To Declaration of Operating and Reciprocal Easement Agreement

(Site Plan depicting: (i) Lots, (v) Common Areas [including Interior Drives and Interior Parking Areas], and (vi) Project Sign Easement Areas)

[Site Plan Attached]

EXHIBIT C
To Declaration of Operating and Reciprocal Easement Agreement
(Sign Exhibit)

EXHIBIT D
To Declaration of Operating and Reciprocal Easement Agreement
(Guzman Use Restrictions)

(a) Exclusive Use. Guzman shall have the exclusive right to sell Mexican and Latin cuisine (the "Exclusive Use").

(b) Other Prohibited Uses. theatre, bowling alley, billiard parlor, funeral parlor, night club, or other place of recreation or amusement (such as laser tag or paint ball facilities; miniature or putt-putt golf facilities; go-cart tracks; rock climbing facilities; party, dance, cheer, tumbling, or gymnastics facilities; flea market, industrial manufacturing facility, skating rink, bar, exercise facility; massage parlor (but therapeutic massage is permitted), modeling studio, adult bookstore or other establishment primarily engaged in the business of selling, exhibiting or distributing pornographic or obscene materials or live models or dancers, an amusement arcade or facility providing coin-operated amusement devices, rides, pinball machines, mechanical or electronic games, and/or similar types of equipment or devices, car repair facility, central laundry or dry cleaning plant (other than a dry cleaning drop-off facility which does not use dry cleaning fluids or similar chemicals or substances on site in connection with the dry cleaning of clothes) or any business which creates unreasonably or unusually strong or offensive odors, fumes, emissions or sounds (the "Prohibited Uses")



Attachment 3
Standards for Special Use Permits

Standards for Special Use Permit

Requested Special Use Permit for the Planned Unit Development at 735 Plainfield Road for the financial institution with a drive-thru component, automobile washing and cleaning facility, and drive-thru-fast food establishment.

9-14-5.2: Standards:

- A. That the establishment, maintenance, or operation of the special use will not be detrimental to or endanger the public health, safety, morals, comfort or general welfare. ***The development will not be detrimental to or endanger the public health, safety, morals, comfort or general welfare of the public.***

- B. That the special use will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish and impair property values within the neighborhood. ***The special use will not be injurious to the use and enjoyment of the other properties in the immediate vicinity for the purposes already permitted, nor substantially diminish and impair property values within the neighborhood.***

- C. That the establishment of the special use will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district. ***The special use will not impede on the normal and orderly development and improvement of surrounding property for uses permitted in the district.***

- D. That adequate utilities, access roads, drainage, and/or other necessary facilities have been or are being provided. ***Adequate utilities, access roads, and drainage are being provided.***

- E. That adequate measures have been or will be taken to provide ingress and egress so designed as to minimize traffic congestion in the public streets. ***Adequate measures have been taking to provide ingress and egress so designed to minimized traffic congestion in the public streets.***

- F. That the special use shall in all other respects conform to the applicable regulations of the district in which it is located except as such regulations may in each instance be modified by the Village Board pursuant to the recommendation of the Plan Commission. ***The special use will conform to the applicable regulations of the district as deemed appropriate by the Plan Commission and Village Board.***

- G. Conditions in the area have substantially changed, and at least one year has elapsed since any denial by the Village Board of any prior application for a special use permit that would have authorized substantially the same use of all or part of the site. ***There has not been an application for this property within the last year.***



Attachment 4
Standards for Variations

Standards for Variations

Requested variation from 9-3-3 (B) to subdivide into three individual lots

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Given the three individual uses including a financial institution, fast food establishment, and automobile washing and cleaning facility, the site will not yield a reasonable return by providing only two subdivided lots as stated per 9-3-3 (B).***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Providing three individual uses is appropriate for the proposed improvements. Allowing only two lots is deemed a hardship given the three separate uses.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation for 9-3-3 (C) – Minimum Lot Areas for Lots 1, 2, and 3 from five acres (5 ac) to two and 79/100ths acres (2.79 ac)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***The site is capable of providing multiple uses otherwise, however, due to the existing overall site area, meeting the five acre minimum requirement is not possible***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the existing site area, relief from this requirement is necessary to provide any development greater than a singular use.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-3-3 (C) to reduce the minimum lot area for Lot 3 (financial institution with drive-thru) from two acres (2 ac) to seventy-nine hundredths of an acre (0.79 ac)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***The site is capable of providing multiple uses otherwise, however, due to the existing overall site area, meeting the five acre minimum requirement is not possible***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the existing site area, relief from this requirement is necessary to provide any development greater than a singular use.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Noted that under Section 9-6B-2, the drive-thru fast food establishment is identified as a special use. Requested variation from 9-6B-3 to reduce the minimum lot area for the drive-thru fast food establishment (Lot 2) from one acre (1 ac) to 0.795 acres

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the size of the existing parcel, meeting the minimum lot area of one-acre cannot be achieved for the fast food establishment use.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the size of the existing parcel and configuration of the site layout, the lot area identified in the Plat of Subdivision meets the minimum area necessary for the proposed use and thus is within the intent of the regulation.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Noted that under Section 9-6B-2, the drive-thru financial institution is identified as a special use.

Requested variation from 9-6B-3 to reduce the minimum lot area for the drive-thru financial institution (Lot 3) from one acre (1 ac) to 0.792 acres

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the size of the existing parcel, meeting the minimum lot area of one-acre cannot be achieved for financial institution use.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the size of the existing parcel and configuration of the site layout, the lot area identified in the Plat of Subdivision meets the minimum area necessary for the proposed use and thus is within the intent of the regulation.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-10-5 (L) 1 to allow for parallel parking for spaces to serve employee parking for Lot 1 (automobile washing and cleaning facility)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Given the site constraints, utilizing parallel parking along the south of the building for employee use is the most necessary to meet parking count requirements and best utilize site space.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the site layout and parking required for the site, allowing parallel parking for employee use is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-10-5 (K) to reduce the vehicle staking requirement from 50 vehicles to 27 vehicles for the approach to the car wash on the south side of Lot 1 (automobile washing and cleaning facility)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Given the site constraints, reducing the required stacking element***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the site layout and parking required for the site, allowing parallel parking for employee use is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-6B-3 (E) 2 to reduce the side yard setback from 30' to 20' for Lot 2 (fast food establishment with drive-thru)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the side yard setback of 30' cannot be achieved for the fast food establishment use.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the parking required for the site, relief from the side yard setback requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-10-5 (G) to reduce the minimum required front yard pavement setback from 15' to 0' for Lot 1 (automobile washing and cleaning facility)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the pavement setback of 15' cannot be achieved for the bypass exit lane from the car wash***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent right-of-way and need for internal vehicular movements, relief from the pavement setback requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-10-5 (G) to reduce the minimum required parking areas, access drives, or drive aisles setback from 10' to 4.4' (west side), 3.9' (south side), and 5.0' (east side) for Lot 1 (automobile washing and cleaning facility)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the interior side yard paving setback of 10' cannot be achieved and allow vehicular access around the perimeter of the car wash building and meet the minimum parking requirements for the site.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the parking required for the site, relief from the interior side yard paving setback requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-10-4 (H) to reduce the minimum required loading berths for Lot 1 (automobile washing and cleaning facility) from one (1) to zero (0)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to proposed architecture and building use, a loading berth is not necessary.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to proposed architecture and building use, a loading berth is not necessary.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-10-4 (H) to reduce the minimum required loading berths for Lot 3 (financial institution with drive-thru) from one (1) to zero (0)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to proposed architecture and building use, a loading berth is not necessary.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to proposed architecture and building use, a loading berth is not necessary.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-6-1 (E) to allow for aluminum composite (ACM) panels on the building within Lot 3 (financial institution with drive-thru)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***As shown in the proposed architecture, the ACM panels are a contemporary cladding material that is long lasting, has high corrosion and water resistance and will blend well with the architecture of the overall development and adjacent commercial architecture within the surrounding area.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to proposed architecture and building use, allowing ACM panels is requested given the functionality of the building.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-6B-3 (G) to increase the allowable building height from thirty-feet (30') to forty-feet (40') for Lot 1 (automobile washing and cleaning facility)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***A request to a variation of this requirement is due to the proposed architecture with the water tower included. The main structure is at the required maximum height per code and only the accessory water tower feature exceeds the maximum allowable.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to proposed architecture and building use, allowing a greater building height is requested given the functionality of the building.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-6B-3 (A) to reduce the minimum lot area from one acre (1 ac) to 0.79 acres or Lot 2 (fast food establishment with drive-thru)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the minimum lot area reduction is necessary for proposed site use while still meeting requirements for minimum parking.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the site orientation, relief from the minimum lot area requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-6B-3 (C) 1 to reduce the minimum required lot width from 200' to 118.68' for Lot 2 (fast food establishment with drive-thru)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the minimum lot width reduction is necessary for proposed site use while still meeting requirements for minimum parking.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the site orientation, relief from the minimum lot width requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-10-5 (G) to reduce the minimum required front yard pavement setback from 25' to 24' for Lot 2 (fast food establishment with drive-thru)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the pavement setback of 25' cannot be achieved while still meeting the parking requirements for the site.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent right-of-way and need for internal vehicular movements and minimum parking requirements, relief from the pavement setback requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-10-5 (G) to reduce the minimum required interior side yard parking area, access drives, and drive aisle setback from 10' to 2.5' on the east side and 0' on the south and west sides for Lot 2 (fast food establishment with drive-thru)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the interior side yard paving area setback of 10' cannot be achieved and allow vehicular access between the fast food establishment and financial institution, while still meeting minimum parking requirements.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the parking required for the site, relief from the interior side yard paving area setback requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-10-5 (G) 3 to reduce the minimum required end parking island width from 9' to 5.0' for Lot 2 (fast food establishment with drive-thru)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the end parking island requirement of 9' is too wide to allow for vehicular movement throughout the site and meet the minimum parking requirements.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the site, the reduced parking island width is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-3-7 (A) 4 to reduce the minimum required specific setback from Plainfield Road from 100' to 71.8' for Lot 3 (financial institution with drive-thru)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the specific Plainfield Road setback cannot be achieved while meeting minimum parking requirements and providing ATM drive-up access entirely within the Lot 3 limits. Note this setback is in general conformance to the existing western adjacent shopping center property.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and proximity of the Plainfield Road right-of-way, relief from the front yard setback requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-6-1 (A) to reduce the minimum lot area with two uses from two acre (2 acres) to 0.79 acres or Lot 2 (fast food establishment with drive-thru)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the minimum lot area reduction is necessary for proposed site use while still meeting requirements for minimum parking.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the site orientation, relief from the minimum lot area requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-6B-3 (A) to reduce the minimum lot area from one acre (1 ac) to 0.79 acres or Lot 3 (financial institution with drive-thru)

9-14-4.5: Standards for Variation:

- H. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the minimum lot area reduction is necessary for proposed site use while still meeting requirements for minimum parking.***
- I. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the site orientation, relief from the minimum lot area requirement is necessary for the site to function as needed.***
- J. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- K. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- L. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- M. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- N. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-6-1 (A) to reduce the minimum lot area with two uses from two acre (2 acres) to 0.79 acres or Lot 3 (financial institution with drive-thru)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the minimum lot area reduction is necessary for proposed site use while still meeting requirements for minimum parking.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the site orientation, relief from the minimum lot area requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-6B-3 (E) 2 to reduce the side yard setback from 30' to 5' for Lot 3 (financial institution with drive-thru)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the side yard setback of 30' cannot be achieved for the financial institution use while still providing vehicular access around the building and meeting minimum parking requirements.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the parking required for the site, relief from the side yard setback requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-10-5 (G) to reduce the minimum required interior side yard parking area, access drive, and drive aisle setback from 10' to 9' on the west lot line and 0' for the south and east lot lines for Lot 3 (financial institution with drive-thru)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the interior side yard parking area setback of 10' cannot be achieved and allow vehicular access around the perimeter of the financial institution building and meet the minimum parking requirements for the site.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the parking required for the site, relief from the interior side yard paving setback requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-10-5 (G) to reduce the minimum required front yard pavement setback from 25' to 24' for Lot 3 (financial institution with drive-thru)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the pavement setback of 25' cannot be achieved while still meeting the parking requirements for the site.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent right-of-way and need for internal vehicular movements and minimum parking requirements, relief from the pavement setback requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-10-5 (L) 2(g) to reduce the site egress driveway storage area providing access to Plainfield Road from 100' to 38' as measured from the property line to the end of the driveway median

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***The proposed driveway location matches the site access in the current condition. Due to the existing site constraints, increasing the driveway storage will be detrimental to providing a usable site while still providing vehicular circulation.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the parcel dimensions, relief from the driveway storage requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-10-5 (L) 2 (d) to reduce the minimum spacing between the center of the proposed driveway entrance and the adjoining property line from 70' to 55.4'

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***The proposed driveway location matches the site access in the current condition and cannot shift due to proximity to the existing intersection to the west. Without this site entrance, the site does not meet the minimum requirement for access.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***The proposed driveway location is located at the same location as the existing access and shifting the driveway will cause potential concerns with the intersection to the west. The driveway location shall remain as-is.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-10-5 (L) 2 (e) to reduce the minimum spacing between separate driveway entrances from 400' to 153.8' for site access the Planned Unit Development

9-14-4.5: Standards for Variation:

- H. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Meeting the requirement per the ordinance is impossible as the limits of the existing intersection to the west and existing driveway access for the neighboring adjoiner to the east is less than 500-feet.***
- I. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***The proposed driveway location is located at the same location as the existing access and shifting the driveway will cause potential concerns with the intersection to the west. The driveway location shall remain as-is.***
- J. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- K. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- L. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- M. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- N. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-12-2 to allow for building canopy encroachment for Lot 1 (automobile washing and cleaning facility) to encroach beyond 1/3 of the required yard

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints including the minimal rear yard dimension and the proposed use, restricting the canopy to encroach less than 1/3 into the required yard would not allow for coverage across all drives.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the parcel dimensions, relief from the allowable distance for the canopy to encroach into the required yard is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-12-2 to allow for building canopy encroachment for Lot 3 (financial institution) to encroach beyond 1/3 of the required yard

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints including the minimal rear yard dimension and the proposed use, restricting the canopy to encroach less than 1/3 into the required yard would not allow for coverage for the bank drive-thru.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the parcel dimensions, relief from the allowable distance for the canopy to encroach into the required yard is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-13-6 (F) to reduce the minimum required yards or setbacks along the periphery of the planned unit development ("PUD") be at least equal in width or depth to the of the applicable side yard within the adjacent zoning district

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the minimum required yard widths and depths cannot meet or exceed those of the surrounding adjacent parcels while still providing vehicular access around the perimeter of all buildings and meeting the minimum parking requirements for the site.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the parking required for the site, relief from the required yard setback requirements is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-13-6 (B) to waive the requirement that the subject realty be under single ownership and/or unified control

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the nature of the subdivision, the individual lots should be available for separate ownership due to separate uses.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the site and nature of the subdivision, multiple ownership is required for multiple uses.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 10-4-3 (B) to waive the requirement that Lot 1 (automobile washing and cleaning facility) front on a public street

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, providing Lot 1 frontage to a public street would cause adverse effects to traffic in the public right-of-way. Lot 1 is provided an accessible ingress/egress at two locations.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent public roadways, providing site access to Lot 1 via a public street is not a feasible option.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 10-4-3 (A) to waive the requirements to meet minimum lot areas and widths for Lots 2 and 3

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the minimum lot area and widths reduction is necessary for proposed site use while still meeting requirements for minimum parking.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the site orientation, relief from the minimum lot area and widths requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 10-4-3 (A) to waive the requirements to meet minimum building setback requirements for Lots 2 and 3

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the minimum building setbacks cannot be achieved for the financial institution and fast food establishment use while still providing vehicular access around the building and meeting minimum parking requirements.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the parking required for the Lots 2 and 3, relief from the minimum building setback requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-11-5 (E) 2 to increase the allowable maximum illuminated window sign requirement from 4 SF to 5.35 SF for Lot 3 (financial institution with drive-thru)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***ATM signage is crucial for financial institution purposes.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***ATM signage is crucial for financial institution purposes.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-11-12 (8) 1(a) and 9-11-12 (2) 2 to increase the allowable maximum SSA from 160.70 SF to 315.20 SF for Lot 2 (fast food establishment with a drive-thru)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the narrow site frontage for Lot 2, the allowable SSA is reduced from the typical 350 SF maximum to 160.70 SF (calculated as 1.25 SF per LF of site frontage). Given this constraint, exceeding the requirement is necessary to provide proper advertising and wayfinding.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the narrow site width, providing an increase in the allowable SSA is necessary for the site to function properly regarding informing the public of the site use.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-11-11 (B) 2 to allow two wall signs to exceed the maximum allowed height (grade at top of sign) of 20-feet for Lot 2 (fast food establishment)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to how this restaurant must be located and positioned on this property the building is set 160ft from Plainfield Rd. This very real hardship directly impacts the commercial viability of this site. A Sign Visibility Study was conducted as this site was being evaluated and the result was two signs being proposed at height of 26ft-6in.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***In the case of this restaurant strict adherence to the sign code essentially rules out this location as a potential site. The setback from Plainfield Rd. and the angle at which Plainfield Rd. runs would render the GyG branding almost invisible in this highly competitive QSR market.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***This hardship was existing by virtue of how the sign code relates to this property.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***This variation only affects the placement of two wall signs and this will not have a negative effect on the public welfare or adjacent properties.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***This variation relates only to aesthetic sign issues and not any construction that impacts the neighboring properties' supply of light, street congestion, danger of fire, or be a public safety issue.***
- F. The proposed variation will not alter the essential character of the locality. ***The proposed relief from the sign code does not change the character of this corridor as it pertains only to sign height on two wall signs.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***This variation is aligned with the goals of Title 9 of the Willowbrook sign code.***

Standards for Variations

Requested variation from 9-11-4 (C) and 9-11-4 (D) to waive requirement and allow roof signage and projecting signage for Lot 1 (automobile washing and cleaning facility)

9-14-4.5: Standards for Variation:

- H. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the proposed roofline architecture, providing main signage by projecting from the bottom of the roof is necessary to be at an appropriate height for safety and advertising purposes.***
- I. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***The roof gradually slopes upward, starting at about 1½ stories. Due to the architecture, the proposed signage must project from the roof for height and visibility purposes.***
- J. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- K. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- L. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- M. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- N. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-11-11 (B) 2 to allow for building signage to extend higher than one foot (1') below the top line of the face of the building and exceed a height of twenty feet (20') for Lot 1 (automobile washing and cleaning facility).

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the proposed roofline architecture, providing main signage by projecting from the bottom of the roof is necessary. Providing signage one-foot below the face of the building would conflict with the buildings windows and doors at the main entrance.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***The roof gradually slopes upward, starting at about 1½ stories. Due to the architecture, the proposed signage must project from the roof for height and visibility purposes.***
- A. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- B. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- C. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- D. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- E. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-11-12 (B) 1(a) to increase the maximum allowable sign surface area from 193.80 SF (calculated as 1.25 SF times the length of the site frontage) to 339.50 SF for Lot 3 (financial institution with drive-thru)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Given the site layout and the effective site frontages along the west and north side, multiple signs are necessary for advertising and wayfinding purposes. The proposed signage is in-line with other financial institutions in the vicinity.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Providing appropriate signage is necessary for site functionality.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-12-4 (C) 3 to decrease the minimum clearance height for the canopy within Lot 1 (automobile washing and cleaning facility) from fourteen-feet (14.0-feet) to twelve-feet (12.0-feet)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the proposed use and vehicles allowed to enter the automobile washing area, providing a clearance canopy of 12.0-feet maximum is necessary.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Reducing the minimum clearance height is necessary for the car wash facility to function based on proposed use.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-14-2(D)2(c)4 to decrease the minimum allowable foundation landscaping from seven feet (7-feet) to zero feet (0-feet) for all building facades within Lot 1, Lot 2, and Lot 3

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Given the site layout and building entrances, meeting the landscaping requirement and allowing pedestrian maneuverability and access to each building is not feasible.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the site configuration and need for pedestrian access to each building while also meeting drive width and parking requirements, relief from the foundation landscaping requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***

Standards for Variations

Requested variation from 9-10-5 (G) to eliminate requirement for one tree and three small shrubs required within the parking islands within Lot 2 (fast food establishment with drive-thru)

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Given the site dimensions and need to meet parking and drive access requirements, the parking islands do not have the capacity to provide one tree and three small shrubs.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Increasing the parking islands enough to provide the landscaping identified will restrict parking and paved areas necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



Attachment 5
Standards for Planned Unit Development
Findings of Fact for Planned Unit Developments

Standards for Planned Unit Development

9-13-6: Standards for Planned Unit Development

- A. Comprehensive Plan: A planned unit development must conform with the intent and spirit of the planning goals and objectives of the Village Comprehensive Plan. ***No variance required – the proposed PUD will meet the goals and intents of the Village Comprehensive Plan, as the uses proposed to be appropriate for the area and beneficial the community overall.***
- B. Size And Ownership: The site of the planned unit development must be under single ownership and/or unified control and be not less than two (2) acres in area. ***Variance is requested, due to the nature of the planned unit development, the individual lots should be available for separate ownership due to separate uses. No variance is required regarding minimum size as the lot area of the total PUD exceeds two (2) acres in area.***
- C. Compatibility: The uses permitted in the planned unit development must be of a type and so located as to exercise no undue detrimental influence upon surrounding properties. ***No variance required – the proposed PUD will align with current adjacent zoning and will not have detrimental influence upon the surrounding properties.***
- D. Stormwater Management: Adequate facilities, both on site and off site, shall be provided to ensure that all drainage related issues are addressed. ***No variance required – the engineering plans and drainage reports will identify that the all stormwater management proposed on-site will conform to current municipal stormwater ordinances.***
- A. Traffic: That adequate provision be made to provide ingress and egress so designed as to minimize traffic congestion in the public streets. ***No variance required – ingress and egress are designed to minimize traffic congestion in the public streets to the extents possible.***
- B. Preservation Standards: Due regard shall be demonstrated for any existing site conditions worthy of consideration including: desirable trees of a minimum six inch (6") caliper or greater, natural bodies of water, designated wetlands and/or flood hazard areas, etc. All desirable vegetation shall be identified by location, caliper size, type, condition and a method by which preservation efforts will be conducted. ***A tree preservation plan and landscape plans are provided as part of our submittal package.***
- K. Uses For PUDs Greater Than Five Acres: Any permitted or special use available under any of the various zoning districts classifications, whether singly or in combination, and any other use permitted by law, may be allowed as either a permitted or special use within a planned unit development consisting of more than five (5) acres, but only to the extent specifically approved by the Village Board, after a finding that such use is consistent with the objectives of this chapter. ***Variance requested as existing site area is below five (5) acres, however, plan meets intent of a planned unit development given combination of uses proposed.***

Findings of Fact

9-13-7: Findings of Fact for Planned Unit Developments

- A. In what respects the proposed plan is consistent with the stated purpose of the planned unit development regulations. ***The proposed plan is consistent with the stated purposes identified specifically including providing diversification in uses (9-13-1 (A) 2) and rational and economic development (9-13-1 (A) 6).***
- B. The extent to which the proposed plan meets the requirements and standards of the planned unit development regulations. (Ord. 97-O-05, 1-27-1997). ***The proposed plan meets the requirements and standards of the planned unit development regulations aside from the items presented within the standards for special use & variations document.***
- C. The extent to which the proposed plan departs from the zoning and subdivision regulations otherwise applicable to the subject property, including, but not limited to, the density, dimension, area, bulk and use, and the reasons why such departures are deemed to be in the public interest. ***The proposed plan departs from the zoning and subdivision regulations as noted in the standards for special use & variations document.***
- D. The method by which the proposed plan makes adequate provision for public services, provides adequate control over vehicular traffic, provides for and protects designated common open space, and furthers the amenities of light and air, recreation and visual enjoyment. ***The proposed plan will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- E. The relationship and compatibility of the proposed plan to the adjacent properties and neighborhood. (Ord. 75-O-3, 3-10-1975). ***The proposed plan fronting a major route provides similar use as in the current condition and is not a departure from current uses of the adjacent properties along the route.***
- F. The desirability of the proposed plan with respect to the physical development, tax base and economic well being of the Village. ***The proposed plan will provide physical and economic benefits to the community in comparison to the existing use.***
- G. The conformity with the intent and spirit of the current planning objectives of the Village. (Ord. 97-O-05, 1-27-1997). ***The proposed PUD will meet the goals and intents of the Village Comprehensive Plan, as the uses proposed to be appropriate for the area and beneficial the community overall.***



Attachment 6
Public Hearing Notice

NOTICE OF PUBLIC HEARING
ZONING HEARING CASE NO. 21-03

NOTICE IS HEREBY GIVEN that the Plan Commission of the Village of Willowbrook, DuPage County, Illinois, will conduct a public hearing at a regular meeting of the Plan Commission on the 3rd of February, 2021 at the hour of 7:00 P.M. This meeting would typically take place in the Willowbrook Police Department Training Room, 7760 S. Quincy St, Willowbrook, IL 60527. However, due to the current circumstances concerning Covid-19, this meeting will be held virtually. Internet address and access instructions will be provided on the Village of Willowbrook's Plan Commission website once available:

<https://www.willowbrookil.org/Archive.aspx?AMID=44>

The purpose of this public hearing shall be to consider a petition requesting approval of a special use permit for a planned unit development, including a financial institution with drive through, fast-food establishment with drive through, an automobile washing and cleaning facility, including certain relief, exceptions and variations from Title 9 and Title 10 of the Village Code; approval of a Preliminary Plat of Subdivision; and approval of a Preliminary Plat of PUD on property legally described as follows on the property legally described as follows:

LOT 1 IN WILLOWBROOK CENTER UNIT NO. 1, OF PART OF LOT 7 IN OWNER'S SUBDIVISION IN SECTIONS 23 AND 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED OCTOBER 16, 1963, AS DOCUMENT R63-37895, IN DUPAGE COUNTY, ILLINOIS.

PIN: 09-23-406-003

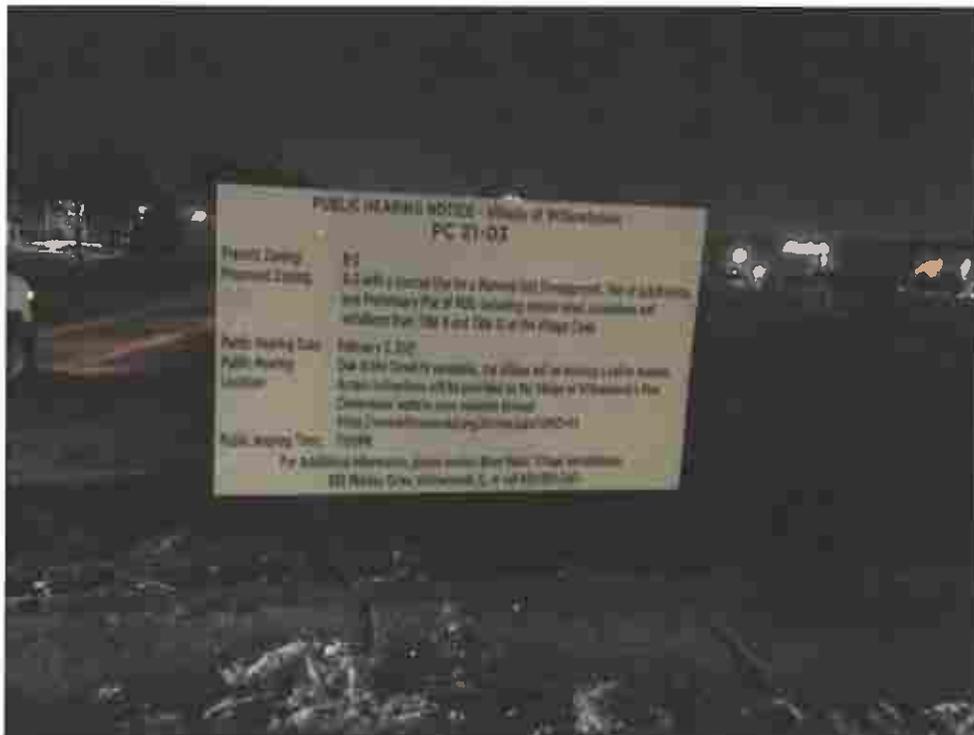
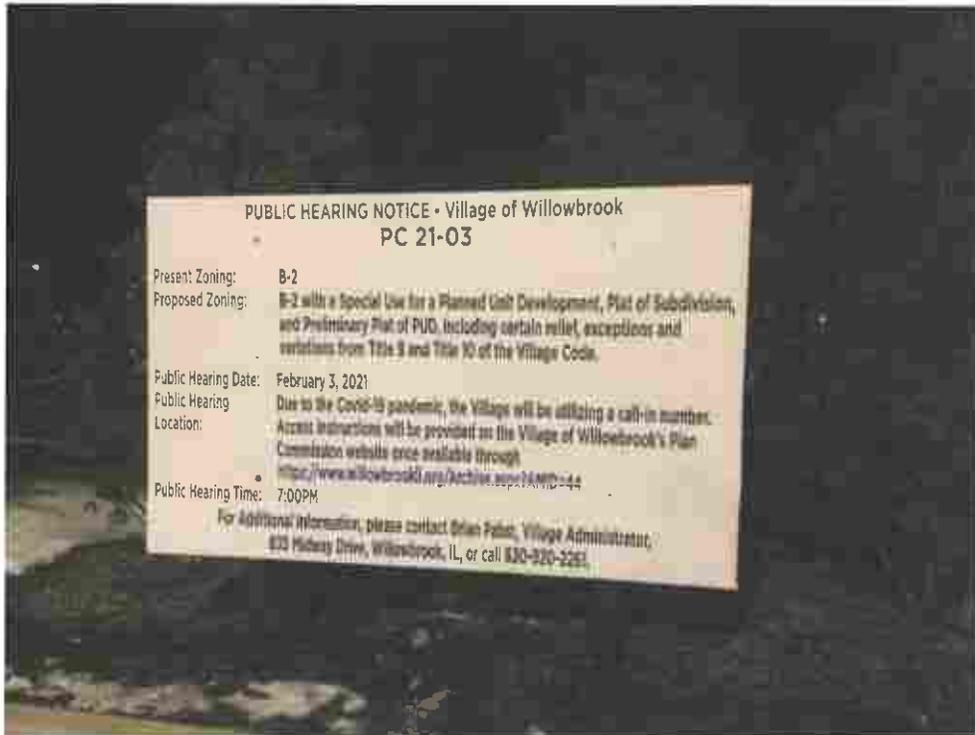
ADDRESS: 735 Plainfield Road, Willowbrook IL 60527.

The applicant for this petition is Alex Katz of G.W. Property Group LLC, 2211 N. Elston Avenue, Suite 304. The property owner is Louis W. Viren Jr. of Viren-Gill Ltd., LLC, 735 Plainfield Road, Willowbrook IL 60527.

A copy of the Village's petition is on file in the Office of the Village Planner, Village of Willowbrook, 835 Midway, Willowbrook, Illinois, and is available for public inspection. Any individual with a disability requiring a reasonable accommodation in order to participate in any public meeting held under the authority of the Village of Willowbrook should contact Ann Choi, Village of Willowbrook, 835 Midway, Willowbrook, IL 60527, or call (630) 920-2233, Monday through Friday, between 8:30 A.M. and 4:30 P.M. All persons desiring to be heard in support or opposition to the application shall be afforded an opportunity and may submit their statements orally, in written form, or both. Written comments may be submitted up to the hour of 6:00pm on February 3, 2021 to planner@willowbrook.il.us. This hearing may be recessed to another date if not concluded on the evening scheduled.

/s/ Brian Pabst
Village Administrator
(630) 920-2261

Published in the January 14, 2021 edition of *The Doings* Newspaper.





Attachment 7
Village of Willowbrook Review Letter(s)



EST. 1960

Willowbrook

835 Midway Drive
Willowbrook, IL 60527-5549

Phone: (630) 323-8215 Fax: (630) 323-0787 www.willowbrookil.org

Mayor

Frank A. Trilla

Village Clerk

Deborah Hahn

Village Trustees

Sue Berglund

Umberto Davi

Michael Mistele

Gayle Neal

Paul Oggerino

Gregory Ruffolo

Village Administrator

Brian Pabst

Chief of Police

Robert Schaller

Director of Finance

Carrie Dittman



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January 27, 2021

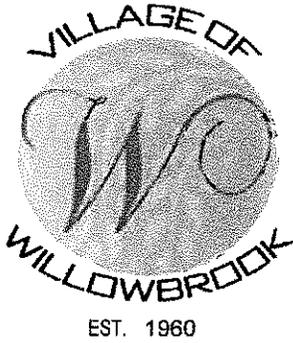
Atten: Alex Katz
GW Properties
2211 N. Elston Avenue, Suite 304
Chicago, IL 60614

Re: **GW Properties – Willowbrook Planning Review #2**

Mr. Katz,

Planning staff has reviewed the revised submittal received by the Village on January 21, 2021, January 24, 2021, and January 25, 2021, for the GW Properties – Willowbrook planned unit development including an automobile cleaning and washing facility, a fast food establishment with drive-through, and a financial institution with drive-through, on the lot currently occupied by the vacant Willowbrook Bowling Alley located at 735 Plainfield Road in Willowbrook, IL 60527. The following plans have been reviewed per the requirements of the Village of Willowbrook Zoning Ordinance:

1. **“Preliminary Plat of PUD”** as prepared by Kimley-Horn and Associates, Inc., KHA Project No. 168726009, Sheet No. C1.0, bearing an original issue date of 12/15/2020, and bearing the latest revision date of 01/21/21.
2. **“Car Wash First Floor Plan”** as prepared by Mark Shively Architecture, Sheet No. A2.1, and bearing a revision date of 01.14.2021.
3. **“Preliminary Floor Plan” for Fast Food Establishment** as prepared by Interplan LLC, Project No. 2020.0919, 1 page, Sheet PFP, and dated 01.20.21.
4. **“Floor Plan” for Chase Bank** as prepared by The Architects Partnership, TAP Project No. 17073, 1 page, and dated 01.22.2021.
5. **“Master Sign Key Plan” for Fast Food Establishment** – 1 page, undated and provided by GW Properties on January 24, 2021.
6. **“Exterior Perspective” for Fast Food Establishment** as prepared by Interplan LLC, 1 page, and dated 01.20.21.
7. **“Architectural Elevations with Signs” for Fast Food Establishment** as prepared by Interplan LLC, 2 pages, and dated 01.20.21.
8. **“Freestanding Sign” for Fast Food Establishment** – 1 page, Sheet ST1.1, bearing the latest revision date of 09.28.20, and provided by GW Properties on January 21, 2021.
9. **“Architectural Elevations” for Chase Bank** as prepared by The Architects Partnership, TAP Project No. 19108, 2 pages, and dated 1.18.2021.
10. **“Sign Package” for Chase Bank** as prepared by Signtech, Drawing No. 18-01613, Project No. CHASE_730_1, 41 pages, bearing an original issue date of 09/14/18, and bearing the latest revision date of 01.19.2021.
11. **“Pay Station & Trash Enclosure Elevations for Car Wash”** as prepared by Mark Shively Architecture, Sheet No. A1.2, and bearing a revision date of 01.14.2021.
12. **“Car Wash Exterior Elevations”** as prepared by Mark Shively Architecture, Sheet No. A4.0, and bearing a revision date of 12.01.2020.
13. **“Vacuum Canopy Plans, Elevations, Sections”** as prepared by Mark Shively Architecture, Sheet No. A1.1, and bearing a plot date of 01.25.2021.



Willowbrook

835 Midway Drive
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14. "Vacuum Canopy Plan Diagram" as prepared by Mark Shively Architecture, Sheet No. A1.2, and bearing a plot date of 01.25.2021.
15. "PUD Plans" as prepared by Kimley-Horn and Associates, Inc., KHA Project No. 168726009, 9 Sheets, bearing an original issue date of 12/15/2020, and bearing the latest revision date of 01/21/21.
16. "Traffic Impact Study" as prepared by KLOA, 130 pages, and dated January 20, 2021.
17. "Traffic Comment Response Letter – DuDOT" as prepared by KLOA, 2 pages, and dated January 20, 2021.
18. "Traffic Comment Response Letter – Willowbrook" as prepared by KLOA, 6 pages, and dated January 20, 2021.
19. "Landscape Plan" and "Landscape Notes and Details" as prepared by Kimley-Horn and Associates, Inc., KHA Project No. 168726009, 2 Sheets, Sheet Nos. L2.0 and L3.0, bearing an original issue date of 12/15/2020, and bearing the latest revision date of 01/21/21.
20. "Preliminary Plat of Subdivision" as prepared by Compass Surveying Group Ltd., Project No. 20.0006, Sheet 1 of 1, and bearing the latest revision date of 01/19/21.
21. "Standards for Special Use & Variations" – 51 pages and undated.

Staff offers the following comments:

General

1. The requirements of the Village Code for Preliminary PUD Approval under Section 9-13-5(B)15 and Section 9-13-5(B)17 include submission of a Market Study and a Tax Impact Study. It is the Village's understanding that the developer of 735 Plainfield Road has not submitted either document.
2. The proposed preliminary site plan should include a depiction of all existing and proposed easements. It is the Village's understanding that the applicant has failed to include any proposed easements on the preliminary plat.
3. The absence of the above-referenced information will be noted in the staff report and should be addressed by the applicant at the Plan Commission Hearing.
4. The Findings of Fact under Section 9-13-7(B) and Section 9-13-7(F) of the Zoning Ordinance cannot be made without the review of these two studies.

Site Layout Plan

1. Include the Lot Areas, Lot Width, Lot Depth, Lot Coverage, Maximum Height and FAR of each lot on the Site Plan according to the definitions provided in Section 9-2-2. Provide a table summarizing this information.

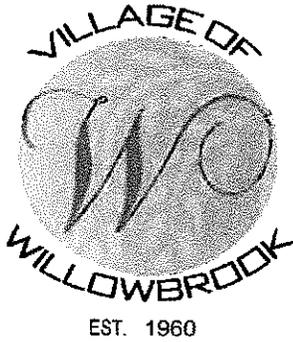
Traffic Regulation Agreement

2. A copy of the Certificate of Insurance will be required as an attachment to the Traffic Regulation Agreement.

Traffic Impact Study and Traffic Control Plan

Per your request, Gewalt Hamilton Associates, Inc. (GHA) has reviewed the following materials for the above captioned project pertaining to traffic, parking and on-site circulation:

- A. KLOA response to GHA's traffic comments dated January 20, 2021.



Willowbrook

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Willowbrook, IL 60527-5549

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- B. KLOA response to DuDOT's comments dated January 20, 2021.
- C. Traffic Impact Study (TIS) prepared by KLOA dated January 20, 2021
- D. Site Plan, Sheet Number C1.0, prepared by Kimley Horn dated January 21, 2021.

Conclusions of this effort and recommendations are presented below:

3. GHA has found the documentation and/or responses provided, in general, to adequately address the comments made and concerns raised in our previous Project Traffic Review dated January 15, 2021.
4. The applicant should provide documentation of the DuDOT approval of TIS and Plainfield Road access, upon receipt.
5. The response letter adequately addressed the comment pertaining to the trip rates assumed in the traffic study for the proposed carwash component of the development during the weekday morning peak hour; however, the concern raised pertaining to the Saturday morning timeframe was not addressed. Please provide response and/or supporting documentation for use of the lower trip generation estimates based on the tunnel independent variable versus the square-footage of the use.
6. The updated study appropriately provides a queuing exhibit that illustrates the stacking capability of each of the proposed uses, as well as additional supporting documentation. However, the supporting carwash queue data provided is insufficient to verify the projected queue lengths. It references a study that was performed in Minnesota in 2012, where 5 of the 6 carwash locations surveyed were at gasoline station facilities, as well as the referenced study did not include average daily traffic for a measure of comparison. While we understand it may be difficult to obtain peak observations at similar facilities, as carwash operations tend to peak in the wintertime on a sunny day following a snowfall, please provide supporting operational characteristics (e.g., transaction information), as available, pertaining to peak operations and/or supporting operations / documentation from similar Chicagoland area existing sites during average, typical conditions.

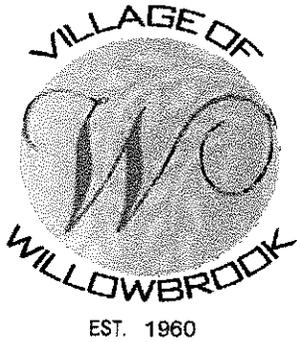
In addition, as we understand, the quick service restaurant use has been identified as Guzman Y Gomez. There is an existing drive-through location in Naperville. Again, please provide either data from observations and/or transaction data to support the capacity provided is adequate to support the peak queue.

7. GHA does not believe the comments noted above will materially impact the findings / recommendations of the TIS.

Police Department Comments

Police Chief Robert Schaller reviewed the proposed Traffic Regulation Agreement and Site Plan and identified the following three main areas of concern that could impact police/fire:

8. To the west of the development is the entrance/exit into the shared Town Center drive which is heavily congested at times and will lead to traffic backing up in all directions. Currently at the Chase bank location across the street, the teller lanes often are 5-6 vehicles deep during peak times. Similar queues at the proposed bank location may lead to vehicles stacking up impacting the west entrance/exit and creating an even greater traffic bottleneck.
9. To the east at the restaurant drive through, Chief Schaller agrees with the concerns raised at the November 4th planning commission concept review. If the drive-through restaurant



Willowbrook

835 Midway Drive
Willowbrook, IL 60527-5549

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Village Trustees

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is in any way close to the volume of traffic Chick-fil-A produces, this will no doubt impact the ability to access the car wash.

10. Lastly, the northeast entrance/exit onto Plainfield could present an issue. During peak rush hour times there are occurrences where westbound Plainfield traffic is backed up past the northeast entrance/exit. Regardless, if there is right turn only lane, it is inevitable that traffic will turn left from that lane.

Bulk Regulations and Landscaping

11. Confirm that the requested variation from Section 9-6B-3(E)2 is for 7 feet, not 5 feet as stated in your Standards for Variations.
12. Sheet L2.0 Landscape Plan labels the 53 "AB" perennials (near the ground sign of the shared drive with Town Center) when these should be 53 "HO2" perennials to correspond to the total number of HO2 perennials (110). The total number of "AB" perennials is correct at 22.

Architectural Elevations

13. The architectural elevations for the car wash indicate that black corrugated metal will be used along the south, west and elevations and are not in compliance with Section 9-6-1(E)2. **A waiver from this code section may be required.**

Loading & Parking

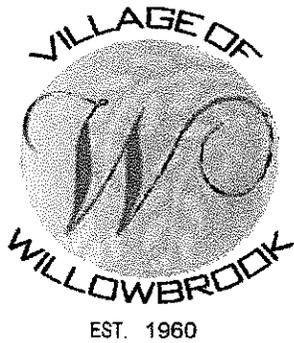
14. Section 9-10-5(G) In-Yards, also requires that all parking areas located within a required yard adjacent to Plainfield Road shall be set back a minimum of twenty five feet (25') from the property line. A landscaped earth berm of a minimum three foot (3') height shall be required and appropriately landscaped with densely planted evergreen trees or shrubs to an effective minimum height of four feet (4') upon installation. Currently there is only two feet of landscape berm along the Plainfield Road frontage. **Staff recommends that the Landscape Plan and Grading Plan be revised to accommodate the required landscape berm of three feet (3') across the entire length of the frontage. A waiver from this code section will be required.**
15. Your responses to the Standards for Variations state under Comment #43, the minimum stacking requirement is 27 vehicles. The Traffic Site study states that is 30 vehicles. Please confirm which is correct. **A waiver from this code section will be required.**

Signage for Lot 1

16. The total sign surface area for Lot 1 is calculated as follows:

This is considered a freestanding business and therefore business site frontage shall be based on the lot width. Therefore, $289.9' \times 1.25 = 362.375$ SF. Total sign surface area permitted is 350 SF. The total sign surface area of 350 SF will also be reduced due to the proposed ground sign that exceeds a height of 8 feet as required by Section(B)1(d)6. **Confirm that the total sign surface area permitted is 335 SF. Adjust the total sign surface area accordingly.**

17. Provide the total sign surface area of each of the painted signs on the east and west elevations to count towards the total sign surface area for the lot.
18. The freestanding sign must contain brick or other similar material in keeping with the design of the principal structure under Section (B)1(d)1.



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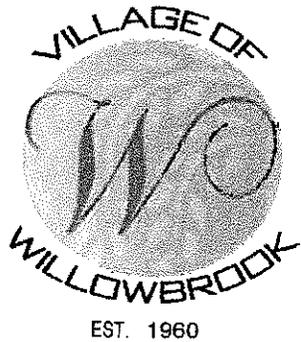
19. The freestanding sign must also be located a minimum of 20 feet away from the existing self-storage ground sign under Section (B)1(d)3. **A waiver is required for total sign surface area.**
20. The proposed digital display sign for Encore Car Wash is located on the Lot 3. **Staff recommends relocating this freestanding sign to Lot 1.** There will be a condition that advertising provided by the digital displays shall be restricted to Encore Car Wash advertising only. It cannot contain advertisements not related to the conduct of the proposed car wash. There will also be conditions imposed on the operation limitations, minimal display time, message change sequence, illumination, etc.
21. Please confirm that Lot 1 is proposing the following signs:
 - a. Wall Sign E.1 (not a roof sign).
 - b. Two (2) painted wall signs E.7 (prohibited). **A waiver is required.**
 - c. Three (3) roof signs E.2, E.3 and E.4 (prohibited). **A waiver is required.**
 - d. One (1) Free standing Sign with Digital Display E.5 – digital display signs are prohibited. **A waiver is required.**
 - e. Three (3) Menu Signs E.8 and one (1) Exit Indicator Sign will not count towards the total SSA of 350 SF, but the Exit Indicator Sign will be considered a Private Traffic Direction Sign and shall not exceed four (4) square feet in area and four feet (4') in height for each sign. **A waiver is required for height and SSA.**
22. In your Standard for Variations, you included a waiver for Projecting Signs. **Please confirm where these projecting signs are located.**
23. For signs E1 and E2, provide the dimensions for the entire length of the signs, not just the width of the individual letters. Otherwise, each letter would be considered a wall sign. **Please adjust the total SSA accordingly.**
24. Sign E.5 needs to count both faces which is a total SSA of 95 square feet. **Please adjust the total proposed SSA accordingly.**
25. The three signs E.6 should also count both faces towards the SSA requirement of 4 SF. Each sign is 12 SF. Please adjust on those sheets. **A waiver for SSA is required.**

Signage for Lot 2

26. Section 9-11-12(B)1(a) permits 1.25 square feet of sign surface area for each foot of business site frontage up to a maximum of 350 square feet of sign surface area. **Confirm that the total sign surface area permitted is 138.4 SF (a deduction was taken for the ground sign height above 8').**
27. Staff calculates the following:
 - a. Signs A and D = 46.7 SF each.
 - b. Signs B, C and E = 49 SF each.
 - c. Sign F = 75 SF
 - d. Sign G = 68 SF (double faces)

Therefore, Staff calculates the total sign surface area proposed to be three hundred eight-three and four tenths square feet (383.4 SF). Please confirm.

28. Please confirm if Signs H, I and J are double faced and what the copy is on each sign. There are only two copies proposed "No Entry" and "Thank You" when there are three signs proposed. **Indicated which copy goes with the corresponding letter. Please also confirm if both faces of each sign were included in the SSA.**
29. The proposed digital display sign for the fast food establishment is located on the bank lot (Lot 3). **Staff recommends relocating this freestanding sign to Lot 2.** Each ground



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- sign should be located on the lot that for which it is intended under Section 9-11-12(B)1(d)4.
30. The sign plan for the fast food establishment indicates a multi-tenant sign. **Staff cannot provide any comments since no drawings were submitted for this. Staff recommends eliminating this from the plans.**
 31. Section 9-11-12(B)1(b)2 permits a maximum of three wall signs on any one facade and have a minimum separation distance upon the building facade equal to or greater than twenty percent (20%) of the linear dimension of the business site frontage and further provided that the total sign surface area of all signs shall not exceed the total sign surface area permitted under subsection (B)1(a) of this section. **Include the linear distances between each sign on the fast food establishment to comply with said section. A waiver may be required.**

Signage for Lot 3

32. Section 9-11-12(B)1(a) permits 1.25 square feet of sign surface area for each foot of business site frontage up to a maximum of 350 square feet of sign surface area. **Confirm that the total sign surface area permitted is 197.9 SF (a deduction was taken for the ground sign height above 8').**
33. Staff calculates the proposed sign surface area for E.1 as 233.3 SF and for E.8 as 4.6 SF, therefore the total proposed SSA is 344 SF (not 339.5 SF). **Please confirm and change the area on the table accordingly.**
34. Staff calculates the sign surface area for Signs E.29 and I.30 to be 8.34 SF each. **Please confirm and change the area on the table accordingly.**

Fences

35. Staff cannot provide any comments on fence height and % openness as no detailed fence drawings were included as part of the submittal. **Any fences shall comply with Section 9-12-4(D)2 of the Zoning Ordinance.**

Canopies

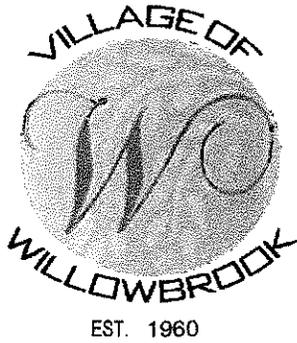
36. The plan for the pay canopy, Sheet A1.2, does not correspond to what is shown on the site plan. There are only two columns/vertical support structures shown on the site plan when Sheet A1.2 indicated there are four columns. Dimensions from the west and south lot lines to the column closest to the south lot line must be provided.

Engineering

As requested by email on January 21, 2021, we have reviewed the PUD Plans and supporting documents. Based on our discussion, this is intended to be the Preliminary PUD submittal and this review is for compliance with Section 9-13-5.B of the Village Code.

Preliminary PUD and Subdivision Plat

We have discussed with the design engineer that the Preliminary PUD and Preliminary Plat of Subdivision will be combined into a two page document and that, combined with the Preliminary Engineering Plans will meet the plan content requirements for both the Preliminary PUD and Preliminary Plat of Subdivision. The following comments will be incorporated into the document:



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37. On the Preliminary Plat of Subdivision, all of the certificates required on the Final Plat will be removed.
38. The Preliminary Plat will provide clarification that Public Utility and Drainage Easements will be provided as required. It remains to be determined if that will be specific easements for each utility, or a blanket easement covering all of the property except for building envelopes.
39. Section 9-13-5.B.4 provides that a statement of ownership be included listing the existing ownership and certification that there are no delinquent taxes on the property. We presume that this has been provided separately or will be added to the Preliminary PUD.
40. Section 9-13-5.B.7 provides for covenants providing various information be submitted. We presume that this will be reviewed by others.
41. Section 9-13-5.B.12 provides for preliminary landscape plans. We will defer review of these drawings to the Village Planner.
42. Section 9-13-5.B.13 provides for preliminary engineering information to be shown. These comments are provided in the next section of this letter.
43. Sections 9-13-5.B.14 – 17 provides for various studies to be provided. We presume these are being reviewed by others.

Preliminary Engineering Plans

Cover Sheet – C0.0

44. Under Utility and Governing Agencies, change the Sanitary Sewer Service to Du Page County Public Works.

Site Plan – C1.0

45. No comments.

Grading Plan – C2.0

46. No comments.

Utility Plan – C4.0

47. The alignment for the proposed watermain has been discussed with the design engineer and will be revised. The connection to the existing main in the town center will be made at the driveway connection between the two sites, rather than south near the proposed car wash building. Also, the proposed watermain will be an 8" main. This will result in less new public watermain and is expected to be a lower cost for the applicant. A 20 foot wide utility easement would be required over this main.
48. Each building service shall have an 8" x 6" Tee with valve. This will allow the public main to be constructed, and then each building service can be installed at the time of building construction without interrupting the public main.
49. The last segment of storm sewer west of the car wash, where the south end of the site connects to the off-site sewer, is shown to be 18". We presume this will be revised down to 12" on the final plans.
50. We presume the intent is for the sanitary sewer serving the bank and restaurant to be a public sewer. This will be owned by Du Page County Public Works and will be reviewed by that agency. We presume they will require a 20 foot wide easement over the main.

General Comments

51. A permit will be required from Du Page County Division of Transportation for work within the Plainfield Road right of way.



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52. A permit will be required from Du Page County Public Works for the proposed sanitary sewer and connections.
53. The Construction Schedule and Engineer's Opinion of Cost will be reviewed with the Final PUD Plat.
54. The fire truck turning templates indicate that presumed truck can maneuver the site. We presume that prior to final approval the plans will be provided to the fire district for comment on both the geometrics, but also the location of hydrants and fire department connections on each building.

Tri-State Fire Protection District

55. Curb radius around the entire site to have a 25' right hand turn radius.
56. Drive width of 20' clear aisles. This includes the drive adjacent to the car wash. The curb radius and drive space is less than 20' and our fire apparatus will have a difficult time accessing the building.
57. Clear height around drive of 13'6".
58. An Auto-Turn exhibit shall be conducted prior to any approval of a site plan.

Relief/waivers Requested

Zoning Ordinance

Approval of a Special Use Permit for a planned unit development consisting of a financial institution with drive through, fast food establishment with drive through, an automobile washing and cleaning facility, and associated site improvements. The approval of the special use permit will include, but is not limited to, the following waivers:

Overall Site

1. That Section 9-3-3(B), Division of Lots, be varied to permit the division of the SUBJECT REALTY into more than two (2) lots that do not conform with all the applicable bulk regulations of the B-2 zoning district.
2. That Section 9-3-3(C), Multiple Uses on a Lot, be varied to permit a reduction in the minimum lot area for two (2) or more special uses from five acres (5 AC) to two and 79/100ths acres (2.79 AC).
3. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required front yard parking area setback from twenty-five feet (25') to twenty-four feet (24') along Plainfield Road.
4. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required height of a landscaped earth berm along the Plainfield Road frontage from three feet (3') to two feet (2'). **STAFF RECOMMENDS REVISIONS TO THE LANDSCAPE PLAN TO ELIMINATE THIS WAIVER.**
5. That Section 9-10-5(L)2(d), Driveway Location On Lots For All Uses Other Than Single-Family Residential, be varied to permit a reduction in the minimum separation requirement between an access driveway entrance and an adjoining lot line from seventy feet (70') to fifty-five point four feet (55.4').
6. That Section 9-10-5(L)2(e), Spacing Between Separate Driveway Entrances On All Lots Other Than In Single-Family Attached Districts, be varied to permit a reduction in the minimum access driveway spacing for separate driveways from four hundred feet (400') to one hundred fifty-three and 8/10ths feet (153.8').



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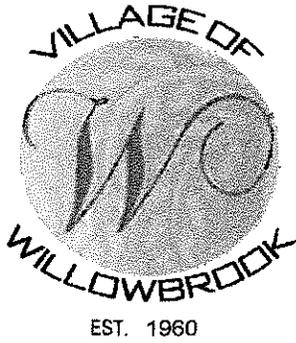


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- That Section 9-10-5(L)2(g), Access Driveways From Arterial Streets For All Uses Other Than Single-Family Residential, be varied to permit a reduction in the driveway storage area from one hundred feet (100') to thirty-eight feet (38').

LOT 1 (Automobile Washing and Cleaning Facility)

- That Section 9-6-1(E)2, General Conditions, Building Façade Materials, be varied to permit metal siding and metal panels as building façade materials, as shown on the architectural elevations.
- That Section 9-6B-3(D), Minimum Lot Depth, be varied to permit a reduction in the minimum required lot depth from two hundred feet (200') to one hundred eighty and three tenths feet (180.3').
- That Section 9-6B-3(E)1, Required Setbacks, Front Yard, be varied to permit a reduction in the minimum required front yard setback from sixty feet (60') to thirteen and five tenths feet (13.5') for the detached canopy along the north side of the proposed car wash building.
- That Section 9-6B-3(E)4, Required Setbacks, Rear Yard, be varied to permit a reduction in the minimum required rear yard setback from forty feet (40') to eight tenths feet (0.8') for the detached canopy along the south side of the proposed car wash building.
- That Section 9-6B-3(G), Maximum Height, be varied to permit an increase in the maximum building height from thirty feet (30') to forty feet (40') to the top of the proposed water tank.
- That Section 9-10-4(H) Off-Street Loading, Required Berths, be varied to permit a reduction of the minimum number of required loading berths from one (1) loading berth to zero (0) loading berths.
- That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required interior side and rear yard parking area setbacks from ten feet (10') to four and four tenths feet (4.4') along the west lot line, to five feet (5') along the east and south lot lines.
- That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required front yard parking area setback from fifteen feet (15') to zero feet along the north lot line.
- That Section 9-10-5(K), Off-Street Parking, Required Spaces, be varied to permit a reduction in the minimum number of stacking spaces for automobile laundries from fifty (50) spaces to thirty (30) spaces for the automobile washing and cleaning facility.
- That Section 9-10-5(L)1, Off-Street Parking, Minimum Parking Space And Aisle Dimensions, be varied to permit five (5) parallel parking spaces for employee parking along the southern building façade.
- That Section 9-11-4(C), Prohibited Signs, Roof Signs, be varied to permit the roof signs, as shown in the elevations.
- That Section 9-11-4(K), Prohibited Signs, Painted Signs, be varied to permit painted signs on the east and west building facades.
- That Section 9-11-4(R), Prohibited Signs, Multiple Message, Digital, Dynamic and/or Video Display Signs, be varied to permit the digital displays on the ground sign located near the shared access driveway with the Willowbrook Town Center, as shown in the elevations.
- That Section 9-11-12(B)1(a), Sign Surface Area, Number and Height Limitations, Business Districts, be varied to permit an increase in the maximum total sign surface area from three hundred thirty-five square feet (335 SF) to [??] (?? SF) for the one (1)



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- freestanding sign proposed at the northwest corner of the lot, for each wall sign proposed on the north, east and west building facades for a total of three wall signs, and for the three roof signs proposed on each of the three detached canopies, as shown in the sign package.
22. That Section 9-11-11(B)2 "Sign Location", be varied to allow building mounted sign to extend higher than one foot (1') below the top line of the face of the building and exceed a height of twenty feet (20') for the signage on the north elevation of the automobile washing and cleaning facility.
 23. That Section 9-12-2, Permitted Accessory Buildings, Structures and Uses, Awning/Canopies/Marquees, be varied to permit an increase in the permitted encroachment from 1/3 bulk district standard to allow the detached canopy located to the south of the building to be set back eight tenths feet (0.8') from the south lot line, and to allow the northernmost detached canopy located to the north of the building to be set back thirteen and five tenths feet (13.5') from the north lot line.
 24. That Section 9-12-4(C)3, Bulk Regulations, Detached Accessory Structures, be varied to permit a reduction in the minimum clearance above grade from fourteen feet (14') to twelve feet (12') for the detached pay canopy located to the south of the building and the detached canopies over the vacuum stations.
 25. That Section 9-14-2(D)2(c)4, Border Plantings and Foundation Plantings, be varied to permit a reduction in the minimum foundation landscape area width from seven feet (7') to zero feet along the north, east and west building facades.

LOT 2 (Fast Food Establishment and Drive-Through)

26. That Section 9-3-3(C), Multiple Uses on a Lot, be varied to permit a reduction in the minimum lot area for two (2) or more special uses from two acres (2 AC) to seven hundred ninety-five thousandths acre (0.795 AC).
27. That Section 9-6-1(A), Minimum Lot Area; Two Or More Uses On A Lot, be varied to permit a reduction in the minimum lot area for two (2) or more special uses (fast food establishment and drive-through) from two acres (2 AC) to seven hundred ninety-five thousandths acre (0.795 AC).
28. That Section 9-6B-3(A), Minimum Lot Area, be varied to permit a reduction in the minimum lot area for the fast food establishment and drive-through from two acres (2 AC) to seven hundred ninety-five thousandths acre (0.795 AC).
29. That Section 9-6B-3(C)1, Minimum Lot Width, be varied to permit a reduction in the minimum lot width for restaurants from two hundred feet (200') to one hundred twenty-two and seven tenths feet (122.7').
30. That Section 9-6B-3(E)2, Minimum Interior Side Yard Setback, be varied to permit a reduction in the minimum interior side yard setback along the east lot line from thirty feet (30') to fifteen and five tenths feet (15.5') for the proposed fast food establishment, to eleven and four tenths feet (11.4') for the proposed attached canopies, to sixteen and one tenths feet (16.1') and to twenty-six and two tenths feet (26.2') for the menu/order boards closest to the east lot line.
31. That Section 9-6B-3(E)4, Required Setbacks, Rear Yard, be varied to permit a reduction in the minimum required rear yard setback from forty feet (40') to twenty-two and nine tenths feet (22.9') and twenty-three feet (23') for the menu/order boards located closest to the south lot line.
32. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required interior side and rear yard parking area setbacks from ten feet (10') to



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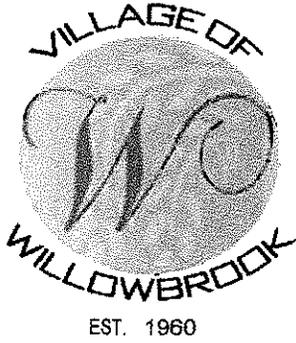


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- two and four tenths feet (2.4') along the east lot line and to zero feet along the west and south lot lines.
33. That Section 9-10-5(G)3 Off-Street Parking, Interior Parking Lot Landscaping, be varied to permit a reduction in the minimum end parking island width from nine feet (9') to five feet (5') and six and eight tenths feet (6.8') for two end parking islands.
 34. That Section 9-10-5(G)3 Off-Street Parking, Interior Parking Lot Landscaping, be varied to eliminate the requirement for one (1) shade tree and nine (9) dwarf variety shrubs required within the three parking islands.
 35. That Section 9-11-6(L), Signs, Exemptions, be varied to permit an increase in the permitted sign surface area for private traffic direction signs from four (4) square feet to eight and one tenths square feet (8.1 SF) for each of the three private traffic direction signs and fifteen and six tenths square feet (15.6 SF) for the private traffic direction sign located underneath the freestanding sign.
 36. That Section 9-11-12(B)1, Sign Surface Area, Number and Height Limitations, Business Districts, be varied to permit an increase in the total sign surface area from one hundred thirty-eight and four tenths square feet (138.4 SF) to three hundred eighty-three and four tenths square feet (383.4 SF), to permit an increase in the maximum number of wall signs from three (3) wall signs to six (6) wall signs, to permit building mounted signage to exceed a height of twenty feet (20') for the wall signs on the north and west building elevations, and to permit a reduction in the minimum separation distance between wall signs on the north and west building facades to less than 20% of the linear dimension of the business site frontage, in accordance with the building elevations.
 37. That Section 9-12-2, Permitted Accessory Buildings, Structures and Uses, Awning/Canopies/Marquees, be varied to permit an increase in the permitted encroachment from 1/3 bulk district standard to eleven and four tenths feet (11.4') from the east lot line for the two attached canopies located along the east building façade.
 38. That Section 9-14-2(D)2(c)4, Border Plantings and Foundation Plantings, be varied to permit a reduction in the minimum foundation landscape area width from seven feet (7') to zero feet along the north, west and south building facades, and to less than seven feet (7') along the east building façade.

LQT 3 (Financial Institution and Drive-Through)

39. That Section 9-3-3(C), Multiple Uses on a Lot, be varied to permit a reduction in the minimum lot area for two (2) or more special uses from two acres (2 AC) to seven hundred ninety-two thousandths acre (0.792 AC).
40. That Section 9-3-7(A)4, Specific Setbacks, Plainfield Road, be varied to permit a reduction in the minimum special setback from for one hundred feet (100') to seventy-one and 8/10ths feet (71.8') for the proposed building.
41. That Section 9-6-1(A), Minimum Lot Area; Two Or More Uses On A Lot, be varied to permit reduction in the minimum lot area for two (2) or more special uses (financial institution and drive-through) from two acres (2 AC) to seven hundred ninety-two thousandths acre (0.792 AC).
42. That Section 9-6-1(E)2, General Conditions, Building Façade Materials, be varied to allow aluminum composite panels as building façade materials, as shown on the architectural elevations.



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43. That Section 9-6B-3(A), Minimum Lot Area, be varied to permit a reduction in the minimum lot area for the financial institution and drive-through from two acres (2 AC) to seven hundred ninety-two thousandths acre (0.792 AC).
44. That Section 9-6B-3(E)2, Minimum Interior Side Yard Setback, be varied to permit a reduction in the minimum interior side yard setback along the east lot line from thirty feet (30') to seven feet (7') for the proposed building.
45. That Section 9-6B-3(E)4, Required Setbacks, Rear Yard, be varied to permit a reduction in the minimum required rear yard setback from forty feet (40') to thirty six and two tenths feet (36.2') for the attached canopy along the south side of the building.
46. That Section 9-10-4(H) Off-Street Loading, Required Berths, be varied to permit a reduction of the minimum number of required loading berths from one (1) loading berth to zero (0) loading berths.
47. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required interior side and rear yard parking area setbacks from ten feet (10') to eight and five tenths feet (8.5') along the west lot line and to zero feet along the east and south lot lines.
48. That Section 9-11-6(L), Signs, Exemptions, be varied to permit an increase in the permitted sign surface area for private traffic direction signs from four (4) square feet to four and six tenths square feet (4.6 SF) for the private traffic direction sign located to the southwest of the building.
49. That Section 9-11-5(E)2, Illuminated Window Signs, be varied to permit an increase in the maximum sign surface area for an illuminated window sign for the Chase ATM located in the vestibule of the west building facade from four square feet (4 SF) to eight and thirty-four hundredths square feet (8.34 SF).
50. That Section 9-11-6(M), Exemptions, ATM Signs, be varied to permit an increase in the maximum sign surface area from four square feet (4 SF) to eight and thirty-four hundredths square feet (8.34 SF).
51. That Section 9-11-12(B)1(a), Sign Surface Area, Number and Height Limitations, Business Districts, be varied to permit an increase in the maximum total sign surface area from one hundred ninety-seven and eighty-five hundredths square feet (197.85 SF) to three hundred forty-four square feet (344 SF) for the one (1) freestanding sign proposed at the northwest corner of the lot and for each wall sign proposed on the north, east and west building facades for a total of three wall signs, as shown in the sign package.
52. That Section 9-11-12(B), Sign Surface Area, Number and Height Limitations, Business Districts, be varied to permit the signs for the financial institution in accordance with the building elevations.
53. That Section 9-12-2, Permitted Accessory Buildings, Structures and Uses, Awning/Canopies/Marquees, be varied to permit an increase in the permitted encroachment from 1/3 bulk district standard to allow the attached canopy located to the south of the building to be set back eleven and five tenths feet (11.5') from the east lot line.
54. That Section 9-14-2(D)2(c)4, Border Plantings and Foundation Plantings, be varied to permit a reduction in the minimum foundation landscape area width from seven feet (7') to zero feet along the north, east, west and south building facades.

PUD Standards

Pursuant to Section 9-13-6 of the Village Code, the following variations from the provisions of the Planned Unit Development Regulations include but are not limited to:



EST. 1960

Willowbrook

835 Midway Drive
Willowbrook, IL 60527-5549

Phone: (630) 323-8215 Fax: (630) 323-0787 www.willowbrookil.org

Mayor

Frank A. Trilla

Village Clerk

Deborah Hahn

Village Trustees

Sue Berglund

Umberto Davi

Michael Mistele

Gayle Neal

Paul Oggerino

Gregory Ruffolo

Village Administrator

Brian Pabst

Chief of Police

Robert Schaller

Director of Finance

Carrie Dittman

1. That Section 9-13-6(B) PUD Standards, Size and Ownership, be varied to waive the requirement that the subject realty be under single ownership and/or unified control.
2. That Section 9-13-6(F) PUD Standards, Yards, be varied to waive the requirement that the required yards or setbacks along the periphery of the planned unit development be at least equal in width or depth to that of the applicable required yard within the adjacent zoning district.
3. That Section 9-13-6(L) PUD Standards, Other Standards, be varied to waive the requirement that the planned unit development must comply with the minimum standards set forth in subsections (A), (B), (C), (D), (H), (I) and (K) of this section.

Subdivision Ordinance

4. That Section 10-4-3(A)2, Lots, Sizes and Shapes, be varied to waive the requirement that the lot areas and lot widths conform to at least the minimum requirements of the zoning ordinance for the district in which the subdivision is proposed for Lots 2 and 3.
5. That Section 10-4-3(A)3, Lots, Sizes and Shapes, be varied to waive the requirement that building setback lines conform to at least the minimum requirements of the zoning ordinance for the district.
6. That Section 10-4-3(B), Lots, Arrangement, be varied to waive the requirement that Lot 1 front on a public street.

If you have any questions, you may contact me at planner@willowbrook.il.us or 630.920.2233.

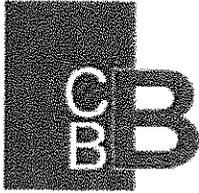
Sincerely,

Ann Choi
Village Planner

Cc: Brian Pabst
Michael S. Mertens
Dan Lynch
Roy Giuntoli



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CHRISTOPHER B. BURKE ENGINEERING, LTD.

9575 W Higgins Road, Suite 600 Rosemont, Illinois 60018-4920 Tel (847) 823-0500 Fax (847) 823-0520

January 27, 2021

Village of Willowbrook
835 Midway Drive
Willowbrook, IL 60527

Attention: Ann Choi

Subject: 735 Plainfield Road – Final Engineering and PUD
2nd Submittal
(CBBEL Project No. 900144.H160)

Dear Ann:

As requested by email on January 21, 2021, we have reviewed the PUD Plans and supporting documents. Based on our discussion, this is intended to be the Preliminary PUD submittal and this review is for compliance with Section 9-13-5.B of the Village Code.

Preliminary PUD and Subdivision Plat

We have discussed with the design engineer that the Preliminary PUD and Preliminary Plat of Subdivision will be combined into a two page document and that, combined with the Preliminary Engineering Plans will meet the plan content requirements for both the Preliminary PUD and Preliminary Plat of Subdivision. The following comments will be incorporated into the document:

1. On the Preliminary Plat of Subdivision, all of the certificates required on the Final Plat will be removed.
2. The Preliminary Plat will provide clarification that Public Utility and Drainage Easements will be provided as required. It remains to be determined if that will be specific easements for each utility, or a blanket easement covering all of the property except for building envelopes.
3. Section 9-13-5.B.4 provides that a statement of ownership be included listing the existing ownership and certification that there are no delinquent taxes on the property. We presume that this has been provided separately or will be added to the Preliminary PUD
4. Section 9-13-5.B.7 provides for covenants providing various information be submitted. We presume that this will be reviewed by others.
5. Section 9-13-5.B.12 provides for preliminary landscape plans. We will defer review of these drawings to the Village Planner.
6. Section 9-13-5.B.13 provides for preliminary engineering information to be shown. These comments are provided in the next section of this letter.
7. Sections 9-13-5.B.14 – 17 provides for various studies to be provided. We presume these are being reviewed by others.

Preliminary Engineering Plans

Cover Sheet – C0.0

1. Under Utility and Governing Agencies, change the Sanitary Sewer Service to Du Page County Public Works.

Site Plan – C1.0

1. No comments.

Grading Plan – C2.0

1. No comments.

Utility Plan – C4.0

2. The alignment for the proposed watermain has been discussed with the design engineer and will be revised. The connection to the existing main in the town center will be made at the driveway connection between the two sites, rather than south near the proposed car wash building. Also, the proposed watermain will be an 8" main. This will result in less new public watermain and is expected to be a lower cost for the applicant. A 20 foot wide utility easement would be required over this main.
3. Each building service shall have an 8" x 6" Tee with valve. This will allow the public main to be constructed, and then each building service can be installed at the time of building construction without interrupting the public main.
4. The last segment of storm sewer west of the car wash, where the south end of the site connects to the off-site sewer, is shown to be 18". We presume this will be revised down to 12" on the final plans.
5. We presume the intent is for the sanitary sewer serving the bank and restaurant to be a public sewer. This will be owned by Du Page County Public Works and will be reviewed by that agency. We presume they will require a 20 foot wide easement over the main.

General Comments

1. A permit will be required from Du Page County Division of Transportation for work within the Plainfield Road right of way.
2. A permit will be required from Du Page County Public Works for the proposed sanitary sewer and connections.
3. The Construction Schedule and Engineer's Opinion of Cost will be reviewed with the Final PUD Plat.
4. The fire truck turning templates indicate that presumed truck can maneuver the site. We presume that prior to final approval the plans will be provided to the fire district for comment on both the geometrics, but also the location of hydrants and fire department connections on each building.

If you have any questions, please do not hesitate to contact me.

Sincerely,

A handwritten signature in cursive script, appearing to read "Daniel Lynch".

Daniel L. Lynch, PE, CFM
Head, Municipal Engineering Department

Cc Brian Pabst
Mike Mertens
Roy Guintoli
AJ Passero

Project Traffic Review #2

To: Ms. Ann Choi
Planning Consultant, Village of Willowbrook

From: Lynn M. Means, P.E., PTOE
Senior Transportation Engineer

Date: January 26, 2021

Subject: Proposed Retail Development
735 Plainfield Road
Willowbrook, Illinois

Per your request, Gewalt Hamilton Associates, Inc. (GHA) has reviewed the following materials for the above captioned project pertaining to traffic, parking and on-site circulation:

1. KLOA response to GHA's traffic comments dated January 20, 2021.
2. KLOA response to DuDOT's comments dated January 20, 2021.
3. Traffic Impact Study (TIS) prepared by KLOA dated January 20, 2021.
4. Site Plan, Sheet Number C1.0, prepared by Kimley Horn dated January 21, 2021.

Conclusions of this effort and **recommendations** are presented below:

1. GHA has found the documentation and/or responses provided, in general, to adequately address the comments made and concerns raised in our previous Project Traffic Review dated January 15, 2021.
2. The applicant should provide documentation of the DuDOT approval of TIS and Plainfield Road access, upon receipt.
3. The response letter adequately addressed the comment pertaining to the trip rates assumed in the traffic study for the proposed carwash component of the development during the weekday morning peak hour; however, the concern raised pertaining to the Saturday morning timeframe was not addressed. Please provide response and/or supporting documentation for use of the lower trip generation estimates based on the tunnel independent variable versus the square-footage of the use.
4. The updated study appropriately provides a queuing exhibit that illustrates the stacking capability of each of the proposed uses, as well as additional supporting documentation. However, the supporting carwash queue data provided is insufficient to verify the projected queue lengths. It references a study that was performed in Minnesota in 2012, where 5 of the 6 carwash locations surveyed were at gasoline station facilities, as well as the referenced study did not include average daily traffic for a measure of comparison. While we understand it may be difficult to obtain peak observations at similar facilities, as carwash operations tend to peak in the wintertime on a sunny day following a snowfall, please provide supporting operational characteristics (e.g., transaction information), as available, pertaining to peak operations and/or supporting operations / documentation from similar Chicagoland area existing sites during average, typical conditions.

In addition, as we understand, the quick service restaurant use has been identified as Guzman Y Gomez. There is an existing drive-through location in Naperville. Again, please provide either data from observations and/or transaction data to support the capacity provided is adequate to support the peak queue.

* * * * *

We hope you find these comments helpful in your assessment of the proposed development. *GHA does not believe the comments noted above will materially impact the findings / **recommendations** of the TIS.* Should you have any questions please do not hesitate to contact GHA at 847-478-9700.

5670.900_735 Plainfield_Traffic Review 2_01.26.21

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FIRE PROTECTION DISTRICT

419 PLAINFIELD ROAD • DARIEN, ILLINOIS 60561 • (630) 323-6445

January 28, 2021

Kimley Horn and Associates Inc.
Attn: Eric Tracy
4201 Winfield Road Suite 600
Warrenville, IL 60555
630-487-5560

planner@willowbrook.il.us
eric.tracy@kimley-horn.com

RE: Site Plan Review
735 Plainfield Road,
Willowbrook, IL 60527

To whom it may concern,

We have received a copy of the site plans and turn exhibits for the above listed project. The following comments are from this office:

1. Drive width of 20' clear aisles. This includes the drive adjacent to the car wash. The curb radius and drive space is less than 20' and our fire apparatus will have a difficult time accessing the building.
2. Fire truck dimensions are not an inner city bus and shall be for a vehicle that is 47.68' long with a clear height of 13'6" and a width of 10'4".
3. The trash enclosures located in that area make it impossible for a fire vehicle to access the drive that is adjacent to the building. Prove that the turn radius is available.
4. The canopies for the vacuum stations may not be an issue if they don't extend into the drive area. This needs to maintain a 20' clear path in the drive with a 13'6" clear height.
5. The exit from the car wash is less than 20' and the fire vehicles will not be able to exit without hitting the curbs and causing damage.
6. All the other canopies will not be an issue. There needs to be a verification with the car wash.

Necessary inspections are to be performed along with any outstanding fees paid before occupancy is granted.

The Bureau of Fire Prevention has been asked to review the plans, specifications or other documents submitted to see if compliance has been made with the Fire Prevention Codes and Ordinances of the Tri-State Fire Protection District. Errors or omissions by representatives of the Bureau of Fire Prevention do not constitute permission to cancel, set aside or waive any provision of any applicable Code or Ordinance of the Tri-State Fire Prevention District. Approvals by the Bureau of Fire Prevention will be in writing only.

Sincerely,



Chris Drews
Plan Review Specialist
Bureau of Fire Prevention
cdrews@tristatefd.com
630-654-6284

