

A G E N D A

REGULAR MEETING OF THE PLAN COMMISSION OF THE VILLAGE OF WILLOWBROOK TO BE HELD ON WEDNESDAY, February 3, 2021 AT 7:00 P.M. AT THE WILLOWBROOK POLICE DEPARTMENT, TRAINING ROOM, 7760 QUINCY STREET, WILLOWBROOK, ILLINOIS.

DUE TO THE COVID 19 PANDEMIC, THE VILLAGE WILL BE UTILIZING A CONFERENCE CALL FOR THIS MEETING.

THE PUBLIC CAN UTILIZE THE FOLLOWING CALL IN NUMBER:

Dial in Phone Number: 312-626-6799

Meeting ID: 870 3339 8604

Password: 604471

Written public comments can be submitted by no later than 6:00pm on February 3, 2021 to planner@willowbrook.il.us.

1. CALL TO ORDER
2. ROLL CALL
3. OMNIBUS VOTE AGENDA
 - A. Waive Reading of Minutes (APPROVE)
 - B. Minutes - January 13, 2021
4. PLAN COMMISSION CONSIDERATION: Continuation of Zoning Hearing Case 21-01: Consideration of a petition requesting approval of a special use permit for a fast food establishment and a special use permit for a drive-through in the B-2 Community Shopping District, including certain variations from Title 9 of the Village Code. The applicant seeks to demolish the existing gas/service station and construct a one-story building and drive-through with associated site improvements on the property. The Applicant is Hakim Yala of Panda Express, Inc., 1683 Walnut Grove Avenue, Rosemead CA 91770. The property owner is True North Energy, LLC, 10346 Brecksville Road, Brecksville OH 44141.
 - A. PUBLIC HEARING
 - B. DISCUSSION/RECOMMENDATION

5. PLAN COMMISSION CONSIDERATION: Zoning Hearing Case 21-03:
Consideration of a petition requesting approval of a special use permit for a planned unit development, including a financial institution with drive through, fast-food establishment with drive through, an automobile washing and cleaning facility, including certain relief, exceptions and variations from Title 9 and Title 10 of the Village Code; approval of a Preliminary Plat of Subdivision; and approval of a Preliminary Plat of PUD. The applicant for this petition is Alex Katz of G.W. Property Group LLC, 2211 N. Elston Avenue, Suite 304, Chicago IL 60614. The property owner is Viren-Gill Ltd., LLC, 735 Plainfield Road, Willowbrook IL 60527.
 - A. PUBLIC HEARING
 - B. DISCUSSION/RECOMMENDATION
6. VISITOR'S BUSINESS
7. COMMUNICATIONS
8. ADJOURNMENT

MINUTES OF THE REGULAR MEETING OF THE PLAN COMMISSION HELD ON WEDNESDAY,
JANUARY 13, 2021 AT THE WILLOWBROOK POLICE DEPARTMENT, TRAINING ROOM, 7760
QUINCY STREET, WILLOWBROOK, ILLINOIS

**DUE TO THE COVID19 PANDEMIC THE VILLAGE WILL BE UTILIZING A ZOOM
CONFERENCE CALL FOR THIS MEETING**

1. CALL TO ORDER

Chairman Kopp called the meeting to order at the hour of 7:13p.m.

2. ROLL CALL

Those physically present at VOW Police station were Chairman Kopp and Commissioner Soukup
Present Via Zoom COVID -19 Pandemic were Commissioners Remkus, Kaczmarek, Kaucky, Walec,
Vice Chairman Wagner and Building Official Roy Giuntoli
Also, present Via Zoom were Planning Consultant Anne Choi and Lisa Shemroske from the Village Hall

3. OMNIBUS VOTE AGENDA

The items on the Omnibus Vote Agenda were as follows:

- A. Waive Reading of Minutes (APPROVE)
- B. Minutes – Special Meeting, December 16,2020

MOTION: Made by Commissioner Remkus seconded by Commissioner Soukups approve the
Omnibus Vote Agenda as presented.

MOTION DECLARED CARRIED

4. PLAN COMMISSION CONSIDERATION: Zoning Hearing Case 21-01; This hearing will be
continued due to an incomplete application. (Applicant: Hakim Yala of Panda Express, Inc.,1683
Walnut Grove Avenue, Rosemead CA 91770.The Property owner is True North Energy, LLC,
10346 Brecksville Road Brecksville OH 44141)

- A. PUBLIC HEARING
Opened at 7:15pm
- B. DISCUSSION/RECOMMENDATION

See Court Reporter Minutes for Discussion and Recommendation

MOTION: The following motion was to continue Zoning Hearing Case 21-01 till the February
3, 2021 Regular Plan Committee meeting made by Vice Chairman Wagner and Second by
Commissioner Remkus, All in favor.

MOTION DECLARED CARRIED

5. PLAN COMMISSION CONSIDERATION: Zoning Hearing Case 21-02: Consideration of a petition to approve a variation from Section 9-5B-3(D)4 to reduce the rear yard setback from thirty feet (30') to twenty-five feet (25') and approval of a variation from Section 9-5B-3(G) to increase the maximum FAR from 0.03 to 0.39 to allow construction of a 1,081 square foot addition to house an indoor swimming pool, and other such relieve from Title 9 of the Village Code necessary.

A. PUBLIC HEARING

Closed public hearing at 8:03pm

B. DISCUSSION/RECOMMENDATION

See Court Reporter Minutes for Discussion and Recommendation.

MOTION: The Plan Commission does not recommend the petition presented for Zoning Hearing Case 21-02 was made by Commissioner Remkus and second my Vice Chairman Wagner.

Roll Call Vote: AYES: Commissioner Remkus, Soukup, Kaczmarek, Kaucky, Walec, Vice Chairman Wagner, and Chairman Kopp NAYS: None

6. VISTOR'S BUSINESS.

None

7. COMMUNICATIONS

Planner Choi informed Commissioners the two items that will be discussed at the February 3, 2021 Plan Commission meeting. One being for the Continued Zoning case hearing 21-01 second one for Willowbrook Bowl now known at 735 Plainfield road PUD. Hinsdale Commons drive thru will be dropping off an application and hopefully will be on Agenda for March 2021 meeting. Building Official Giuntoli permanent signal at Kingery and Plainfield should be on by the end of the month. Side entrance on Plainfield making progress.

8. ADJOURNMENT

MOTION: Made by Commissioner Remkus seconded by Vice Chairman Wagner to adjourn the meeting of the Plan Commission at the hour of 8:13 p.m. all in favor

UNANIMOUS VOICE VOTE
PRESENTED, READ, AND APPROVED,

MOTION DECLARED CARRIED

February 3, 2021

Chairman

1/13/2021

**VILLAGE OF WILLOWBROOK PROCEEDINGS BEFORE THE
PLANNING & ZONING COMMISSION OF THE VILLAGE OF
WILLOWBROOK**

ROBIN HEJNAR

VILLAGE OF WILLOWBROOK
PROCEEDINGS BEFORE THE
PLANNING & ZONING COMMISSION
OF THE VILLAGE OF WILLOWBROOK

WEDNESDAY, JANUARY 13, 2021

7:00 p.m.

RECORD OF THE PROCEEDINGS VIA ZOOM had at the meeting held before the Planning & Zoning Commission of Willowbrook, on Wednesday, the 13th day of January 2021, commencing at 7:00 p.m., as reported by Robin Hejnar, a Certified Shorthand Reporter and Registered Professional Reporter and Notary Public in and for the County of DuPage and State of Illinois.

1 APPEARANCES:

2
3 Daniel Kopp - Chairman

4 John Wagner - Vice-Chairman

5 Lisa Shemroske - Secretary

6 Commissioner Soukup - Member

7 Maciej Walec - Member

8 Catherine Kaczmarek - Member

9 Leonard Kaucky - Member

10 William Remkus - Member

11
12
13
14
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16
17 Staff Also Present:

18 Ann Choi - Planning Consultant

19 Roy Giuntoli - Building Official

1 (Wherein, begins the Zoom-recorded audio
2 transcribed by Court Reporter.)

3 CHAIRMAN KOPP: Let's get started then.
4 This is the regular meeting of the Plan Commission of
5 the Village of Willowbrook. I ask -- I call this
6 meeting to order and ask the Plan Commission secretary
7 to call the role.

8 MS. SHEMROSKE: Commissioner Remkus?

9 MR. REMKUS: Here.

10 MS. SHEMROSKE: Commissioner Soukup?

11 MR. SOUKUP: Here.

12 MS. SHEMROSKE: Commissioner Kaczmarek?

13 MS. KACZMAREK: Here.

14 MS. SHEMROSKE: Commissioner Kaucky?

15 MS. KAUCKY: Here.

16 MS. SHEMROSKE: Commissioner Walec?

17 MR. WALEC: Here.

18 MS. SHEMROSKE: Vice Chairman Wagner?

19 MR. WAGNER: Here.

20 MS. SHEMROSKE: Chairman Kopp?

21 CHAIRMAN KOPP: Here.

22 MS. SHEMROSKE: Planner Ann Choi?

23 MS. CHOI: Here.

24 MS. SHEMROSKE: Building Official Roy

1 Giuntoli?

2 MR. GIUNTOLI: Present via Zoom.

3 MS. SHEMROSKE: And for the record, I'm Lisa
4 Shemroske.

5 CHAIRMAN KOPP: Next item on the agenda is
6 the omnibus vote agenda. Would any of the commissioners
7 like an item removed from the omnibus vote agenda; if
8 not, would someone make a motion to approve the omnibus
9 vote agenda?

10 MR. REMKUS: Remkus so moved.

11 CHAIRMAN KOPP: Soukup seconded here.

12 MS. SHEMROSKE: Thank you.

13 CHAIRMAN KOPP: So all in favor of the
14 omnibus vote agenda, please say, Aye.

15 ALL MEMBERS: Aye.

16 CHAIRMAN KOPP: Opposed, say Nay. So that
17 motion carries.

18 Next item, item four on the agenda, is
19 Zoning Hearing Case 21-01. The applicant for that is
20 Hakim Yala of Panda Express of Rosemead, California.
21 The property is -- the property owner is True North
22 Energy of Brecksville, Ohio.

23 Because of an incomplete application,
24 [reporter joins Zoom meeting live] we are going to

1 continue this hearing until the next regular hearing
2 on -- what date, Ann?

3 MS. CHOI: February 3rd, 2021. So I believe
4 we have to open up the hearing, continue it, and then
5 not close the hearing.

6 CHAIRMAN KOPP: Okay. So I am opening
7 Zoning Hearing Case 21-01. The applicant, as I said, is
8 Hakim Yala of Panda Express, of Rosemead, California,
9 and the property owner is True North Energy, LLC, of
10 Brecksville, Ohio.

11 What's the address of the property, Ann?

12 MS. CHOI: I'm sorry, it's 7505 Kingery. It
13 is right across the street from the Potbelly. So it's
14 that Shell Gas Station.

15 CHAIRMAN KOPP: Do you think we need to do
16 it by motion, or can I just continue it?

17 MS. CHOI: I think we do everything by
18 motion, so I would say --

19 CHAIRMAN KOPP: Okay. Will someone make a
20 motion to continue Zoning Hearing Case 21-01 to the next
21 regular meeting of the Plan Commission on February 3rd?

22 MR. WAGNER: Wagner, so moved.

23 MR. REMKUS: Remkus second.

24 CHAIRMAN KOPP: All in favor say, Aye.

1 ALL MEMBERS: Aye.

2 CHAIRMAN KOPP: Opposed, say Nay.

3 Okay. So that hearing is continued.

4 Next item on the agenda, No. 5, the purpose
5 is public hearing for Zoning Hearing Case No. 21-02.

6 The purpose of this public hearing shall be to consider
7 a petition for approval of a variation from Section
8 9-5B-3(D)4 to reduce the rear yard setback from 30 feet
9 to 25 feet, and approval of a variation from Section
10 9-5B-3(G) to increase the maximum floor area ratio from
11 0.30 to 0.39 to allow construction of a 1,081 square
12 foot addition to house an indoor pool, and such other
13 relief from Title 9 of the Village Code as necessary,
14 and the property with and address of 6401 Meadow Lane,
15 Willowbrook, Illinois.

16 The applicant for this petition is Pete
17 Baftiri, and the property owner is Jain Bhagwan of 5S541
18 Radcliffe Road, Naperville, Illinois 60563. A copy of
19 this notice was published in the December 24, 2020,
20 Edition of the Doings Newspaper.

21 Ann, would you like to make your report?

22 MS. CHOI: Yes. Thank you, Chairman; and
23 good evening, Commissioners.

24 The petitioner, Pete Baftiri, of A&E Luxury

1 Homes, on behalf of the property owners at 6401 Meadow
2 Lane, the subject property of this petition are
3 requesting the following:

4 For the construction of a 26-and-a-half foot
5 by 42-foot, 10-inch addition off the rear exterior wall
6 of the existing residence. The addition is proposed to
7 a maximum height of 21 feet, and would house an indoor
8 swimming pool for therapeutic purposes to serve its
9 elderly residents.

10 The construction of the addition of the size
11 desired would encroach, approximately, 5.54 feet into
12 the required rear yard setback of the property, and
13 increase the allowable floor area. As such, the
14 applicant has applied for the following two variations.

15 Again, as you stated, approval of a
16 variation from Section 9-5B-3(D)4 to reduce the rear
17 yard setback from 30 feet to 24.46 feet. The public
18 hearing notice actually stated 25 feet, but it is
19 actually 24.46 feet. I just wanted to highlight that.

20 And the second variation is approval of a
21 variation from Section 9-5B-3(G) to increase the maximum
22 FAR from 0.30 to 0.39 to allow for the construction of a
23 1,081 square foot addition.

24 The subject property is zoned R2

1 single-family residence, and are adjacent to properties
2 also in the R2 single-family residence to the north,
3 south, east and west.

4 This application is subject to review by the
5 Plan Commission regarding the requested zoning relief,
6 and some background, this parcel was platted back in
7 1978 as part of the Gallagher and Henry Waterford Unit
8 No. 2 Subdivision.

9 The existing single-family resident
10 currently -- residents currently conform to the minimum
11 requirements of the zoning ordinance. However, the
12 proposed addition would add just under 1,100 square feet
13 of floor area, thereby increasing that FAR by
14 130 percent. The required rear yard setback is
15 currently 30 feet, and the proposed addition would
16 encroach into this area by 5-and-a-half feet.

17 It should also be noted that the current lot
18 depth does not conform to the current zoning regulations
19 since this subdivision was platted in 1978. If the
20 subject property was platted to today's standards, the
21 variation request for reduced rear yard setback would
22 likely be unnecessary. However, the variation request
23 for increased FAR would still be necessary.

24 Staff included the following recommended

1 conditions for this petition:

2 The applicant shall revise the site plan
3 included as attachment five of the staff report to
4 reflect the correct variation to reduce the rear yard
5 from 25 feet to 24.46 feet. I believe this has already
6 been revised.

7 The variations granted shall only apply to
8 the proposed addition to accommodate an indoor swimming
9 pool. The proposed addition shall not be used as
10 habitable space or for non-recreational purposes,
11 including, but not limited to bedrooms, living rooms,
12 kitchens, et cetera.

13 The variations shall be null and void of
14 construction if the proposed use is not commenced, and a
15 certificate of completion is not granted within
16 12 months of the date of any approval of the variation
17 by the Village Board.

18 I did want to note that I received three
19 letters -- actually, two letters and an email from
20 neighbors, which the Plan Commission have now received
21 by email, at least two of them. These letters include
22 16 signatures in opposition to the petition; and I can
23 read these letters now, to include as part of the
24 transcript or during public comment, whichever you

1 prefer, Chairman Kopp; and I just want to say that these
2 letters highlight the concerns over existing water
3 drainage problems in the area that might be exacerbated
4 by increased impervious area.

5 CHAIRMAN KOPP: Why don't we -- why don't
6 you go ahead and read the letters now.

7 MS. CHOI: Okay. They're quite lengthy, so
8 bear with me.

9 "To the Plan Commission of the Village of
10 Willowbrook:

11 We, the adjacent neighboring property owners
12 of the subject property offer the following concerns for
13 consideration in review of proposal of the following
14 Case No. PC21-02.

15 A petition for approval of a variation from
16 Section 9-5B-3(D)4 to reduce the rear yard setback from
17 30 feet to 25 feet," which is now 24.46 feet, "and
18 approval of a variation from Section 9-5B-3(G) to
19 increase the maximum FAR from 0.30 to 0.39 to allow
20 construction of a 1,081 square foot addition to house an
21 indoor swimming pool at the property located at 6401
22 Meadow Lane.

23 The design plans prepared by Agama Design
24 Architecture do not adequately depict or address how

1 storm water will be controlled, or the potential impact
2 to neighboring properties. The impervious area of the
3 rear yard will be greatly reduced, impacting the natural
4 drainage flow. Drainage flow is a very real concern of
5 the adjacent property owners.

6 The plans depict graphically what seems to
7 be a gutter with no vertical leaders indicated on the
8 pitched gable sides of the elevations of the proposed
9 addition. Without indication of where the storm water
10 is running, we assume the storm water will sheet flow on
11 the existing grading, thereby increasing the flow rate
12 of the storm water on the existing grading.

13 In review of the proposed engineering plans,
14 the notation that all existing grading is to remain can
15 not be accurate. The proposed addition crosses the site
16 contour line of spot elevation 719.54 feet to
17 716.51 feet in the northeastern corner of the site.
18 Northeastern corner of the site is the lowest point of
19 the site, and holds the most storm water runoff
20 currently.

21 As indicated on the engineering plans, a
22 storm water manhole is located in the southeast corner
23 of the site. However, all storm water runoff is being
24 directed to the northeast corner. Although the proposed

1 addition shows the grade to be flat, this is not
2 accurate, nor are the engineering plans and design plans
3 coordinated to provide same information.

4 We request that consideration of this
5 petition be continued to allow adequate time for the
6 applicant/property owner to properly study and then
7 present to the Plan Commission the impact of the
8 proposed variation upon storm water drainage and what
9 types of remedial measures, if any, might adequately
10 prevent or mitigate the issue.

11 In terms of whether the proposed variation
12 will alleviate some demonstratable and unusual hardship
13 posed by the COVID-19 outbreak, we suggest that the Plan
14 Commission take a fresh look at this factor after a
15 study of the storm water drainage issue. The hardships
16 and restrictions associated by the outbreak may be
17 alleviated over the first half of 2021 as a vaccination
18 program continues to ramp. Health clubs may reopen
19 prior to completion of construction, if not prior to
20 commencement.

21 Finally, as part of the proposal, we would
22 like to see a site logistics plan to ensure the limits
23 of construction are held off the adjacent properties.
24 The existing vegetation along the property lines is

1 requested to be protected from any and all site
2 disturbances.

3 We also object to the variation to increase
4 the maximum FAR to 0.30 to 0.39. We believe the Village
5 of Willowbrook intended for the maximum FAR to align
6 with open space and green space ideals of suburban
7 community desires.

8 We hope you will take our concerns into
9 consideration, and make the decision to disapprove for
10 any request for variance or building that may cause harm
11 to our properties. Thank you."

12 I believe this was from Kevin Webb, who's a
13 resident to the rear.

14 MR. WEBB: That is correct.

15 MS. CHOI: I have a second letter from Jan
16 and Carol Miecznikowski. I'm sorry if I'm butchering
17 the last name. They live at 6420 Tremont Street.

18 And this letter reads:

19 "We are a neighboring property owner of the
20 subject property offering the following two concerns for
21 consideration in review of the Case No. PC21-02.

22 Our first concern relates to the drainage of
23 water in the area. Having recently made an investment
24 to finally solve a persistent drainage problem on our

1 property, we are concerned that the proposed
2 construction would cause the problems to return.
3 Therefore, we are not in favor of granting any
4 exceptions to the ordinances unless assurances can be
5 given that the planned construction will not cause any
6 drainage issues on neighboring properties.

7 Our second concern is with the precedence
8 that would be set by the proposed changes to the setback
9 in FAR. Although, seemingly a single home on a single
10 street, such an accommodation could become precedent,
11 and, therefore, a slippery slope, which would transform
12 the look and feel of the neighborhood incrementally
13 without a broader hearing of the matter, and without an
14 exclusive decision on the part of the Village.

15 Thank you in advance for considering our
16 inputs. Best regards, Jan and Carol Miecznikowski."

17 And then the third I just received via email
18 at 6:00 p.m., and this resident is Rob Goodridge at 6354
19 Meadow Lane; and he writes:

20 "Dear Mr. Pabst, we would prefer to maintain
21 the rear yard setback to remain at 30 feet and the
22 maximum FAR of 0.30. Thank you."

23 And that was it.

24 CHAIRMAN KOPP: If you're done, Ann, we'll

1 allow the applicant to make his presentation.

2 MR. BAFTIRI: Good evening, everyone. My
3 name is Ardi Baftiri. I am here with Pete Baftiri, A&E
4 Luxury Homes. Thank you, everyone, for hearing our
5 petition.

6 CHAIRMAN KOPP: I'm sorry to interrupt you.
7 You will need to be sworn in.

8 (Whereupon, Ardi Baftiri is duly
9 sworn.)

10 MR. BAFTIRI: Again, thanks, everyone; and
11 thank you, Ann, for your help and guidance to this
12 point.

13 So thank you for also reading those concerns
14 from the neighbors, understandable, and we want to point
15 out some of the key points from our proposal.

16 We believe that this is a neighborhood in
17 transition. There's a lot of new construction homes,
18 some remodeling. We think that this remodel to the
19 house will add value to the neighborhood, and as it's
20 revitalizing one of the older houses in that
21 neighborhood, I just want to point out there's also new
22 windows --

23 (Phone rings.)

24 MR. BAFTIRI: So the remodel has --

1 THE REPORTER: Anil, if you can mute
2 yourself. Continue.

3 MR. BAFTIRI: So the remodel also consists
4 of new windows, new tile roof, new stucco, and just
5 high-quality construction generally.

6 Overall, we think it's going to increase the
7 value of the surroundings properties and for the
8 neighborhood. The purpose of the addition is because
9 it's for therapeutic purposes. The residents are
10 elderly, and it is for therapy, because of the COVID
11 pandemic, they are not able to go to other public
12 facilities, and, so, this is kind of the only reasonable
13 alternative given the risks to their health.

14 And regarding the points that were noted
15 from the neighbors, the rear yard setback would, I
16 believe, to my understanding, would not be required if
17 the house was built to date, but regardless still, it's
18 a very minor five-foot setback. The vegetation would be
19 preserved and protected to the best of our abilities,
20 and most likely we'll be adding vegetation there,
21 screening and privacy for all the neighbors, including
22 the residents.

23 As for the drainage issues, our engineers
24 are very experienced and they will comply with any

1 drainage requirements that the Village requests, if
2 approved here, during the review process with Village
3 staff afterwards, and, so, we don't believe that
4 drainage will be an issue given the design proposed.

5 And I'll pass it here to Pete Baftiri for
6 any additional comments.

7 MR. P. BAFTIRI: I just want --

8 CHAIRMAN KOPP: Mr. Baftiri will need to be
9 sworn in.

10 (Whereupon, Pete Baftiri is duly
11 sworn.)

12 MR. P. BAFTIRI: Thank you, everyone, again.

13 So I just wanted to add an additional
14 statement for the project, that the remodel that's
15 already done, it's very high-end remodeling. The owners
16 spent a lot of money, and the house was completely brand
17 new; and by adding pool to this property, we think that
18 this will add value to the neighborhood. It's not going
19 to hurt the neighborhood. Actually, the neighborhood
20 will benefit a lot by having a brand new house remodeled
21 with the new standards, with today's standards, and
22 having a pool on this neighborhood.

23 This neighborhood is changing. I've been a
24 builder for 23 years, and I've seen changes in different

1 neighborhoods, Willowbrook -- I lived in Willowbrook and
2 all western suburbs.

3 So this neighborhood kind of is in
4 transition with some old homes. I have clients who want
5 to tear it down, build new ones. It's very good
6 location. Some of them are between doing the remodel.
7 So in the future, I think this subdivision will change;
8 and by initiating this change, in FAR, by asking for
9 this variation by adding a pool to this property, I
10 think it's going to add a lot of value to the
11 neighborhood.

12 For the drainage, neighbors are right, they
13 should be concerned, but one thing, I can promise that
14 our engineers, civil engineers will address every issue,
15 and we'll comply with all the requirements Village
16 requires, without affecting the neighborhood.

17 And the other thing is the way how it's
18 designed, and I know that the neighbor next door on the
19 north side -- it's going to be next to the property on
20 the north side, actually, that neighbor will get more
21 privacy from these neighbors. By adding the pool on the
22 back, they'll have really good privacy; and the pool
23 house, it won't be that tall, if you see on the
24 blueprint.

1 On the rear backyard, only 5.4 -- like 5.6
2 feet, I think we ask for variance; and that one --
3 again, there is a lot of vegetation, existing one; and I
4 did talk to the owner, they're going to do, also,
5 landscaping changes. They're going to add a lot of
6 vegetation to beautify this lot.

7 If you have any additional questions, let me
8 know.

9 CHAIRMAN KOPP: Do any of the commissioners
10 have any questions for either Ann or for the applicant?
11 I'll get started.

12 So, Ann, the letter from the Village's
13 engineer about drainage, he just said that it's not of
14 a -- if I'm reading this correctly, that it's not of the
15 size that -- he doesn't make an opinion about whether
16 this is going to affect the drainage.

17 MS. CHOI: Well -- so I followed up with him
18 after I received this email, and he wanted to clarify a
19 few things.

20 So he said that the grading plan that was
21 referenced in the first letter is not a plan for the
22 currently proposed project. I believe that maybe Agama
23 Design Architecture, they presumably took a plan from a
24 2016 submittal. I think back in 2016 there was an

1 application with the Building Department for an addition
2 to the home, which was never built. So they likely took
3 that 2016 submittal, and superimposed a proposed
4 addition on that plan.

5 In our engineer's letter, he stated that, at
6 the time of building permit, they will need a grading
7 plan, if the variations are approved; and based on what
8 was submitted, it was difficult for him to make more
9 than general comments on what the code requires.

10 However, he will definitely take into
11 consideration that there are existing drainage problems
12 at the northeast corner of the site because he wasn't
13 aware of that prior.

14 CHAIRMAN KOPP: And my other question to you
15 has to go with FAR; and I don't know that you can answer
16 this, because we have this aerial, your Exhibit 2, and
17 some of these houses, at least, based on lot coverage,
18 looked like they're probably much larger houses than
19 this house, even this house with the addition.

20 MS. CHOI: Yes, you are correct.

21 So I actually went on Google Street View to
22 look at some of those homes. I believe the ones that
23 look like they -- that are more L-shaped, and they have
24 an extension towards the back, they're actually

1 one-story, if I'm not mistaken.

2 I was looking for older, perhaps, zoning
3 cases that might have approved variations for FAR. I
4 couldn't find any in this neighborhood, so I believe
5 they're probably complying with current FAR, because
6 they're 1-to-1-and-a-half stories.

7 CHAIRMAN KOPP: Okay. That's it for my
8 questions.

9 Any other commissioners have questions of
10 Ann or the applicant?

11 MR. REMKUS: One question I had was that,
12 one of the things -- that we're doing this, wanting a
13 variation because of the COVID. I think -- myself, I
14 don't want to get into granting variations because of
15 COVID. I think that's a really slippery slope for us to
16 go down.

17 This is something -- it's a pandemic, it's
18 worldwide, but it's not going to be here forever. When
19 we grant a variation to do something, the variation we
20 grant is going to be there forever, so I'm a little
21 worried about that.

22 MR. WAGNER: Commissioner Wagner. I guess I
23 would point out a couple things, or ask a question.

24 This subdivision, when it was developed, was

1 developed within the guidelines of the Village of
2 Willowbrook, I believe, and it may not meet the current
3 zoning standard, which typically changes over time. I
4 don't think it was developed in the county. I think
5 because that and Rogers Farm was also developed at a
6 time when it was a subdivision to Willowbrook, which
7 this backs up to. That's No. 1.

8 No. 2, I think, to some degree, the hearing
9 may be moot because the notice is incorrect, and I
10 believe -- the request is for greater than what was
11 published, even though it's minor, I would question it.

12 The third thing is, these standards for
13 variation -- I'd have a hard time with the -- many of
14 the things that have been brought up within that; and,
15 No. 1, that the property in question can't have a
16 reasonable return without this variation being granted.
17 I question that.

18 And I would agree that the reasoning for the
19 COVID is not something that should be recognized,
20 because it's something that we're dealing with, but
21 isn't a long-term situation that looks like it would be
22 running with the property as the variation does. So I
23 would raise those questions. Thank you.

24 CHAIRMAN KOPP: Anything else from -- any

1 questions from any other commissioners?

2 MS. KACZMAREK: I have a question for the
3 applicant.

4 You mentioned that this is going to add
5 value to the property. I'm just curious, do you have a
6 Realtor, or anyone that you reached out to just to see
7 if at all this would provide value to the property in
8 the long run?

9 MR. P. BAFTIRI: So when I said it's going
10 to add value to the property -- the existing property is
11 proposing -- this is old house, but, also, I said it's
12 going to add value to the neighborhood by having the
13 house with a pool, high-end -- high finishes, it's going
14 to be a positive impact to the neighborhood and to
15 property itself.

16 So based on my experience -- and, also, I'm
17 real estate agent -- yeah, definitely when we get these
18 kind of homes, and nice neighborhood, this location is
19 very preferred location in Willowbrook, very good
20 schools.

21 When they see people in these kind of
22 homes -- because that's true, right now we have COVID,
23 but in the future it will be gone, with the vaccines and
24 all that, but, still, adding this pool to this house and

1 to this neighborhood is going to help and increase the
2 value of old properties, not just for this house.

3 MS. KACZMAREK: Ann, in the neighborhood, do
4 you know offhand if we have other properties that have
5 indoor pools?

6 MS. CHOI: I'm not that familiar with the
7 Willowbrook neighborhood and property. I don't know if
8 Roy Giuntoli knows more.

9 MR. GIUNTOLI: This is Roy Giuntoli,
10 building official.

11 None that come right off the top of my head
12 on the inside in this neighborhood. I know of one in a
13 different neighborhood, but not within Waterford. It's
14 nearby, it's on the other side of Madison Street, but
15 it's not in Waterford.

16 And then I know another one in town that
17 really isn't a swimming pool, it's more of a -- one of
18 those type of pools that you swim in place in. There's
19 a name for it, and I'm sorry I'm drawing a blank for it.

20 MS. KACZMAREK: I think I know what you're
21 saying, yeah.

22 MR. GIUNTOLI: And that's in another
23 neighborhood, a little bit further to the east, but none
24 come to mind -- over the last 15 years -- of being

1 permitted in this subdivision. Of course, the map shows
2 some outdoor pools, and there's been a couple here and
3 there, new ones over the years in Waterford, but as far
4 as inside, I can't recall anything off the top of my
5 head.

6 MS. KACZMAREK: Okay. Thank you.

7 MR. GIUNTOLI: You're welcome. Thank you.

8 CHAIRMAN KOPP: If the commissioners are
9 done, do any members of the public have any questions or
10 comments?

11 All right, go ahead, Mr. Webb.

12 (Whereupon, Kevin Webb is duly sworn.)

13 MR. WEBB: So I'm one of the authors of the
14 one letter that you received an objection, and that is
15 the rather -- kind of longer one there, that is dated
16 January 12th.

17 If you can see attached to it is a signature
18 page. I collected all those signatures yesterday
19 afternoon. So within an approximate three-hour period,
20 I was able to get -- what do we got there? I think it
21 was 15 different households that all surround this
22 particular property that are objecting.

23 So in regards to some of the things that the
24 applicant is saying, that this will build value in the

1 neighborhood, well, you've got, just from yesterday,
2 16 -- well, 16 property owners, the other one that Ann
3 pointed out -- that would object to that. I don't see
4 where this would be adding value to my property by
5 having a house that's got an indoor pool, which that is
6 a very extremely unique type of thing, as far as I would
7 believe, that people would want when buying a house.
8 Because if you've ever walked into an indoor pool, and
9 you walk in that front door of that natatorium, what's
10 it smell like? Chlorine. So unless they're okay with
11 having their entire house smell like chlorine, an indoor
12 pool is something that I would never ever desire to
13 have.

14 As far as all the work that they're doing to
15 the exterior, that, again, is just of their opinion,
16 that it's adding value to the neighborhood. If you,
17 Commissioners, ever have a chance, please drive by and
18 look at what they're doing to the house. Again, that
19 is -- as far as I believe, and so do all the other ones
20 that I've spoke with, this is a personal preference of
21 the design to the exterior -- to the house.

22 Now, we can't object to how they want to do
23 the exterior of their house; that's their choice, but
24 from my own personal standpoint, I don't think that that

1 exterior of the house is adding any value between the
2 roof and the pink walls.

3 As far as our objection to the FAR -- so
4 when that was put in place, that was done for a reason,
5 and that was to leave open space within this community.

6 They're talking -- the applicant's talking
7 about a transition of our community. I'm not too
8 sure -- I don't know of any transition. People can put
9 on new sidings on their home, and that's not a
10 transition. New roofs need to be put on if these houses
11 were built in the 1970's. They've reached their maximum
12 life, and then some, and need to be replaced, but as far
13 as the community transitioning, again, that is a
14 personal opinion that is just exclusive to them, and not
15 any of the 15 individuals that I spoke with yesterday;
16 and if I needed to, I could probably, within a week, if
17 not quadruple the number of people that I have on this
18 list, get many, many more than that by just going around
19 the neighborhood and finding out people's opinion on
20 that.

21 So I feel that, when we start letting just
22 one individual build more on their land in this aspect,
23 then what's to keep -- how can you deny the next one and
24 then the next one? And before we know it, we look like

1 Clarendon Hills, or we look like Hinsdale, that are on
2 top of each other.

3 Me, personally, I like the open space. I
4 want that extra 30 feet -- even though it's the
5 neighbor's yard, I want to have a nice distance between
6 us just for that open feeling, okay?

7 I don't have any questions for them.

8 One other concern would just be as far as
9 the drainage. I want to say it was three years ago that
10 there was major work done on a storm drainage pipe in
11 the -- along the property line there. That did a little
12 bit of mediation to the flooding problem that all of us
13 are experiencing, but as stated by the Miecznikowskis,
14 they had to do additional work.

15 I know Fred Stein, who's on the call, he did
16 major work in his backyard to put in tiles, I believe it
17 was, but he can attest to that. My neighbor, who's
18 directly behind them has done a lot of work to help with
19 the flooding, but it still floods; and the corner, my
20 northwest corner, which is adjacent to this property, it
21 floods, and there's a storm drain right there.

22 So as far as mitigation of that, we have a
23 lot of concern when you're going to put up a nearly
24 1,100 square foot structure of concrete, then you're

1 going to pour an in-ground pool -- I don't know how many
2 yards of concrete that's going to be or how far down
3 it's gonna, but another thing that people need to
4 realize is, we're high on the water table here.

5 My sump pump, even when it's not raining, it
6 runs. When it runs, it runs constantly. Why? I have
7 two. I got two sump pumps to suck that water out of
8 here; and if you're going to displace a permeable area
9 and drop all that concrete, then where's that water
10 going to go? Into our yards and the potential to our
11 basements. So that's an objection I have in regards to
12 this type of work.

13 I hope you all take this into consideration,
14 and thank you for your time.

15 CHAIRMAN KOPP: Mr. Webb, is your address
16 6410 Tremont?

17 MR. WEBB: That's correct, sir.

18 CHAIRMAN KOPP: Okay. Anyone else? So we
19 have three callers. Since I can't see your hands --
20 Mr. Stein?

21 THE REPORTER: I'm sorry, I didn't
22 understand that.

23 CHAIRMAN KOPP: Mr. Stein's going to speak;
24 and, Robin, you can swear him in.

1 (Whereupon, Fred Stein is duly sworn.)

2 MR. STEIN: Thanks for the opportunity. As
3 mentioned, I am one of the neighboring homeowners. I'm
4 at 6350 Tremont Street. I've been here for 16 years. I
5 just want to give a little bit of color to some of the
6 water issues.

7 So my property touches that corner that's
8 been noted -- that northeast corner of the applicant's
9 property, the low point. I mean, I'll tell you that
10 during heavy rain storms; and sometimes not so heavy,
11 you can actually see the water flow coming from the
12 yards to mine, and I've seen it over the 16 years. It
13 got to be such a problem, frankly, that it almost looked
14 like an irrigation ditch running through the middle of
15 my yard, towards the well at the far corner from the
16 applicant's property.

17 Grass wouldn't grow, and water would stand
18 there; and I mean, for a persistently long time, more
19 than a week, more than ten days sometimes. Ducks would
20 land in it. Even before I fixed it a year ago, ducks
21 would land in there, and squawk around. That's how bad
22 it was.

23 And this last year I finally resolved to do
24 something about it. I got a quote. They wanted to

1 charge me \$910,000 to put down a hundred feet of drain
2 tile, to do some other things. I personally didn't want
3 to spend that money; and seeing that I was home every
4 day, working there with COVID, and wanted to get some
5 exercise, I resolved to do it myself.

6 Neighbors being good neighbors around me --
7 frankly, everyone on this call that signed that letter
8 stopped by to see what I was doing, and neighbors on
9 each side helped me dig the trench for the hundred feet.
10 It was a true community effort. Matter of fact, all the
11 neighbors from the adjacent lots came over and spent
12 time as I was digging; I'll say everyone except for the
13 neighbor at 6401, the applicant's property.

14 I never met them. I'm not sure they're
15 truly aware of the issue being caused by the runoff, but
16 there's been substantial time and effort put in to
17 resolving the problem. I believe it is resolved. I've
18 had no further problems after that. We did a lot of
19 work. I put a rain guard in as well, doing everything I
20 could to help soak up that water. I'd hate to see all
21 that hard effort by all the neighbors go to waste; and,
22 so, I really do think it's incumbent upon the applicant,
23 prior to any building permit, or anything else moving
24 forward, to take a really hard look at this.

1 This has been a long time issue here; and
2 what they're asking for, indoor pool -- again, I've been
3 in this neighborhood 16 years, and been in these close
4 western suburbs for 55 of my 56 years. There isn't any
5 indoor pools around here, and it is somewhat of an
6 extraordinary request. I was very surprised when I saw
7 that someone was willing to sink that money -- and I
8 mean sink, sink that money into that kind of a
9 structure.

10 Thanks for your time and consideration.

11 CHAIRMAN KOPP: Either Mr. Pennino or any of
12 the Zemans -- I can't tell if you wanted to speak to
13 this matter. You don't have to, but if you want to.

14 MR. ZEMAN: This is Jerry Zeman.

15 (Whereupon, Jerry Zeman is duly
16 sworn.)

17 MR. ZEMAN: First off, I would just like to
18 state that I did sign Mr. Webb's petition, and I concur
19 with both -- my wife and I both concur with both
20 Mr. Webb -- I'm sorry, I didn't get the last name,
21 Mr. Stein's notation of the problems that they're
22 having, and I would like to comment relative to the
23 water.

24 I live immediately across the street from

1 the property at question, at 6404 Meadow Lane, and at my
2 property, my sump pump, in times of heavy rain, also
3 runs almost constantly, to the point where, when I did
4 have a sump pump failure, I did get water in my
5 basement's addition, and we've had to put in a
6 water-energized sump pump to deal with that issue should
7 our sump pump fail. So we have an extremely high water
8 table.

9 In addition, we do get some seepage during
10 heavy rains already, which is why we had -- when we put
11 our addition on, and increased the size of our basement
12 at the same time, we had put in additional weeping tile
13 per code to deal with all of this additional water; and
14 I actually have to have a piece of null board at the
15 bottom of my basement going down to the weeping tile, so
16 if I do get any seepage coming down the basement walls,
17 it goes right into the weeping tile and goes into the
18 sump.

19 We have been a resident of this neighborhood
20 now for 32 years. We did an addition on our house in
21 2009, which was a code-compliant addition, to the point
22 where we went so far as to ensure that we matched the
23 brick on the side of our house, on the 300-square foot
24 addition that we did. We used the same color of roofing

1 tile so that the property looked like it was the
2 original build of the structure. So it's already been
3 expressed on this property, that people around here do
4 not recognize the improvements that have been done to
5 the property as being, let's say, consistent with the
6 look of the surrounding homes, and we don't understand
7 the assertion that this is going to improve the value of
8 our properties.

9 Thank you very much for the opportunity to
10 speak, and look forward to your favorable consideration
11 from the petitions that you've received.

12 CHAIRMAN KOPP: All right. Anyone else?

13 Okay. Do the commissioners have any final
14 questions of Ann or the applicant?

15 MR. PENNINO: Hello?

16 THE REPORTER: Mr. Martin?

17 CHAIRMAN KOPP: I'm sorry, did you want to
18 speak Mr. Pennino?

19 MR. PENNINO: Yes. Just to confirm --

20 THE REPORTER: Before you continue, I'll
21 swear you in. Can you raise your right hand?

22 (Whereupon, Martin Pennino is duly
23 sworn.)

24 MR. PENNINO: I live a little bit further

1 away from the house -- not too far, on Hiddenbrook, that
2 backs up to Creekside Park, and I just want to confer,
3 that, yes, whenever it rains heavily, ours -- we have
4 two sump pumps and they're constantly going on and off
5 all night long, all day long. There definitely is water
6 issues in this area. Even though we're high above the
7 Creekside Park, there's still a lot of water issues.
8 I've been a resident for 18 years, and pretty much -- we
9 had to get two new sump pumps recently because they just
10 ran out from being used so much.

11 One other question I had is, the size of the
12 pool sounds pretty large. If this is really a
13 therapeutic pool, why wouldn't a smaller therapeutic
14 pool be used, one that would just be a little bit bigger
15 than a human body or something like that? I don't know
16 why they need so much space for a pool. It sounds more
17 like a multiple use than just therapy.

18 I think that's all I have right now. That's
19 it. Thank you.

20 CHAIRMAN KOPP: Okay. Was one of the
21 commissioners starting to ask a question or say
22 something?

23 All right. If not, the applicant, you have
24 the opportunity to have the last word before we close

1 this public hearing.

2 MR. P. BAFTIRI: Thank you again.

3 So all these comments -- the neighbors --
4 they make, they're very valid, especially for this
5 drainage; and I'm doing something every day, in running
6 into these issues, but I want, again, to confirm that,
7 whatever we do is Village requirement. Design -- we
8 meet all the requirements, we can comply with all
9 requirements, because we know the water, it's a problem.

10 There are multiple options now to address
11 that. We can do dry well, or we could do other testing
12 to see what's the absorption in the area. So I know
13 we're going to meet this requirement, and not affect the
14 neighbors.

15 And with regard to the FAR -- I'm looking at
16 the aerial pictures that is in Exhibit 2 -- all three
17 neighbors, north, south and west, they have bigger homes
18 than this neighbor, higher FAR. So what they're asking
19 to meet comes to the same size house. All they have a
20 protrusion, except like my client is asking. If you
21 look at Exhibit 2, I think it's going to be -- after
22 this pool house, it's going to look almost similar to
23 the existing homes and all three neighbors next to this
24 property.

1 That's all I have to add. Thank you.

2 CHAIRMAN KOPP: All right. If that's it, we
3 will close the public hearing for Zoning Hearing Case
4 21-01 [sic], and then the commissioners will have their
5 discussion, and then we will vote whether to recommend
6 this matter to the Village trustees.

7 For myself, when I first read this
8 application and looked at the materials, I assumed that
9 this was -- that I was going to approve this just based
10 on what was presented. However, I look at a zoning
11 ordinance as if it's almost a contract between people,
12 so that when people buy a property, they can rely on the
13 fact that the zoning ordinance is going to somewhat
14 protect their neighborhood. As Mr. Webb said, you can't
15 stop someone from painting their house purple or doing
16 something like that, but you have some expectations
17 based on the zoning ordinance; and the fact that the
18 neighbors -- all of the immediate neighbors are opposed
19 to granting a variance here means that, for me, I am not
20 going to vote in favor of this; and for me, it's purely
21 because of the zoning ordinance. It's not the
22 drainage -- not that the drainage is not an important
23 issue. I deal with it. On the Plan Commission here, we
24 constantly hear about drainage, because that's just

1 endemic to Willowbrook, but we have to trust that
2 engineers can solve that issue, but I am not in favor of
3 the increased FAR or the rear yard reduction variation
4 just because the neighbors are not.

5 Now, the neighbors have to understand, these
6 folks could scale down their pool, and perhaps redesign
7 it so it fits within -- if this isn't approved by the
8 way, we're going to have a vote, and then it's going to
9 go to the trustees, but the neighbors obviously have to
10 understand, that they could scale this down and fit
11 within the zoning ordinance, and then there would be
12 nothing to say, other than building code issues, which
13 the drainage is.

14 MS. CHOI: Actually, can I interject?

15 So if they were to even revise and put in a
16 smaller footprint -- their current house is already
17 maxed out at FAR, so they would still have to probably
18 come back and request a variation to FAR.

19 CHAIRMAN KOPP: Okay. Then, in that case --
20 so I strike that comment, that -- again, I'm not in
21 favor then of the increased FAR or the zoning variance
22 because the neighbors aren't, and just then -- in the
23 same case, if one of the neighbors shows up, we're
24 not -- I'm not going to be in favor of that one.

1 I have no idea how the other commissioners
2 feel. Would any commissioners like to state their
3 opinions before we take the vote?

4 MR. REMKUS: I'd like to say one thing, that
5 I agree with you, that it's an awfully large addition;
6 and the drainage is a concern to me, because being --
7 serving on this Plan Commission for so long, it's an
8 issue that constantly and constantly comes up, and it's
9 getting worse and worse, and we have to be a lot more
10 conscious about things like that; and going through all
11 the papers and everything, I didn't see anything -- one
12 of the neighbors mentioned that they built a rain guard.
13 Well, that's one of the approved things that has come
14 about the last few years for -- to slow down the
15 transition of water so it's retained on property, and
16 even something -- a small addition like this, I would
17 look on it more favorable if they were doing something
18 with the water they're displacing, but there's no --
19 nothing that forces them to do that, but it's something
20 that you look on more favorably when you're showing that
21 you're going to do something with the access water that
22 you're going to displace; and the FAR, that's what
23 they're doing, they're displacing more water. So I'm
24 just not in favor of it.

1 CHAIRMAN KOPP: All right. Our sample
2 motion assumes that we are going to approve this
3 application, or recommend this application, and I
4 suspect we are not.

5 So will someone make a motion that the Plan
6 Commission not recommend the petition for -- presented
7 at Zoning Hearing Case PC21 -- oh, I identified it as
8 21-01 before, but this one was 21-02.

9 Would someone make the motion that we not
10 recommend this?

11 MR. REMKUS: So moved.

12 THE REPORTER: Wagner, I didn't hear you.

13 MR. WAGNER: Wagner so moved, or second.

14 MR. WALEC: I second.

15 CHAIRMAN KOPP: I ask the Plan Commission
16 Secretary to call the vote.

17 MS. SHEMROSKE: Commissioner Remkus?

18 MR. REMKUS: Are we asking to disapprove,
19 right? So I would say "yes" to disapprove?

20 CHAIRMAN KOPP: Yeah, sorry. "Yes" to
21 disapprove.

22 MS. SHEMROSKE: Commissioner Soukup?

23 MR. SOUKUP: Not approve.

24 MS. SHEMROSKE: Commissioner Kaczmarek?

1 MS. KACZMAREK: Yes.

2 MS. SHEMROSKE: Commissioner Kaucky?

3 MS. KAUCKY: Yes.

4 MS. SHEMROSKE: Commissioner Walec?

5 MR. WALEC: Disapprove.

6 MS. SHEMROSKE: Vice Chairman Wagner?

7 MR. WAGNER: Yes.

8 MS. SHEMROSKE: And Chairman Kopp?

9 CHAIRMAN KOPP: Yes.

10 All right. For the petitioners, again, this
11 is a recommendation, and it's not binding. Only the
12 trustees can make the binding determination. Although,
13 I will warn you, they usually follow our
14 recommendations.

15 MR. BAFTIRI: Thank you very much.

16 CHAIRMAN KOPP: So that's the end of that
17 matter.

18 (WHICH WERE ALL THE PROCEEDINGS HAD.)

19

20

21

22

23

24

1 STATE OF ILLINOIS)
) SS:
2 COUNTY OF DUPAGE)

3
4 I, ROBIN HEJNAR, a certified shorthand reporter
5 and registered professional reporter do hereby certify:

6 That prior to being examined, the witness in
7 the foregoing proceeding was by me duly sworn to testify
8 to the truth, the whole truth, and nothing but the
9 truth;

10 That said proceedings were taken remotely
11 before me at the time and places therein set forth and
12 were taken down by me in shorthand and thereafter
13 transcribed into typewriting under my direction and
14 supervision;

15 I further certify that I am neither counsel
16 for, nor related to, any party to said proceedings, not
17 in anywise interested in the outcome thereof.

18 In witness whereof, I have hereunto subscribed
19 my name.

20 Dated: January 25, 2021

21
22
23 _____
24 ROBIN HEJNAR, RPR
 CSR No. 084-004689





Village of Willowbrook
Staff Report to the Plan Commission

Plan Commission Date: February 3, 2021

Prepared By: Ann Choi, Village Planning Consultant

Case Title: Zoning Hearing Case No. 21-01: 7505 Kingery Highway Panda Express SUP

Applicant: Hakim Yala
Panda Express, Inc.
1683 Walnut Grove Avenue
Rosemead, CA 91770

Property Owner: True North Energy, LLC
10346 Brecksville Road
Brecksville, OH 44141

Action Requested: Consideration of a petition requesting approval of a special use permit for a fast food establishment and a special use permit for a drive-through in the B-2 Community Shopping District, including certain variations from Title 9 of the Village Code. The Applicant seeks to demolish the existing gas/service station and construct a one-story building and drive-through with associated site improvements.

Applicable Regulations: Zoning Ordinance

Location: 7505 Kingery Highway, Willowbrook IL 60527

PINs: 09-26-400-013

Existing Zoning: B-2 Community Shopping with a Special Use for a Gasoline Service Station

Proposed Zoning: B-2 Community Shopping with a Special Use for a Fast Food Establishment and Drive-through

Existing Land Use: Highway Commercial

Property Size: 0.28 acres

Surrounding Land Use:	Use	Zoning
North	Willowbrook/Wingren Plaza	B-2/Special Use
South	Red Roof PLUS	B-4
East	Light Manufacturing	M-1
West	Potbelly's Restaurant	B-2/Special Use

Necessary Action by Plan Commission Open Public Hearing, accept testimony, and approve a recommendation to the Village Board. A sample motion can be found on pages 13 and 14.



Documents Attached:

Attachment 1: Legal Description

Attachment 2: Submitted Plans

- a) **"ALTA-NSPS Land Title Survey"** – 2 sheets (Job. No. 18003769), dated 8/10/2020, bearing the latest revision date of 9/23/2020, resubmitted on January 19, 2021 and prepared by NORR LLC.
- b) **"Site Layout Plan"** – 1 sheet, Sheet C04.0, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, bearing the latest revision date of 01/19/2021 and prepared by NORR Architects Engineers Planners.
- c) **"Architectural Drawings"** – 5 sheets, Sheets A-101, A-200, A-201, A-202, A-300, Panda Project #: D7058, Arch. Project #: JCDT180453, bearing an issue date of 11-04-2020, and prepared by NORR Architects Engineers Planners.
- d) **"Proposed Sign Package"** – 17 pages, Project #: C59897, dated 12.19.2018, bearing the latest revision date of 01.19.2020, and prepared by Priority Sign.
- e) **"Final Engineering Plans"** – 18 sheets, Panda Project #: D7058, Atwell Project #: 18003769, bearing an issue date of 11/20/2020, bearing the latest revision date of 01/19/2021 and prepared by NORR Architects Engineers Planners.
(Includes Landscape Plan)
- f) **"Engineer's Opinion of Probable Construction Cost"** – 3 pages, dated 11/20/2020, and prepared by Atwell Group.
- g) **"Site Traffic Study"** – 11 pages, dated January 5, 2020 and prepared by Atwell Group.
- h) **"Traffic Turn Analysis"** – 1 sheet, Sheet EX-01, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, and prepared by NORR Architects Engineers Planners.
- i) **"Traffic Plan"** – 1 sheet, Sheet EX-02, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, and prepared by NORR Architects Engineers Planners.
- j) **"Photometric Plan"** – 1 sheet, Sheet L01.0, dated 10/29/2020, and prepared by Villa Lighting.
- k) **"Construction Schedule"** – 1 page, dated 11/17/2020, and prepared by Atwell Group.

Attachment 3: Standards for Special Use Permits

Attachment 4: Standards for Variations

Attachment 5: Traffic Regulation Agreement – 11 pages, resubmitted by Atwell Group on 01/19/2021.

Attachment 6: Public Hearing Notice

Attachment 7: Village of Willowbrook Review Letter(s)



Background

Description of Site

The subject property is located at the southeast corner of Illinois (IL) Route 83/Kingery Highway and 75th Street, just south of Willowbrook/Wingren Plaza shopping center and east of the Potbelly's restaurant. The subject property is currently located in the B-2 Community Shopping Zoning District, is adjacent to the north, northwest and west by properties also located in the B-2 Community Shopping Zoning District and is immediately adjacent to property located in the B-4 Highway and Service Business District to the south and the M-1 Light Manufacturing District to the east. The one-acre property is regular in shape and is currently served by three existing driveways.

Exhibit 1: Map View of the Subject Property



Surrounding Uses

Surrounding zoning and uses include a shopping center (Willowbrook Plaza Shopping Center) including a Club Champion, Orange Theory Fitness, Fedex Print and Ship Center, Kabob-Q restaurant, a couple of nail salons, and associated parking lots in the B-2 zoning district to the north, a hotel (Red Roof Inn) and associated parking lot in the B-4 zoning district to the south, a fast-casual restaurant (Potbelly's) in the B-2 zoning district to the west, and manufacturing/sales uses in the M-1 zoning district to the east. The Village of Willowbrook's Comprehensive Plan designates the site for "Highway Commercial" uses, which corresponds to the B-2 Community Shopping Zoning District.



Exhibit 2: Zoning Map (B-2 Community Shopping)



Existing Streets and Circulation

Illinois Route 83/Kingery Highway is designated as a Regional Arterial and forms the western boundary of the site with a required right-of-way width of 200 feet. A right-in/right-out access drive on IL Route 83 is located approximately 100 feet south of 75th Street. The access drive provides one inbound lane and one outbound lane restricted to right-turn movements by the median on IL Route 83.

75th Street is designated as a Major Arterial west of IL Route 83 and is designated as an Industrial Collector east of IL Route 83. 75th Street forms the northern boundary of the subject property with a required right-of-way width of 60 feet. There are two access drives on 75th Street, with the westernmost access drive allowing right-in only into the subject property. The current allows full movement access on 75th Street.

History

Prior Zoning Request

The previous ordinances regulating this facility are as follows:

- Ordinance No. 77-O-14 – Original special use ordinance establishing the use, but without specific bulk standards.
- Ordinance No. 94-O-16 – Amended original special use to permit the construction of a second driveway on 75th Street and various site and landscaping improvements and granted the following variations:
 1. That Section 9-3-7(A)1 of the Village Code of the Village of Willowbrook establishing a minimum required setback on Illinois Route 83 be varied by reducing same from one hundred feet (100') to five feet, four inches (5'4") for the existing overhead canopy, to fifteen feet (15') for the existing



gasoline pump dispenser islands, and to fifty-eight feet (58') for the existing service station building.

2. That Section 9-6-1(A) of the Village Code of the Village of Willowbrook establishing the minimum lot area for two or more uses on a lot be varied by reducing same from two (2) acres to one (1) acre.
3. That Section 9-6B-3(A) of the Village Code of the Village of Willowbrook establishing minimum lot area be varied by reducing same from two (2) acres to one (1) acre.
4. That Section 9-6B-3(E)1 of the Village Code of the Village of Willowbrook establishing front yard setbacks be varied by reducing same for the existing canopy from sixty feet (60') to five feet, four inches (5'4"), for the existing gasoline pump dispenser islands from sixty feet (60') to fifteen feet (15'), and for the existing service station building from sixty feet (60') to fifty-eight feet (58').
5. That Section 9-6B-3(E)3 of the Village Code of the Village of Willowbrook establishing exterior side yard setbacks be varied by reducing same from sixty feet (60') to forty-three feet (43').
6. That Section 9-10-5(G) of the Village Code of the Village of Willowbrook establishing parking area setbacks within required yards, be varied by reducing same from fifteen feet (15') to zero (0) setback for the front yard impervious surface setback, and from fifteen feet (15') to ten feet (10') for the exterior side yard setback.
7. That Section 9-10-5(I)2(b) of the Village Code of the Village of Willowbrook establishing parking area pavement improvements be varied to permit a substitution from the required all-weather hard surface pavement improvement for all parking areas within a non-residential district to a gravel surface for the screened vehicle storage area.
8. That Section 9-10-5(I)2(c) of the Village Code of the Village of Willowbrook establishing required parking area improvements be varied to permit a substitution for the required concrete barrier curb and gutter for all parking areas within a non-residential district to permit individual wheel stops for the southern parking area.
9. That Section 9-10-5(L)2(b)2(a) of the Village Code of the Village of Willowbrook establishing required minimum access driveway curb radius be varied by reducing same from thirty feet (30') to fifteen feet (15').
10. That Section 9-10-5(L)2(b)2(b) of the Village Code of the Village of Willowbrook establishing required minimum access driveway inside curb radius be varied by reducing same from twenty-five feet (25') to ten feet (10').
11. That Section 9-10-5(I)2(b) of the Village Code of the Village of Willowbrook establishing parking area pavement improvements be varied to permit a substitution from the required all-weather hard surface pavement improvement for all parking areas within a non-residential district to a gravel surface for the screened vehicle storage area.
12. That Section 9-10-5(I)2(c) of the Village Code of the Village of Willowbrook establishing required parking area improvements be varied to permit a substitution for the required concrete barrier curb and gutter for all parking areas within a non-residential district to permit individual wheel stops for the southern parking area.
13. That Section 9-10-5(L)2(b)2(a) of the Village Code of the Village of Willowbrook establishing required minimum access driveway curb radius be varied by reducing same from thirty feet (30') to fifteen feet (15').
14. That Section 9-10-5(L)2(b)2(b) of the Village Code of the Village of Willowbrook establishing required minimum access driveway inside curb radius be varied by reducing same from twenty-five feet (25') to ten feet (10').



Overview

Development Proposal

The subject property is currently occupied by a single building that houses a Shell gasoline and service station and a convenience mart, including a detached canopy covering four existing gasoline pump dispenser islands. The subject property likely contains contaminants due to the existing use. The current property owner, True North Energy, LLC, will be responsible for securing a No Further Remediation (NFR) letter from the Illinois Environmental Protection Agency (IEPA).

Panda Express, Inc. (the "Applicant"), proposes to demolish the existing 2,131 square foot Shell gasoline and service station with convenience mart and is requesting approval of a special use for the construction of a 2,300 square foot Panda Express fast food restaurant with a drive-through and associated site improvements. A special use permit is requested to allow for both the fast food establishment and a proposed drive-through within the Village of Willowbrook's B-2 Community Shopping Zoning District. The fast food establishment will include over fifty (50) interior seats and no permanent outdoor dining is proposed. The proposed single-story building is situated towards the northwest portion of the subject property and will require several variations because the proposed building will not meet required setbacks, minimum required lot area for the proposed uses, and certain landscape requirements.

Requested Action

Approval of a special use permit within the B-2 Community Shopping District zoning classification for a fast food establishment and a special use permit for a drive-through in the B-2 Community Shopping District, including the following variations from Title 9 of the Village Code. The Applicant seeks to demolish the existing gas/service station and construct a one-story building and drive-through with associated site improvements.

Requested Variations

The proposed improvements for the use include the variations listed below.

1. A variation from Section 9-3-3(C) to decrease the minimum lot area for two (2) or more special uses from two acres (2 AC) to one acre (1 AC).
2. A variation from Section 9-3-7(A)1 to decrease the minimum special setback on Illinois Route 83 from one hundred feet (100') to fifty feet (50') for the proposed building, to fifty-six and six tenths feet (56.6') for the proposed menu board, and to sixty-two and four tenths feet (62.4') to the order canopy.
3. A variation from Section 9-6-1(A) to decrease the minimum lot area for two (2) or more special uses (fast food establishment and drive-through) from two acres (2 AC) to one acre (1 AC).
4. A variation from Section 9-6B-3(A) to decrease the minimum lot area for all uses from two acres (2 AC) to one acre (1 AC).
5. A variation from Section 9-6B-3(C)1 to decrease the minimum lot width of 200 feet for restaurants from two hundred feet (200') to one hundred seventy feet (170').
6. A variation from Section 9-6B-3(E)1 to decrease the minimum front yard setback on Illinois Route 83 from sixty feet (60') to fifty feet (50') for the proposed building and to fifty-six and six tenths feet (56.6') for the proposed menu board.
7. A variation from Section 9-6B-3(E)3 to decrease the minimum exterior side yard setback on 75th Street from sixty feet (60') to forty-three and four tenths feet (43.4') for the proposed building, to thirty-two and seven tenths feet (32.7') for the proposed menu board, and to twenty-five feet (25') to the order canopy.
8. A variation from Section 9-10-5(L)2(b)2(a) to decrease the radius connecting street pavement edge and driveway edge on 75th street from thirty-five feet (35') to twenty-five feet (25').



9. A variation from Section 9-10-5(L)2(d) to decrease the driveway location on Route 83 from seventy feet (70') to twenty-five and seven tenths feet (25.7').
10. A variation from Section 9-10-5(L)2(e) to decrease the minimum spacing between separate driveway entrances from four hundred feet (400') to approximately three hundred thirteen feet (+/- 313') on Illinois Route 83 and to approximately one hundred sixty-nine feet (+/- 169') on 75th Street.
11. A variation from Section 9-10-5(L)2(f) to decrease the minimum spacing between driveway entrance and right-of-way of an adjacent intersecting street from five hundred feet (500') to ninety-four and three tenths feet (94.3') on Illinois Route 83 and to one hundred forty-six and two tenths feet (146.2') on 75th Street.
12. A variation from Section 9-10-5(L)2(g) to eliminate the requirement of a concrete median separation and to reduce the driveway storage from one hundred feet (100') to zero feet on Illinois Route 83 and 75th Street.
13. A variation from Section 9-14-2(D)2(c)4 to decrease the minimum foundation landscape area from seven feet (7') to four and five tenths feet (4.5') along the southern façade, to five and two tenths feet (5.2') along the northern portion of the eastern building façade and from seven feet (7') to zero feet along the western building façade and remaining portion of the eastern building façade.

While it appears that there are a number of variations, staff met with the developer early in the process to discuss all of them, and it should be noted that five (5) of the thirteen (13) of the requested variations are due to existing conditions such as lot width and existing driveway locations. A more detailed description of the variations is outlined below.

Staff Analysis

Access, Circulation and Parking/Loading

A Site Traffic Study and a Truck Turn Analysis were submitted, and planning staff supplied a review letter dated January 13, 2021 which includes the Village traffic consultant's comments as part of **Attachment 7** for the Plan Commission's review. The subject property is currently accessed from both IL Route 83/Kingery Highway and 75th Street. The Panda Express site layout proposes constructing new access driveways in the approximate location of the existing access driveways, with the exception of the westernmost right-in only site access drive on 75th Street which will be closed off and curbed. The access on 75th Street will be a full movement for inbound traffic and limited to a right-out only. The access on IL Route 83/Kingery Highway will remain to be a right-in/right-out access.

The single-lane drive-through has a clockwise rotation around the proposed building and offers a second bypass lane after patrons submit their orders at the menu board. The proposed fast food establishment can be accessed via the driveway on IL Route 83/Kingery Highway for northbound travelers. Vehicles will enter from this driveway and proceed along the south edge of the site, past a row of surface parking spaces. Vehicles will make two left turns after driving past the proposed building to enter the drive-through. The approach from the 75th Street driveway also has a clockwise rotation where vehicles will make an immediate right to proceed directly into the drive-through lane.

According to the Traffic Plan, the proposed site layout is designed to provide a total storage length of seven (7) vehicles for the drive-through window queue, with placement of the menu/order board at the fifth car. Furthermore, two (2) drive-through parking pick up spaces are proposed to help reduce drive-through queueing as needed during peak business hours.

Data was collected from three (3) similar nearby Panda Express drive-through restaurants and includes seven (7) consecutive days of drive-through transaction counts, divided into 30-minute time spans. The traffic study



Indicates there is an average service time of five (5) minutes starting with the food order at the menu board and ending with food pickup at the drive-through window.

Parking for restaurants is based on gross square footage. A 2,300 square foot restaurant requires twenty-three (23) parking spaces, or 1 space for every 100 square feet of gross floor area. The facility is providing forty-six (46) parking spaces which exceeds the requirement and will be useful in servicing the additional seasonal outdoor seating as well, which is currently not proposed and not technically included in the parking requirement. The proposed fast food establishment is below the threshold of 5,001 square feet to trigger the requirement of providing a loading berth.

Appropriateness of Use

The property is zoned B-2 Community Shopping District, which lists fast food establishments as a special use and allows a drive-through for permitted uses by special use approval. The introduction of this use on this property will demolish an aging building and the proposed fast food establishment will benefit from a location that is easily accessible by motorists as well as students and visitors to Hinsdale South High School, which is nearby.

Building Elevations

The Applicant has submitted building elevations, including the south elevation as shown in **Exhibit 3** below. **Refer to Attachment 2** for the remaining elevations. The proposed fast food establishment provides variations in the choice of building materials, and introduces a sufficient number of breaks in the façade with material changes and variations of the roof line. Proposed building materials include a combination of brick, stone, EIFS and metal. Glass windows are provided on each elevation to allow natural light to filter into the building and provide street views for its patrons. Furthermore, the proposed building elevations will conform to the General Conditions of the Business Districts under Section 9-6-1(E) Building Façade Materials. Since the submitted building elevations were only drawn in black and white, colored perspective renderings are included in the architectural elevations and included as **Exhibit 4** and **Attachment 2** of the staff report.

Exhibit 3 South Elevation

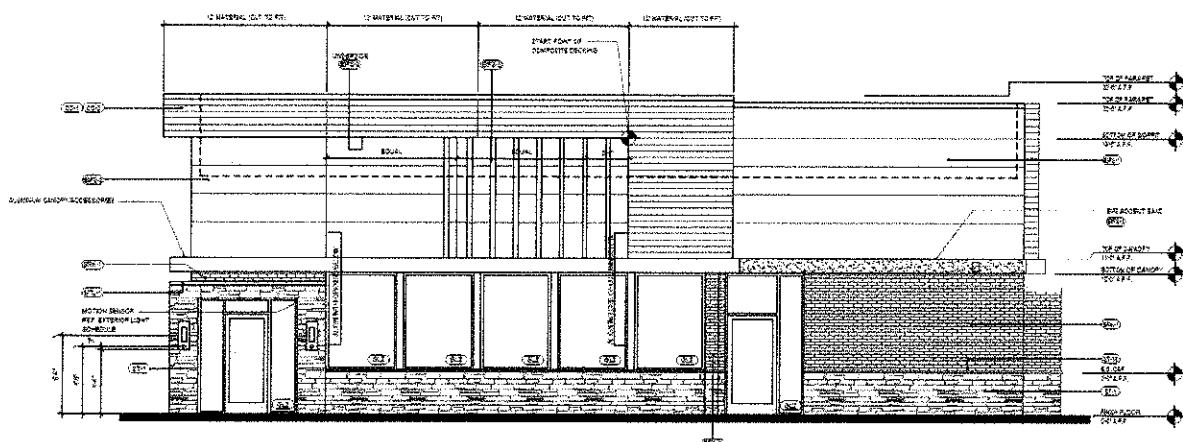




Exhibit 4 Perspective Rendering (East and South Elevations Facing Parking)



Zoning Bulk Requirements

Setbacks: The Applicant is requesting three variations from the setback requirements of the Zoning Ordinance. The property is zoned B-2 (with a special use). A detailed discussion of specific setbacks and important bulk exceptions and variations for the proposed project is provided in **Exhibit 4** and **Exhibit 5** shown below and on the following page. As previously discussed in the History section of this report, the subject property is further governed by Ordinance No. 94-O-16, which provided significant zoning relief from the specific setbacks and B-2 bulk regulations. Staff also considered the reduced setbacks of adjoining properties and the Village's history of allowing reduced setbacks in similar instances.

Exhibit 4: Specific Setbacks Compliance Table

Specific Setbacks				
Item	Code Section or Ordinance	Code Requirement	Proposed	Departure
Route 83 Setback	9-3-7(A)1 94-O-16 Sect. 3	100'	50' (building) 56.6' (menu board) 62.4' (order canopy) 5'-4" for existing overhead canopy 15' for existing gasoline pump dispenser islands 58' existing service station building	Yes

It should be noted that relief is requested from the one hundred foot (100') specific setback and the sixty foot (60') front yard setback on Illinois Route 83 for the proposed building, menu board, and order canopy which encroaches into the front and exterior side yards. Staff recommends approval of these variations since similar variations were granted through Ordinance No. 94-O-16 for the existing service station building.



Exhibit 5: B-2 Zoning District Compliance Table

B-2 Zoning District Bulk Regulations				
Min. Lot Area	9-6B-3(A)	2 acres	1.0 acre	Yes
Min. District Area	9-6B-3(B)	2 or more acres	N/A	
Min. Lot Width	9-6B-3(C)1	200'	170' See 9-3-12	Yes (Existing Condition)
Min. Lot Depth	9-6B-3(D)	200'	256.24'	None
Min. Front Yard Setback (Kingery Hwy)	9-6B-3(E)1	60' 5'-4" (overhead canopy) 15' (bill board) 55' (street lighting)	50' (building) 56.6' (menu board)	Yes
Min. Interior Side Yard Setback (south lot line)	9-6B-3(E)2	30'	63.6'	None
Min. Exterior Side Yard Setback (75 th St)	9-6B-3(E)3	60'	43.4' (building) 32.7' (menu board) 25' (order canopy)	Yes
Min. Rear Yard Setback (east lot line)	9-6B-3(E)4	40'	162.8'	None
Min. Transition Yard	9-6B-3(E)5		N/A	
Max. Lot Coverage	9-6B-3(F)	50%	6% (2,607 SF)	None
Max. Height	9-6B-3(G)	30'	23'	None
Max. Floor Area Ratio	9-6B-3(H)	0.30	0.06 (2,607 SF/43,561 SF)	None

Lot Dimensions: The subject property has a minimum lot area of approximately 43,561 square feet, or 1.0 acre. The subject property has a lot width of one hundred seventy feet (170') and a lot depth of two hundred fifty-six feet (256'). A variation for reduced lot width requested and granted under Ordinance No. 94-0-16. However, Staff is including a variation request for reduced lot width as part of this petition.

Building Height: The maximum height of a building in the B-2 Zoning District is forty feet (30'). The proposed building height for the fast food establishment is twenty-three feet (23') and is therefore in compliance with the Zoning Ordinance.

Density and Coverage: The maximum lot coverage (building coverage) in the B-2 Zoning District is 50% per Section 9-6B-3(F). The proposed building has a building footprint of approximately 2,300 square feet. On a one-acre lot, the building coverage is approximately 6% and is below the maximum 50% threshold. As a one-story building, the floor area ratio (FAR) is approximately 0.06, which is below the minimum required 0.30 FAR. Both the requirements for lot coverage and FAR have been satisfied.

Parking & Loading

The proposed site layout will provide a total of forty-six (46) parking spaces, including two (2) accessible spaces, located along the east and south lot lines, east of the proposed building and within the interior of the subject property. There will also be two (2) dedicated drive-through pick-up spaces located along the southwest area of the site to help reduce drive-through queueing as needed during peak business hours. There are no loading berths proposed as the Zoning Ordinance does not require any for structures that have a gross floor area of 5,000 square feet or below.



Parking Area/Pavement Setbacks

Parking area/pavement setbacks are regulated in the Parking Section of the Zoning Ordinance. A twenty-five foot (25') parking area setback is required along the Illinois Route 83 frontage, a fifteen-foot (15') parking area setback is required along the 75th Street frontage, and a 10-foot (10') setback is required along the interior side and rear lot lines. The proposed site plan complies with the pavement setback requirements required by the Village Code.

Contamination Issues

The Applicant has a purchase agreement in place with the current property owner/seller (True North Energy, LLC) for the property that includes an addendum outlining the removal procedure for the contaminants. This agreement states a No Further Remediation (NFR) letter from the Illinois Environmental Protection Agency (IEPA) is the responsibility of the current property owner/seller; therefore, the purchase of the property is contingent on the current property owner/seller to obtain the NFR letter from the IEPA (that is acceptable to the Applicant for its intended use of the property). The Village is agreeable to this and has added a condition of approval that the NFR letter will be required at time of building permit if the seller and buyer have the NFR letter as a condition of the sale closing.

Stormwater Management

An abbreviated Stormwater Submittal was provided to the Village. The Village Engineer confirmed the findings of the study stating that the subject property does not contain floodplain or wetlands, and the development does not reach the net new impervious thresholds to trigger Best Management Practices or Detention. The Village Engineer has reviewed the engineering for stormwater compliance and identified no issues. A copy of his review letter is included as **Attachment 7** for your review.

Trash Enclosure

The Applicant is proposing a six-foot (6') height trash enclosure on the southeast portion of the site lot, to the east side of the building, where the open side of the enclosure is oriented so that, to the greatest extent possible, it does not face towards an abutting property or street. A black and white trash enclosure plan and details were provided by the Applicant. Per Section 9-12-11 of the Village Code, the trash enclosure must be constructed to look like masonry and shall appear to the color and style of the building. Staff recommends that this be included as a condition for the Plan Commission's future recommendation to the Village Board.

Site Lighting

The Village Engineer's December 21, 2020 memo states that site lighting is in general compliance with Village Code and standard engineering methods. Please note that the Village reserves the right to require glare shields to be installed, should it deem appropriate.

Landscaping

The modifications to the site are an improvement over the existing conditions. Additional foundation, interior parking lot, and perimeter landscape areas will improve circulation and safety, and are an engineering asset since it means that no additional stormwater improvements are required. A significant amount of new landscaping is being provided site wide. The proposed Landscape Plan is shown in **Attachment 2**. Staff anticipates that only minor changes will be required as the proposed development appears to provide sufficient landscaping, but a condition has been added to ensure that the landscape plan will address the comments in the planning review letter outlined in **Attachment 7**. There are some minor revisions that need to be included in the current landscape plan; however, staff is confident that appropriate landscaping can be achieved on the site. Therefore, staff is recommending a condition of approval that requires future approval of the landscape plan by staff prior to Village Board approval.



Signage

The new free-standing sign on a brick base is proposed in generally the same location as the existing free-standing sign, at the northeast corner of the subject property. The proposed free-standing sign complies with the current ordinance and current configuration of the property. The sign has dimensions of 12 feet x 6'-5 7/8" feet, or a total of 36 square feet per side (72 square feet total). It will be set back a minimum of five feet (5') from the north and west lot lines, contains brick in keeping with the design of the principal structure, and will be constructed to a maximum height of twelve feet (12'). Because the proposed free-standing sign exceeds a height of eight feet (8') above average surrounding grade, the total sign surface area allowed is reduced by thirty square feet (30 SF).

Exhibit 6: Proposed Free-Standing Sign



Based on the length of the exterior side lot line and the reductions taken because of the height of the free-standing sign, a total of two hundred ninety square feet (290 SF) of signage is allowed, which covers both wall and free-standing signs. The sum of all signs shown on the plans is two hundred thirty-three and nineteen hundredths square feet (233.19 SF). One wall sign is proposed on each of the north, east and west sides of the building as shown in the sign package. The south wall sign has 53.27 square feet of sign surface area, while the north, east and west wall signs have 36 square feet of sign surface area each. The Zoning Ordinance allows only one wall sign for each street frontage or "business site frontage". With its corner location, the tenant is assured a total of four (4) wall signs. Staff is interpreting all four elevations as having "business site frontage", which allows for four wall signs. A condition of approval is added that confirms no additional wall signage will be allowed as part of this approval.

Exhibit 7: Proposed Wall Signs



Findings of Fact



Standards for Special Use

Section 9-14-5(B) of the Willowbrook Zoning Ordinance establishes seven (7) standards for a Special Use Permit that must be evaluated by the Plan Commission and Village Board. Recommendations may include conditions of approval if appropriate to mitigate any negative impacts created by the special use permit. The Applicant's responses are provided in **Attachment 3**.

Standards for Variations

Section 9-14-4(E) of the Willowbrook Zoning Ordinance establishes seven (7) standards that must be evaluated by the Plan Commission and Village Board. Recommendations may include conditions of approval if appropriate to mitigate any negative impacts created by the variations. A list of the variation standards is provided in **Attachment 4**, along with the proposed findings to be incorporated in the Plan Commission's recommendation and the Village Board's ordinance if approved.

Staff Recommendation

Generally, staff has been supportive of the demolition of the existing building on this site for a new fast food establishment with a drive through. The building meets the user's needs, and the site improvements create an improved situation than exists today even though variations are required. The use will provide nearby dining opportunities for students and visitors to Hinsdale South High school. The special use and variations are supported. Planning staff would also recommend acceptance of the submitted written Findings of Fact in response to the Standards for Special Use Permits and Standards for Variations from the Zoning Ordinance sought, which are included as **Attachments 3 and 4** of this report. If the Plan Commission wishes to support the project, staff recommends approval of the following sample motion:

Sample Motion

Based on the submitted petition and testimony presented, I move that the Plan Commission approve and adopt the Findings of Fact submitted in response to the Standards for Special Use Permits and Standards for Variations outlined in **Attachments 3 and 4**, respectively, of the staff report prepared for PC 21-01 for the February 3, 2021 Plan Commission meeting; and that the Plan Commission recommend to the Village Board approval of a special use for a fast food establishment with a drive-through, including the list of variations as outlined in the staff report prepared for PC 21-01 for the February 3, 2021 Plan Commission meeting, and as deliberated by the Plan Commission, according to the plans listed in the Staff Report for PC 21-01 for the February 3, 2021 Plan Commission meeting, and subject to the following conditions:

1. Improvements shall be in substantial compliance with all plans described in **Attachment 2** of the staff report prepared for PC 21-01 except as may be modified in response to compliance with Village Codes or conditions recommended by the Plan Commission and as approved by the Village.
2. Approved signage is limited to two hundred ninety square feet (290 SF) of signage including one monument sign and four wall signs in substantial conformance with the sign package included in **Attachment 2** of the staff report.
3. The Landscape Plan shall be revised, resubmitted, and approved by the Village in compliance with Village Codes prior to final zoning approval from the Village Board.
4. Plans shall be revised prior to being considered by the Village Board in compliance with the planning comments outlined in **Attachment 7** of the staff report prepared for PC 21-01 for the February 3, 2021 Plan Commission meeting.
5. The trash enclosure shall be constructed to look like masonry and shall appear similar to the color and style of the building.



6. All landscaped areas shall be constructed, and landscape material installed prior to the issuance of any permanent occupancy permit for the subject realty, or such earlier time, as is reasonably practical.
7. A separate sign permit shall be obtained for the proposed building signage, pursuant to the Village Code.
8. That as part of the final approval by the Village, the Applicant shall enter into a Traffic Agreement in substantial conformance outlined in **Attachment 6** of the staff report prepared for PC 21-01 for the February 3, 2021 Plan Commission meeting.
9. A permit is required from IDOT prior to any work in the Route 83 right of way.
10. A permit is required from the Du Page County Public Works Department for the sanitary sewer connection.
11. The Applicant must file a Notice of Intent with the IEPA prior to the start of construction.
12. The property owner shall provide the Village with a letter of No Further Remediation (NFR) from the IEPA prior to a building permit being issued.
13. Prior to building permit, the Village will require a Plat of Easement to grant access to the B-Box at the building. This can be a 10-foot-wide non-exclusive easement over the water service from 75th Street to the building.
14. All outstanding comments from the Village must be addressed prior to final zoning approval from the Village Board.

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Attachment 1
Legal Description

THE NORTH 170.0 FEET OF THAT PART OF THE SOUTHEAST 1/4 OF SECTION 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING EAST OF THE EAST RIGHT OF WAY OF STATE ROUTE 83, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT 102.65 FEET EAST OF THE NORTHWEST CORNER OF SAID SOUTHEAST 1/4; THENCE EAST, ALONG THE NORTH LINE OF SAID SOUTHEAST 1/4, A DISTANCE OF 256.24 FEET; THENCE SOUTH, ALONG A LINE PARALLEL WITH THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170 FEET; THENCE WEST, ALONG A LINE PARALLEL WITH THE NORTH LINE OF SAID SOUTHEAST 1/4, A DISTANCE OF 256.24 FEET TO THE INTERSECTION OF SAID PARALLEL LINE WITH THE EAST RIGHT OF WAY OF SAID STATE ROUTE 83, WHICH POINT IS 102.45 FEET EAST OF THE WEST LINE OF SAID SOUTHEAST 1/4; THENCE NORTH, ALONG THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170.0 FEET TO THE POINT OF BEGINNING, IN DUPAGE COUNTY, ILLINOIS.

Property Address: 7505 South Kingery Highway, Willowbrook, IL 60527

PIN #: 09-26-400-013

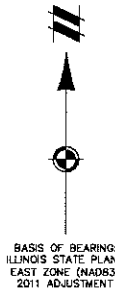


Attachment 2
Submitted Plans

1. **"ALTA-NSPS Land Title Survey"** – 2 sheets (Job. No. 18003769), dated 8/10/2020, bearing the latest revision date of 9/23/2020, resubmitted on January 19, 2021 and prepared by NORR LLC.
2. **"Site Layout Plan"** – 1 sheet, Sheet C04.0, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, bearing the latest revision date of 01/19/2021 and prepared by NORR Architects Engineers Planners.
3. **"Architectural Drawings"** – 5 sheets, Sheets A-101, A-200, A-201, A-202, A-300, Panda Project #: D7058, Arch. Project #: JCDT180453, bearing an issue date of 11-04-2020, and prepared by NORR Architects Engineers Planners.
4. **"Proposed Sign Package"** – 17 pages, Project #: C59897, dated 12.19.2018, bearing the latest revision date of 01.19.2020, and prepared by Priority Sign.
5. **"Final Engineering Plans"** – 18 sheets, Panda Project #: D7058, Atwell Project #: 18003769, bearing an issue date of 11/20/2020, bearing the latest revision date of 01/19/2021 and prepared by NORR Architects Engineers Planners.
6. **"Engineer's Opinion of Probable Construction Cost"** – 3 pages, dated 11/20/2020, and prepared by Atwell Group.
7. **"Site Traffic Study"** – 11 pages, dated January 5, 2020 and prepared by Atwell Group.
8. **"Traffic Turn Analysis"** – 1 sheet, Sheet EX-01, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, and prepared by NORR Architects Engineers Planners.
9. **"Traffic Plan"** – 1 sheet, Sheet EX-02, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, and prepared by NORR Architects Engineers Planners.
10. **"Landscape Plan"** – 2 sheets, Sheets L01.0 and L01.1, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, bearing the latest revision date of 01/19/2021 and prepared by NORR Architects Engineers Planners.
11. **"Photometric Plan"** – 1 sheet, Sheet L01.0, dated 10/29/2020, and prepared by Villa Lighting.
12. **"Construction Schedule"** – 1 page, dated 11/17/2020, and prepared by Atwell Group.

ALTA/NSPS LAND TITLE SURVEY

THAT PART OF THE SOUTHEAST QUARTER OF SECTION 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, DUPAGE COUNTY, ILLINOIS.



ALTA TABLE NOTES:

ITEM 1 - MONUMENTS PLACED
FOUND MONUMENTATION SHOWN HEREON.

ITEM 2 - ADDRESSES
ADDRESSES SHOWN HEREON ARE PER COUNTY GIS DATA.

ITEM 3 - FLOOD ZONE CLASSIFICATION
THE SURVEYED PROPERTY LIES WITHIN ZONE X, AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN PER F.I.R.M. MAP NO. 1704300189, EFFECTIVE AUGUST 1, 2019.

ITEM 4 - GROSS LAND AREA
THE MEASURED ACREAGE OF THE SURVEYED PROPERTY IS SHOWN HEREON.

ITEM 5 - VERTICAL RELIEF
THE VERTICAL RELIEF SHOWN HEREON WAS COMPLETED BY GROUND SURVEY WITH A CONTOUR INTERVAL OF 1 FOOT ON THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88) BASED ON MEASUREMENTS MADE ON THE VRS CONTROL NETWORK. (SEE SHEET 2 OF 2 FOR TOPOGRAPHIC DETAILS.)

ITEM 6 - CURRENT ZONING CLASSIFICATION
ZONING REPORT NOT PROVIDED BY CLIENT.

ITEM 7A - EXTERIOR DIMENSIONS OF BUILDINGS
EXTERIOR DIMENSIONS OF BUILDING IS SHOWN HEREON.

ITEM 7B1 - SQUARE FOOTAGE OF BUILDINGS
SQUARE FOOTAGE OF BUILDING IS SHOWN HEREON.

ITEM 7C - BUILDING HEIGHTS
MEASURED HEIGHT OF BUILDING IS AS SHOWN HEREON.

ITEM 8 - SUBSTANTIAL FEATURES
ALL SUBSTANTIAL FEATURES OBSERVED IN THE PROCESS OF CONDUCTING THE SURVEY ARE SHOWN HEREON.

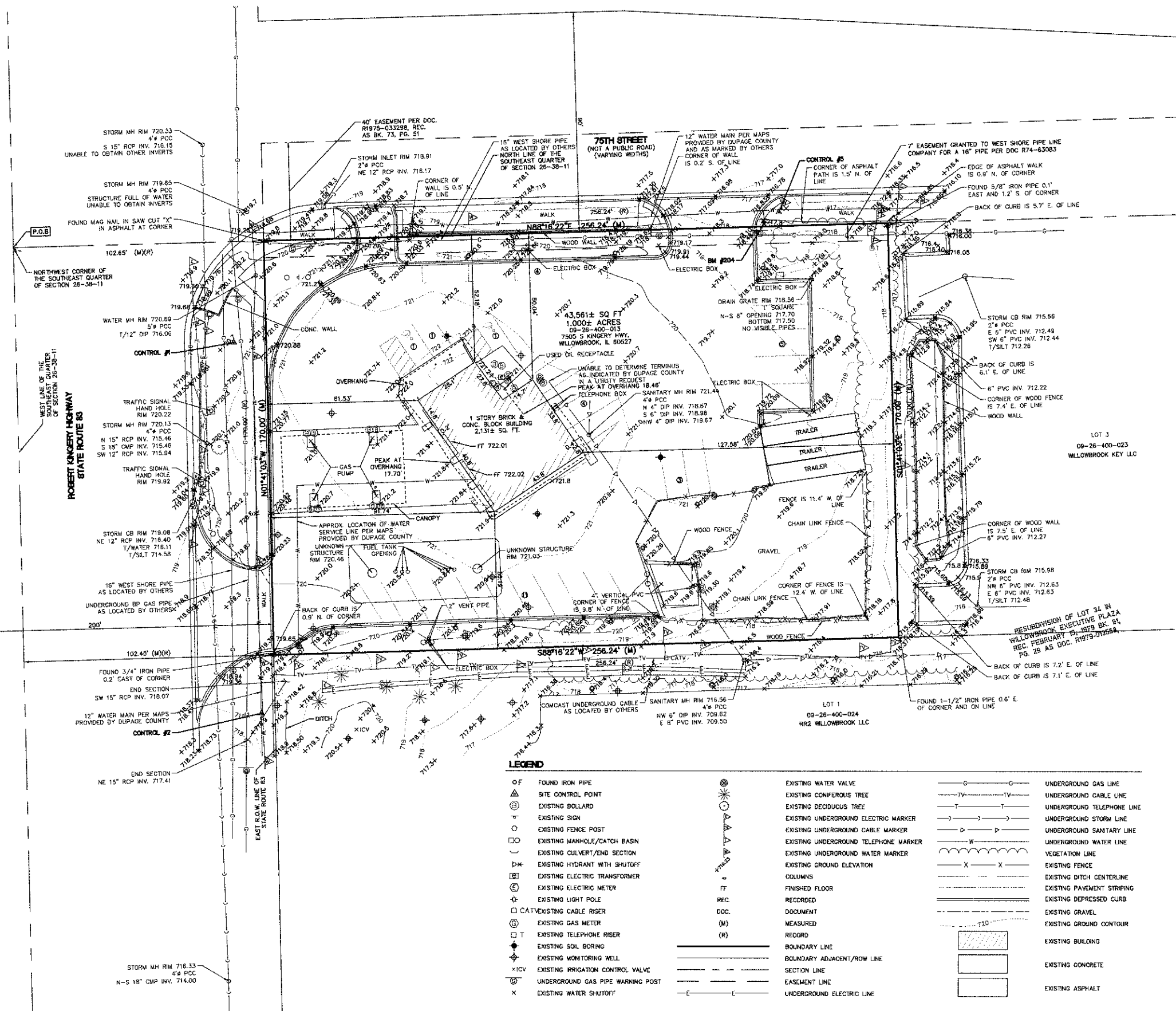
ITEM 9 - PARKING SPACES
PARKING SPACES EXIST WITHIN THE SURVEYED PROPERTY: 30 TOTAL SPACES, 2 HANDICAPPED SPACES AND 28 REGULAR SPACES.

ITEM 11 - UTILITIES
UTILITIES SHOWN HEREON ARE BY VISIBLE LOCATION OF ABOVE GROUND STRUCTURES AND MAPS OBTAINED FROM THE UTILITY COMPANIES FROM JULIE DESIGN TICKET NUMBER X2190860. NOTE TO THE CLIENT, INSURER, AND LENDER - WITH REGARD TO TABLE A, ITEM 11, SOURCE INFORMATION FROM PLANS AND DRAWINGS WILL BE COMBINED WITH OBSERVED EVIDENCE OF UTILITIES PURSUANT TO SECTION 5.6.IV. TO DEVELOP A VIEW OF THE UNDERGROUND UTILITIES, HOWEVER, LACKING EXCAVATION, THE EXACT LOCATION OF UNDERGROUND FEATURES CANNOT BE ACCURATELY, COMPLETELY, AND RELIABLY DEPICTED. IN ADDITION IN SOME JURISDICTIONS, 811 OR OTHER SIMILAR UTILITY LOCATE REQUESTS FROM SURVEYORS MAY BE IGNORED OR RESULT IN THE SURVEYOR'S ASSESSMENT OF THE LOCATION OF THE UTILITIES, WHERE ADDITIONAL OR MORE DETAILED INFORMATION IS REQUIRED, THE CLIENT IS ADVISED THAT EXCAVATION AND/OR A PRIVATE UTILITY LOCATE REQUEST MAY BE NECESSARY.

ITEM 12 - NAMES OF ADJOINING OWNERS
NAMES OF ADJOINING OWNERS ARE SHOWN HEREON PER COUNTY GIS DATA.

ITEM 14 - DISTANCE TO NEAREST INTERSECTING STREET
SURVEYED PARCEL IS AT THE NORTHWEST CORNER OF THE INTERSECTION OF 75TH STREET AND ROBERT KINGSLEY HIGHWAY, STATE ROUTE 83.

ITEM 16 - CONSTRUCTION
NO EVIDENCE OF RECENT EARTH MOVING WORK, BUILDING CONSTRUCTION OR BUILDING ADDITIONS WERE OBSERVED IN THE PROCESS OF THE FIELDWORK.



LEGEND

OF	FOUND IRON PIPE	EXISTING WATER VALVE	EXISTING BUILDING
△	SITE CONTROL POINT	EXISTING CONIFEROUS TREE	EXISTING CONCRETE
⊙	EXISTING BOLLARD	EXISTING DECIDUOUS TREE	EXISTING ASPHALT
+	EXISTING SIGN	EXISTING UNDERGROUND ELECTRIC MARKER	
○	EXISTING FENCE POST	EXISTING UNDERGROUND CABLE MARKER	
□	EXISTING MANHOLE/CATCH BASIN	EXISTING UNDERGROUND TELEPHONE MARKER	
—	EXISTING CULVERT/END SECTION	EXISTING UNDERGROUND WATER MARKER	
⊗	EXISTING HYDRANT WITH SHUTOFF	EXISTING GROUND ELEVATION	
⊕	EXISTING ELECTRIC TRANSFORMER	COLUMNS	
⊖	EXISTING ELECTRIC METER	FINISHED FLOOR	
☆	EXISTING LIGHT POLE	RECORDED	
⊕	CATV/EXISTING CABLE RISER	DOCUMENT	
⊖	EXISTING GAS METER	MEASURED	
⊕	EXISTING TELEPHONE RISER	RECORD	
⊖	EXISTING SOIL BORING	BOUNDARY LINE	
⊕	EXISTING MONITORING WELL	BOUNDARY ADJACENT/ROW LINE	
⊖	EXISTING IRRIGATION CONTROL VALVE	SECTION LINE	
⊕	UNDERGROUND GAS PIPE WARNING POST	EASEMENT LINE	
×	EXISTING WATER SHUTOFF	UNDERGROUND ELECTRIC LINE	

811

Know what's below.
Call before you dig.

THE LOCATION OF EXISTING UNDERGROUND UTILITIES ARE SHOWN BY AN APPROXIMATE WAY ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVE. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF EXISTING UTILITIES BEFORE COMMENCING WORK AND SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCURRED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

NOTICE: CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. WITHIN THE OWNER, THE ENGINEER SHALL BE EXPECTED TO ASSURE ANY RESPONSIBILITY FOR SAFETY OF THE WORK OF PERSONS ENGAGED IN THE WORK OF ANY NEARBY STRUCTURES BY OTHER PERSONS.

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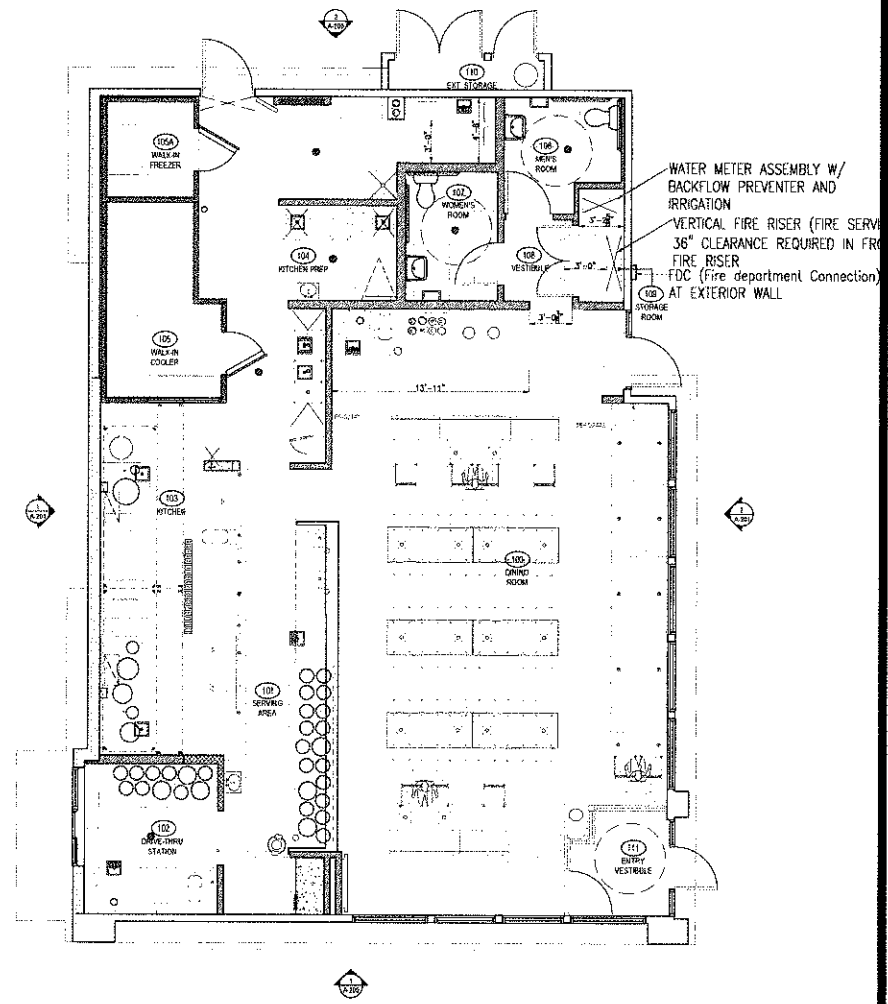
SECTION 26
TOWN 38 NORTH, RANGE 11 EAST
DOWNS GROVE TOWNSHIP
DUPAGE COUNTY, ILLINOIS

CLIENT
NORR LLC
ALTA/NSPS LAND TITLE SURVEY
7505 S KINGSLEY HWY
WILLOWBROOK, IL 60057

DATE	8/10/2020
5/23/2020	UPDATED TITLE COMMITMENT
REVISIONS	
SCALE	0 10 20
1" = 20 FEET	
DR. TLA	CH. KOA
P.M. B. STYCK	
BOOK	246
JOB	18003769
SHEET NO.	2 OF 2

Q:\18003769\18003769AS-01.DWG

- GENERAL NOTES
- BUILDING GROSS AREA 2,381 SF
 - BUILDING NETUSABLE AREA 2,233 SF
 - PROPOSED USE: ASSEMBLY (A-2)



PANDA EXPRESS, INC.
1685 Walnut Grove Ave.
Rosemead, California
91770
Telephone: 626.799.9885
Facsimile: 626.972.8288

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REVISIONS	

ISSUE DATE	
PC SUBMITTAL	11.04.2020

DRAWN BY: CP

PANDA PROJECT # 07068

ARCH PROJECT # JCDT180453

NORR

PANDA EXPRESS
TRUE WARM & WELCOME 2300
7505 KINGSWAY HIGHWAY
WILLOWBROOK, IL 60077

A-00

OUTRIP PLAN

TRUE WARM & WELCOME 2300 R3

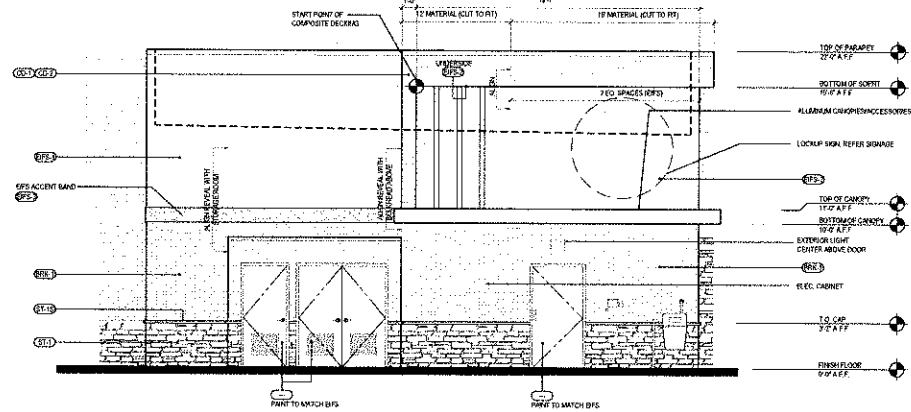
FLOOR PLAN 1
Scale: 1/4" = 1'-0" A-101

EXTERIOR FINISH SCHEDULE

NO.	MANUFACTURER	NAME	COLOR	FINISH	NOTES
001	TBO		BEIGE		BUILDING BODY
002	STO	STOTHERM ESSENCE SYSTEM	SW 6148 ROCK SUDON	PINE	BUILDING BODY
003	STO	STOTHERM ESSENCE SYSTEM	SW 7067 ZYXOCAP	PINE	BUILDING BODY
004	STO	STOTHERM ESSENCE SYSTEM	SW 7065 PINK ONE	PINE	EPS ACCENT BAND
005	COROMACO STONE PRODUCTS	ONE SERIES	#0 GREY		STONE CAP (NO VERTICAL JOINT - MANTLE TO MATCH STONE CAP)
006	COROMACO STONE PRODUCTS	INDUSTRIAL LEDGE	SHALE GREY		ENTRY PORTAL & WARDROBE CONTACT (USE ALUMINUM BEAMS 1/2" PROVIDE 3/8" MORTAL JOINTS. MFC: ARCADIS COLOR PUTTY
007	FIBERON	HORIZON	PC		COMPOSITE DECORING CONTRACT: BILL RODES @ 704-794-1800 EMAIL: BRS@billrodes.com
008	FIBERON	HORIZON	TUDOR BROWN		COMPOSITE DECORING CONTRACT: BILL RODES @ 704-794-1800 EMAIL: BRS@billrodes.com
009	EXCEPTIONAL METALS		"PANDA EXPRESS-BRONZ ONE"		CAP FLASHING
010	CLEAR GLAZING				

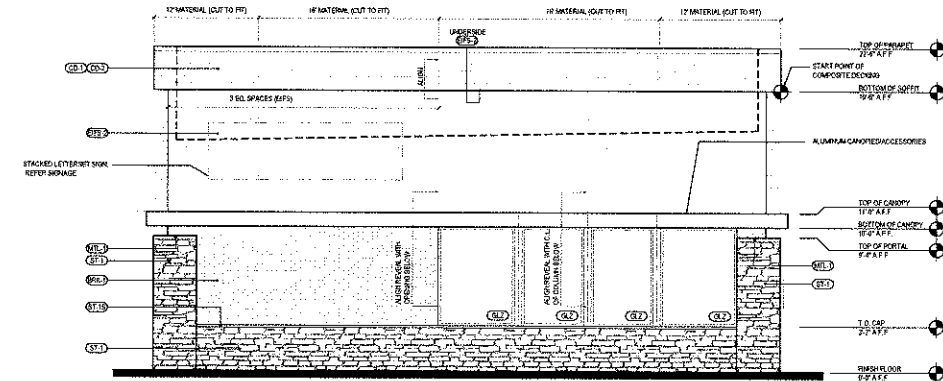
FACADE COVERAGE

	FRONT ELEV. (WEST)	REAR ELEV. (EAST)	ENTRY ELEV. (SOUTH)	DRIVE THRU ELEV. (NORTH)
BRICK	12%	34%	10%	20%
EIFS	116 SF	287 SF	127 SF	346 SF
EIFS	30%	40%	18%	40%
EIFS	352 SF	372 SF	246 SF	541 SF
STONE	16%	8%	14%	28%
STONE	174 SF	78 SF	192 SF	248 SF
FIBERON	10%	2%	10%	17%
FIBERON	147 SF	72 SF	184 SF	100 SF



EAST ELEVATION 2

Scale: 1/4" = 1'-0" A-200



WEST ELEVATION 1

Scale: 1/4" = 1'-0" A-200



PANDA EXPRESS, INC.
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1	FOR SUBMITTAL	11-01-2020

ISSUE DATE:

FOR SUBMITTAL 11-01-2020

DRAWN BY: CP

PANDA PROJECT #: 07058

ARCH PROJECT #: JCDT18043

NORR

PANDA EXPRESS

TRUE WARM & WELCOME 2300
7505 MINISTEY AVE. N.W.
WILLOWBROOK, IL 60027

A-200

EXTERIOR
ELEVATIONS

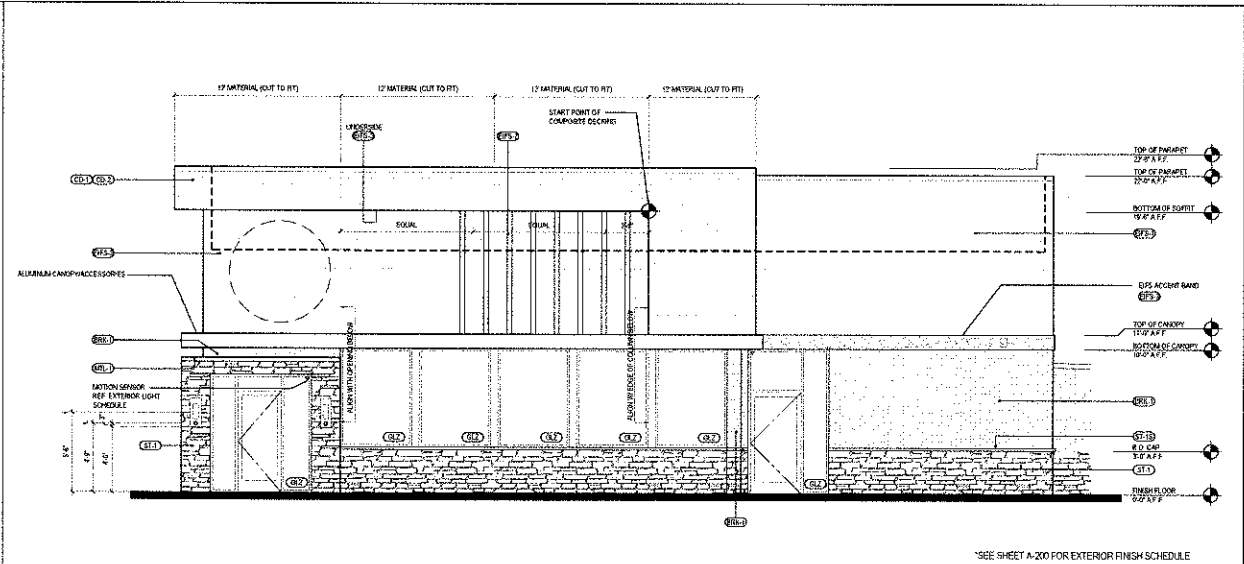
TRUE WARM & WELCOME 2300

EXTERIOR FINISH SCHEDULE

NO.	MANUFACTURER	WFOUR	DESCRIPTION	FINISH
001	T&B		STONE/TERAZZO SYSTEM	PINE
002	STC		STONE/TERAZZO SYSTEM	PINE
003	STC		STONE/TERAZZO SYSTEM	PINE
004	STC		STONE/TERAZZO SYSTEM	PINE
005	COORDINATED STONE PRODUCTS		STONE/TERAZZO SYSTEM	PINE
006	COORDINATED STONE PRODUCTS		STONE/TERAZZO SYSTEM	PINE
007	HARRIS		STONE/TERAZZO SYSTEM	PINE
008	FLORIAN		STONE/TERAZZO SYSTEM	PINE
009	EXCEPTIONAL METALS		STONE/TERAZZO SYSTEM	PINE
010	CLEAR GLASS		STONE/TERAZZO SYSTEM	PINE

FACADE COVERAGE

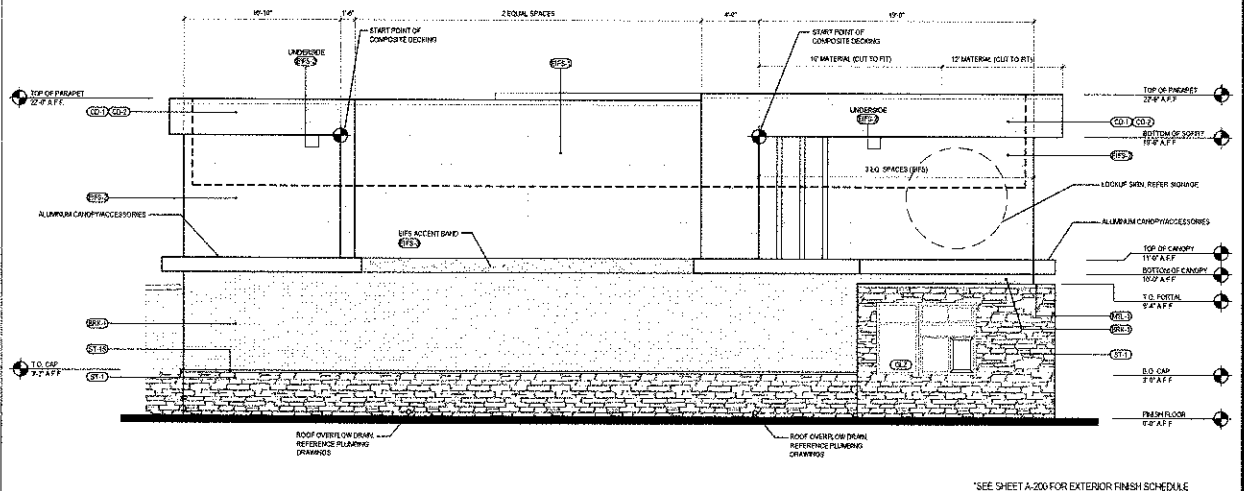
	FRONT ELEV. (WEST)	REAR ELEV. (EAST)	ENTRY ELEV. (SOUTH)	DRIVE THRU ELEV. (NORTH)
BRICK	12%	24%	10%	38%
EIFS	116 SF	207 SF	137 SF	340 SF
STONE	38%	40%	18%	4%
EIFS	352 SF	328 SF	296 SF	341 SF
STONE	18%	1%	14%	18%
EIFS	174 SF	79 SF	192 SF	248 SF
BRICK	15%	1%	14%	17%
EIFS	147 SF	72 SF	184 SF	199 SF



*SEE SHEET A-200 FOR EXTERIOR FINISH SCHEDULE

SOUTH ELEVATION 2

Scale: 1/4" = 1'-0" A-201



*SEE SHEET A-200 FOR EXTERIOR FINISH SCHEDULE

NORTH ELEVATION 1

Scale: 1/4" = 1'-0" A-201



PANDA EXPRESS, INC.
1635 Walnut Grove Ave.
Rosemead, California
91770
Telephone: 626.799.9905
Facsimile: 626.372.8286

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ISSUE DATE:

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NO.	DESCRIPTION	DATE

DRAWN BY: CP

PANDA PROJECT # D7058
ARCH PROJECT # JCD1150453

NORR

PANDA EXPRESS
TRUE WARM & WELCOME 2300
7509 HUNTERS HIGHWAY
WILLOWBROOK, IL 60527

A-201

EXTERIOR
ELEVATIONS

TRUE WARM & WELCOME 2300 RD

FACADE COVERAGE

	FRONT ELEV (WEST)	REAR ELEV (EAST)	ENTRY ELEV (SOUTH)	DRIVE THRU ELEV (NORTH)
AREA:	17%	30%	9%	55%
	116 SF	207 SF	137 SF	346 SF
GLAZING:	30%	40%	18%	48%
	272 SF	278 SF	249 SF	341 SF
PAINT:	18%	27%	14%	28%
	174 SF	193 SF	139 SF	245 SF
FINISHES:	19%	8%	14%	17%
	167 SF	72 SF	174 SF	138 SF



PERSPECTIVE FACING SOUTHWEST 2

Scale: NTS: A-202



PERSPECTIVE FACING NORTHEAST 1

Scale: NTS: A-202



PANDA EXPRESS, INC.
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PANDA PROJECT # 07054
ARCH PROJECT # JCD1180453

NORR
THE ARCHITECTS

PANDA EXPRESS

TRUE WARM & WELCOME 2300
7505 KINGSWAY HIGHWAY
WILLOWBROOK, IL 60077

A-202

EXTERIOR
PERSPECTIVES

TRUE WARM & WELCOME 2300 R3



PANDA EXPRESS, INC.
1083 Walnut Grove Ave.
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91770
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Facsimile: 626.772.8288

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1	FOR SUBMITTAL	11-24-2000

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ARCH PROJECT # JCD1180453

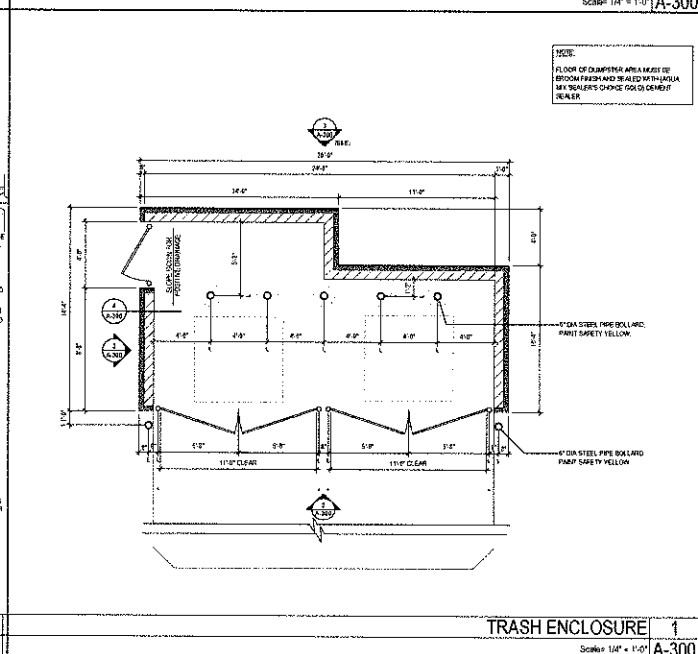
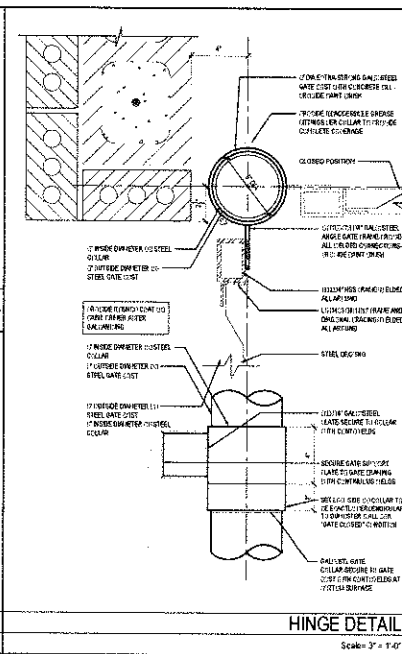
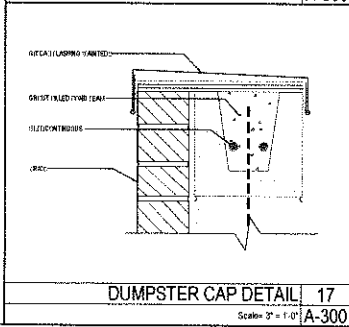
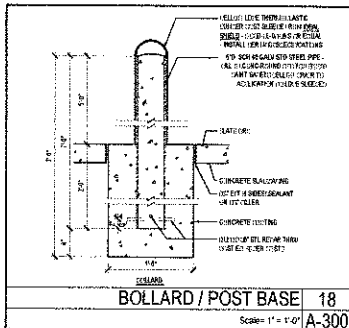
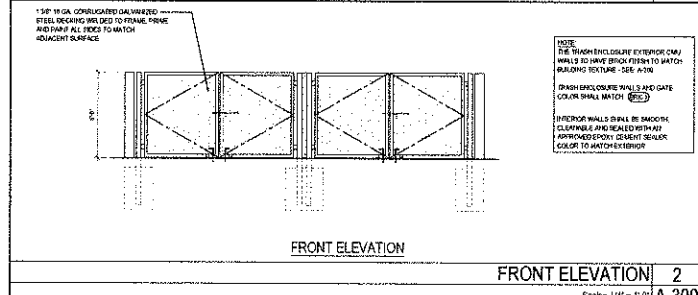
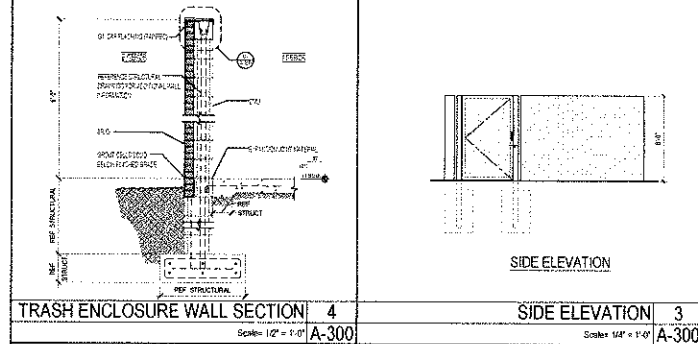
NORR
ARCHITECTS

PANDA EXPRESS
TRUE WARM & WELCOME 2300
7505 KINGSWAY HIGHWAY
WILLOWBROOK, IL 60527

A-300

TRASH ENCLOSURE
DETAILS

TRUE WARM & WELCOME 2300 P3





Proposed Sign Package
Willowbrook, IL
7505 S. Kingery
D7058

12.19.2018

Revisions:

01.07.2019		Added new site plan, removed rear and drive thru wall signs
01.08.2019		Added new site plan, added S8, added new elevations
03.04.2019		Added new site plan and changed S2 placement
03.12.2019		Added new site plan and added signs
03.27.2019		Removed short storefront lockup logo and drive thru left side lockup logo
03.28.2019		Added new site plan
10.30.2020		Added new site plan
11.02.2020		Adjusted code balances for S3, S4, S9
12.16.2020		Added S11
01.19.2021		Revised size of D8 faces, added brick base to monument

C59897

Code Summary

SIGN	LOCATION	TYPE	HEIGHT	WIDTH	ALLOWED	PROPOSED	BALANCE
S1	SHORT STOREFRONT	STACKED RED LETTERSET	2'-6"	13'-5 1/2"	320	53.19	266.81
S2	ENTRANCE	LOCKUP LOGO	6'-0"	6'-0"	266.81	36	230.81
S3	BUILDING REAR	LOCKUP LOGO	6'-0"	6'-0"	230.81	36	194.81
S4	DRIVE THRU	LOCKUP LOGO	6'-0"	6'-0"	194.81	36	158.81
S9	STREET	MONUMENT	12'-0"	6'-0"	158.81	72	86.81

NOTE: Exception required for two additional wall signs

Code Allowance

1.25 sq ft X 256 = 320 sq ft
allowed for entire lot.

Elevation

Long Storefront	320 aggregate
Short Storefront	320 aggregate

Allowed

Elevation

Drive Thru	320 aggregate
Rear	320 aggregate

Allowed



Revisions:

Removed S3 & S4 / KB / 1.7.19
Added red note / KB / 3.4.19
Added new signs / KB / 3.13.19

Removed S2 and S3 / KB / 3.27.19

Added illuminated cabinets (10/30/20) PB
Adjusted code balances | PV | 11.02.2020
X

File Location:
Drive/Clients/

STND
CSTM

☐ AS ☐ CR ☒ EN

Date: 12.19.2018

Designer: KB PM: MC

City/State: Willowbrook, IL

Address: 7505 S. Kingery

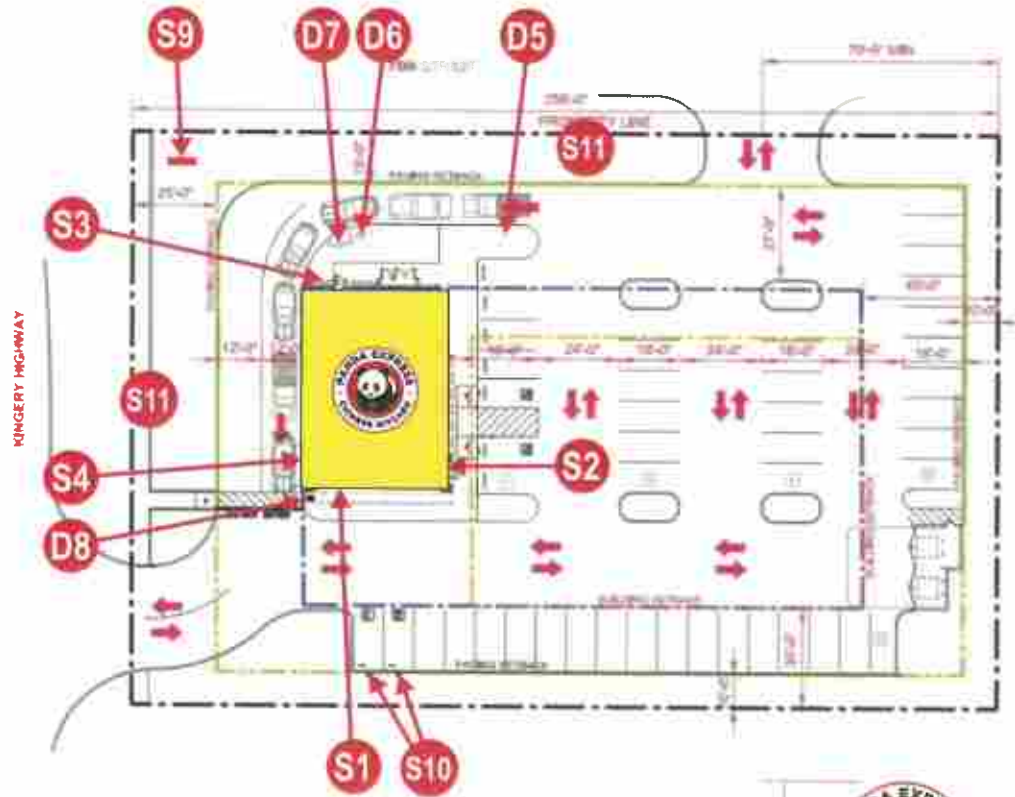
Drawing #

OE #

C59897 - 6

1200000

Site Plan



PANDA EXPRESS

S1 2'-6" stacked red letterset

S2

S3

S4



6' Lock Up Logo



D5



D6



D7



D8

3'-6"



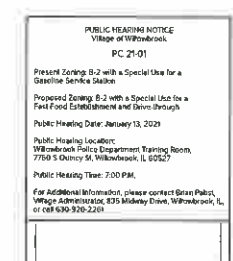
12'-0"

S9

Monument



S10



S11



Revisions:

Added new site plan / KB / 1.7.19
Added new site plan / KB / 1.7.19
Added new site plan / KB / 1.8.19
Added new site plan / KB / 3.4.19

Added new site plan / KB / 3.13.19

Removed S2 and S3 / KB / 3.27.19

Added new site plan / KB / 3.28.19

Added new site plan (10/30/20) PB

File Location:

Drive/Clients/

STND

CSTM

☐ AS

☐ CR

☐ EN

Date: 12.19.2018

City/State: Willowbrook, IL

Designer: KB

PM: MC

Address: 7505 S. Kingery

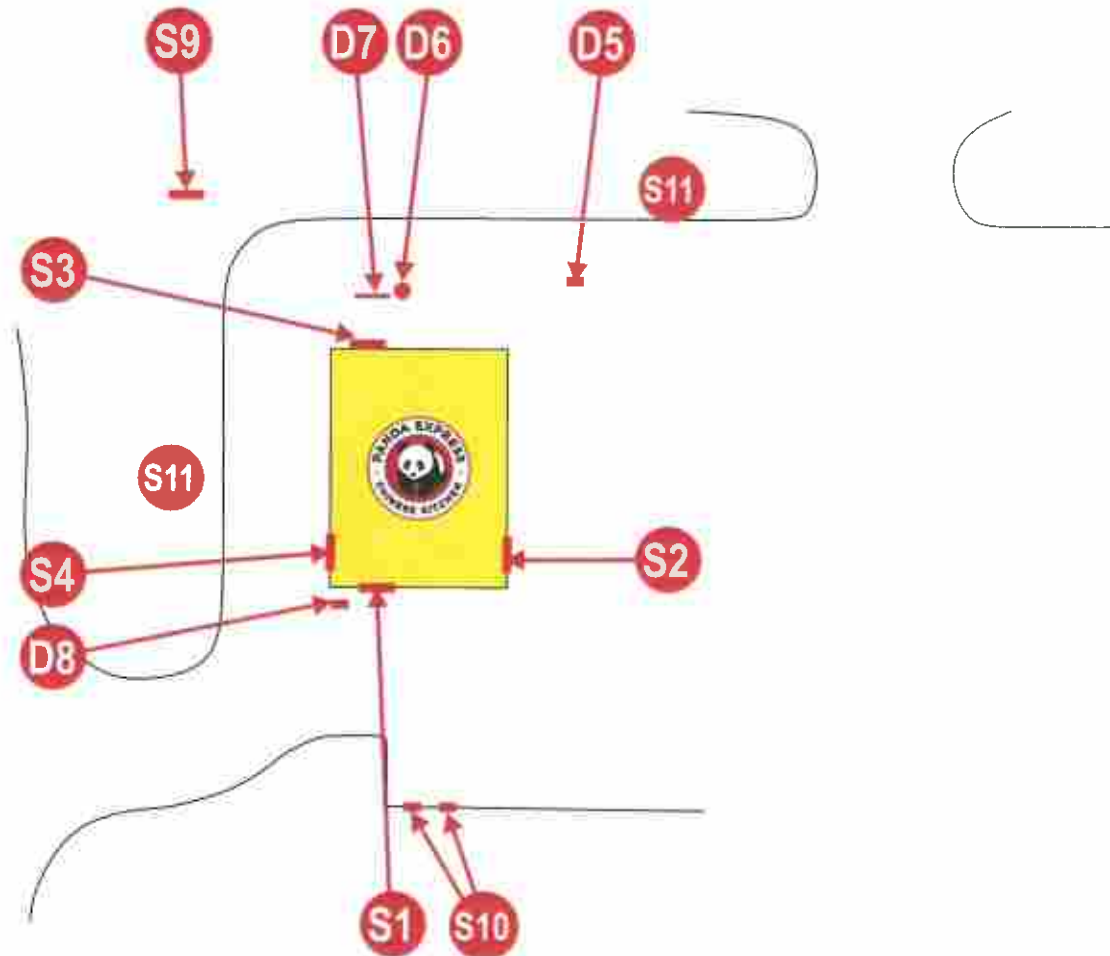
Drawing #

C59897-7

OE #

110700

Site Plan



Revisions:

Added new site plan / KB / 1.7.19
Added new site plan / KB / 1.8.19
Added new site plan / KB / 2.4.19

Removed S2 and S3 / KB / 3.27.19

Added new site plan / KB / 3.28.19

Added new site plan (10/30/20) PB

X

File Location:
Drive/Clients/

STND
CSTM

☐ AS ☐ CR ☐ EN

Date: 12.19.2018

Designer: KB PM: MC

City/State: Willowbrook, IL

Address: 7505 S. Kingery

Drawing #

OE #

C59897-6

1201901

Elevations

Scale | 3/32" = 1'



Scale | 1/4" = 1'

SIGN	LOCATION	TYPE	HEIGHT	WIDTH	ALLOWED	PROPOSED	BALANCE
S1	SHORT STOREFRONT	STACKED RED LETTERSET	2'-6"	13'-5 1/2"	320	53.19	266.81



Revisions:

Added new elevation / KB / 1.8.19
X
Added S2 / KB / 3.13.19
X
Removed lockup logo, made letters red / KB / 3.27.19
X

Added new elevation (10/30/20) PB

X

X

X

File Location:
Drive/Clients/

STND
CSTM



Date: 12.19.2018

Designer: KB PM: MC

City/State: Willowbrook, IL

Address: 7505 S. Kingery

Drawing #

OE #

CS9897-4

100000

Elevations

Scale | 3/32" = 1'



Scale | 1/4" = 1'

SIGN	LOCATION	TYPE	HEIGHT	WIDTH	ALLOWED	PROPOSED	BALANCE
S2	DRIVE THRU	LOCKUP LOGO	6'-0"	6'-0"	205.81	36	169.81



Revisions:

Added new elevation / KB / 1.8.19
 Changed to drive thru elevation / KB / 3.4.19
 Added S3 & S4 / KB / 3.13.19

Removed 5" lockup logo on left / KB / 3.27.19

Added new elevation (10/30/20) PB

X

X

File Location:
 Drive/Clients/

STND
 CSTM

AS CR EN

Date: 12.19.2016

Designer: KB PM: MC

City/State: Willowbrook, IL

Address: 7505 S. Kingery

Drawing #

OE #

C59897-5

1200000

Elevations

Scale | 3/32" = 1'



Scale | 1/4" = 1'

SIGN	LOCATION	TYPE	HEIGHT	WIDTH	ALLOWED	PROPOSED	BALANCE
S3	DRIVE THRU	LOCKUP LOGO	6'-0"	6'-0"	230.81	36	194.81



Revisions:

Revised code balance | PV | 11.02.2020

x

x

x

x

x

x

x

File Location:
Drive/Clients/

STND
CSTM



Date: 10.30.2020

City/State: Willowbrook, IL

Designer: PB

PM: MC

Address: 7505 S. Kingery

Drawing #

C59897-1

OE #

1.000000

Elevations

Scale | 3/32" = 1'



Scale | 1/4" = 1'

SIGN	LOCATION	TYPE	HEIGHT	WIDTH	ALLOWED	PROPOSED	BALANCE
S4	DRIVE THRU	LOCKUP LOGO	6'-0"	6'-0"	194.81	36	158.81



Revisions:

Revised code balance | PV | 11.02.2020

x

x

x

x

x

x

x

File Location:
Drive/Clients/

STND
CSTM



Date: 10.30.2020

City/State: Willowbrook, IL

Designer: PB

PM: MC

Address: 7505 S. Kingery

Drawing #

C59897-1

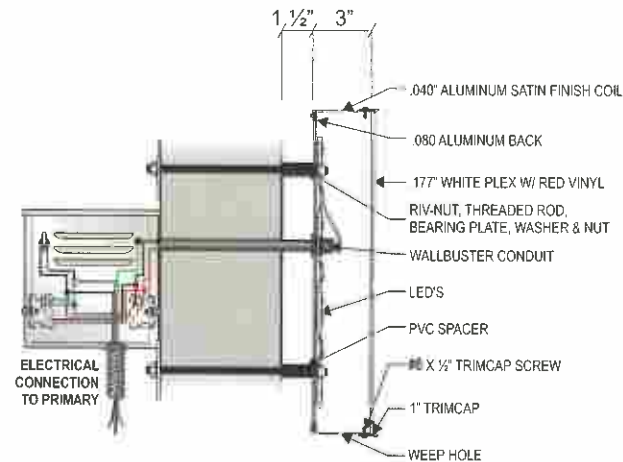
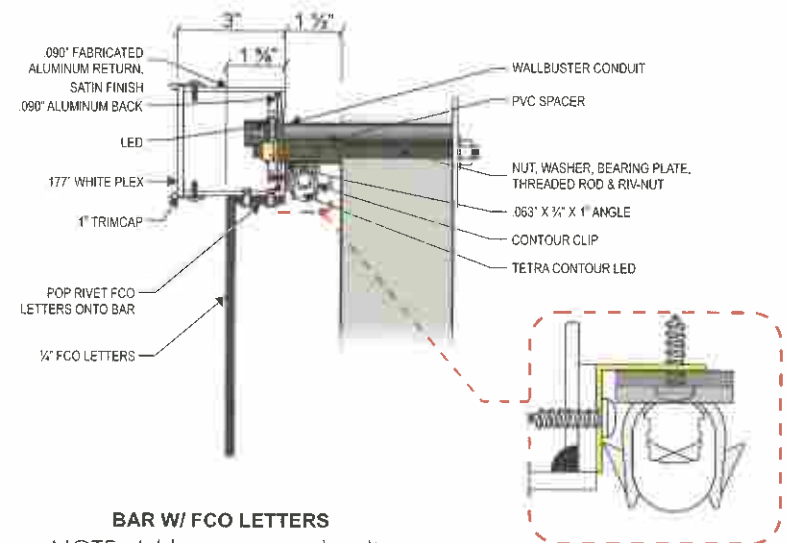
OE #

1703101

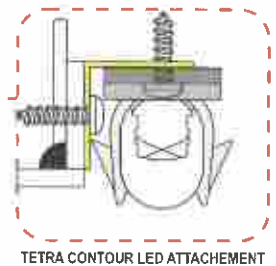
S1

Red Face-Lit Letterset - Qty: 1

Install new face-lit illuminated letterset pin-mounted to wall with aluminum FCO tagline.

**DUAL LIT CHANNEL LETTER****BAR W/ FCO LETTERS**

NOTE: Add a tie wire to the clip that holds the LED strip in

**TETRA CONTOUR LED ATTACHMENT****GENERAL SPECIFICATIONS:****FACE-LIT LETTERS:**

FACES..... .177" WHITE #7328 PLEX W/ 1ST SURFACE 3M #3630-33 RED

TRIMCAP..... 1" BLACK

RETURNS.. .040" X 3" BLACK / WHITE COIL PAINTED TO MATCH BLACK 7C, SATIN FINISH

BACKS..... .080" ALUMINUM

BAR W/ FCO LETTERS:

FACES......177" WHITE #7328 PLEX

TRIMCAP.....1" WHITE

RETURNS.....125" X 3" FABRICATED WHITE COIL RETURN, SATIN FINISH

BACKS......090" ALUMINUM, INTERIOR PAINTED WHITE

DOWNLIGHT....063" BRAKE FORMED ALUMINUM BOX BEHIND THE BAR WITH DOWNLIGHT LENS AND WHITE LED'S

EXPRESS.....25" FCO ALUMINUM LETTER PAINTED TO MATCH BLACK 7C SATIN FINISH



	A	B	C	D	E	F	SF (F x H)
PE-CL-30R	2'-6"	13'-5 1/2"	9 1/8"	7'-7 11/16"	2 13/16"	3'-11 1/4"	53.19

**Revisions:**

x		x
x		x
x		x
x		x

File Location:
Drive/Clients/

STND
CSTM



Date: 10.30.2020

City/State: Willowbrook, IL

Designer: PB

PM: MC

Address: 7505 S. Kingery

Drawing #

C59897

OE #

12/20/20

S2

Face Illuminated Lockup Logo - Qty: 1

S3

Install new face-lit lockup logo as shown.

S4

GENERAL SPECIFICATIONS:

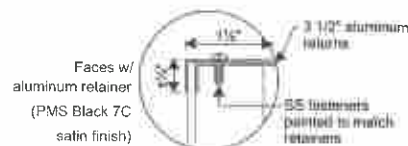
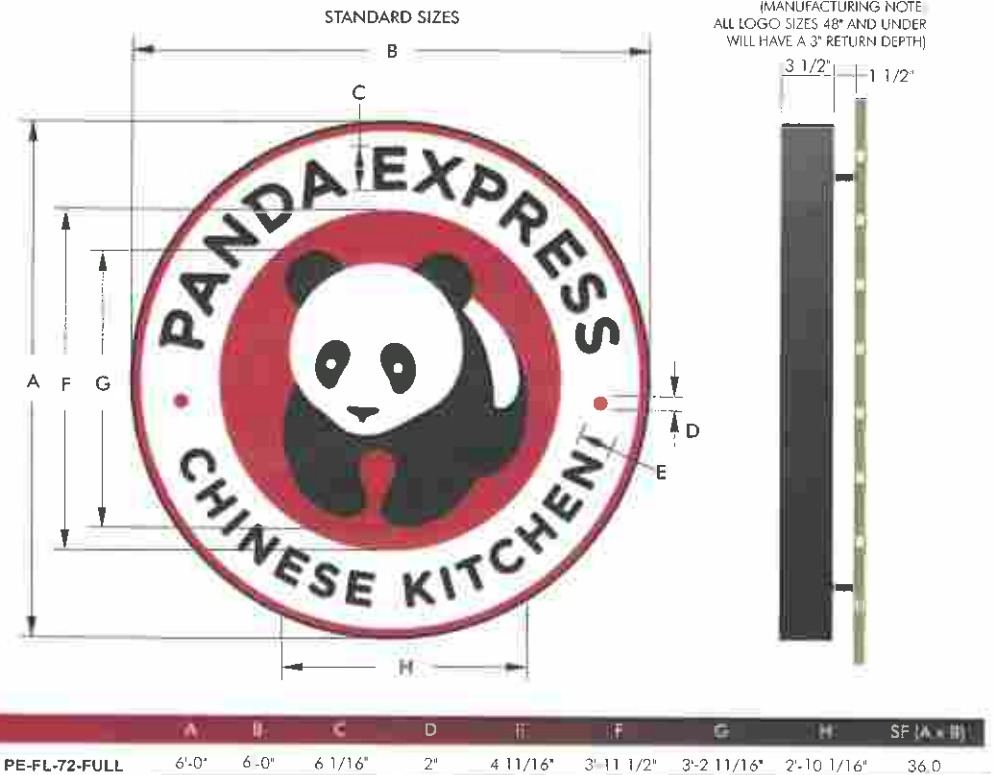
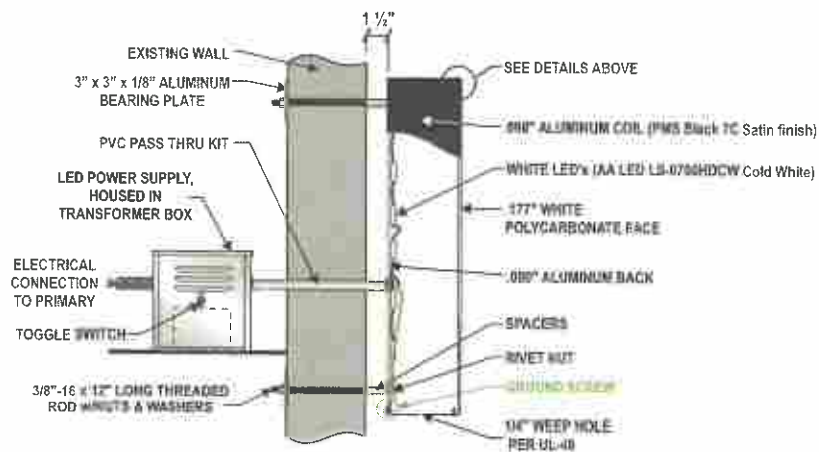
FACES:..... 177" WHITE POLYCARBONATE W/APPLIED VINYL GRAPHICS

RETAINER:.... 1" BLACK TRIMCAP / 1 1/2" ALUMINUM RETAINER (SEE DETAILS BELOW)

RETURNS:.... .040" X 3" BLACK / WHITE COIL PAINTED TO MATCH BLACK 7C SATIN FINISH

COLOR SPECIFICATIONS FOR FACEVinyl: 3M #3630-33 Red film
applied first surface

Face: 177" #7328 White Acrylic

Vinyl: 3M #3630-22 Black film
applied first surface**4'-0" AND OVER LOGO****Revisions:**

x	
x	
x	
x	

File Location:

Drive/Clients/

STND

CSTM



Date: 10.30.2020

City/State: Willowbrook, IL

Designer: PB

PM: MC

Address: 7505 S. Kingery

Drawing #

C59897

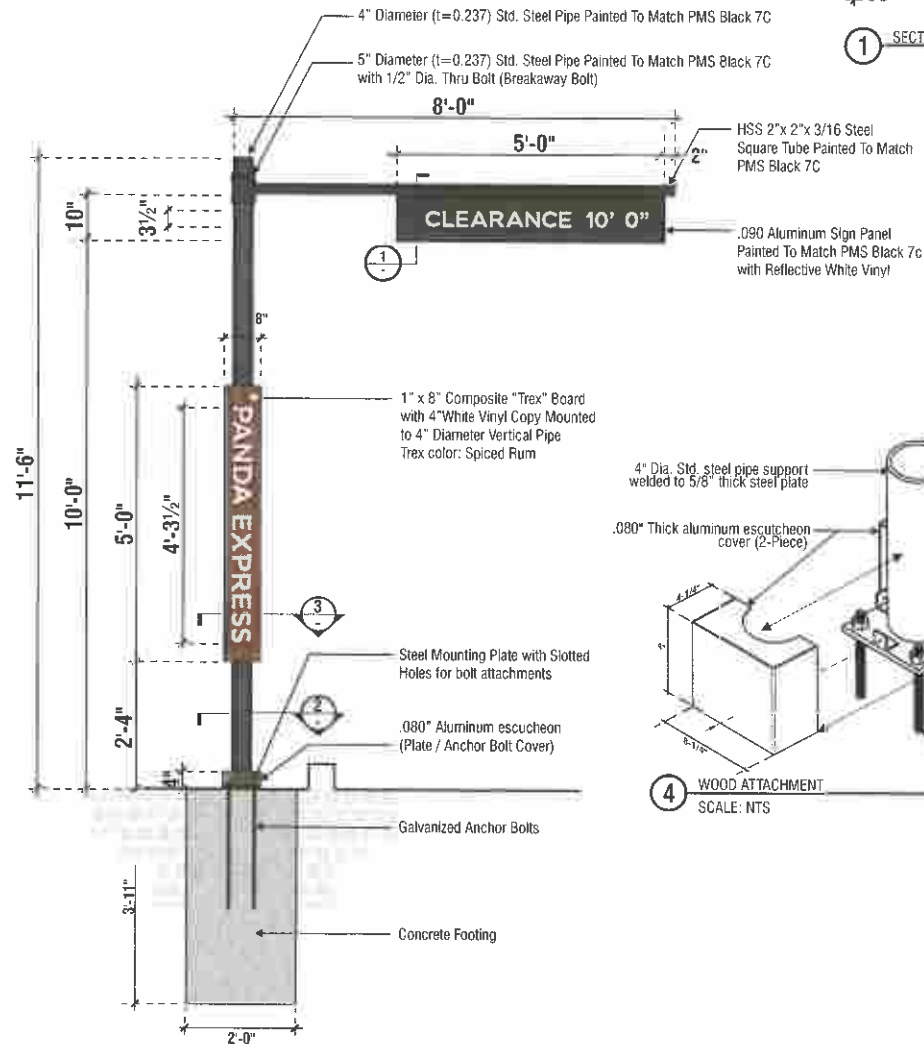
OE #

132200

D5

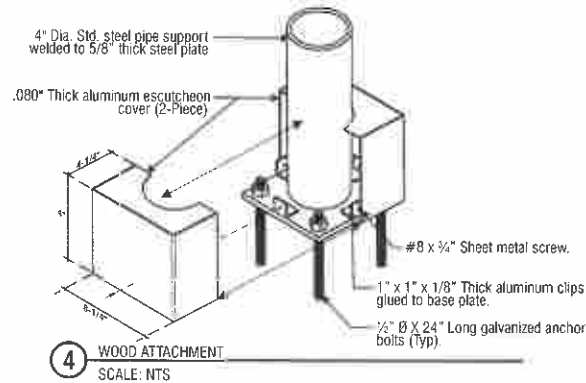
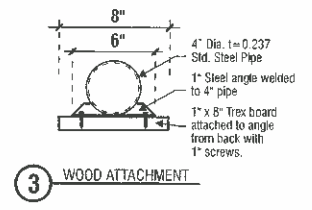
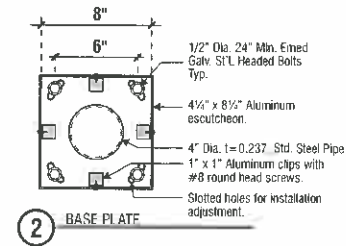
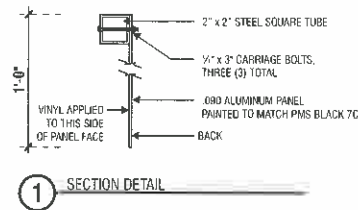
DT-L Clearance Bar - (Qty: 1)

Install new DT-L Clearance Bar sign. Refer to site plan for exact location.



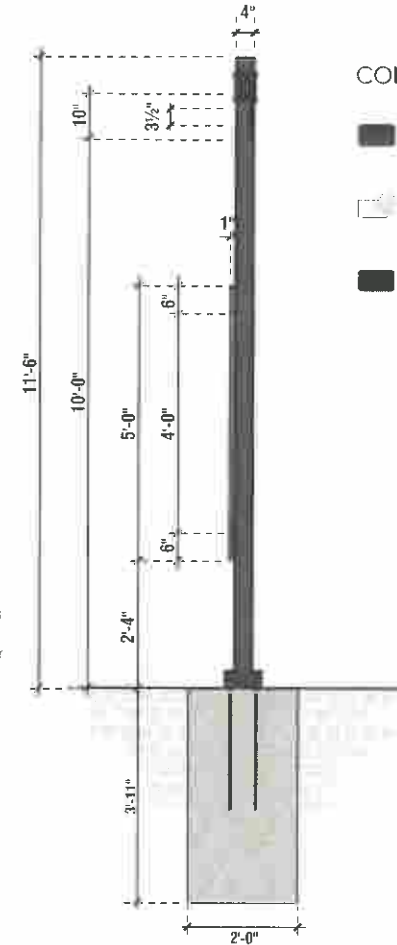
J NON-ILLUMINATED CLEARANCE BAR
QUANTITY: ONE (1) SET REQUIRED

SCALE: 1:40



COLOR SPECIFICATIONS

- Painted to match PMS Black 7C
- Arlon #4100-02 Reflective White Film
- Composite "Trex" Board



SIDE VIEW - NON-ILLUMINATED CLEARANCE BAR

SCALE: 1:40



Revisions:	x
x	x
x	x
x	x

File Location:	STND
Drive/Clients/	CSTM
AS	CR
EN	

Date: 10.30.2020

City/State: Willowbrook, IL

Drawing #

C59897

Designer: PB

PM: MC

Address: 7505 S. Kingery

OE #

12/1/2020

D6

DT COD Canopy - (Qty: 1)



Install new DT COD Canopy per site plan for exact location.

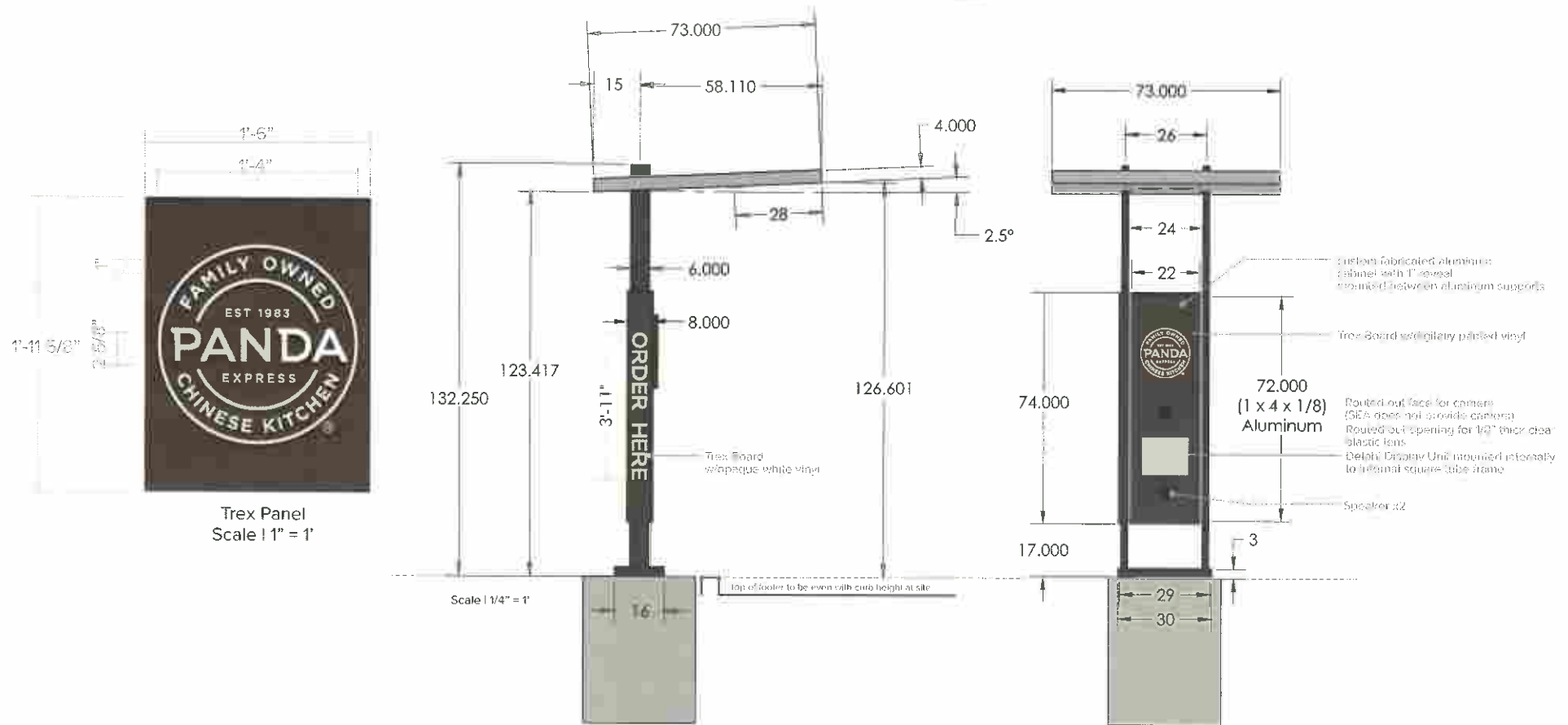
REFER TO CONSTRUCTION DRAWINGS FOR EXACT FOUNDATION & MANUFACTURING SPECIFICATIONS

COLOR SPECIFICATIONS

-  Trex Board
<https://www.lowes.com/pd/Trex-Transcend-12-ft-Spiced-Rum-Composite-Fascia-Deck-Board/1000712838>
-  Clear Vinyl Digitally Printed (FAMILY SEAL)
-  Opaque White Vinyl (ORDER HERE TEXT ONLY)

CABINET CANOPY PAINT COLORS

-  Aluminum painted to match PMS Black 7C
-  Aluminum painted to match Matthews Brushed Silver



Revisions:

x		x
x		x
x		x
x		x

File Location:
Drive/Clients/STND
CSTM

Date: 10.30.2020

City/State: Willowbrook, IL

Designer: PB

PM: MC

Address: 7505 S. Kingery

Drawing #

C59897

OE #

1253001

D7

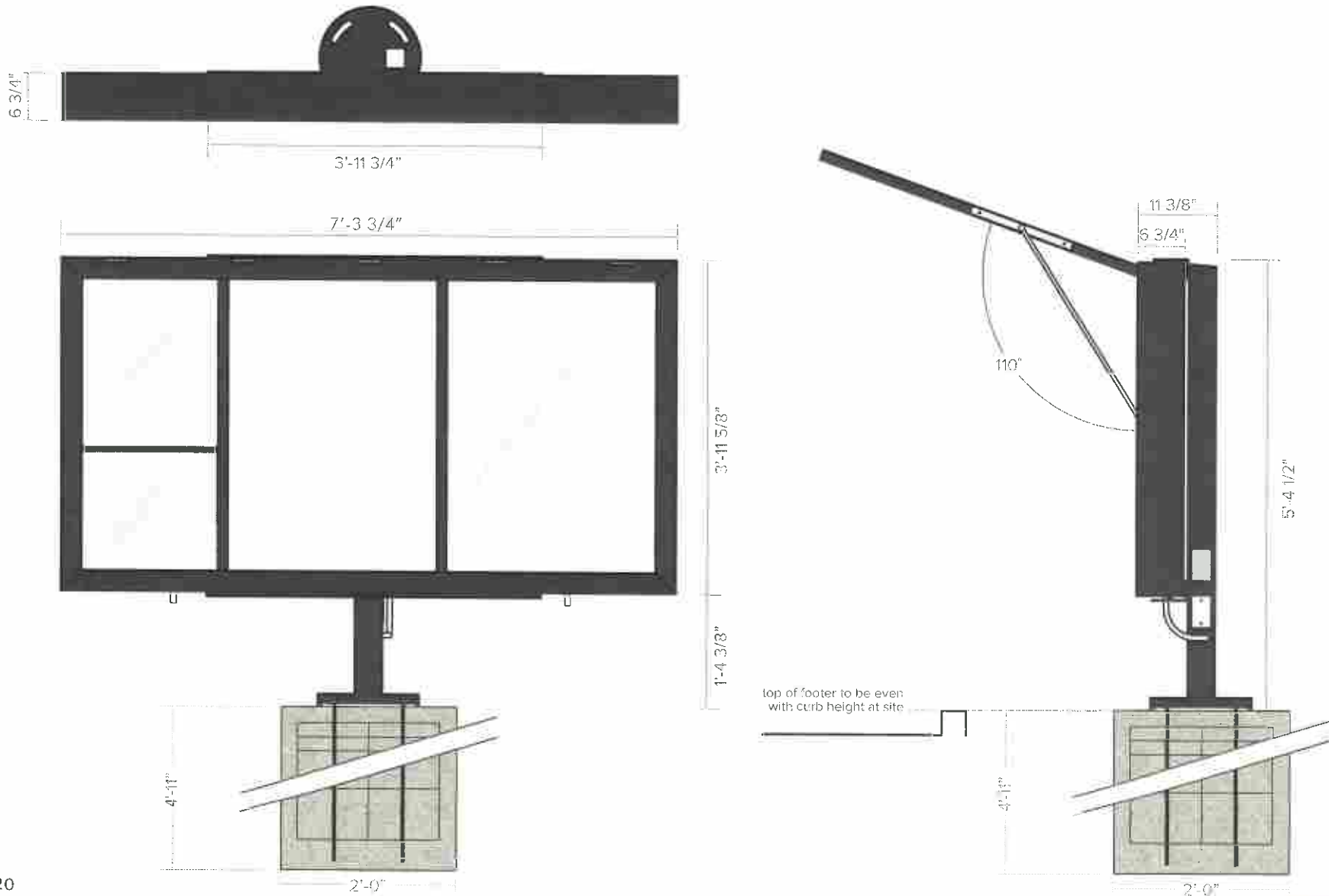
DT Menu Board - (Qty: 1)

Install new DT Menu Board. Refer to site plan for exact location.

COLOR SPECIFICATIONS

MAIN CABINET

Painted to match PMS Black 7C



Scale 1:20



Revisions:

x		x	
x		x	
x		x	
x		x	

File Location:

Drive/Clients/

STND

CSTM



AS



CR



EN

Date: 10.30.2020

Designer: PB

PM: MC

City/State: Willowbrook, IL

Address: 7505 S. Kingery

Drawing #

OE #

C59897

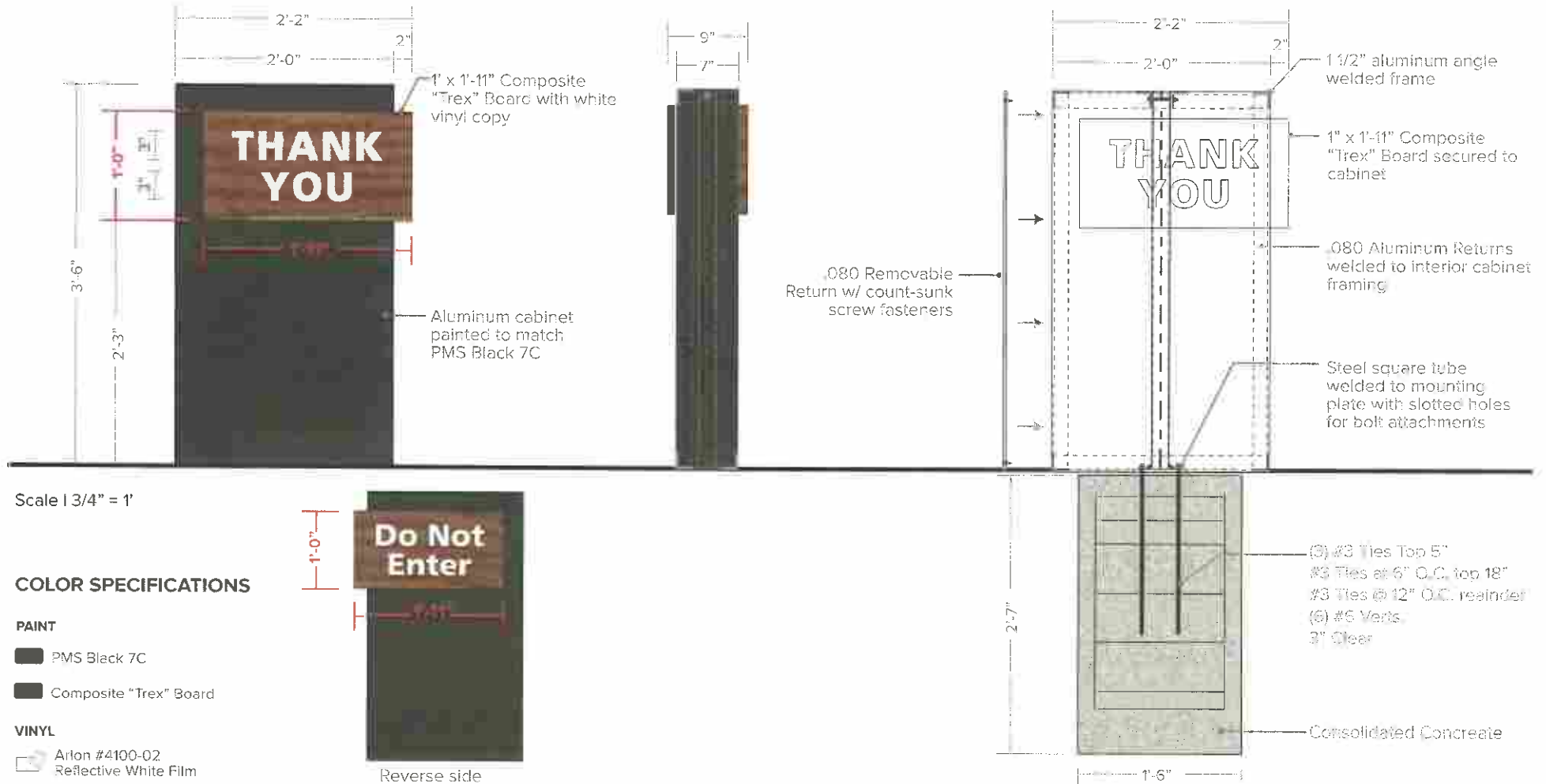
100100

D8

Non-Illuminated Directional - (Qty: 1)

Install new non-illuminated directional. Refer to site plan for exact location.

1'-0" x 1'-11" = 1.92 sq.ft.
 1.92 sq.ft. x 2 = **3.84 sq. ft. total**

**Revisions:**

Calculated face sq ft - KB - 119.21

x

x

x

x

File Location:
Drive/Clients/STND
CSTM

Date: 10.30.2020

City/State: Willowbrook, IL

Designer: PB

PM: MC

Address: 7505 S. Kingery

Drawing #

C59897-1

OE #


10/30/2020

S9

Custom Illuminated Monument - (Qty: 1)

Install new internally illuminated double-faced monument sign.
Refer to site plan for exact location.
Sign is custom due to height.


**COLOR SPECIFICATIONS****TOP CLADDING**

 .080" Aluminum sign cladding
Color: To match PMS Black 7C

RETAINER

 .080" Routed aluminum ring with .040" welded return
Color: To match PMS Black 7C on return edge only
To match PMS 711C on face

FACE

 .125" Routed aluminum cladding
Color: To match PMS Black 7C


BASE/SKIRT

 Brick to match building

ILLUMINATED PANDA LOGO

 Bayer Makrolon .77"
#7328 White LD Polycarbonate

 3M Scotchcal #3630-33 Red film

 3M #3630-22 Black film

"DRIVE THRU" COPY

 Bayer Makrolon .77"
#7328 White LD Polycarbonate

Revisions:

Changed to brick base - KB - 1.19.2021

X

X

X

X

X

X

X

File Location:
Drive/Clients/

STND
CSTM



Date: 12.19.2018

Designer: KB

PM: MC

City/State: Willowbrook, IL

Address: 7505 S. Kingery

Drawing #

C59897-1

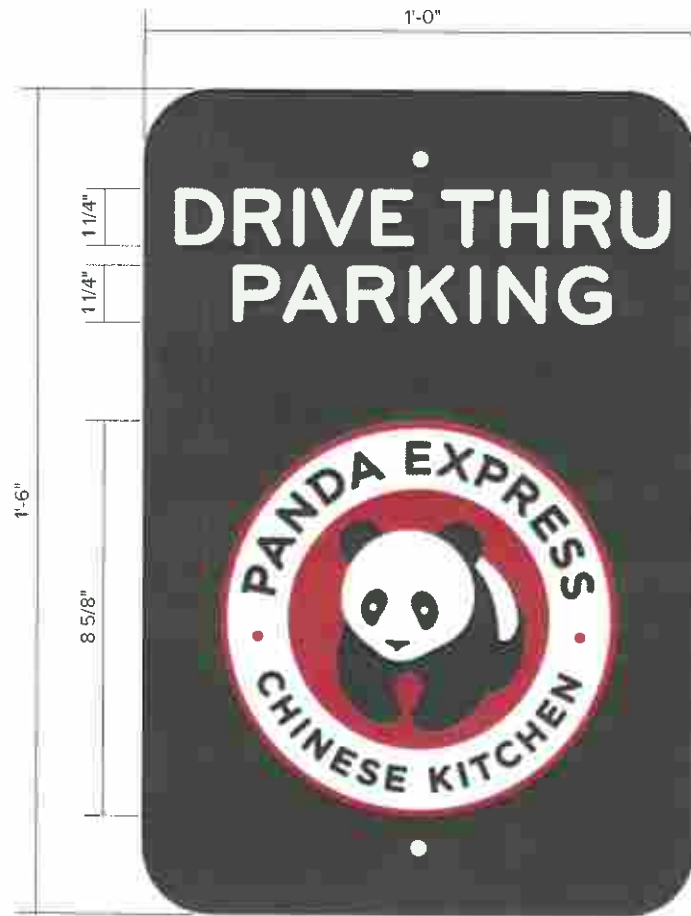
OE #

1/23/2019

S10

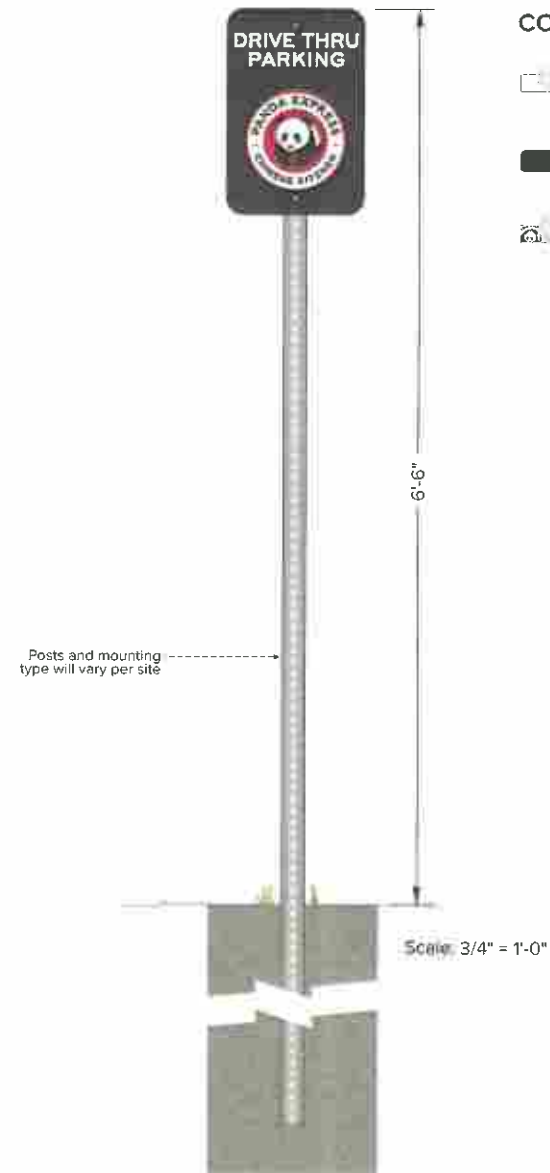
Aluminum Parking Panel - (Qty: 2)

Install new aluminum parking panels to existing posts OR with new posts.
 Refer to site plan for exact location.
 Verify ground mounting type.



Scale 1 1:4

PE-Park-DT

**COLOR SPECIFICATIONS**

- 3M #680-10 White Scotchlite reflective vinyl
- Background printed to match PMS Black 7C
- Digitally printed graphics

**Revisions:**

Revised copy / KB / 3.27.19

X

X

X

X

X

X

File Location:

Drive/Clients/

STND

CSTM



AS



CR



EN

Date: 12.19.2018

Designer: KB

PM: MC

City/State: Willowbrook, IL

Address: 7505 S. Kingery

Drawing #

C59897-1

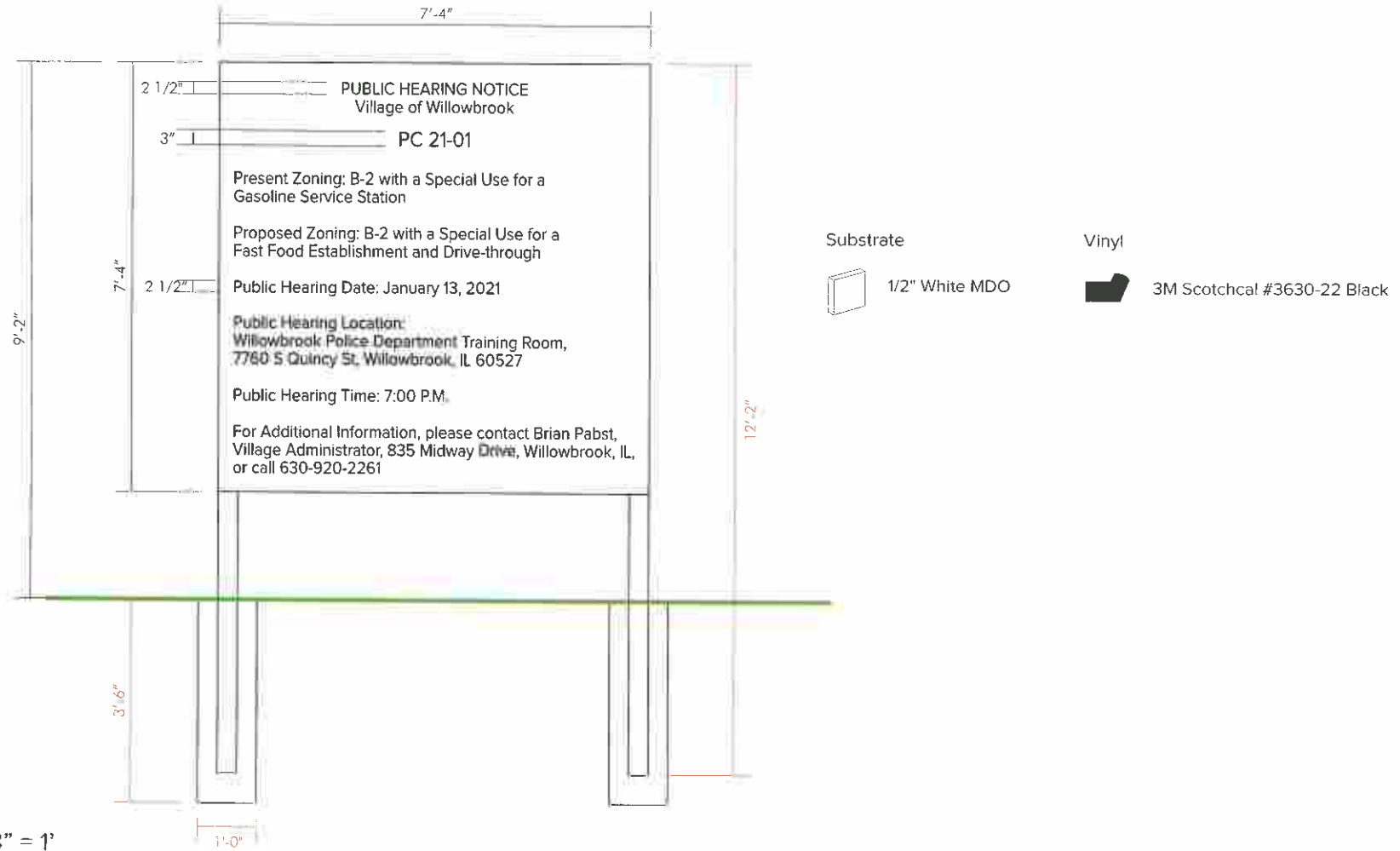
OE #

120390

S11

Custom S/F Post and Panel Sign (Qty. 2)

Install new custom s/f post and panel signs as shown. 4" square wood posts painted white with 1/2" thick white MDO panel with black vinyl graphics applied. Faces to be screw mounted to posts. Direct bury.



Scale 1 3/8" = 1'



Revisions:

Added to book KB - 12.16.2020

X

X

X

X

X

X

File Location:
Drive/Clients/STND
CSTM

Date: 12.19.2018

City/State: Willowbrook, IL

Designer: KB PM: MC

Address: 7505 S. Kingery

Drawing #

C59897

OE #

120000



WILLOWBROOK PANDA EXPRESS

PANDA PROJECT #D7058

7505 KINGERY HIGHWAY
WILLOWBROOK, IL 60527

FINAL ENGINEERING PLANS

DEVELOPMENT TEAM

CURRENT OWNER
TRUE NORTH ENERGY, LLC
10346 BRECKSVILLE ROAD
BRECKSVILLE, OH 44141
PHONE: (440) 792-4200
CONTACT: RYAN HOWARD

DEVELOPER
PANDA EXPRESS, INC.
1683 WALNUT GROVE AVENUE
ROSEMead, CA 91770
PHONE: (949) 477-5125
FAX: (926) 372-8280
CONTACT: HAKIM YALA

CIVIL ENGINEER/SURVEYOR/
LANDSCAPE ARCHITECT
ATWELL, LLC
1250 EAST DIEHL ROAD, SUITE 300
NAPERVILLE, IL 60563
PHONE: (630) 577-0800
FAX: (630) 577-0900
CONTACT: BRIAN STYCK, P.E.

ARCHITECT
NORR
150 W. JEFFERSON AVENUE, SUITE 1300
DETROIT, MI 48225
PHONE: (313) 324-3085
CONTACT: DINA MEKAWY

GOVERNING AGENCIES & UTILITY CONTACTS

MUNICIPALITY/ZONING
VILLAGE OF WILLOWBROOK
835 MIDWAY DRIVE
WILLOWBROOK, IL 60527
PHONE: (630) 920-2233
CONTACT: ANN CHOI

SOIL EROSION/STORMWATER
ENGINEERING DEPARTMENT
835 MIDWAY DRIVE
WILLOWBROOK, IL 60527
PHONE: (630) 920-2235
CONTACT: DAN LYNCH

WATER SEWER
VILLAGE OF WILLOWBROOK WATER
DIVISION
835 MIDWAY DRIVE
WILLOWBROOK, IL 60527
PHONE: (630) 920-2238
CONTACT: ANDREW PASSERO

SEWER
DUPAGE COUNTY PUBLIC WORKS
421 N. COUNTY FARM ROAD
WHEATON, IL 60187
PHONE: (830) 407-8800
CONTACT: EDWARD BUGA

ELECTRIC
COMMON WEALTH EDISON
PHONE: 1 (877) 429-6331

GAS
NICOR - WATCH AND PROTECT
REPRESENTATIVE
PHONE: (847) 344-0111
CONTACT: RAHUL

TELEPHONE
AT&T
PHONE: (630) 573-5687
CONTACT: ROBERT GILBERT

COMCAST
PHONE: (224) 229-5849
CONTACT: ROBERT STROLL

PANDA EXPRESS STANDARD NOTES:

- THE GEOTECHNICAL INVESTIGATION PREPARED BY TERRACON CONSULTANTS, INC. DATED 12/28/2020 AND ANY SUBSEQUENT ADDENDUMS IS CONSIDERED PART OF THE CONTRACT DOCUMENTS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE THE REPORT'S RECOMMENDATIONS AND FINDINGS WITH THE OWNER, ENGINEER AND ARCHITECT PRIOR TO CONSTRUCTION. IMPLEMENTATION OF THE REPORT'S RECOMMENDATIONS MAY REQUIRE THE CONTRACTOR TO PERFORM ADDITIONAL WORK NOT SHOWN ON THE CIVIL PLANS INCLUDING BUT NOT LIMITED TO EXCAVATION, REMEDIATION, DEWATERING, COMPACTION ETC.
- CONTRACTOR SHALL COORDINATE AND VERIFY LOCATION OF ALL SIGNAGE WITH OWNER PRIOR TO CONSTRUCTION.
- CONTRACTOR SHALL COORDINATE AND ADJUST LOCATION OF LOOP DETECTORS TO AVOID UTILITY CONFLICTS PRIOR TO CONSTRUCTION.
- CONTRACTOR SHALL ENSURE 100% COVERAGE OF ALL LANDSCAPED AREAS WITHIN LIMITS OF WORK, INCLUDING POTENTIAL OFFSITE AREAS. COVERAGE SHALL INCLUDE BOTH LANDSCAPING AND IRRIGATION.

LEGAL DESCRIPTION

FIRST AMERICAN TITLE INSURANCE COMPANY
COMMITMENT NO.: NCS 968541-CLE
COMMITMENT DATE: AUGUST 28, 2020

THE NORTH 170.0 FEET OF THAT PART OF THE SOUTHEAST 1/4 OF SECTION 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING EAST OF THE EAST RIGHT OF WAY OF STATE ROUTE 83, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT 102.85 FEET EAST OF THE NORTHWEST CORNER OF SAID SOUTHEAST 1/4, THENCE EAST ALONG THE NORTH LINE OF SAID SOUTHEAST 1/4, A DISTANCE OF 256.24 FEET; THENCE SOUTH ALONG A LINE PARALLEL WITH THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170 FEET; THENCE WEST ALONG A LINE PARALLEL WITH THE NORTH LINE OF SAID SOUTHEAST 1/4, A DISTANCE OF 256.24 FEET TO THE INTERSECTION OF SAID PARALLEL LINE WITH THE EAST RIGHT OF WAY OF SAID STATE ROUTE 83, WHICH POINT IS 102.45 FEET EAST OF THE WEST LINE OF SAID SOUTHEAST 1/4; THENCE NORTH, ALONG THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170.0 FEET TO THE POINT OF BEGINNING, IN DUPAGE COUNTY, ILLINOIS.

BENCHMARKS:

SOURCE BENCHMARK:

NGS 0135 - PID (DK3296)

TO REACH THE STATION FROM THE INTERSECTION OF SR 83 AND PLAINFIELD ROAD, GO EAST ALONG PLAINFIELD ROAD FOR APPROXIMATELY 0.5 MI (0.8 KM) TO THE STATION ON THE RIGHT. THE STATION IS LOCATED AT THE SOUTHEAST CORNER OF THE INTERSECTION OF PLAINFIELD ROAD AND MADISON STREET.

THE STATION IS 7.0 FT (2.1 M) SOUTH OF THE CENTERLINE OF PLAINFIELD ROAD AND 38.5 FT (11.7 M) EAST OF THE CENTERLINE OF MADISON STREET. THE MONUMENT IS A 3.5 INCH (9 CM) BRASS DISK ON THE BASE OF A TRAFFIC SIGNAL, 0.8 FT (0.2 M) ABOVE GRADE.

ELEVATION: 732.18 (NAVD 88)

SITE BENCHMARKS AND CONTROL
BM #204

SET BENCHMARK ON THE SOUTHERLY BOLT, UNDER THE WORD "CITY" ON A HYDRANT ON THE SOUTH SIDE OF 75TH STREET, 308' EAST OF THE INTERSECTION OF 75TH STREET AND ROUTE 83. ROBERT KINGERY HIGHWAY
ELEVATION: 719.56 (NAVD 88)

CONTROL #1

SET CAPPED IRON ROD AT THE SOUTHEAST CORNER OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY, 15' SOUTHEAST OF A TRAFFIC SIGNAL AND 18' EAST OF THE BACK OF CURB OF ROUTE 83
N: 1852589.52 (NAD 83)
E: 1090379.82 (NAD 83)

CONTROL #2

SET OUT "X" IN A CONCRETE SIDEWALK ON THE EAST SIDE OF ROUTE 83, ROBERT KINGERY HIGHWAY, 235' SOUTH OF THE INTERSECTION OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY, 26' EAST OF THE BACK OF CURB OF ROUTE 83.

N: 1852417.41 (NAD 83)
E: 1090392.03 (NAD 83)

CONTROL #3

SET OUT "X" IN A CONCRETE SIDEWALK ON THE SOUTH SIDE OF 75TH STREET, 315' EAST OF THE INTERSECTION OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY, AND 7' NORTH OF A FIRE HYDRANT

N: 1852623.35 (NAD 83)
E: 1090603.32 (NAD 83)

SITE INFORMATION:

STAKE MARKED WITH AN INTEREST VARY FROM THE VILLAGE CODE AND WILL REQUIRE A VARIANCE

JURISDICTION: VILLAGE OF WILLOWBROOK
ZONING DISTRICT: B-2 COMMUNITY SHOPPING

SITE AREA CALCULATIONS:
TOTAL SITE AREA 1.0 ACRES
PERVIOUS AREA 0.29 ACRES
IMPERVIOUS AREA 0.71 ACRES

RESTAURANT BUILDING AREA 2,300 SF
TRASH ENCLOSURE AREA 307 SF
TOTAL BUILDING AREA 2,607 SF

	REQUIRED	PROVIDED
MIN. LOT AREA	2.0 ACRES	1.00 ACRES (43,561 SF)
MIN. LOT WIDTH	200'	170'
MIN. LOT DEPTH	200'	256.24'

MAX. LOT COVERAGE	50%	6.0%
MAX. BUILDING HEIGHT	30'	23'
MAX. FLOOR AREA RATIO	0.30	0.06

EXISTING BUILDING SETBACK	REQUIRED	PROVIDED
FRONT (WEST)	60'	61.5'
ROUTE 83 (WEST)	100'	61.5'
SIDE EXTERIOR (NORTH)	40'	52.2'
SIDE INTERIOR (SOUTH)	100'	56.2'
REAR (EAST)	30'	127.6'

PROPOSED BUILDING SETBACK	REQUIRED	PROVIDED
FRONT (WEST)	60'	50.0'
ROUTE 83 (WEST)	100'	50.0'
SIDE EXTERIOR (NORTH)	60'	43.4'
SIDE INTERIOR (SOUTH)	30'	63.6'
REAR (EAST)	40'	162.8'

PARKING SETBACK	REQUIRED	PROVIDED
FRONT (WEST)	25'	25'
SIDE EXTERIOR (NORTH)	15'	15'
SIDE INTERIOR (SOUTH)	10'	10'
REAR (EAST)	10'	10.3'

DRIVE AISLE WIDTH: 24' UNLESS OTHERWISE NOTED

FLOOD HAZARD: ZONE X
FIRM MAP NO. 17043C0189U, DATED 8/1/2019

EXISTING INFORMATION:
EXISTING SITE CONDITIONS ARE BASED ON AN ALTAIRNSP LAND TITLE SURVEY BY ATWELL, LLC DATED 9/23/2020

SITE LIGHTING:
PHOTOMETRICS DESIGNED BY OTHERS. POLE LOCATIONS ARE SHOWN FOR REFERENCE ONLY. CONTRACTOR SHALL VERIFY FINAL LOCATIONS WITH PHOTOMETRICS PLAN AND OWNER PRIOR TO CONSTRUCTION.

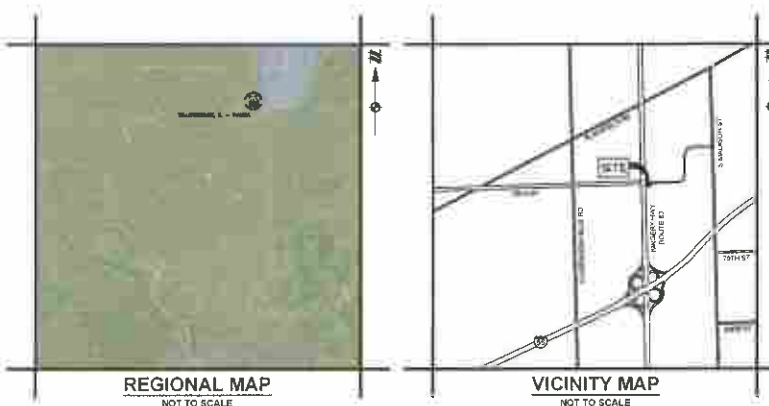
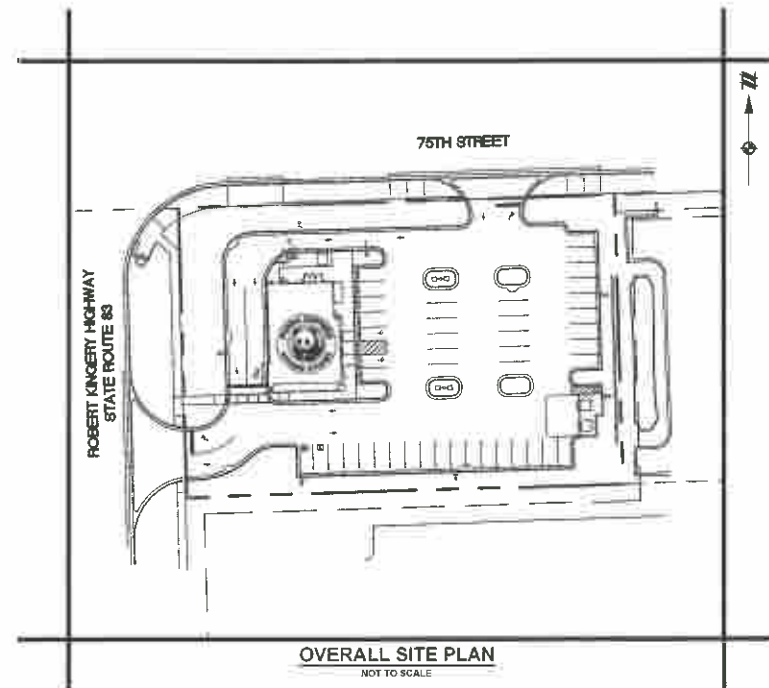
PARKING DATA

	REQUIRED	PROVIDED
LOADING BERTHS	0	0

REQUIRED PARKING CALCULATION:
1 PARKING SPACE PER 100 SF OF FLOOR AREA
1 SPACE/100 SF = 2,300 SF = 23 SPACES

TOTAL REQUIRED SPACES 23 SPACES

PROVIDED		
REGULAR (9' X 18')	42 SPACES	
DRIVE THRU PICKUP (9' X 18')	2 SPACES	
ADA (8' X 12')	2 SPACES	
TOTAL PROVIDED SPACES	46 SPACES	



SHEET INDEX

- | | |
|-------|--|
| C01.0 | COVER SHEET |
| C01.1 | GENERAL NOTES |
| C01.2 | GENERAL NOTES |
| C02.0 | EXISTING CONDITIONS |
| C02.1 | DEMOLITION PLAN |
| C03.0 | SOIL EROSION AND SEDIMENTATION CONTROL PLAN |
| C03.1 | SOIL EROSION AND SEDIMENTATION CONTROL NOTES AND DETAILS |
| C04.0 | SITE LAYOUT PLAN |
| C04.1 | DETAILED SITE LAYOUT PLAN |
| C05.0 | GRADING PLAN |
| C05.1 | DETAILED GRADING |
| C06.0 | STORM SEWER PLAN |
| C07.0 | UTILITY PLAN |
| C07.1 | DETAILED UTILITY PLAN |
| C08.0 | CONSTRUCTION DETAILS |
| C08.1 | CONSTRUCTION DETAILS |
| C08.2 | CONSTRUCTION DETAILS |
| C08.3 | CONSTRUCTION DETAILS |
| C08.4 | CONSTRUCTION DETAILS |
| L01.0 | LANDSCAPE PLAN |
| L01.1 | LANDSCAPE NOTES AND DETAILS |



PANDA EXPRESS, INC.
1683 Walnut Grove Ave.
Rosemead, California
91770
Telephone: 626.799.9898
Facsimile: 626.372.8288

All ideas, designs, arrangements and plans indicated or represented by this drawing are the property of Panda Express Inc. and shall remain confidential and shall not be used by or disclosed to any person, firm, or corporation without the written permission of Panda Express Inc.

REVISIONS:

VILLAGE PLAN REV #1 01/19/2021

ISSUE DATE:

11/20/2020

DRAWN BY: LEH

PANDA PROJECT #: D7058

ATWELL PROJECT #: 18003769

NORR
ARCHITECTS ENGINEERS PLANNERS

ATWELL
866.850.4200 www.atwell-group.com
1250 EAST DIEHL ROAD, SUITE 300
NAPERVILLE, IL 60563
DESIGN FIRM: gja-008078

PANDA EXPRESS

WARM & WELCOME 2300
7505 KINGERY HIGHWAY
WILLOWBROOK, IL 60527

COVER SHEET

C01.0

TRUE WARM & WELCOME 2300



Know what's below.
Call before you dig.

THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE MANNER AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVE. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREE TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MAY BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXCAVATE, LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

NOTICE: CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. NEITHER THE OWNER NOR THE ENGINEER SHALL BE DEEMED TO ASSUME ANY RESPONSIBILITY FOR SAFETY OF THE WORK OF PERSONS ENGAGED IN THE WORK OF ANY NEARBY STRUCTURES, OR OF ANY OTHER PERSONS. COPYRIGHT © 2021 ATWELL, LLC. NO REPRODUCTION SHALL BE MADE WITHOUT THE PRIOR WRITTEN CONSENT OF ATWELL, LLC.

24-HOUR CONTACT:
PANDA PM
HAKIM YALA
847-477-5125



1. ALL SITE WORK AND GRADING OPERATIONS WITHIN THE LIMITS OF THIS PROJECT SHALL BE DONE IN ACCORDANCE WITH THE CURRENT EDITION, AND ALL REVISIONS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION. THE CONTRACTOR SHALL ALSO FOLLOW ALL STATE, COUNTY AND LOCAL JURISDICTIONAL ORDINANCES AND REQUIREMENTS.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL REQUIRED PERMITS FOR CONSTRUCTION ALONG OR ACROSS EXISTING ALLEYS, STREETS OR HIGHWAYS. THE CONTRACTOR SHALL MAKE ALL ARRANGEMENTS FOR THE PROPER BRACING, SHORING AND OTHER REQUIRED PROTECTION OF ALL ROADWAYS BEFORE CONSTRUCTION BEGINS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO THE ALLEYS, STREETS OR ROADWAYS AND ASSOCIATED STRUCTURES AND SHALL MAKE REPAIRS AS NECESSARY TO THE SATISFACTION OF THE OWNER.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF IDOT STANDARD TRAFFIC CONTROL, AND PROTECTION DURING CONSTRUCTION TO INFORM AND PROTECT THE PUBLIC IN COMPLIANCE WITH IDOT STANDARD TRAFFIC CONTROL AND PROTECTION SPECIFICATIONS.
4. EASEMENTS FOR EXISTING UTILITIES, BOTH PUBLIC AND PRIVATE, AND UTILITIES WITHIN PUBLIC RIGHT OF WAY ARE SHOWN ON THE PLANS ACCORDING TO INFORMATION AVAILABLE IN THE RECORDS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE EXACT LOCATION IN THE FIELD OF ALL UTILITIES, AND PROVIDING PROTECTION FROM DAMAGE DUE TO CONSTRUCTION OPERATIONS. IF EXISTING UTILITY LINES OF ANY NATURE ARE ENCOUNTERED WHICH CONFLICT IN LOCATION WITH NEW CONSTRUCTION THE CONTRACTOR SHALL NOTIFY THE OWNER SO THAT THE CONFLICT MAY BE RESOLVED.
5. BEFORE ACCEPTANCE BY THE OWNER AND FINAL PAYMENT, ALL WORK SHALL BE INSPECTED AND APPROVED BY THE OWNER AND MUNICIPALITY.
6. THE CONTRACTOR SHALL GUARANTEE HIS WORK FOR A PERIOD OF ONE YEAR FROM THE DATE OF ACCEPTANCE AND SHALL BE HELD RESPONSIBLE FOR ANY DEFECTS IN MATERIAL ON WORKSMANSHIP DURING THAT PERIOD.
7. ALL WORK AND MATERIALS SHALL COMPLY WITH ALL MUNICIPAL REGULATIONS, CODES AND O.S.H.A. STANDARDS.
8. CONTRACTOR SHALL REFER TO THE MOST RECENT ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF BUILDINGS.
9. ALL DIMENSIONS AND RADII ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
10. SITE BOUNDARY, TOPOGRAPHY, UTILITY AND ROAD INFORMATION TAKEN FROM A SURVEY PREPARED BY ATWELL, LLC DATED 9/23/2020.
11. TOTAL PROPERTY IS 1.00 ACRES.
12. REFER TO ARCHITECTURAL PLANS FOR SITE LIGHTING ELECTRICAL PLANS.
13. ALL STRIPING SHALL BE 4" WHITE PAINT UNLESS OTHERWISE NOTED. ALL STRIPING SHALL BE APPLIED WITH TWO COATS OF PAINT AT MANUFACTURER'S RECOMMENDED RATE.
14. THE BUILDING AND PARKING ARE PARALLEL AND/OR PERPENDICULAR TO THE WEST PROPERTY LINE, N01°41'03"W.

1. THE CONTRACTOR IS RESPONSIBLE FOR THE PROPER DISPOSAL OF ALL WASTE MATERIAL IN A LOCATION AND IN A MANNER APPROVED BY ALL GOVERNING AUTHORITIES INCLUDING, BUT NOT LIMITED TO, STRUCTURES, FOUNDATIONS, CONCRETE, ASPHALT, STEEL UTILITIES, DRAINAGE STRUCTURES, ETC.
2. CONTRACTOR TO DEMOLISH THE SITE SUCH THAT THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS CAN BE CONSTRUCTED.
3. ALL UNSUITABLE MATERIAL THAT WOULD CONFLICT WITH THE PROPER CONSTRUCTION OF THE PROPOSED IMPROVEMENTS SHALL BE REMOVED.
4. THE CONTRACTOR SHALL OBTAIN ALL REQUIRED PERMITS FROM ALL GOVERNING AGENCIES PRIOR TO THE START OF SITE DEMOLITION.
5. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING THAT ALL EXISTING UTILITY SERVICES ARE DISCONNECTED OR PROTECTED PRIOR TO THE START OF DEMOLITION OF THE SITE.
6. THE CONTRACTOR SHALL COORDINATE WITH THE RESPECTIVE UTILITY COMPANIES PRIOR TO REMOVAL AND/OR RELOCATION OF UTILITIES. THE CONTRACTOR IS RESPONSIBLE FOR PAYING ALL FEES AND CHARGES ASSOCIATED WITH UTILITY COORDINATION.
7. THE EXISTING UTILITIES SHOWN ON THE PLANS HAVE BEEN LOCATED BASED ON THE BEST INFORMATION AVAILABLE AND ARE GIVEN FOR THE CONVEYANCE OF REFERENCE OF THE CONTRACTOR. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THEIR ACCURACY. CONTRACTOR SHALL VERIFY THE LOCATIONS PRIOR TO STARTING CONSTRUCTION AND REPORT ANY DISCREPANCIES TO THE OWNER.
8. PRIOR TO THE START OF ANY SITE DEMOLITION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES FOR ONSITE LOCATIONS OF EXISTING UTILITIES.
9. CONTRACTOR MUST PROVIDE SAFETY FENCING FROM PUBLIC ACCESS TO THE SITE PRIOR TO THE START OF DEMOLITION ACTIVITY AND UNTIL SITE CONSTRUCTION HAS BEEN COMPLETED.
10. SITE DEMOLITION SHALL NOT CONFLICT WITH ACCESS TO ADJACENT SITES OR TRAFFIC AND PEDESTRIAN FLOW IN ADJACENT PUBLIC R.O.W.'S.
11. ALL EROSION CONTROL DEVICES ARE TO BE INSTALLED PRIOR TO THE START OF DEMOLITION ACTIVITY.
12. ANY IMPROVEMENTS, STRUCTURES, PAVEMENTS, UTILITIES, OR PROPERTY, EITHER ON-SITE OR OFF-SITE, THAT BECOMES DAMAGED DURING SITE CONSTRUCTION SHALL BE REPLACED BACK TO ITS ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE.
13. EARTH EXCAVATION AND/OR BUILDING DEMOLITION THAT RESULTS IN SOIL AND/OR BUILDING DEMOLITION DEBRIS HAIL-OFF SHALL BE EXECUTED IN ACCORDANCE WITH THE ILLINOIS POLLUTION CONTROL BOARD CLEAN CONSTRUCTION OR DEMOLITION DEBRIS RULE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT ALL DEMOLITION MATERIAL IS CLEAN OR CONTAMINATED AND WITHIN THE ALLOWABLE LIMITS OF THE RULE.

1. PAVEMENT DESIGN REQUIREMENTS FOR ALL PUBLIC AND PRIVATE STREETS, SHALL BE IN ACCORDANCE WITH ALL LOCAL/MUNICIPAL CODES AND THE QUALITY CONTROL OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
2. THE SUBGRADE OF ALL ROADS SHALL BE GRADED AND ROLLED IN ACCORDANCE WITH SECTION 301 OF THE STANDARD SPECIFICATIONS FOR THE ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION, BY THE DEPARTMENT OF TRANSPORTATION.
3. SUBGRADE MATERIAL HAVING AN IBV LESS THAN 8.0 SHALL BE REMOVED AND REPLACED WITH SUITABLE ALL MATERIAL OR THE PAVEMENT MUST BE DESIGNED TO COMPENSATE FOR THE EXISTING SOIL CONDITIONS.
4. THE BITUMINOUS CONCRETE BINDER AND SURFACE COURSE SHALL COMPLY WITH THE STATE OF ILLINOIS CLASS 1, AS SET FORTH IN SECTION 408 OF THE STANDARD SPECIFICATIONS. A PRIME TACK COAT SHALL BE APPLIED TO THE BASE COURSE AT A RATE OF 0.40 GALLONS PER SQUARE YARD.
5. AFTER A BASE CURING PERIOD OF NOT MORE THAN FIVE (5) DAYS AFTER PRIMING, A BITUMINOUS CONCRETE BINDER COURSE (MIXTURE B) SHALL BE CONSTRUCTED UPON THE BASE COURSE.
6. A PRIME TACK COAT SHALL BE APPLIED AT A RATE OF 0.15 GALLONS PER SQUARE YARD TO THE SURFACE OF THE SUBGRADE COURSE, CLASS 1, MODIFIED, SHALL BE CONSTRUCTED.
7. ONE (1) INCH PER-MADE BITUMINOUS EXPANSION JOINTS HAVING TWO (2) ONE (1) INCH DOWEL BARS SHALL BE INSTALLED AT 45 FOOT INTERVALS AND AT ALL POINTS OF CURVATURE ALONG THE CURB WHERE THE RADIUS IS LESS THAN 100 FEET. JOINTS SHALL BE PROTECTED WITH JOINT SEALER BE PROPERLY GREASED AND HAVE EXPANSION CASPS. CONTRACTION JOINT SHALL BE FORMED AT MAXIMUM OF 15-FOOT INTERVALS.
8. CONCRETE PAVEMENT CONTRACTION JOINTS SHALL BE PROVIDED AT SPACING OF 12' CENTERS, MAXIMUM EACH WAY. CONSTRUCTION JOINTS SHALL BE PLACED AT LOCATIONS WHERE PLACEMENT OPERATIONS HAVE BEEN STOPPED FOR A PERIOD OF MORE THAN 1 HOUR.
9. SIDEWALKS SHALL BE CONSTRUCTED OF PORTLAND CEMENT CONCRETE TO A MINIMUM OF 4 INCHES THICKNESS. ALL SIDEWALKS SHALL HAVE A MINIMUM OF A 6 BAG MIX WITH A MINIMUM OF 28 DAY COMPRESSIVE STRENGTH OF 4000 PSI. THE SIDEWALKS WHEN CONSTRUCTED SHALL CONTAIN 4 PERCENT AIR ENTRAINMENT. JOINTS SHALL BE NOT MORE THAN 20 PERCENT OF THE ENTIRE AREA. THE SLUMP SHALL BE NOT LESS THAN TWO (2) INCHES, NOT MORE THAN FOUR (4) INCHES. CONTRACTION JOINTS SHALL BE PROVIDED AT 5' MAXIMUM SPACING. EXPANSION JOINTS SHALL BE PROVIDED AT 45' MAXIMUM SPACING.
10. ALL EXPOSED CONCRETE WORK INCLUDING SIDEWALKS AND CURBS MUST HAVE A FINISHING COMPOUND APPLIED IN ACCORDANCE WITH THE STATE STANDARDS.

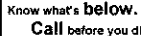
11. THE CONSTRUCTION OF ALL PAVEMENT, CURBS AND GUTTERS, AND SIDEWALK IMPROVEMENTS, INCLUDING THE USE OF ALL MACHINERY, EQUIPMENT AND TOOLS, SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONSTRUCTION OF ALL OTHER WORK NECESSARY TO COMPLETE THE PROPOSED IMPROVEMENTS, SHALL BE CONSTRUCTED ACCORDING TO THE REQUIREMENTS AS OUTLINED IN THE STANDARD SPECIFICATIONS. THE IMPROVEMENTS FOR THOSE IMPROVEMENTS SHALL BE IN CONFORMANCE WITH THE APPROPRIATE SECTION OF THE STATE STANDARD SPECIFICATIONS.
12. ALL EXISTING MANHOLES AND BASINS SHALL BE ADJUSTED TO MEET FINISHED GRADE. THIS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
13. ALL PHASES OF PAVING IMPROVEMENTS MAY BE SUBJECT TO PERIODIC INSPECTION BY A REPRESENTATIVE OF THE ENGINEER.
14. ALL PAVING IMPROVEMENTS INSTALLED WILL BE SUBJECT TO TESTING, AS PROVIDED FOR IN THESE SPECIFICATIONS AND IN THE STANDARD SPECIFICATIONS. IF ANY DEFECTS ARE FOUND TO BE DEFECTS IN WORKSMANSHIP OR MATERIALS, THEN THEY SHALL BE REMOVED AND REPLACED.
15. ALL STREETS, CURBS, GUTTERS AND SIDEWALKS WHICH ARE CONSTRUCTED WITHIN PUBLIC RIGHT-OF-WAY SHALL BE UNDER WARRANTY FOR ALL DEFECTS AND FAILURES FOR A PERIOD OF TWO (2) YEARS AFTER THE DATE OF COMPLETION.
16. PAVEMENT SUBGRADE PREPARATION SHALL BE COMPLETED IN ACCORDANCE WITH THE RECOMMENDATIONS OF THE GEOTECHNICAL INVESTIGATION REPORT PREPARED BY TERRACON CONSULTANTS, INC. DATED 128/26/2020.

1. ALL STORM SEWER CROSSING WATERMAIN SHALL BE CONSTRUCTED WITH "O" RING JOINTS PER ASTM C-443.
2. STORM SEWER PIPE SHALL BE AS SPECIFIED ON PLANS AND CONFORM TO THE FOLLOWING (WHEN APPLICABLE):
 - A. CORRUGATED SMOOTH WALLED INTERIOR HIGH DENSITY POLYETHYLENE (HDPE) PIPE SHALL BE USED FOR ALL STORM SEWERS FOR SIZES 12" AND LARGER AND AASHTO M252 TYPE S FOR SIZES SMALLER THAN 12". JOINTS SHALL BE BELL AND SPIGOT AND SHALL BE WATERTIGHT. RUBBER GASKET FOR JOINTS SHALL CONFORM TO ASTM F477.
 - B. REINFORCED CONCRETE PIPE C-76 CL. B UNDER NON PAVED AREAS, AND C-76 CL. IV UNDER PAVED AREAS UNLESS OTHERWISE NOTED.
 - C. MANHOLE CHAMBERS SHALL BE CAST-IN-PLACE CONCRETE WITH BELL AND SPIGOT TYPE WITH ELASTOMERIC SEAL JOINTS AND SMOOTH INNER WALLS IN ACCORDANCE WITH AASHTO M 304. JOINTS AND GASKETS SHALL CONFORM TO ASTM D3212 AND ASTM F 477.
3. STORM SEWER MANHOLES, CATCH BASINS AND INLETS SHALL BE PRECAST CONCRETE (ASTM C-478).
4. ALL STRUCTURE LIDS SHALL BE IMPRINTED "STORM", AND ALL GRATES SHALL BE IMPRINTED WITH THE "DUMP NO WASTE- DRAINS TO CREEK" EMBLEM. FROM THE PROPOSED STORM SEWER SYSTEM OR EXTENDED TO OUTLET INTO A PROPOSED DRAINAGE WAY. IF THIS CANNOT BE ACCOMPLISHED, THEN IT SHALL BE REPAIRED WITH NEW PIPE OF SIMILAR SIZE AND MATERIAL TO THAT OF THE EXISTING. THE CONTRACTOR SHALL MAINTAIN OPERATING CONDITION. A RECORD OF THE LOCATION OF ALL FIELD LIE OR DRAIN PIPE ENCOUNTERED SHALL BE KEPT BY THE CONTRACTOR AND TURNED OVER TO THE ENGINEER UPON COMPLETION OF THE PROJECT.
5. ALL FIELD LIE ENCOUNTERED DURING CONSTRUCTION OPERATIONS SHALL BE CONNECTED TO THE PROPOSED STORM SEWER SYSTEM OR EXTENDED TO OUTLET INTO A PROPOSED DRAINAGE WAY. IF THIS CANNOT BE ACCOMPLISHED, THEN IT SHALL BE REPAIRED WITH NEW PIPE OF SIMILAR SIZE AND MATERIAL TO THAT OF THE EXISTING. THE CONTRACTOR SHALL MAINTAIN OPERATING CONDITION. A RECORD OF THE LOCATION OF ALL FIELD LIE OR DRAIN PIPE ENCOUNTERED SHALL BE KEPT BY THE CONTRACTOR AND TURNED OVER TO THE ENGINEER UPON COMPLETION OF THE PROJECT.
6. ALL STORM DRAINS THAT FALL WITHIN PAVED AREAS SHALL BE BACKFILLED WITH GRANULAR MATERIAL TO THE FINISH GRADE. TO ILLINOIS DEPARTMENT OF TRANSPORTATION SPECIFICATIONS.
7. RIM ELEVATIONS FOR STORM SEWER STRUCTURES IN CURB LINE ARE FLOW LINE ELEVATIONS.

1. EARTH EXCAVATION SHALL INCLUDE CLEARING, STRIPPING AND STOCKPILING TOPSOIL, REMOVING UNSUITABLE MATERIALS, THE CONSTRUCTION OF EMBANKMENTS, CONSTRUCTING NONSTRUCTURAL RAIL, AND SPALL SHAPING AND TRIMMING TO THE LINES GRADES AND CROSS SECTIONS SHOWN IN THE PLANS. THIS WORK SHALL BE PERFORMED IN ACCORDANCE WITH APPLICABLE EDITIONS OF DIVISION 200 OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION. ALL UNSUITABLE OR EXCESS MATERIAL SHALL BE DISPOSED OF AS DETERMINED, AS SHOWN ON THE PLANS OR AS DIRECTED BY THE OWNER, PROOF ROLLING, DISING AND DEWATERING ARE EXCAVATORS RESPONSIBILITY AND CONSIDERED INCIDENTAL.
2. THE GRADING AND CONSTRUCTION OF PROPOSED IMPROVEMENTS SHALL NOT CAUSE PONDING OF STORM WATER. ALL AREAS ADJACENT TO THESE IMPROVEMENTS SHALL BE GRADED TO ALLOW POSITIVE DRAINAGE.
3. THE PROPOSED GRADING ELEVATIONS SHOWN ON THE PLANS ARE FINISH GRADE. A MINIMUM OF FOUR (4") INCHES OF TOPSOIL IS TO BE PLACED, OR AS SUBSTITUTED, NOTED OR IMPROVED LATER, BEFORE FINISH GRADE ELEVATIONS ARE ACHIEVED IN AREAS TO RECEIVE VEGETATIVE COVER.
4. SOILS INFORMATION TAKEN FROM A GEOTECHNICAL INVESTIGATION REPORT DATED 12/28/2020, PREPARED BY TERRACON CONSULTANTS, INC., IN THE EVENT OF A CONFLICT BETWEEN THESE NOTES AND THE GEOTECHNICAL INVESTIGATION REPORT, THE GEOTECHNICAL INVESTIGATION REPORT SHALL GOVERN.
5. CONTRACTOR SHALL REFER TO THE GEOTECHNICAL REPORT PRIOR TO EARTH MOVING AND UTILITY INSTALLATION. CONTRACTOR SHALL FOLLOW THE RECOMMENDATIONS IN THE GEOTECHNICAL REPORT.
6. THE CONTRACTOR SHALL FURNISH THE OWNER WITH CERTIFICATES OF COMPLIANCE WITH THE SPECIFICATIONS FOR ALL MATERIALS USED IN THE CONSTRUCTION OF THE IMPROVEMENTS. THE CERTIFICATE SHALL INCLUDE TEST RESULTS AND SKETCHES SHOWING COMPLIANCE WITH THE APPLICABLE ASTM SPECIFICATIONS.
7. ALL BUILDING PADS SHALL HAVE A MINIMUM BEARING CAPACITY OF 2,000 P.S.F. FOR BUILDING SUPPORT.
8. THE SELECTED FILL MATERIAL SHALL BE PLACED IN LEVEL, UNIFORM LAYERS SO THAT THE COMPACTED THICKNESS IS APPROXIMATELY NINE INCHES (9") FOR EACH LAYER. THE CONTRACTOR SHALL STABILIZE THE FABRIC TO ALL SLOPES AS DETERMINED BY THE "MODIFIED PROCTOR" COMPACTION TEST (ASTM D 1557). HOWEVER, IF COMPACTION EQUIPMENT DEMONSTRATES THE ABILITY TO COMPACT TO GREATER DENSITIES, THE CONTRACTOR SHALL FOLLOW THE SPECIFIED, EACH LAYER SHALL BE THOROUGHLY MIXED DURING SPREADING TO INSURE UNIFORMITY.
9. OVERLAND DRAINAGE ROUTING AND SWALES SHALL BE CONSTRUCTED AS SHOWN ON PLANS.
10. CONTRACTOR IS RESPONSIBLE FOR DEMOLITION OF EXISTING STRUCTURES INCLUDING REMOVAL OF ANY EXISTING UTILITIES SERVING THE STRUCTURE. UTILITIES ARE TO BE REMOVED TO THE RIGHT-OF-WAY.
11. ALL OUT OR FILL SLOPES SHALL BE 3:1 OR FLATTER UNLESS OTHERWISE SPECIFIED. CONTRACTOR SHALL STABILIZE STATIONARY FABRIC TO ALL SLOPES 4H:1V OR STEEPER. CONTRACTOR SHALL GRASS DISTURBED AREAS IN ACCORDANCE WITH STATE, LOCAL, AND COUNTY SPECIFICATIONS, INCLUDING IN ADDITION WITH THE ILLINOIS D.P.E.S. GENERAL PERMIT, UNTIL A HEALTHY STAND OF GRASS IS OBTAINED
12. EXISTING GRADE CONTOUR INTERVALS SHOWN AT 1 FOOT INTERVALS.
13. PROPOSED GRADE CONTOUR INTERVALS SHOWN AT 1 FOOT INTERVALS.
14. ALL SPOT GRADES ALONG CURB LINE ARE FLOWLINE GRADES UNLESS OTHERWISE NOTED. ALL R/W ELEVATIONS ARE FLOWLINE ELEVATIONS.
15. ALL STORM SEWER AND UTILITY STRUCTURE RIMS SHALL BE FLUSH WITH PAVEMENT OR FINISHED GRADE.
16. THE CONTRACTOR SHALL ADHERE TO ALL TERMS & CONDITIONS AS OUTLINED IN THE GENERAL N.P.D.E.S. PERMIT FOR STORMWATER DISCHARGE ASSOCIATED WITH CONSTRUCTION ACTIVITIES.
17. CONTRACTOR SHALL ASSURE POSITIVE DRAINAGE AWAY FROM BUILDINGS FOR ALL NATURAL, AND PAVED AREAS.
18. AT THE START OF EARTHWORK OPERATIONS, ANY VEGETATION, TOPSOIL, UNDESIRABLE MATERIALS, AND EXCESS MATERIAL SHALL BE REMOVED AND COMPLETELY REMOVED FROM PROPOSED BUILDING AND PAVEMENT AREAS.

1. WORK AND MATERIAL SHALL BE IN ACCORDANCE WITH ALL STATE, COUNTY, AND LOCAL GOVERNING JURISDICTIONS AND IN ACCORDANCE WITH THE ADDITIONAL SPECIFICATIONS FOR SEWER AND WATER MAIN CONSTRUCTION IN ILLINOIS.
2. ALL SANITARY SEWER PIPE TO BE PVC SDR 38 CONFORMING TO ASTM D-3034 SPECIFICATIONS, UNLESS OTHERWISE SPECIFIED. JOINTS SHALL BE FACTORY MANUFACTURED WITH LOCKING RUBBER RING CONFORMING TO ASTM D-3212. PVC PRESSURE PIPE SHALL CONFORM TO ASTM D-2241 AND ASTM D-3039 JOINT SPECIFICATIONS.
3. INFILTRATION OR EXFILTRATION SHALL NOT EXCEED 100 GALLONS PER 24 HOURS PER LINEAL FOOT PER INCH DIAMETER OF THE SEWER PIPE FOR ANY SECTION OF THE SYSTEM AND AT ANY TIME DURING ITS SERVICE LIFE. TESTING IS REQUIRED PER THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS".
4. ALL STRUCTURE LIDS SHALL BE IMPRINTED "SANITARY", FRAMES AND LIDS SHALL BE OF THE FOLLOWING: NENEMAN R-17729, NENEMAN R-17729, THE APPROPRIATE GOVERNING AGENCIES AND THE LOCAL MUNICIPALITY. SHALL APPROVE ANY VARIATION FROM THESE TYPES OF FRAMES AND LIDS.
5. CONTRACTOR TO PROVIDE A 2' x 2' x 8" CONCRETE COLLAR AROUND ALL CLEANOUTS OUTSIDE OF THE BUILDING.
6. SANITARY MANHOLES SHALL BE INSPECTED AND LEAKAGE TESTED FOR WATER TIGHTNESS IN ACCORDANCE WITH ASTM C898-24, PRIOR TO BEING PLACED INTO SERVICE.
7. PIPE DEFLECTION SHALL NOT EXCEED THE MANUFACTURER'S RECOMMENDED DEFLECTION LIMITS OR A MAXIMUM OF 50% OF THE INTERNAL DIAMETER OF THE PIPE. PIPE DEFLECTION SHALL BE MEASURED BY DEFLECTION TESTING SHALL BE IN ACCORDANCE WITH SECTION 31-113 OF THE "STANDARD SPECIFICATIONS FOR SEWER AND WATER MAIN CONSTRUCTION IN ILLINOIS".
8. ALL SANITARY SEWERS SHALL PASS AN AIR TEST IN CONFORMANCE WITH THE "STANDARD SPECIFICATIONS FOR SEWER AND WATER MAIN CONSTRUCTION IN ILLINOIS". THE SEWER SHALL BE TESTED AT AN INITIAL PRESSURE OF 3.5 PSI FOR THE TIME PERIOD OF 15 MINUTES. THE TIME FOR THE PRESSURE DROP OF 1 PSI SHALL BE DETERMINED.

- WORK AND MATERIAL SHALL BE IN ACCORDANCE WITH ALL STATE, COUNTY, AND LOCAL GOVERNING JURISDICTIONS AND IN ACCORDANCE WITH THE CURRENT EDITION OF STANDARD SPECIFICATIONS FOR SEWER AND WATER MAIN CONSTRUCTION IN ILLINOIS.
- WATER MAIN PIPE SHALL BE DUCTILE IRON CLASS 52, CONFORMING TO AWWA C151 (ANSI A21.51) WITH CEMENT MORTAR LINING. DUCTILE IRON PIPE WATERMAIN SHALL HAVE POLYETHYLENE WRAP IN ACCORDANCE WITH AWWA C-105-82.
- DUCTILE IRON PIPE JOINTS SHALL CONFORM TO AWWA C111 (ANSI A21.11).
- DUCTILE IRON FITTINGS SHALL CONFORM TO AWWA C110 (ANSI 21.10) OR AWWA C153 (ANSI A21.53).
- WATER SERVICES SHALL BE TYPE K COPPER PIPE IN ACCORDANCE WITH AWWA M-22.
- THRUST BLOCKS SHALL BE PROVIDED AT ALL BENDS, TEES AND FIRE HYDRANTS.
- ALL STRUCTURE LIDS SHALL BE IMPRINTED "WALD" FRAMES AND LIDS SHALL BE SEWANA 18" DIA. WITH TYPE B LID, NEMA# 8-1712-18. THE APPROPRIATE GOVERNING AGENCIES AND THE LOCAL MUNICIPALITY SHALL APPROVE ANY VARIATION FROM THIS TYPE OF FRAME AND LID.
- ALL WATER MAIN AND SERVICES SHALL BE INSTALLED A MINIMUM OF 5.5 FEET BELOW FINISHED GRADE UNLESS OTHERWISE SPECIFIED AND APPROVED BY THE ENGINEER.
- HORIZONTAL SEPARATION:
 - A) WATER MAINS SHALL BE LAID AT LEAST TEN FEET HORIZONTALLY FROM ANY EXISTING OR PROPOSED DRAIN, STORM SEWER, SANITARY SEWER OR SEWER SERVICES CONNECTION.
 - B) WATER MAINS MAY BE LAID CLOSER THAN TEN FEET TO A SEWER LINE WHEN:
 - 1) LOCAL CONDITIONS PREVENT A LATERAL SEPARATION OF TEN FEET;
 - 2) THE WATER MAIN INVERT IS AT LEAST 18 INCHES ABOVE THE CROWN OF THE SEWER; AND
 - 3) THE WATER MAIN IS EITHER IN A SEPARATE TRENCH OR IN THE SAME TRENCH ON AN UNDISTURBED EARTH SHELVE LOCATED TO ONE SIDE OF THE SEWER.
 - C) BOTH THE WATER MAIN AND DRAIN OR SEWER SHALL BE CONSTRUCTED OF WATER MAIN QUALITY PIPE WHEN IT IS IMPOSSIBLE TO MEET (A) OR (B) ABOVE. THE DRAIN OR SEWER SHALL BE PRESSURE TESTED TO THE MAXIMUM EXPECTED SURCHARGE HEAD BEFORE BACKFILLING.
- VERTICAL SEPARATION:
 - A) A WATER MAIN SHALL BE LAID SO THAT ITS INVERT IS 18 INCHES ABOVE THE CROWN OF THE EXISTING OR PROPOSED DRAIN, STORM SEWER, SANITARY SEWER OR SEWER SERVICE CONNECTIONS. THE VERTICAL SEPARATION SHALL BE MAINTAINED FOR THAT PORTION OF THE TRENCH LOCATED WITHIN FIVE FEET OF ANY SEWER OR DRAIN CROSSING. A LENGTH OF WATER MAIN PIPE SHALL BE CENTERED OVER THE SEWER TO BE CROSSED WITH JOINTS EQUIDISTANCE FROM THE SEWER OR DRAIN.
 - B) BOTH THE PROPOSED WATER MAIN AND PROPOSED SEWER SHALL BE CONSTRUCTED OF WATER MAIN QUALITY PIPE WHEN:
 - 1) IT IS IMPOSSIBLE TO OBTAIN THE PROPER VERTICAL SEPARATION AS DESCRIBED IN (A) ABOVE; OR
 - 2) THE WATER MAIN PASSES UNDER A SEWER OR DRAIN.
 - C) WHEN A PROPOSED SEWER IS CONSTRUCTED ABOVE AN EXISTING WATER MAIN OR A PROPOSED WATER MAIN IS CONSTRUCTED BELOW AN EXISTING SEWER, WATERMAIN QUALITY CASING MAY BE CONSTRUCTED AROUND THE PROPOSED PIPE IN A LIEU OF CONSTRUCTING BOTH PIPES OF WATERMAIN QUALITY PIPE.
 - D) A 12" VERTICAL SEPARATION OF 18 INCHES BETWEEN THE INVERT OF THE SEWER OR DRAIN AND THE CROWN OF THE WATER MAIN SHALL BE MAINTAINED WHERE A WATER MAIN CROSSES UNDER A SEWER, SUPPORT THE SEWER OR DRAIN LINES TO PREVENT SETTLING AND BREAKING THE WATER MAIN.
 - E) CONSTRUCTION SHALL EXTEND ON EACH SIDE OF THE CROSSING UNTIL THE NORMAL DISTANCE FROM THE WATER MAIN TO THE SEWER OR DRAIN LINE IS AT LEAST TEN FEET.
- THE CONTRACTOR SHALL VERIFY WITH THE PUBLIC SERVICES DEPARTMENT FOR THE CITY OF CHICAGO THE LOCATION OF FIRE HYDRANTS. THE BASE ELEVATION OF THE HYDRANT SHALL BE PROPERLY THRUST BLOCKED AND SHALL BE PROVIDED WITH CLEAN WASHED STONE AND A POLYETHYLENE COVERING.
- ALL FIRE HYDRANTS SHALL BE FURNISHED AND INSTALLED COMPLETE WITH A SIX (6) INCH AUXILIARY VALVE AND CAST IRON VALVE BOX. VALVES SHALL BE RESILIENT VALVE BOXES SHALL BE CLOW F-2454 WITH CLOW F-2495 COVER OR APPROVED EQUAL.
- VALVES SHALL CONFORM TO THE STANDARDS OF THE LATEST AWWA C500, AND SHALL HAVE MECHANICAL JOINT ENDS. VALVES SHALL BE RESILIENT TAPPING SLEEVES SHALL BE ALL STAINLESS STEEL WITH A STAINLESS STEEL FLANGE OR ALL DUCTILE IRON.
- ALL VALVES SHALL BE INSTALLED IN VALVE VAULTS HAVING A MINIMUM DIAMETER OF 4 FEET BELOW THE CONCENTRIC CORE SECTION. THE VAULTS SHALL BE CONSTRUCTED OF PRECAST CONCRETE RINGS. ALL VALVE VAULTS SHALL BE LEAK PROOF.

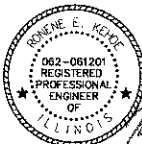


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NOTICE:
CONSTRUCTION SITE SAFETY IS THE
SOLE RESPONSIBILITY OF THE
CONTRACTORS. NEITHER THE OWNER
NOR THE ENGINEER SHALL BE
EXPECTED TO ASSUME ANY
RESPONSIBILITY FOR SAFETY OF THE
WORK, OF PERSONS ENGAGED IN THE
WORK, OF ANY NEARBY STRUCTURES,
OR OF ANY OTHER PERSONS.

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DATE: 1/19/2021
EXP: 11/30/2021

21

EXISTING CONDITIONS LEGEND

	BOUNDARY LINE
	EXISTING ROW
	BOUNDARY ADJACENT LINE
	EXISTING EASEMENT LINE
	EXISTING CONTOUR
	EXISTING CURB AND GUTTER
	EXISTING SANITARY SEWER
	EXISTING STORM SEWER
	EXISTING WATER LINE
	EXISTING UNDERGROUND GAS
	EXISTING UNDERGROUND TELEPHONE
	EXISTING UNDERGROUND ELECTRIC
	EXISTING UNDERGROUND CABLE
	EXISTING CENTERLINE OF DITCH
	EXISTING GRAVEL
	EXISTING FENCE
	EXISTING VEGETATION LINE
	EXISTING MONITORING WELL
	SOIL BORING, REFER TO GEOTECHNICAL REPORT
	FOUND IRON ROD
	SITE CONTROL POINT
	EXISTING BOLLARD
	EXISTING FENCE POST
	EXISTING SIGN
	EXISTING TREE
	EXISTING CLEANOUT
	EXISTING SANITARY MANHOLE
	EXISTING FLARED END SECTION
	EXISTING STORM CATCH BASIN/MANHOLE
	EXISTING WATER SHUTOFF
	EXISTING WATER VALVE
	EXISTING HYDRANT WITH SHUTOFF
	EXISTING FIRE HYDRANT
	EXISTING WATER METER
	EXISTING IRRIGATION CONTROL VALVE
	UNDERGROUND GAS PIPE WARNING POST
	EXISTING TELEPHONE RISER
	EXISTING TRAFFIC SIGNAL
	EXISTING ELECTRIC TRANSFORMER
	EXISTING ELECTRIC METER
	EXISTING UTILITY POLE
	EXISTING LIGHT POLE
	EXISTING CABLE RISER
	EXISTING ASPHALT
	EXISTING CONCRETE
	EXISTING BUILDING

DEMOLITION LEGEND

	BOUNDARY LINE
	EXISTING ROW
	REMOVE CONCRETE CURB AND GUTTER
	FULL DEPTH SAWCUT LIMITS
	TREE PROTECTION FENCE, SEE DETAIL SHEET C03.1
	EXISTING FENCE/UTILITY TO BE REMOVED
	REMOVE EXISTING ASPHALT/CONCRETE PAVEMENT
	REMOVE GRAVEL
	REMOVE EXISTING VEGETATION
	EXISTING BUILDING (TO BE REMOVED BY OTHERS)

LAYOUT LEGEND

	BOUNDARY LINE
	EXISTING ROW
	EXISTING CURB AND GUTTER
	PROPOSED CURB AND GUTTER
	PROPOSED DEPRESSED CURB AND GUTTER
	EXISTING EASEMENT LINE
	PROPOSED EASEMENT LINE
	PROPOSED LANDSCAPE BERM
	SETBACK LINE
	PARKING SPACE COUNT
	ADA RAMP
	DIMENSION TO BACK OF CURB
	DIMENSION TO PROPERTY LINE
	DIMENSION TO CENTER LINE
	DIMENSION TO EDGE OF PAVEMENT
	DIMENSION TO BUILDING
	PROPOSED LIGHT POLE/WALL PACK LIGHT
	PROPOSED SIGN
	PROPOSED ACCESSIBLE PARKING SPACE SIGN
	PROPOSED STOP SIGN
	PROPOSED DO NOT ENTER SIGN
	PROPOSED RIGHT TURN ONLY SIGN
	PROPOSED REINFORCED CONCRETE PAVEMENT, SEE DETAIL SHEET C08.2
	PROPOSED CONCRETE SIDEWALK, SEE DETAIL SHEET C08.2
	PROPOSED STANDARD DUTY ASPHALT PAVEMENT, SEE DETAIL SHEET C08.2
	PROPOSED HEAVY DUTY ASPHALT PAVEMENT, SEE DETAIL SHEET C08.2
	TRASH ENCLOSURE PAVEMENT, SEE DETAIL SHEET C08.2

SOIL EROSION AND SEDIMENTATION CONTROL LEGEND

	BOUNDARY LINE
	EXISTING ROW
	LIMITS OF DISTURBANCE
	EXISTING CONTOUR
	PROPOSED CONTOUR
	TEMPORARY SILT FENCE, SEE DETAIL SHEET C03.1
	PROPOSED RIDGE LINE
	TREE PROTECTION FENCE, SEE DETAIL SHEET C03.1
	EXISTING STORM SEWER
	PROPOSED STORM SEWER
	TEMPORARY CONSTRUCTION ENTRANCE, SEE DETAIL SHEET C03.1
	EROSION CONTROL BLANKET WITH PERMANENT STABILIZATION PER LANDSCAPE PLAN
	PERMANENT STABILIZATION PER LANDSCAPE PLAN
	TEMPORARY DROP-IN INLET PROTECTION, SEE DETAIL SHEET C03.1

GRADING LEGEND

	BOUNDARY LINE
	EXISTING ROW
	EXISTING CONTOUR
	PROPOSED CONTOUR
	PROPOSED RIDGE LINE
	EXISTING STORM SEWER
	PROPOSED STORM SEWER
	PROPOSED PITCH-OUT CURB AND GUTTER
	EXISTING STORM CATCH BASIN/MANHOLE
	PROPOSED STORM CATCH BASIN/MANHOLE
	PROPOSED SPOT ELEVATION
	PROPOSED RIM ELEVATION
	MATCH EXISTING ELEVATION
	PROPOSED FINISHED GRADE ELEVATION
	PROPOSED TOP OF CURB ELEVATION
	PROPOSED EDGE OF PAVEMENT ELEVATION
	PROPOSED FINISHED GRADE AT TOP OF WALL
	PROPOSED FINISHED GRADE AT BOTTOM OF WALL

STORM LEGEND

	BOUNDARY LINE
	EXISTING ROW
	EXISTING EASEMENT LINE
	EXISTING SANITARY SEWER
	PROPOSED SANITARY SEWER
	EXISTING STORM SEWER
	PROPOSED STORM SEWER
	EXISTING WATER LINE
	PROPOSED WATER LINE
	EXISTING UNDERGROUND GAS
	PROPOSED UNDERGROUND GAS
	EXISTING UNDERGROUND TELEPHONE
	PROPOSED UNDERGROUND TELEPHONE
	EXISTING UNDERGROUND ELECTRIC
	PROPOSED UNDERGROUND ELECTRIC
	EXISTING UNDERGROUND CABLE
	PROPOSED UNDERGROUND CABLE
	PROPOSED LIGHT POLE/WALL PACK LIGHT
	EXISTING MANHOLE/CATCH BASIN
	PROPOSED MANHOLE/CATCH BASIN/INLET
	PROPOSED CLEANOUT
	EXISTING END SECTION
	PROPOSED WATER VALVE
	PROPOSED FIRE HYDRANT
	PROPOSED FIRE DEPARTMENT CONNECTION
	PROPOSED UTILITY CROSSING, SEE UTILITY CROSSING CHART THIS SHEET

UTILITY LEGEND

	BOUNDARY LINE
	EXISTING ROW
	EXISTING EASEMENT LINE
	PROPOSED EASEMENT LINE
	EXISTING SANITARY SEWER
	PROPOSED SANITARY SEWER
	EXISTING STORM SEWER
	PROPOSED STORM SEWER
	EXISTING WATER LINE
	PROPOSED WATER LINE
	EXISTING UNDERGROUND GAS
	PROPOSED UNDERGROUND GAS
	EXISTING UNDERGROUND TELEPHONE
	PROPOSED UNDERGROUND TELEPHONE
	EXISTING UNDERGROUND ELECTRIC
	PROPOSED UNDERGROUND ELECTRIC
	EXISTING UNDERGROUND CABLE
	PROPOSED UNDERGROUND CABLE
	PROPOSED LIGHT POLE/WALL PACK LIGHT
	EXISTING MANHOLE/CATCH BASIN
	PROPOSED MANHOLE/CATCH BASIN/INLET
	PROPOSED CLEANOUT
	EXISTING END SECTION
	PROPOSED WATER VALVE
	PROPOSED FIRE HYDRANT
	PROPOSED FIRE DEPARTMENT CONNECTION
	PROPOSED TV PULL BOX
	PROPOSED UTILITY CROSSING, SEE UTILITY CROSSING CHART THIS SHEET

LANDSCAPE LEGEND

	BOUNDARY LINE
	EXISTING CONTOUR
	PROPOSED CONTOUR
	EXISTING ROW
	EXISTING EASEMENT LINE
	EXISTING SANITARY SEWER
	PROPOSED SANITARY SEWER
	EXISTING STORM SEWER
	PROPOSED STORM SEWER
	EXISTING WATER LINE
	PROPOSED WATER LINE
	EXISTING UNDERGROUND GAS
	PROPOSED UNDERGROUND GAS
	EXISTING UNDERGROUND TELEPHONE
	PROPOSED UNDERGROUND TELEPHONE
	EXISTING UNDERGROUND ELECTRIC
	PROPOSED UNDERGROUND ELECTRIC
	EXISTING UNDERGROUND CABLE
	PROPOSED UNDERGROUND CABLE
	PROPOSED IRRIGATION CONDUIT
	PROPOSED ALUMINUM EDGING
	EXISTING LIGHT POLE
	PROPOSED LIGHT POLE/WALL PACK LIGHT
	PROPOSED SHRUBS
	PROPOSED EVERGREEN TREES
	PROPOSED ORNAMENTAL TREES
	PROPOSED DECIDUOUS TREES
	PROPOSED KENTUCKY BLUE GRASS SOD
	PROPOSED STONE MULCH



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REVISIONS:

VILLAGE PLAN REV. #1 01/19/2021

ISSUE DATE:

PC SUBMITTAL 11/20/2020

DRAWN BY: LEH

PANDA PROJECT #: D7058
ATWELL PROJECT #: 18003769



PANDA EXPRESS
WARM & WELCOME 2300
7505 KINGERY HIGHWAY
WILLOWBROOK, IL 60527

GENERAL NOTES

C01.2

TRUE WARM & WELCOME 2300



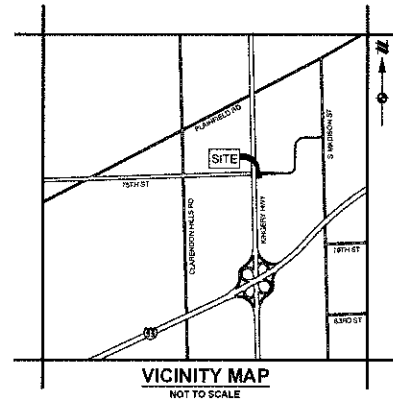
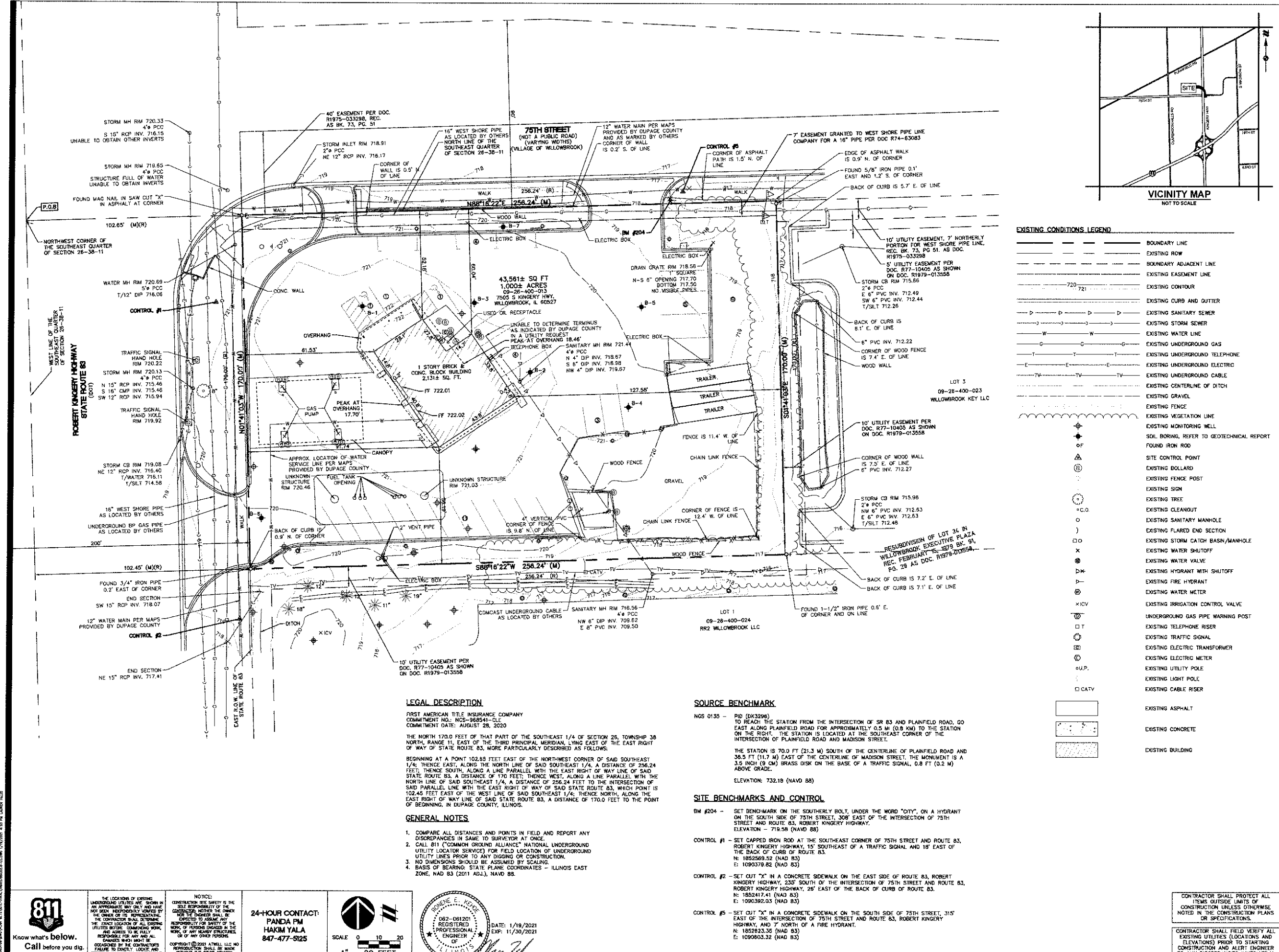
Know what's below.
Call before you dig.

THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE MANNER ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVE. THE CONTRACTOR SHALL OBTAIN THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. ANY DAMAGE TO ANY AND ALL UTILITIES WHICH MAY BE OCCURRED BY THE CONTRACTOR'S FAILURE TO CAREFULLY LOCATE AND PROTECT ANY AND ALL UNDERGROUND UTILITIES.

NOTICE:
CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. NEITHER THE OWNER NOR THE ENGINEER SHALL BE EXPECTED TO ASSUME ANY RESPONSIBILITY FOR SAFETY OF THE WORK OF PERSONS ENGAGED IN THE WORK OF ANY HEAVY STRUCTURES OR OF ANY OTHER PERSONS.
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24-HOUR CONTACT:
PANDA PM
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EXISTING CONDITIONS LEGEND	
	BOUNDARY LINE
	EXISTING ROW
	BOUNDARY ADJACENT LINE
	EXISTING EASEMENT LINE
	EXISTING CONTOUR
	EXISTING CURB AND GUTTER
	EXISTING SANITARY SEWER
	EXISTING STORM SEWER
	EXISTING WATER LINE
	EXISTING UNDERGROUND GAS
	EXISTING UNDERGROUND TELEPHONE
	EXISTING UNDERGROUND ELECTRIC
	EXISTING UNDERGROUND CABLE
	EXISTING CENTERLINE OF DITCH
	EXISTING GRAVEL
	EXISTING FENCE
	EXISTING VEGETATION LINE
	EXISTING MONITORING WELL
	SOIL BORING, REFER TO GEOTECHNICAL REPORT
	FOUND IRON ROD
	SITE CONTROL POINT
	EXISTING BOLLARD
	EXISTING FENCE POST
	EXISTING SIGN
	EXISTING TREE
	EXISTING CLEANOUT
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	EXISTING WATER METER
	EXISTING IRRIGATION CONTROL VALVE
	UNDERGROUND GAS PIPE WARNING POST
	EXISTING TELEPHONE RISER
	EXISTING TRAFFIC SIGNAL
	EXISTING ELECTRIC TRANSFORMER
	EXISTING ELECTRIC METER
	EXISTING UTILITY POLE
	EXISTING LIGHT POLE
	EXISTING CABLE RISER
	EXISTING ASPHALT
	EXISTING CONCRETE
	EXISTING BUILDING

LEGAL DESCRIPTION

FIRST AMERICAN TITLE INSURANCE COMPANY
COMMITMENT NO.: NCS-968541-CLE
COMMITMENT DATE: AUGUST 28, 2020

THE NORTH 170.0 FEET OF THAT PART OF THE SOUTHEAST 1/4 OF SECTION 25, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING EAST OF THE EAST RIGHT OF WAY OF STATE ROUTE 83, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT 102.65 FEET EAST OF THE NORTHWEST CORNER OF SAID SOUTHEAST 1/4; THENCE EAST, ALONG THE NORTH LINE OF SAID SOUTHEAST 1/4, A DISTANCE OF 256.24 FEET; THENCE SOUTH, ALONG A LINE PARALLEL WITH THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170 FEET; THENCE WEST, ALONG A LINE PARALLEL WITH THE NORTH LINE OF SAID SOUTHEAST 1/4, A DISTANCE OF 256.24 FEET TO THE INTERSECTION OF SAID PARALLEL LINE WITH THE EAST RIGHT OF WAY OF SAID STATE ROUTE 83, WHICH POINT IS 102.45 FEET EAST OF THE WEST LINE OF SAID SOUTHEAST 1/4; THENCE NORTH, ALONG THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170.0 FEET TO THE POINT OF BEGINNING, IN DUPAGE COUNTY, ILLINOIS.

GENERAL NOTES

- COMPARE ALL DISTANCES AND POINTS IN FIELD AND REPORT ANY DISCREPANCIES IN SAME TO SURVEYOR AT ONCE.
- CALL 811 ("COMMON GROUND ALLIANCE") NATIONAL UNDERGROUND UTILITY LOCATOR SERVICE FOR FIELD LOCATION OF UNDERGROUND UTILITY LINES PRIOR TO ANY DIGGING OR CONSTRUCTION.
- NO DIMENSIONS SHOULD BE ASSUMED BY SCALING.
- BASIS OF BEARING: STATE PLANE COORDINATES - ILLINOIS EAST ZONE, NAD 83 (2011 ADJ.), NAD 83.

SOURCE BENCHMARK

NGS 0135 - PID (DK3298)
TO REACH THE STATION FROM THE INTERSECTION OF SR 83 AND PLAINFIELD ROAD, GO EAST ALONG PLAINFIELD ROAD FOR APPROXIMATELY 0.5 MI (0.8 KM) TO THE STATION ON THE RIGHT. THE STATION IS LOCATED AT THE SOUTHEAST CORNER OF THE INTERSECTION OF PLAINFIELD ROAD AND MADISON STREET.

THE STATION IS 70.0 FT (21.3 M) SOUTH OF THE CENTERLINE OF PLAINFIELD ROAD AND 38.5 FT (11.7 M) EAST OF THE CENTERLINE OF MADISON STREET. THE MONUMENT IS A 3.5 INCH (9 CM) BRASS DISK ON THE BASE OF A TRAFFIC SIGNAL, 0.8 FT (0.2 M) ABOVE GRADE.

ELEVATION: 732.19 (NAVD 88)

SITE BENCHMARKS AND CONTROL

- BM #204 - SET BENCHMARK ON THE SOUTHERLY BOLT, UNDER THE WORD "CITY", ON A HYDRANT ON THE SOUTH SIDE OF 75TH STREET, 308' EAST OF THE INTERSECTION OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY.
ELEVATION - 719.58 (NAVD 88)
- CONTROL #1 - SET CAPPED IRON ROD AT THE SOUTHEAST CORNER OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY, 15' SOUTHEAST OF A TRAFFIC SIGNAL AND 18' EAST OF THE BACK OF CURB OF ROUTE 83.
N: 1852569.52 (NAD 83)
E: 1090379.82 (NAD 83)
- CONTROL #2 - SET CUT "X" IN A CONCRETE SIDEWALK ON THE EAST SIDE OF ROUTE 83, ROBERT KINGERY HIGHWAY, 235' SOUTH OF THE INTERSECTION OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY, 26' EAST OF THE BACK OF CURB OF ROUTE 83.
N: 1852623.35 (NAD 83)
E: 1090803.32 (NAD 83)
- CONTROL #5 - SET CUT "X" IN A CONCRETE SIDEWALK ON THE SOUTH SIDE OF 75TH STREET, 315' EAST OF THE INTERSECTION OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY, AND 7' NORTH OF A FIRE HYDRANT.
N: 1852623.35 (NAD 83)
E: 1090803.32 (NAD 83)



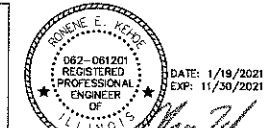
THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY AND HAVE NOT BEEN INDEPENDENTLY LOCATED BY THE OWNER OR ITS REPRESENTATIVE. THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE CONSTRUCTION, AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MAY BE OCCURRED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PREPARE ANY AND ALL UNDERGROUND UTILITIES.

NOTICE: CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. NOTWITHSTANDING THE ENGINEER'S REVIEW, THE ENGINEER SHALL NOT BE RESPONSIBLE FOR SAFETY OF THE WORK OR PERSONS ENGAGED IN THE WORK OF ANY HEAVY STRUCTURES OR OF ANY OTHER PERSONS.

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SCALE 0 10 20
1" = 20 FEET



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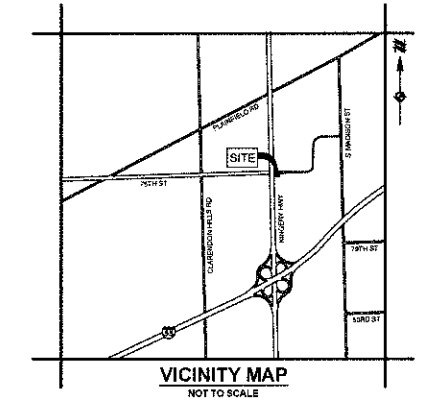
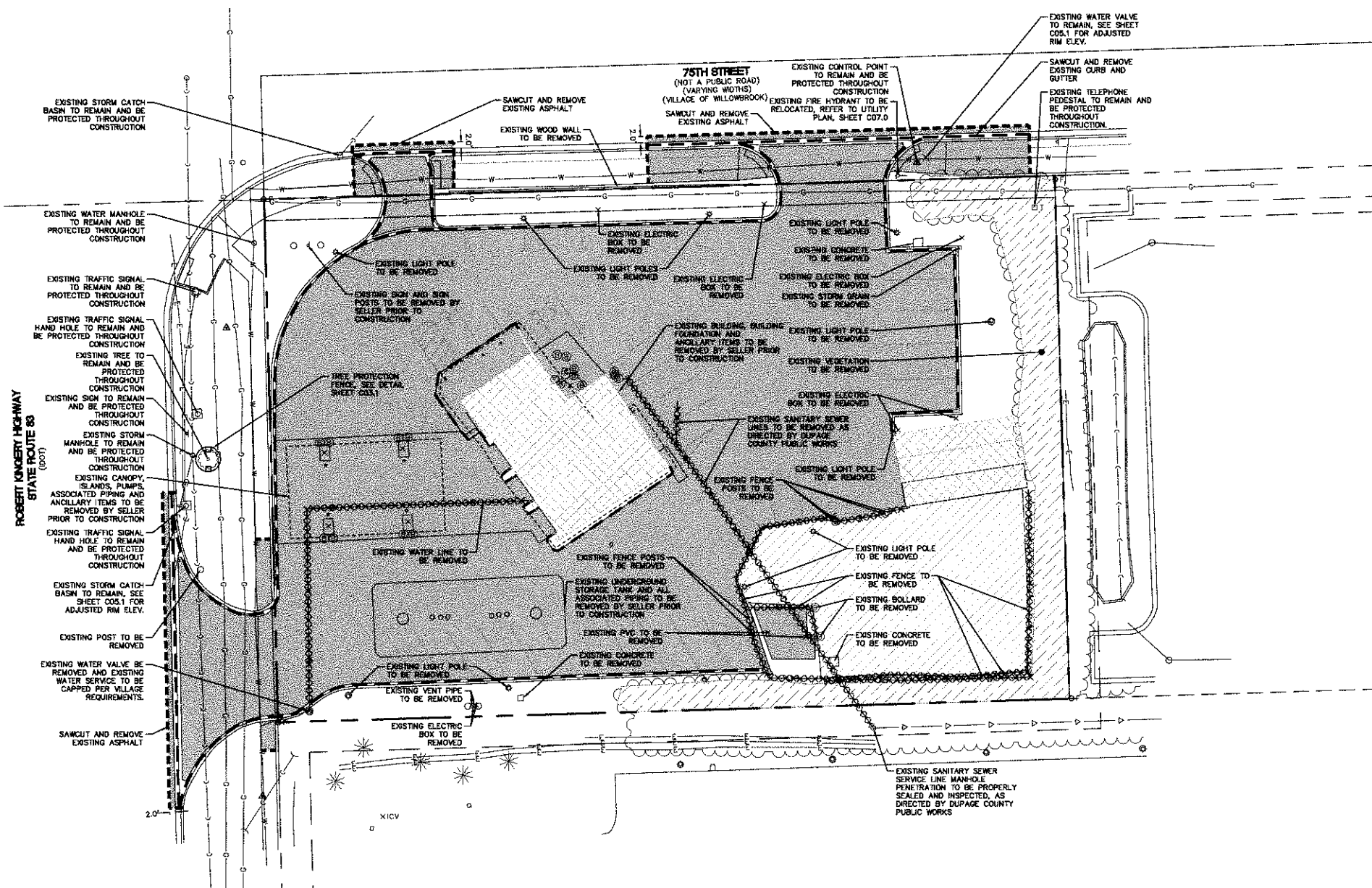


PANDA EXPRESS
WARM & WELCOME 2300
7505 KINGERY HIGHWAY
WILLOWBROOK, IL 60527

EXISTING CONDITIONS

C02.0

TRUE WARM & WELCOME 2300



DEMOLITION LEGEND

---	BOUNDARY LINE
---	EXISTING ROW
---	REMOVE CONCRETE CURB AND GUTTER
---	FULL DEPTH SAWCUT LIMITS
---	TREE PROTECTION FENCE
---	EXISTING FENCE/UTILITY TO BE REMOVED
---	REMOVE EXISTING ASPHALT/CONCRETE PAVEMENT
---	REMOVE GRAVEL
---	REMOVE EXISTING VEGETATION
---	EXISTING BUILDING (TO BE REMOVED BY OTHERS)

- NOTES:**
- SEE SHEET C01.1 FOR DEMOLITION NOTES.
 - PROTECT ALL ITEMS DURING ALL PHASES OF CONSTRUCTION. THE CONTRACTOR SHALL ENSURE THE INTEGRITY OF ALL ITEMS DENOTED TO BE PROTECTED THAT ARE ADJACENT TO ITEMS DENOTED TO BE DEMOLISHED AND WILL SAFELY REPAIR ANY SUCH ITEMS TO THE REQUIRED JURISDICTIONAL STANDARDS.
 - SAWCUT AND REMOVAL OF EXISTING CONCRETE/ASPHALT PAVEMENT, SIDEWALK, CURB AND GUTTER AND ASSOCIATED APPURTENANCES SHALL INCLUDE, BUT NOT BE LIMITED TO, REINFORCEMENT AND STONE BASE.
 - REMOVAL OF EXISTING ON-SITE LANDSCAPE SHALL INCLUDE STUMPS.
 - ANY ITEM NOT INDICATED AS BEING REMOVED SHALL REMAIN.
 - CONTRACTOR SHALL COORDINATE ALL UTILITY REMOVAL AND ABANDONMENT ACTIVITIES WITH LOCAL GOVERNING AGENCY OR UTILITY COMPANY PRIOR TO STARTING DEMOLITION TO ENSURE COMPLIANCE WITH GOVERNING AGENCY AND UTILITY COMPANY REMOVAL AND ABANDONMENT STANDARDS.
 - TREES TO BE REMOVED ARE DESIGNATED ON THIS PLAN FOR REFERENCE. CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVAL OF ALL TREES AND VEGETATION NECESSARY TO ACCOMMODATE CONSTRUCTION.
 - CONTRACTOR MUST PROVIDE A BARRIER FROM PUBLIC ACCESS TO THE SITE PRIOR TO THE START OF DEMOLITION ACTIVITY AND UNTIL SITE CONSTRUCTION HAS BEEN COMPLETED.
 - REMOVAL OF DEMO ITEMS THAT REQUIRE EXCAVATION DEPTHS BELOW EXISTING TOPSOIL SHALL BE BACKFILLED ACCORDING TO THE GEOTECHNICAL REPORT.
 - PRIOR TO CONSTRUCTION OF THE PANDA EXPRESS SITE, THE SELLER SHALL REMOVE EXISTING ITEMS SHOWN ON THIS PLAN AND OBTAIN A "NO FURTHER REMEDIATION" (NFR) LETTER FROM THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY (EPA) THAT IS ACCEPTABLE TO PANDA EXPRESS FOR ITS INTENDED USE OF THE PROPERTY.



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ATWELL PROJECT #: 18003769



PANDA EXPRESS
WARM & WELCOME 2300
7505 KINGERY HIGHWAY
WILLOWBROOK, IL 60527

DEMOLITION PLAN

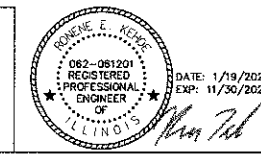
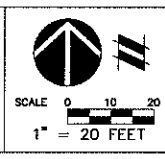
C02.1

TRUE WARM & WELCOME 2300



THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE MANNER ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVE. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREE TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCURRED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

24-HOUR CONTACT:
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CONTRACTOR SHALL PROTECT ALL ITEMS OUTSIDE LIMITS OF CONSTRUCTION UNLESS OTHERWISE NOTED IN THE CONSTRUCTION PLANS OR SPECIFICATIONS.

CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITIES (LOCATIONS AND ELEVATIONS) PRIOR TO STARTING CONSTRUCTION AND ALERT ENGINEER TO ANY DISCREPANCIES IMMEDIATELY.

SOIL EROSION AND SEDIMENTATION CONTROL NOTES

1. ALL EROSION AND SEDIMENT CONTROLS SHALL BE INSPECTED IN ACCORDANCE WITH THE CONDITIONS OF APPLICABLE IL GENERAL NPDES PERMIT.
2. EROSION AND SEDIMENT CONTROLS SHALL BE MAINTAINED AND REPLACED AS NECESSARY AT NO ADDITIONAL COST TO THE OWNER.
3. THE EROSION CONTROL MEASURES INCLUDED IN THE SOIL EROSION AND SEDIMENTATION CONTROL PLAN SHALL BE INSTALLED PRIOR TO INITIAL LAND DISTURBANCE ACTIVITIES OR AS SOON AS PRACTICAL. SEDIMENT SHALL BE PREVENTED FROM DISCHARGING FROM THE PROJECT SITE BY INSTALLING AND MAINTAINING SILT FENCE, STRAW BALES, SEDIMENT BASINS, ETC. AS SHOWN ON THIS PLAN. IF SHOWN ON THESE PLANS, ENERGY-DISSIPATION DEVICES OR EROSION CONTROL AT THE OUTFALL OF THE STORM SEWER SYSTEM SHALL BE INSTALLED AT THE TIME OF THE CONSTRUCTION OF THE OUTFALL.
4. THE CONTRACTOR SHALL CONTROL WASTES, GARBAGE, DEBRIS, WASTEWATER, AND OTHER SUBSTANCES ON THE SITE IN SUCH A WAY THAT THEY SHALL NOT BE TRANSPORTED FROM THE SITE BY THE ACTION OF WINDS, STORM WATER RUNOFF, OR OTHER FORCES. PROPER DISPOSAL OR MANAGEMENT OF ALL WASTES AND UNUSED BUILDING MATERIAL, APPROPRIATE TO THE NATURE OF THE WASTE OR MATERIAL IS REQUIRED. COMPLIANCE IS REQUIRED WITH ALL STATE OR LOCAL REGULATIONS REGARDING WASTE DISPOSAL, SANITARY SEWER, OR SEPTIC SYSTEMS.
5. PUBLIC OR PRIVATE ROADWAYS SHALL BE KEPT CLEARED OF ACCUMULATED SEDIMENT. IF SEDIMENT HAS BEEN TRACKED-OUT FROM THE SITE, ONTO PAVED ROADS, SIDEWALKS, OR OTHER PAVED AREAS OUTSIDE THE SITE, REMOVE THE DEPOSITED SEDIMENT BY THE END OF THE SAME BUSINESS DAY IN WHICH THE TRACK-OUT OCCURS OR BY THE END OF THE NEXT BUSINESS DAY IF TRACK-OUT OCCURS ON A NON-BUSINESS DAY. REMOVE TRACK-OUT BY SWEEPING, SHOVELING, OR VACUUMING THESE SURFACES, OR BY USING OTHER SIMILAR EFFECTIVE MEANS OF SEDIMENT REMOVAL. THE HOUSING OR SWEEPING OF TRACKED-OUT SEDIMENT INTO ANY STORMWATER CONVEYANCE, STORM DRAIN INLET, OR WATER OF THE US IS PROHIBITED. CLEARED SEDIMENT SHALL BE RETURNED TO THE POINT OF LIKELY ORIGIN OR OTHER SUITABLE LOCATION.
6. ALL DISCHARGES FROM DEWATERING ACTIVITIES, INCLUDING DISCHARGES FROM DEWATERING OF TRENCHES AND EXCAVATIONS, SHALL BE MANAGED BY APPROPRIATE CONTROLS.
 - DEWATERING DISCHARGES SHALL BE TREATED OR CONTROLLED TO MINIMIZE DISCHARGES OF POLLUTANTS.
 - THE DISCHARGE SHALL NOT INCLUDE VISIBLE FLOATING SOLIDS OR FOAM.
 - AN OIL-WATER SEPARATOR OR SUITABLE FILTRATION DEVICE SHALL BE USED TO TREAT OIL, GREASE, OR OTHER SIMILAR PRODUCTS IF DEWATERING IS FOUND TO CONTAIN THESE MATERIALS.
 - TO THE EXTENT FEASIBLE, USE VEGETATED, UPLAND AREAS OF THE SITE TO INFILTRATE DEWATERING WATER BEFORE DISCHARGE.
 - BACKWASH WATER USED TO BACKWASH/CLEAN ANY FILTERS USED AS PART OF THE STORMWATER TREATMENT MUST BE PROPERLY TREATED OR HAULED OFF-SITE FOR DISPOSAL.
 - DEWATERING TREATMENT DEVICES SHALL BE PROPERLY MAINTAINED.
7. GENERAL CONTRACTOR SHALL DENOTE ON PLAN THE TEMPORARY PARKING AND STORAGE AREA WHICH SHALL ALSO BE USED AS THE EQUIPMENT MAINTENANCE AND CLEANING AREA, EMPLOYEE PARKING AREA, AND AREA FOR LOCATING PORTABLE FACILITIES, OFFICE TRAILERS, AND TOILET FACILITIES.
8. ALL WASH WATER (CONCRETE TRUCKS, VEHICLE CLEANING, EQUIPMENT CLEANING, ETC.) SHALL BE DETAINED AND PROPERLY TREATED OR DISPOSED.
9. SUFFICIENT OIL AND GREASE ABSORBING MATERIALS AND FLotation ROOMS SHALL BE MAINTAINED ON SITE OR READILY AVAILABLE TO CONTAIN AND CLEAN-UP FUEL OR CHEMICAL SPILLS AND LEAKS.
10. DUST ON THE SITE SHALL BE CONTROLLED. THE USE OF MOTOR OILS AND OTHER PETROLEUM BASED OR TOXIC LIQUIDS FOR DUST SUPPRESSION OPERATIONS IS PROHIBITED.
11. ALL ON-SITE STORM DRAIN INLETS SHALL BE PROTECTED AGAINST SEDIMENTATION WITH STRAW BALES, FILTER FABRIC, OR EQUIVALENT BARRIERS AS SHOWN ON THESE PLANS.
12. EXCEPT AS PREVENTED BY INCIDENT WEATHER CONDITIONS, STABILIZATION OF DISTURBED AREAS MUST BE INITIATED WITHIN ONE (1) WORKING DAY OF PERMANENT OR TEMPORARY CESSATION OF EARTH DISTURBING ACTIVITIES AND SHALL BE COMPLETED AS SOON AS POSSIBLE, BUT NO LATER THAN 14 DAYS FROM THE INITIATION OF THE STABILIZATION WORK IN AN AREA.
13. THIS EROSION CONTROL PLAN SHALL BE IMPLEMENTED ON ALL DISTURBED AREAS WITHIN THE CONSTRUCTION SITE. ALL MEASURES INVOLVING EROSION CONTROL PRACTICES SHALL BE INSTALLED UNDER THE GUIDANCE OF QUALIFIED PERSONNEL EXPERIENCED IN EROSION CONTROL, AND FOLLOWING THE PLANS AND SPECIFICATIONS INCLUDED HEREIN.
14. ALL DISTURBED AREAS, AREAS USED FOR STORAGE OF MATERIALS THAT ARE EXPOSED TO PRECIPITATION, AND ALL AREAS WHERE STORMWATER TYPICALLY FLOWS WITHIN THE SITE SHALL BE INSPECTED FOR EVIDENCE OF, OR THE POTENTIAL FOR, POLLUTANTS ENTERING THE DRAINAGE SYSTEM. EROSION AND SEDIMENT CONTROL MEASURES IDENTIFIED IN THE PLAN SHALL BE OBSERVED TO ENSURE THAT THEY ARE STILL STABILIZED, WHERE DISCHARGE LOCATIONS OR POINTS ARE ACCESSIBLE, THEY SHALL BE INSPECTED TO ASCERTAIN WHETHER EROSION CONTROL MEASURES ARE EFFECTIVE IN PREVENTING SIGNIFICANT IMPACTS TO RECEIVING WATERS. LOCATIONS WHERE VEHICLES ENTER OR EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFF SITE SEDIMENT TRACKING.
15. CONTRACTOR SHALL TAKE CORRECTIVE ACTIONS TO ADDRESS ANY STORMWATER CONTROL THAT NEEDS REPAIR OR REPLACEMENT AS SOON AS POSSIBLE AND DOCUMENT CORRECTIVE ACTIONS WITHIN SEVEN DAYS IN AN INSPECTION REPORT.
16. CONTRACTOR SHALL INSTALL EROSION CONTROL BLANKET PER MANUFACTURER'S RECOMMENDATIONS ON ALL SLOPES 4:1 OR STEEPER.
17. DURING THE PERIOD OF CONSTRUCTION ACTIVITY, ALL SEDIMENT BASINS AND OTHER EROSION CONTROL MEASURES SHALL BE MAINTAINED BY THE CONTRACTOR. AT COMPLETION OF CONSTRUCTION, THE CONTRACTOR SHALL COORDINATE THE TRANSFER OF MAINTENANCE RESPONSIBILITY, IF REQUIRED, WITH THE OWNER. MAINTENANCE SHALL BE IN ACCORDANCE WITH THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL, 1987, AND THE SOIL EROSION AND SEDIMENTATION CONTROL PLAN.
18. ON-SITE & OFF-SITE SOIL STOCKPILE AND BORROW AREAS SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION THROUGH IMPLEMENTATION OF BEST MANAGEMENT PRACTICES (I.E. SILT FENCE, TEMPORARY STABILIZATION, ETC.). STOCKPILE AND BORROW AREA LOCATIONS SHALL BE NOTED ON THE SITE MAP AND PERMITTED IN ACCORDANCE WITH GENERAL PERMIT REQUIREMENTS.
19. EXISTING VEGETATION SHALL BE PROTECTED AS MUCH AS PRACTICAL.
20. ALL TEMPORARY EROSION AND SEDIMENT CONTROL PRACTICES SHALL BE REMOVED AND DISPOSED OF WITHIN THIRTY DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY PRACTICES ARE NO LONGER NEEDED. TRAPPED SEDIMENT SHALL BE PERMANENTLY STABILIZED TO PREVENT FURTHER EROSION.
21. CONTRACTOR SHALL REMOVE ANY ACCUMULATED SEDIMENT FROM DETENTION BASINS AND STORM SEWER SYSTEMS IN CONJUNCTION WITH THE FINAL STABILIZATION OF THE SITE.
22. THIS EROSION CONTROL PLAN MUST BE RETAINED ON-SITE AT ALL TIMES DURING THE PERIOD OF CONSTRUCTION.

NOTE:
THIS SOIL EROSION CONTROL PLAN DOES NOT CONSTITUTE A COMPLETE STORM WATER POLLUTION PREVENTION PLAN (SWPPP). THE CONTRACTOR IS RESPONSIBLE FOR DEVELOPING A COMPLETE STORM WATER POLLUTION PLAN IN ACCORDANCE WITH THE GENERAL NPDES PERMIT REQUIREMENTS INCLUDING BUT NOT LIMITED TO REPORTING, INSPECTIONS, MONITORING, ETC.



THE LOCATION OF EXISTING UNDERGROUND UTILITIES ARE SHOWN ON THIS MAP. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREE TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MAY BE OCCURRED BY THE CONTRACTOR'S FAILURE TO CAREFULLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

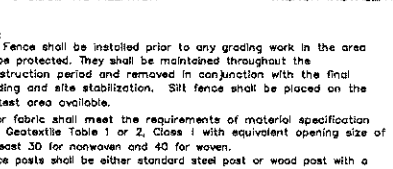
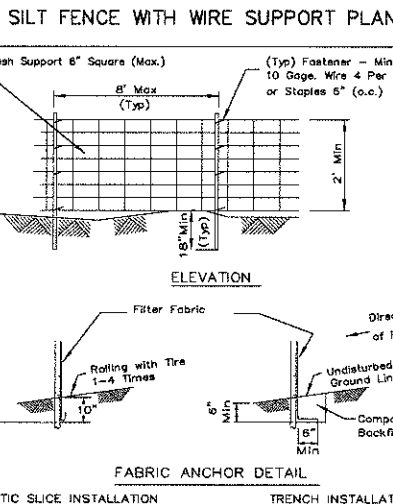
NOTICE:
CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. NEITHER THE OWNER NOR THE ENGINEER SHALL BE EXPECTED TO ASSUME ANY RESPONSIBILITY FOR SAFETY OF THE WORK, OR OF PERSONS OR PROPERTY, OR OF ANY OTHER PERSONS, OR OF ANY OTHER PERSONS.

24-HOUR CONTACT:
PANDA PM
HAKIM YALA
847-477-5125

DATE: 1/19/2021
EXP: 11/30/2021

RECEIVING WATER: RUNOFF FROM THE SITE ENTERS THE PUBLIC STORM SEWER SYSTEM AND DISCHARGES INTO SAWMILL CREEK.

USGS MAP
NOT TO SCALE



NOTES:
1. Silt Fence shall be installed prior to any grading work in the area to be protected. They shall be maintained throughout the construction period and removed in conjunction with the final grading and site stabilization. Silt fence shall be placed on the flattest area available.
2. Filter fabric shall meet the requirements of material specification 592 Geotextile Table 1 or 2, Class I with equivalent opening size of at least 30 for nonwoven and 40 for woven.
3. Fence posts shall be either standard steel post or wood post with a minimum cross-sectional area of 3.0 sq. in.

MAINTENANCE:
SILT FENCE SHALL BE REMOVED ONCE UPLAND AREAS HAVE BEEN PERMANENTLY STABILIZED.
SILT FENCE SHALL BE INSPECTED NO LESS FREQUENTLY THAN EVERY WEEK DURING CONSTRUCTION. SHOULD THE FABRIC DECOMPOSE OR BECOME INEFFECTIVE PRIOR TO THE END OF THE EXPECTED USABLE LIFE, AND THE FENCE STILL IS NECESSARY, THE FABRIC OR THE ENTIRE SYSTEM SHALL BE REPLACED PROMPTLY.
SEDIMENT DEPOSITS MUST BE REMOVED WHEN THE LEVEL OF DEPOSITION REACHES APPROXIMATELY ONE-HALF THE HEIGHT OF THE SILT FENCE.
ANY SEDIMENT DEPOSITS REMAINING IN PLACE AFTER THE SILT FENCE IS NO LONGER REQUIRED SHALL BE DRESSED TO CONFORM TO THE EXISTING GRADE, A SEEDBED PREPARED AND THE SITE VEGETATED.

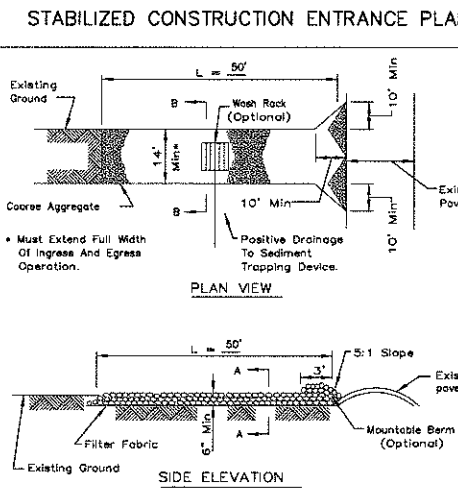
CONCRETE WASHOUT:
INSTALLATION
PREFABRICATED WASHOUT SYSTEMS/CONTAINERS
1. INSTALL AND SECURE ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.
DESIGNED AND INSTALLED SYSTEMS
1. UTILIZE AND FOLLOW THE DESIGN IN THE STORM WATER POLLUTION PREVENTION PLAN TO INSTALL THE SYSTEM.
2. DEPENDENT UPON THE TYPE OF SYSTEM, EITHER EXCAVATE THE PIT OR INSTALL THE CONTAINMENT SYSTEM.
3. A BASE SHALL BE CONSTRUCTED AND PREPARED THAT IS FREE OF ROCKS AND OTHER DEBRIS THAT MAY CAUSE TEARS OR PUNCTURES IN THE POLYETHYLENE LINING.
4. INSTALL THE POLYETHYLENE LINING. FOR EXCAVATED SYSTEMS, THE LINING SHOULD EXTEND OVER THE ENTIRE EXCAVATION. THE LINING FOR BERMED SYSTEMS SHOULD BE INSTALLED OVER THE POOLING AREA WITH ENOUGH MATERIAL TO EXTEND THE LINING OVER THE BERM OR CONTAINMENT SYSTEM. THE LINING SHOULD BE SECURED WITH RIVS, STAPLES, OR OTHER FASTENERS.
5. PLACE FLAGS, SAFETY FENCING, OR EQUIVALENT TO PROVIDE A BARRIER TO CONSTRUCTION EQUIPMENT AND OTHER TRAFFIC.
6. PLACE A NON-WATER HOLDING COVER OVER THE WASHOUT FACILITY PRIOR TO A PREDICTED RAINFALL EVENT TO PREVENT ACCUMULATION OF WATER AND POSSIBLE OVERFLOW OF THE SYSTEM (OPTIONAL).
7. INSTALL SIGNAGE THAT IDENTIFIES CONCRETE WASHOUT AREAS.
8. POST SIGNS DIRECTING CONTRACTORS AND SUPPLIERS TO DESIGNATED LOCATIONS.
9. WHERE NECESSARY, PROVIDE STABLE INGRESS AND EGRESS (SEE TEMPORARY CONSTRUCTION INGRESS/EGRESS PAD) OR ALTERNATIVE APPROACH PAD FOR CONCRETE WASHOUT SYSTEMS.

MAINTENANCE:
1. INSPECT DAILY AND AFTER EACH STORM EVENT.
2. INSPECT THE INTEGRITY OF THE OVERLAP STRUCTURE INCLUDING, WHERE APPLICABLE, THE CONTAINMENT SYSTEM.
3. INSPECT THE SYSTEM FOR LEAKS, SPILLS, AND TRACKING OF SOIL BY EQUIPMENT.
4. INSPECT THE POLYETHYLENE LINING FOR FAILURE, INCLUDING TEARS AND PUNCTURES.
5. ONCE CONCRETE WASTES HARDEN, REMOVE AND DISPOSE OF THE MATERIAL.
6. EXCESS CONCRETE SHOULD BE REMOVED WHEN THE WASHOUT SYSTEM REACHES 90 PERCENT OF THE DESIGN CAPACITY. USE OF THE SYSTEM SHOULD BE DISCONTINUED UNTIL APPROPRIATE MEASURES CAN BE INITIATED TO CLEAN THE STRUCTURE.
7. PREPARED SYSTEMS SHOULD ALSO UTILIZE THIS CRITERION, UNLESS THE MANUFACTURER HAS ALTERNATE SPECIFICATIONS.
8. UPON REMOVAL OF THE SOLIDS, INSPECT THE STRUCTURE. REPAIR THE STRUCTURE AS NEEDED OR CONSTRUCT A NEW SYSTEM.
9. DISPOSE OF ALL CONCRETE IN A LEGAL MANNER. REUSE THE MATERIAL ON SITE, RECYCLE, OR HAUL THE MATERIAL TO AN APPROVED CONSTRUCTION/DEMOLITION LANDFILL SITE. RECYCLING OF MATERIAL IS ENCOURAGED. THE WASTE MATERIAL CAN BE USED FOR MULTIPLE APPLICATIONS INCLUDING BUT NOT LIMITED TO ROADBEDS AND BUILDING THE AVAILABILITY FOR RECYCLING SHOULD BE CHECKED LOCALLY.
10. THE PLASTIC LINER SHOULD BE REPLACED AFTER EVERY LEAKING. THE REMOVAL OF MATERIAL WILL USUALLY DAMAGE THE LINING.
11. THE CONCRETE WASHOUT SYSTEM SHOULD BE REPAIRED OR ENLARGED AS NECESSARY TO MAINTAIN CAPACITY FOR CONCRETE WASTE.

12. CONCRETE WASHOUT SYSTEMS ARE DESIGNED TO PROMOTE EVAPORATION. HOWEVER, IF CONCRETE WASTE IS BEING DISPOSED OF IN A TRENCH OR IF THE BLANKETS THAT HAVE BEEN DISPLACED BY WIND, ANY AREAS WHERE WATER SEEPED UNDER THE BLANKET, MORE STAPLES MAY BE NEEDED OR MORE FREQUENT ANCHORING TRENCHES INSTALLED WITH BETTER COMPACTION. IF SIGNIFICANT EROSION HAS OCCURRED UNDER THE BLANKET THEN GRADING AND RESEEDING MAY ALSO BE NECESSARY. AN EROSION CONTROL BLANKET THAT HAS BEEN DISPLACED WILL NEED TO BE REINSTALLED AND RE-STAPLED. THIS MAY INDICATE THAT THE WRONG TYPE OF BLANKET WAS CHOSEN. ONE MAY NEED TO REVISIT THE SITE CHARACTERISTICS AND THEN SELECT A DIFFERENT TYPE OF EROSION CONTROL BLANKET OR CHOOSE A DIFFERENT PRACTICE.

13. REPAIRS OR REPLACEMENT OF INLET PROTECTION DEVICES SHALL BE MADE IMMEDIATELY. FOR DEVICES TO BE KEPT IN PLACE IN THE WINTER SEASON, AREAS SHALL BE CLEARED OF ANY SEDIMENT ACCUMULATION AND PREPARED OR PROTECTED FOR SNOW REMOVAL OPERATIONS. INLET PROTECTION PRACTICES SHALL BE REMOVED UPON JOB COMPLETION.

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DETAIL 1: STAPLE DETAIL
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1. Staples shall be placed in a diamond pattern at 2 per sq. ft. for stitched blankets. Non-stitched shall use 4 staples per sq. ft. of material. This equates to 200 staples with stitched blanket and 800 staples with non-stitched blanket per 100 sq. ft. of material.
2. Staple or push pin lengths shall be selected based on soil type and conditions. (Minimum staple length is 6").
3. Erosion control material shall be placed in contact with the soil over a prepared seedbed.
4. All anchor stakes shall be staked at approximately 12" intervals.



NOTES:
1. Filter fabric shall meet the requirements of material specification 592 Geotextile, Table 1 or 2, Class I, II, or IV and shall be placed over the cleared area prior to the placing of rock.
2. Rock or reclaimed concrete shall meet one of the following IDOT coarse aggregate gradation, CA-1, CA-2, CA-3 or CA-4 and be placed according to construction specification 25 ROCKFILL using placement Method 1 and Class III compaction.
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MAINTENANCE:
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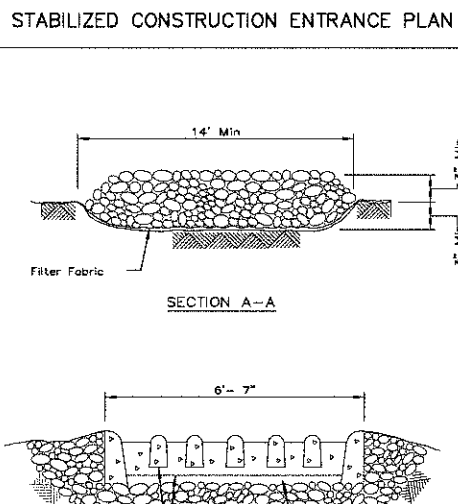
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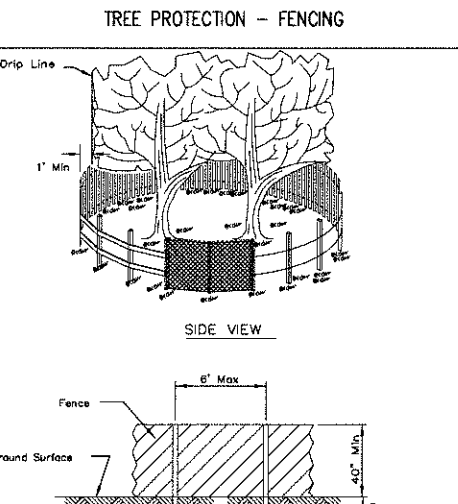
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1. Staples shall be placed in a diamond pattern at 2 per sq. ft. for stitched blankets. Non-stitched shall use 4 staples per sq. ft. of material. This equates to 200 staples with stitched blanket and 800 staples with non-stitched blanket per 100 sq. ft. of material.
2. Staple or push pin lengths shall be selected based on soil type and conditions. (Minimum staple length is 6").
3. Erosion control material shall be placed in contact with the soil over a prepared seedbed.
4. All anchor stakes shall be staked at approximately 12" intervals.



NOTES:
1. The fence shall be located a minimum of 1 foot outside the drip line of the tree to be saved and in no case closer than 5 feet to the trunk of any tree.
2. Fence posts shall be either standard steel posts or wood posts with a minimum cross sectional area of 3.0 sq. in.
3. The fence may be either 40" high snow fence, 40" plastic web fencing or any other material as approved by the engineer/inspector.

MAINTENANCE:
1. INSPECT DAILY AND AFTER EACH STORM EVENT.
2. INSPECT THE INTEGRITY OF THE OVERLAP STRUCTURE INCLUDING, WHERE APPLICABLE, THE CONTAINMENT SYSTEM.
3. INSPECT THE SYSTEM FOR LEAKS, SPILLS, AND TRACKING OF SOIL BY EQUIPMENT.
4. INSPECT THE POLYETHYLENE LINING FOR FAILURE, INCLUDING TEARS AND PUNCTURES.
5. ONCE CONCRETE WASTES HARDEN, REMOVE AND DISPOSE OF THE MATERIAL.
6. EXCESS CONCRETE SHOULD BE REMOVED WHEN THE WASHOUT SYSTEM REACHES 90 PERCENT OF THE DESIGN CAPACITY. USE OF THE SYSTEM SHOULD BE DISCONTINUED UNTIL APPROPRIATE MEASURES CAN BE INITIATED TO CLEAN THE STRUCTURE.
7. PREPARED SYSTEMS SHOULD ALSO UTILIZE THIS CRITERION, UNLESS THE MANUFACTURER HAS ALTERNATE SPECIFICATIONS.
8. UPON REMOVAL OF THE SOLIDS, INSPECT THE STRUCTURE. REPAIR THE STRUCTURE AS NEEDED OR CONSTRUCT A NEW SYSTEM.
9. DISPOSE OF ALL CONCRETE IN A LEGAL MANNER. REUSE THE MATERIAL ON SITE, RECYCLE, OR HAUL THE MATERIAL TO AN APPROVED CONSTRUCTION/DEMOLITION LANDFILL SITE. RECYCLING OF MATERIAL IS ENCOURAGED. THE WASTE MATERIAL CAN BE USED FOR MULTIPLE APPLICATIONS INCLUDING BUT NOT LIMITED TO ROADBEDS AND BUILDING THE AVAILABILITY FOR RECYCLING SHOULD BE CHECKED LOCALLY.
10. THE PLASTIC LINER SHOULD BE REPLACED AFTER EVERY LEAKING. THE REMOVAL OF MATERIAL WILL USUALLY DAMAGE THE LINING.
11. THE CONCRETE WASHOUT SYSTEM SHOULD BE REPAIRED OR ENLARGED AS NECESSARY TO MAINTAIN CAPACITY FOR CONCRETE WASTE.

12. CONCRETE WASHOUT SYSTEMS ARE DESIGNED TO PROMOTE EVAPORATION. HOWEVER, IF CONCRETE WASTE IS BEING DISPOSED OF IN A TRENCH OR IF THE BLANKETS THAT HAVE BEEN DISPLACED BY WIND, ANY AREAS WHERE WATER SEEPED UNDER THE BLANKET, MORE STAPLES MAY BE NEEDED OR MORE FREQUENT ANCHORING TRENCHES INSTALLED WITH BETTER COMPACTION. IF SIGNIFICANT EROSION HAS OCCURRED UNDER THE BLANKET THEN GRADING AND RESEEDING MAY ALSO BE NECESSARY. AN EROSION CONTROL BLANKET THAT HAS BEEN DISPLACED WILL NEED TO BE REINSTALLED AND RE-STAPLED. THIS MAY INDICATE THAT THE WRONG TYPE OF BLANKET WAS CHOSEN. ONE MAY NEED TO REVISIT THE SITE CHARACTERISTICS AND THEN SELECT A DIFFERENT TYPE OF EROSION CONTROL BLANKET OR CHOOSE A DIFFERENT PRACTICE.

13. REPAIRS OR REPLACEMENT OF INLET PROTECTION DEVICES SHALL BE MADE IMMEDIATELY. FOR DEVICES TO BE KEPT IN PLACE IN THE WINTER SEASON, AREAS SHALL BE CLEARED OF ANY SEDIMENT ACCUMULATION AND PREPARED OR PROTECTED FOR SNOW REMOVAL OPERATIONS. INLET PROTECTION PRACTICES SHALL BE REMOVED UPON JOB COMPLETION.

EROSION CONTROL BLANKET INSTALLATION DETAILS
DETAIL 1: STAPLE DETAIL
DETAIL 2: PUSH PIN DETAIL
DETAIL 3: OVERLAP DETAIL
NOTES:
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ISSUE DATE:
PC SUBMITTAL 11/20/2020

DRAWN BY: LEH

PANDA PROJECT # D7058
ATWELL PROJECT #: 18003769

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DESIGN FIRM #18-003769

PANDA EXPRESS
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SOIL EROSION AND SEDIMENTATION CONTROL NOTES AND DETAILS C03.1

TRUE WARM & WELCOME 2300



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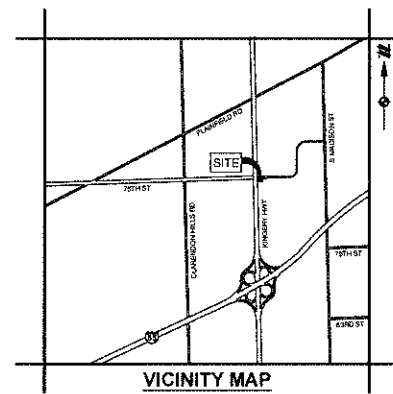
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WILLOWBROOK, IL 60527

SITE LAYOUT PLAN

C04.0

TRUE WARM & WELCOME 2300



VICINITY MAP
NOT TO SCALE

LAYOUT LEGEND

---	BOUNDARY LINE
---	EXISTING ROW
---	EXISTING CURB AND GUTTER
---	PROPOSED CURB AND GUTTER
---	PROPOSED DEPRESSED CURB AND GUTTER
---	EXISTING EASEMENT LINE
---	PROPOSED LANDSCAPE BERM
---	SETBACK LINE
①	PARKING SPACE COUNT
R	ADA RAMP
BC	DIMENSION TO BACK OF CURB
PL	DIMENSION TO PROPERTY LINE
CL	DIMENSION TO CENTER LINE
EP	DIMENSION TO EDGE OF PAVEMENT
EP	DIMENSION TO BUILDING
□-○-□	PROPOSED LIGHT POLE/WALL PACK LIGHT
↑	PROPOSED SIGN
↑ ADA	PROPOSED ACCESSIBLE PARKING SPACE SIGN
↑ S	PROPOSED STOP SIGN
↑ DNE	PROPOSED DO NOT ENTER SIGN
↑ RT	PROPOSED RIGHT TURN ONLY SIGN
---	PROPOSED REINFORCED CONCRETE PAVEMENT, SEE DETAIL SHEET C08.2
---	PROPOSED CONCRETE SIDEWALK, SEE DETAIL SHEET C08.2
---	PROPOSED STANDARD DUTY ASPHALT PAVEMENT, SEE DETAIL SHEET C08.2
---	PROPOSED HEAVY DUTY ASPHALT PAVEMENT, SEE DETAIL SHEET C08.2
---	TRASH ENCLOSURE PAVEMENT, SEE DETAIL SHEET C08.2

NOTES:

- SEE SHEET C01.1 FOR CONSTRUCTION NOTES.
- ALL DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
- ALL RADII ARE 3.0' UNLESS OTHERWISE NOTED.
- REFER TO ARCHITECTURAL PLANS FOR SITE LIGHTING ELECTRICAL PLANS.
- ALL STRIPING SHALL BE 4" WHITE PAINT UNLESS OTHERWISE NOTED. ALL STRIPING SHALL BE APPLIED WITH TWO COATS OF PAINT AT MANUFACTURER'S RECOMMENDED RATE. SEE STRIPING DETAIL SHEET C08.0

INTERIOR LANDSCAPING

SITE AREA EXCLUDING FRONT YARD, ROW, BUILDINGS, AND STRUCTURES: 36,576 SF

REQUIRED INTERIOR LANDSCAPING: 3,658 SF (10%)

PROVIDED INTERIOR LANDSCAPING: 9,408 SF (26%)

ADJACENT LAND USE DATA

ZONING DISTRICT	LAND USE	DEVELOPMENT NAME
NORTH: B2 COMMUNITY SHOPPING	SHOPPING CENTER	WILLOWBROOK PLAZA
SOUTH: B4 HWY & SERVICE BUSINESS	HOTEL	RED ROOF INN
EAST: M1 LIGHT MANUFACTURING	MANUFACTURING/SALES	WILLOWBROOK KEY LLC
WEST: B2 COMMUNITY SHOPPING	RESTAURANT	POTBELLY

PROPOSED BUILDING SETBACK	REQUIRED	PROVIDED
FRONT (WEST)	60'	50.0'
ROUTE 83 (WEST)	100'	50.0'
SIDE EXTERIOR (NORTH)	90'	43.4'
SIDE INTERIOR (SOUTH)	30'	63.8'
REAR (EAST)	40'	162.8'

PARKING SETBACK	REQUIRED	PROVIDED
FRONT (WEST)	25'	25'
SIDE EXTERIOR (NORTH)	15'	15'
SIDE INTERIOR (SOUTH)	10'	10'
REAR (EAST)	10'	10.3'

DRIVE AISLE WIDTH: 24' UNLESS OTHERWISE NOTED

FLOOD HAZARD: ZONE X

FIRM MAP NO. 17043C01894, DATED 8/1/2019

EXISTING INFORMATION:

EXISTING SITE CONDITIONS ARE BASED ON AN ALTA/NSPS LAND TITLE SURVEY BY ATWELL, LLC DATED 9/23/2020

SITE LIGHTING:

PHOTOMETRICS DESIGNED BY OTHERS. POLE LOCATIONS ARE SHOWN FOR REFERENCE ONLY. CONTRACTOR SHALL VERIFY FINAL LOCATIONS WITH PHOTOMETRICS PLAN AND OWNER PRIOR TO CONSTRUCTION.

PARKING DATA

LOADING BERTHS	REQUIRED	PROVIDED
0	0	0

REQUIRED PARKING CALCULATION:
1 PARKING SPACE PER 100 SF OF FLOOR AREA
1 SPACE/100 SF * 2,300 SF = 23 SPACES

TOTAL REQUIRED SPACES	23 SPACES
PROVIDED	
REGULAR (8' X 18')	42 SPACES
DRIVE THRU PICKUP (9' X 18')	2 SPACES
ADA (8' X 18')	2 SPACES
TOTAL PROVIDED SPACES	46 SPACES

CONTRACTOR SHALL PROTECT ALL ITEMS OUTSIDE LIMITS OF CONSTRUCTION UNLESS OTHERWISE NOTED IN THE CONSTRUCTION PLANS OR SPECIFICATIONS.

CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITIES (LOCATIONS AND ELEVATIONS) PRIOR TO STARTING CONSTRUCTION AND ALERT ENGINEER TO ANY DISCREPANCIES IMMEDIATELY.

SITE DATA

ITEMS MARKED WITH AN ASTERISK VARY FROM THE VILLAGE CODE AND WILL REQUIRE A VARIANCE

JURISDICTION: VILLAGE OF WILLOWBROOK

ZONING DISTRICT: B-2 COMMUNITY SHOPPING

SITE AREA CALCULATIONS

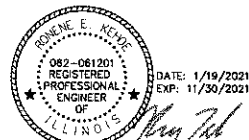
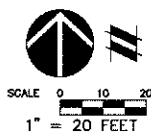
SITE AREA CALCULATIONS:	1.0 ACRES
TOTAL SITE AREA	0.29 ACRES
PERVIOUS AREA	0.71 ACRES

RESTAURANT BUILDING AREA	2,300 SF
TRASH ENCLOSURE AREA	307 SF
TOTAL BUILDING AREA	2,607 SF

	REQUIRED	PROVIDED
MIN. LOT AREA	2.0 ACRES	1.00 ACRES (43,561 SF)*
MIN. LOT WIDTH	200'	170'
MIN. LOT DEPTH	200'	256.24'
MAX. LOT COVERAGE	50%	6.0%
MAX. BUILDING HEIGHT	30'	23'
MAX. FLOOR AREA RATIO	0.30	0.06

EXISTING BUILDING SETBACK	REQUIRED	PROVIDED
FRONT (WEST)	90'	61.5'
ROUTE 83 (WEST)	100'	61.5'
SIDE EXTERIOR (NORTH)	40'	52.2'
SIDE INTERIOR (SOUTH)	100'	58.2'
REAR (EAST)	30'	127.6'

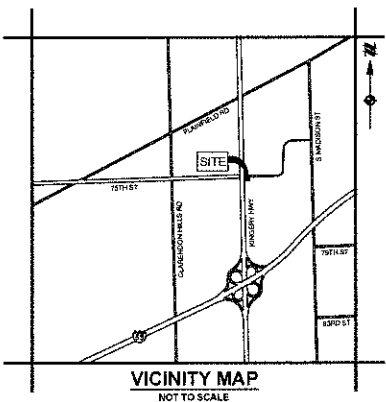
24-HOUR CONTACT:
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HAKIM YALA
847-477-5125



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THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE MANNER ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVE. THE CONTRACTOR SHALL OBTAIN THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PROTECT ANY AND ALL UNDERGROUND UTILITIES.

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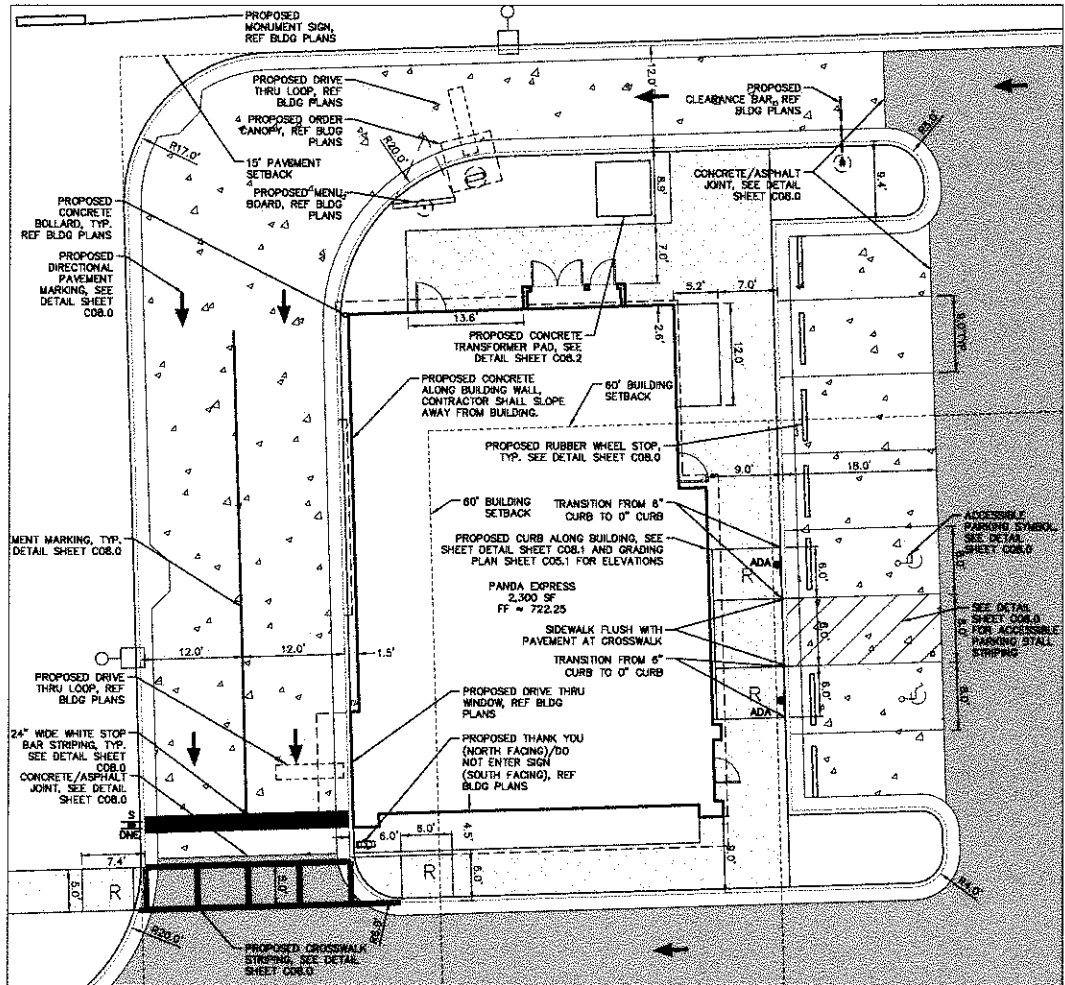
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7505 KINGERY HIGHWAY
WILLOWBROOK, IL 60527

DETAILED SITE
LAYOUT PLAN

C04.1

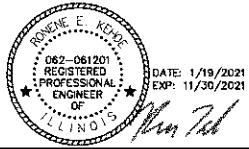
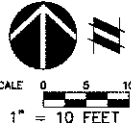
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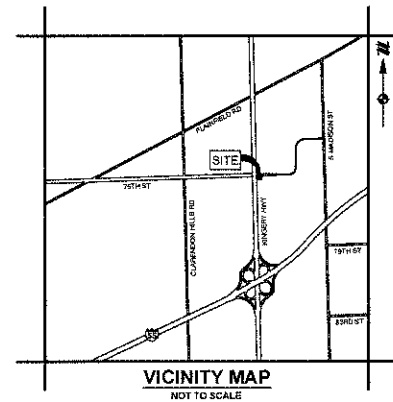
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GRADING PLAN

C05.0

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GRADING LEGEND

---	BOUNDARY LINE
---	EXISTING ROW
---	EXISTING CONTOUR
---	PROPOSED CONTOUR
---	PROPOSED RIDGE LINE
---	EXISTING STORM SEWER
---	PROPOSED STORM SEWER
---	PROPOSED PITCH-OUT CURB AND GUTTER
---	EXISTING STORM CATCH BASIN/MANHOLE
---	PROPOSED STORM CATCH BASIN/MANHOLE
---	PROPOSED SPOT ELEVATION
---	PROPOSED RM ELEVATION
---	MATCH EXISTING ELEVATION
---	PROPOSED FINISHED GRADE ELEVATION
---	PROPOSED TOP OF CURB ELEVATION
---	PROPOSED EDGE OF PAVEMENT ELEVATION
---	PROPOSED FINISHED GRADE AT TOP OF WALL
---	PROPOSED FINISHED GRADE AT BOTTOM OF WALL

NOTES:

- SEE SHEET C01.1 FOR GENERAL NOTES.
- SEE LANDSCAPE PLAN FOR REQUIRED TREES AND GROUND COVER.
- SLOPE OF SURFACE GRADE WITHIN LANDSCAPED AREAS SHALL BE A MINIMUM OF 1.00%.
- MAXIMUM CUT OF FILL SLOPES IS 3H:1V.
- THE CONTRACTOR SHALL PROVIDE CLEAN, SUITABLE MATERIAL FOR REQUIRED FILL. SHOULD A SUFFICIENT QUANTITY OF SUITABLE MATERIAL NOT BE AVAILABLE FROM THE REQUIRED EXCAVATION ON THE SITE.
- ALL FILL SHOULD BE PLACED IN THIN, HORIZONTAL LOOSE LIFTS (MAXIMUM 9-INCH) AND COMPACTED TO AT LEAST 95 PERCENT OF THE MODIFIED PROCTOR MAXIMUM DRY DENSITY (ASTM D 1557). THE UPPER 8 INCHES OF SOIL BENEATH PAVEMENTS AND SLAB-ON-GRADE SHOULD BE COMPACTED TO AT LEAST 98 PERCENT. COMPACTION MUST BE CERTIFIED BY AN ILLINOIS REGISTERED PROFESSIONAL SOILS ENGINEER PRIOR TO THE INSTALLATION OF PAVEMENTS, CURBS, SIDEWALKS OR FOOTINGS OF ANY TYPE.
- JURISDICTIONAL LAND DISTURBANCE PERMIT MUST BE DISPLAYED ON SITE AT ALL TIMES DURING CONSTRUCTION AND IN PLAIN VIEW FROM A PUBLIC ROAD OR STREET.

BUILDING AREA NOTES:

- MAINTAIN ACCESS FOR EMERGENCY VEHICLES AROUND AND TO ALL BUILDINGS UNDER CONSTRUCTION. 14' IN TIMES OF RAIN OR MUD, ROADS SHALL BE PASSABLE TO EMERGENCY VEHICLES BY BEING PAVED OR HAVING A CRUSHED STONE BASE ETC. WITH A MINIMUM WIDTH OF 20 FEET. THE ACCESS TO BUILDINGS HAVING SPRINKLER OR STANDPIPE SYSTEMS SHALL BE TO WITHIN 40 FEET OF THE FIRE DEPARTMENT CONNECTION (NFPA 1141 3-1).
- CONTRACTOR TO PROVIDE POSITIVE DRAINAGE AWAY FROM BUILDING IN ALL AREAS AROUND BUILDING.

HYDROLOGY STATEMENT:

ON-SITE STORMWATER RUN-OFF WILL BE COLLECTED VIA PROPOSED CATCH BASINS. A PROPOSED STORM PIPE SYSTEM WILL CONVEY STORMWATER TO AN EXISTING STORM PIPE LOCATED ALONG ROUTE 83. THE EXISTING 18" PIPE WILL CONVEY THE STORMWATER SAWMILL CREEK.

LEGAL DESCRIPTION

FIRST AMERICAN TITLE INSURANCE COMPANY
COMMITMENT NO.: NOS-988541-01E
COMMITMENT DATE: AUGUST 28, 2020

THE NORTH 170.0 FEET OF THAT PART OF THE SOUTHEAST 1/4 OF SECTION 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING EAST OF THE EAST RIGHT OF WAY OF STATE ROUTE 83, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT 102.65 FEET EAST OF THE NORTHWEST CORNER OF SAID SOUTHEAST 1/4; THENCE EAST, ALONG THE NORTH LINE OF SAID SOUTHEAST 1/4, A DISTANCE OF 256.24 FEET; THENCE SOUTH, ALONG A LINE PARALLEL WITH THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170 FEET; THENCE WEST, ALONG A LINE PARALLEL WITH THE NORTH LINE OF SAID SOUTHEAST 1/4, A DISTANCE OF 256.24 FEET TO THE INTERSECTION OF SAID PARALLEL LINE WITH THE EAST RIGHT OF WAY OF SAID STATE ROUTE 83, WHICH POINT IS 102.45 FEET EAST OF THE WEST LINE OF SAID SOUTHEAST 1/4; THENCE NORTH, ALONG THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170.0 FEET TO THE POINT OF BEGINNING, IN DUPAGE COUNTY, ILLINOIS.

SOURCE BENCHMARK

NGS 0135 - PID (DK3296)
TO REACH THE STATION FROM THE INTERSECTION OF SR 83 AND PLAINFIELD ROAD, GO EAST ALONG PLAINFIELD ROAD FOR APPROXIMATELY 0.5 MI (0.8 KM) TO THE STATION ON THE RIGHT. THE STATION IS LOCATED AT THE SOUTHEAST CORNER OF THE INTERSECTION OF PLAINFIELD ROAD AND MADISON STREET.

THE STATION IS 70.0 FT (21.3 M) SOUTH OF THE CENTERLINE OF PLAINFIELD ROAD AND 38.5 FT (11.7 M) EAST OF THE CENTERLINE OF MADISON STREET. THE MONUMENT IS A 3.5 INCH (9 CM) BRASS DISK ON THE BASE OF A TRAFFIC SIGNAL, 0.8 FT (0.2 M) ABOVE GRADE.

ELEVATION: 732.18 (NAVD 88)

SITE BENCHMARKS AND CONTROL

- BM #204 - SET BENCHMARK ON THE SOUTHERLY BOLT, UNDER THE WORD "CITY", ON A HYDRANT ON THE SOUTH SIDE OF 75TH STREET, 308' EAST OF THE INTERSECTION OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY.
ELEVATION - 719.58 (NAVD 88)
- CONTROL #1 - SET CAPPED IRON ROD AT THE SOUTHEAST CORNER OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY, 15' SOUTHEAST OF A TRAFFIC SIGNAL AND 18' EAST OF THE BACK OF CURB OF ROUTE 83.
N: 1852869.52 (NAD 83)
E: 1090379.82 (NAD 83)
- CONTROL #2 - SET OUT "X" IN A CONCRETE SIDEWALK ON THE EAST SIDE OF ROUTE 83, ROBERT KINGERY HIGHWAY, 235' SOUTH OF THE INTERSECTION OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY, 26' EAST OF THE BACK OF CURB OF ROUTE 83.
N: 1852417.41 (NAD 83)
E: 1090392.03 (NAD 83)
- CONTROL #5 - SET OUT "X" IN A CONCRETE SIDEWALK ON THE SOUTH SIDE OF 75TH STREET, 315' EAST OF THE INTERSECTION OF 75TH STREET AND ROUTE 83, ROBERT KINGERY HIGHWAY, AND 7' NORTH OF A FIRE HYDRANT.
N: 1852823.35 (NAD 83)
E: 1090603.32 (NAD 83)

CONTRACTOR SHALL PROTECT ALL ITEMS OUTSIDE LIMITS OF CONSTRUCTION UNLESS OTHERWISE NOTED IN THE CONSTRUCTION PLANS OR SPECIFICATIONS.

CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITIES (LOCATIONS AND ELEVATIONS) PRIOR TO STARTING CONSTRUCTION AND ALERT ENGINEER TO ANY DISCREPANCIES IMMEDIATELY.

ROBERT KINGERY HIGHWAY
STATE ROUTE 83
(NOT)

REFER TO SHEET C05.1 FOR
WEST ENTRANCE GRADING

REFER TO SHEET C05.1 FOR
NORTH ENTRANCE GRADING

REFER TO SHEET
C05.1 FOR BUILDING
AND DRIVE THRU

REFER TO SHEET C05.1 FOR
TRASH ENCLOSURE AND
RETAINING WALL GRADING

811
Know what's below.
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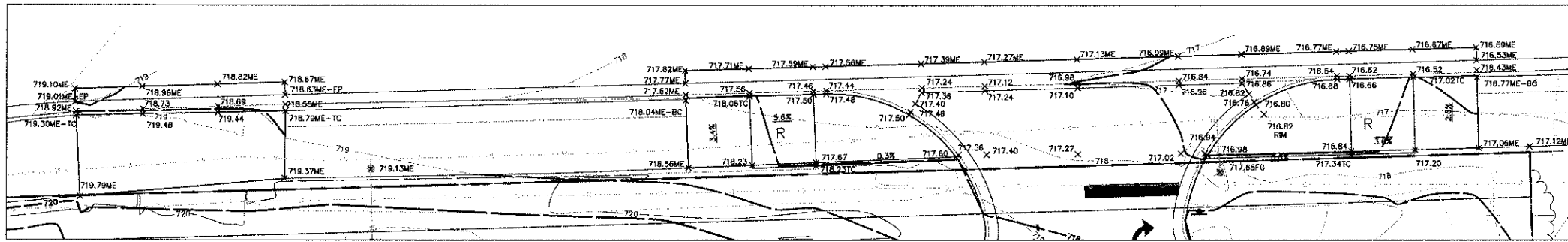
THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROPRIATE MANNER AND ARE NOT BEING INDEPENDENTLY VERIFIED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION OF ALL UTILITIES PRIOR TO COMMENCING WORK, AND AGREE TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MAY BE OCCURRED BY THE CONTRACTOR'S FAILURE TO EXCAVATE, LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

NOTICE:
CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. NEITHER THE OWNER NOR THE ENGINEER SHALL BE EXPECTED TO ASSUME ANY RESPONSIBILITY FOR SAFETY OF THE WORK, OF PERSONS ENGAGED IN THE WORK, OR OF ANY OTHER PERSONS, OR OF ANY OTHER PERSONS.

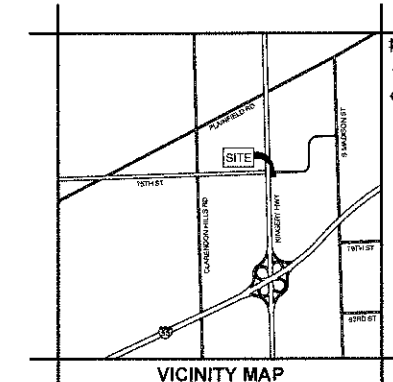
24-HOUR CONTACT:
PANDA FM
HAKIM YALA
847-477-5125

SCALE 0 10 20
1" = 20 FEET

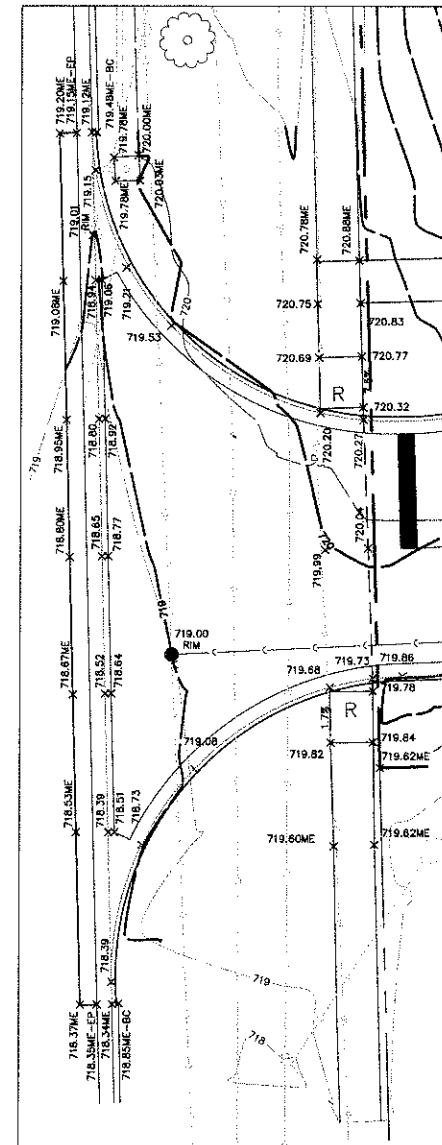
DATE: 1/19/2021
EXP: 11/30/2021
PROFESSIONAL ENGINEER
ILLINOIS



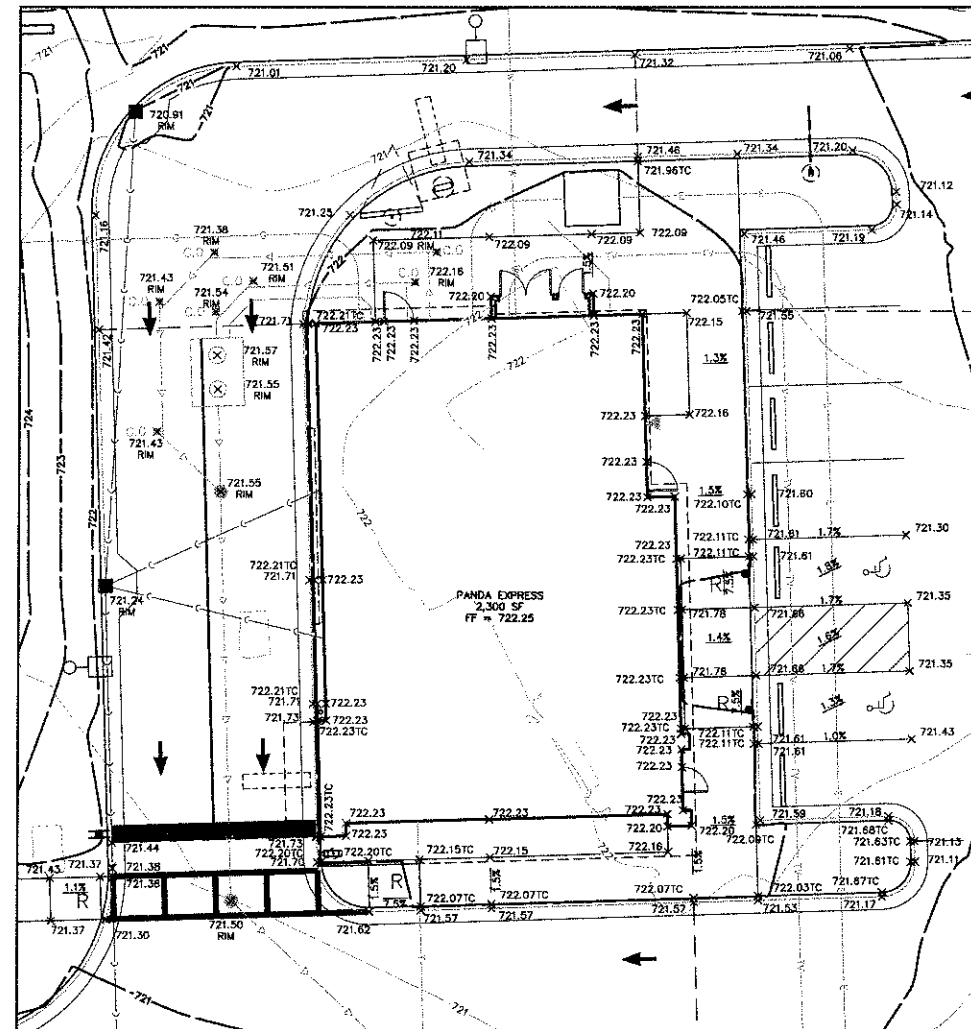
NORTH ENTRANCE GRADING DETAIL
SCALE 1" = 10'



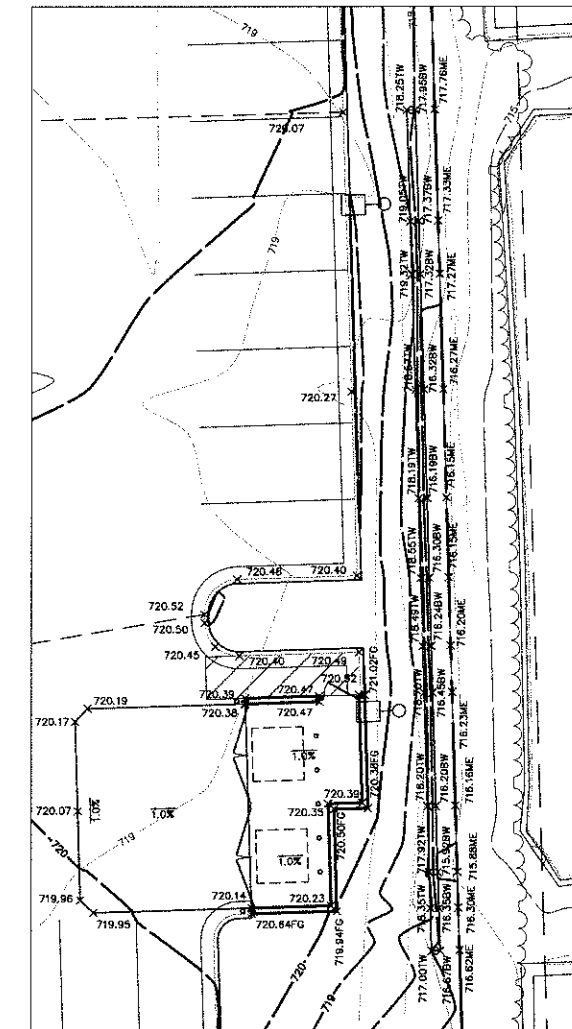
VICINITY MAP
NOT TO SCALE



WEST ENTRANCE GRADING DETAIL
SCALE 1" = 10'



BUILDING AND DRIVE THRU GRADING DETAIL
SCALE 1" = 10'



TRASH ENCLOSURE AND RETAINING WALL GRADING DETAIL
SCALE 1" = 10'

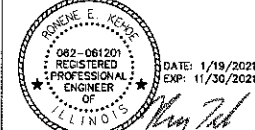
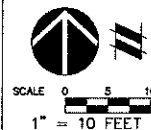


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NOTICE: CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. WHETHER THE OWNER OR THE CONTRACTOR SHALL BE EXPECTED TO ASSUME ANY RESPONSIBILITY FOR SAFETY OF THE WORK OF PERSONS ENGAGED IN THE WORK OF ANY NEWER STRUCTURES, OR OF ANY OTHER PERSONS. COPYRIGHT © 2021 ATWELL LLC. NO REPRODUCTION SHALL BE MADE WITHOUT THE PRIOR WRITTEN CONSENT OF ATWELL LLC.

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847-477-5125



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Facsimile: 626.372.8288

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REVISIONS:
VILLAGE PLAN REV. #1 01/19/2021

ISSUE DATE:
PC SUBMITTAL 11/20/2020

DRAWN BY: LEH

PANDA PROJECT # D7058
ATWELL PROJECT #: 18003789

NORR
NORTHWESTERN ENGINEERING & ARCHITECTS

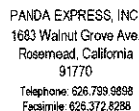
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DESIGN FIRM #18-025876

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DETAILED GRADING

C05.1

TRUE WARM & WELCOME 2300



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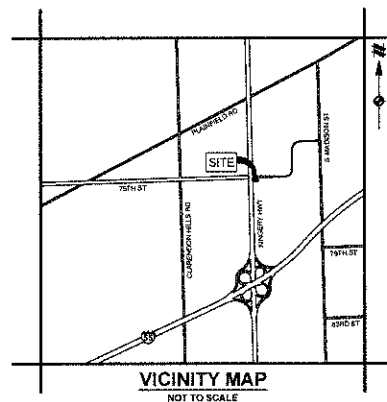
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STORM SEWER PLAN

C06.0

TRUE WARM & WELCOME 2300



	BOUNDARY LINE
	EXISTING ROW
	EXISTING EASEMENT LINE
	EXISTING SANITARY SEWER
	PROPOSED SANITARY SEWER
	EXISTING STORM SEWER
	PROPOSED STORM SEWER
	EXISTING WATER LINE
	PROPOSED WATER LINE
	EXISTING UNDERGROUND GAS
	PROPOSED UNDERGROUND GAS
	EXISTING UNDERGROUND TELEPHONE
	PROPOSED UNDERGROUND TELEPHONE
	EXISTING UNDERGROUND ELECTRIC
	PROPOSED UNDERGROUND ELECTRIC
	EXISTING UNDERGROUND CABLE
	PROPOSED UNDERGROUND CABLE
	PROPOSED LIGHT POLE/WALL PACK LIGHT
	EXISTING MANHOLE/CATCH BASIN
	PROPOSED MANHOLE/CATCH BASIN/INLET
	PROPOSED CLEANOUT
	EXISTING END SECTION
	PROPOSED WATER VALVE
	PROPOSED FIRE HYDRANT
	PROPOSED FIRE DEPARTMENT CONNECTION
	PROPOSED UTILITY CROSSING, SEE UTILITY CROSSING CHART THIS SHEET

CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION WITH APPROPRIATE UTILITY COMPANIES TO ENSURE THAT THE REQUIRED VERTICAL AND HORIZONTAL CLEARANCES AT ALL LOCATIONS WHERE ANY UTILITY LINE CROSSES AN EXISTING OR PROPOSED GAS, TELEPHONE OR ELECTRIC LINE ARE MET.

NOTES:

1. SEE SHEET CD01.1 FOR GENERAL NOTES.
2. SEE MEP PLANS FOR CONTINUATION OF ALL UTILITIES TO BUILDING.
3. CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITIES AND THEIR LOCATIONS AND ELEVATIONS PRIOR TO STARTING CONSTRUCTION.
4. THE CONTRACTOR SHALL CONTACT PUBLIC UTILITIES INSPECTIONS AT LEAST 72 HOURS PRIOR TO ANY CONSTRUCTION ACTIVITY.
5. ALL WORK TO BE DONE IN STRICT ACCORDANCE WITH LOCAL GOVERNING CODES.
6. CONTROLLED BACK FILL TO BE PLACED IN 9" LOOSE LIFT AND COMPACTED PER GEOTECHNICAL REPORT RECOMMENDATIONS PRIOR TO STORM AND SANITARY SEWER CONSTRUCTION. BACK FILL SHALL BE PLACED TO A MINIMUM OF $\pm 2'$ ABOVE THE CROWN ELEVATION OF THE PIPES.
7. STORM SEWER AND SANITARY SEWER LENGTHS ARE MEASURED FROM CENTER LINE OF STRUCTURE TO CENTERLINE OF STRUCTURE OR FACE OF HEADWALL.
8. ALL PIPE LENGTHS SHOWN ARE ROUNDED TO THE NEAREST FOOT.
9. ALL STORM SEWER PIPING SHALL BE TRENCHED, BEDDED AND BACK FILLED ACCORDING TO DETAIL ON SHEET CD08.3 UNLESS SPECIFICALLY NOTED OTHERWISE.
10. UNFORESEEN SUBSURFACE CONDITIONS SHALL BE BROUGHT TO THE OWNER'S AND THE OWNER'S ATTENTION IMMEDIATELY UPON IMPLEMENTATION OF CORRECTIVE BEDDING MEASURES WITHOUT THE OWNER'S APPROVAL. SHALL BE AT THE CONTRACTOR'S OWN RISK AND AT NO ADDITIONAL COMPENSATION.
11. EXISTING GRASSES SHOWN ARE APPROXIMATE AND DO NOT REFLECT TOP SOIL REMOVAL, CLEARING, AND GRUBBING OPERATIONS. THE CONTRACTOR SHALL ASCERTAIN FOR HIMSELF THE EXTENT OF DISTURBANCE FOR THESE ACTIVITIES.
12. THE CONTRACTOR SHALL REFERENCE THE GEOTECHNICAL REPORT PREPARED BY THE OWNER'S CONSULTING SUBCONSULTANT. THE GEOTECHNICAL REPORT IS NOT A PART OF THE CONTRACT DOCUMENTS.
13. EXCAVATIONS FOR STRUCTURES SHALL BE TAKEN AS A TRENCHING EXCAVATION WITHOUT FURTHER COMPENSATION.

CONTRACTOR SHALL COORDINATE AND
ADJUST LOCATION OF LOOP DETECTORS
TO AVOID UTILITY CONFLICTS PRIOR TO
CONSTRUCTION.

CONTRACTOR SHALL COORDINATE AND
VERIFY LOCATION OF ALL SIGNAGE WITH
OWNER PRIOR TO CONSTRUCTION.

CONTRACTOR SHALL INSTALL GENERAL UTILITY CONDUITS TO PLANTERS AROUND BUILDING AND PATIO. SEE ARCHITECTURAL/MEP PLANS FOR CONTINUATION.

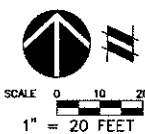
CONTRACTOR SHALL PROTECT ALL
ITEMS OUTSIDE LIMITS OF
CONSTRUCTION UNLESS OTHERWISE
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OR SPECIFICATIONS.

CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITIES (LOCATIONS AND ELEVATIONS) PRIOR TO STARTING CONSTRUCTION AND ALERT ENGINEER TO ANY DISCREPANCIES IMMEDIATELY.

UTILITY CROSSING CHART							
CROSSING NUMBER	FINISHED GROUND	TOP UTILITY	8" ELEVATION	BOTTOM UTILITY	TOP ELEVATION	CLEARANCE	COMMENT
1	721.54	6" STORM	717.32	6" SANITARY	715.73	1.56	FIELD VERIFY DEPTH OF EXISTING WATER MAIN PRIOR TO CONSTRUCTION OF STORM SEWER. IF WATER MAIN NEEDS TO MAINTAIN 12" MINIMUM SEPARATION, RAINAGE
2	721.51	6" STORM	717.33	6" SANITARY	715.40	1.93	
3	716.53	12" STORM	715.92	6" SANITARY	712.17	3.06	
	719.76	12" STORM	714.91	12" WATERMAIN	713.41	1.50	

STRUCTURE NUMBER	STRUCTURE TYPE AND SIZE	GRATE TYPE	NOTES
ST-1	4 DIA. TYPE 1 MANHOLE	NEENAH R 3271-A	CLOSED LID
ST-2	4 DIA. TYPE A CATCH BASIN	NEENAH R 3281-A	
ST-3	4 DIA. TYPE A CATCH BASIN	NEENAH R 3281-A	
ST-4	4 DIA. TYPE A CATCH BASIN	NEENAH R 3281-A	
ST-5	4 DIA. TYPE A CATCH BASIN	NEENAH R 3281-A	
ST-6	4 DIA. TYPE A CATCH BASIN	NEENAH R 3281-A	

24-HOUR CONTACT:
PANDA PM
HAKIM YALA
847-477-5125



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THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVE. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREE TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

NOTICE:
CONSTRUCTION SITE SAFETY IS THE
SOLE RESPONSIBILITY OF THE
CONTRACTOR. NEITHER THE OWNER
NOR THE ENGINEER SHALL BE
EXPECTED TO ASSUME ANY
RESPONSIBILITY FOR SAFETY OF THE
WORK OF PERSONS ENGAGED IN THE
WORK OF ANY NEARBY STRUCTURES
OR OF ANY OTHER PERSONS.

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ISSUE DATE:
PC SUBMITTAL 11/20/2020

DRAWN BY: LEH

PANDA PROJECT # D7058
ATWELL PROJECT #: 18003769

NORR
REGISTERED PROFESSIONAL ENGINEER

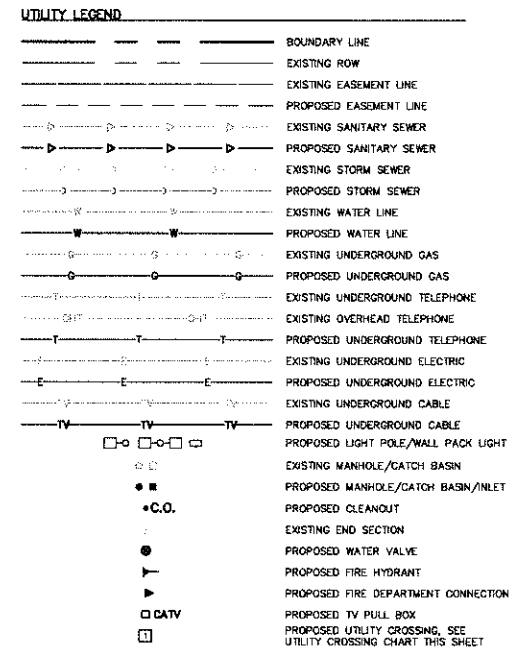
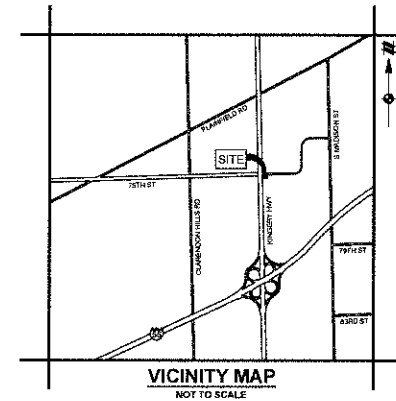
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UTILITY PLAN

C07.0

TRUE WARM & WELCOME 2300

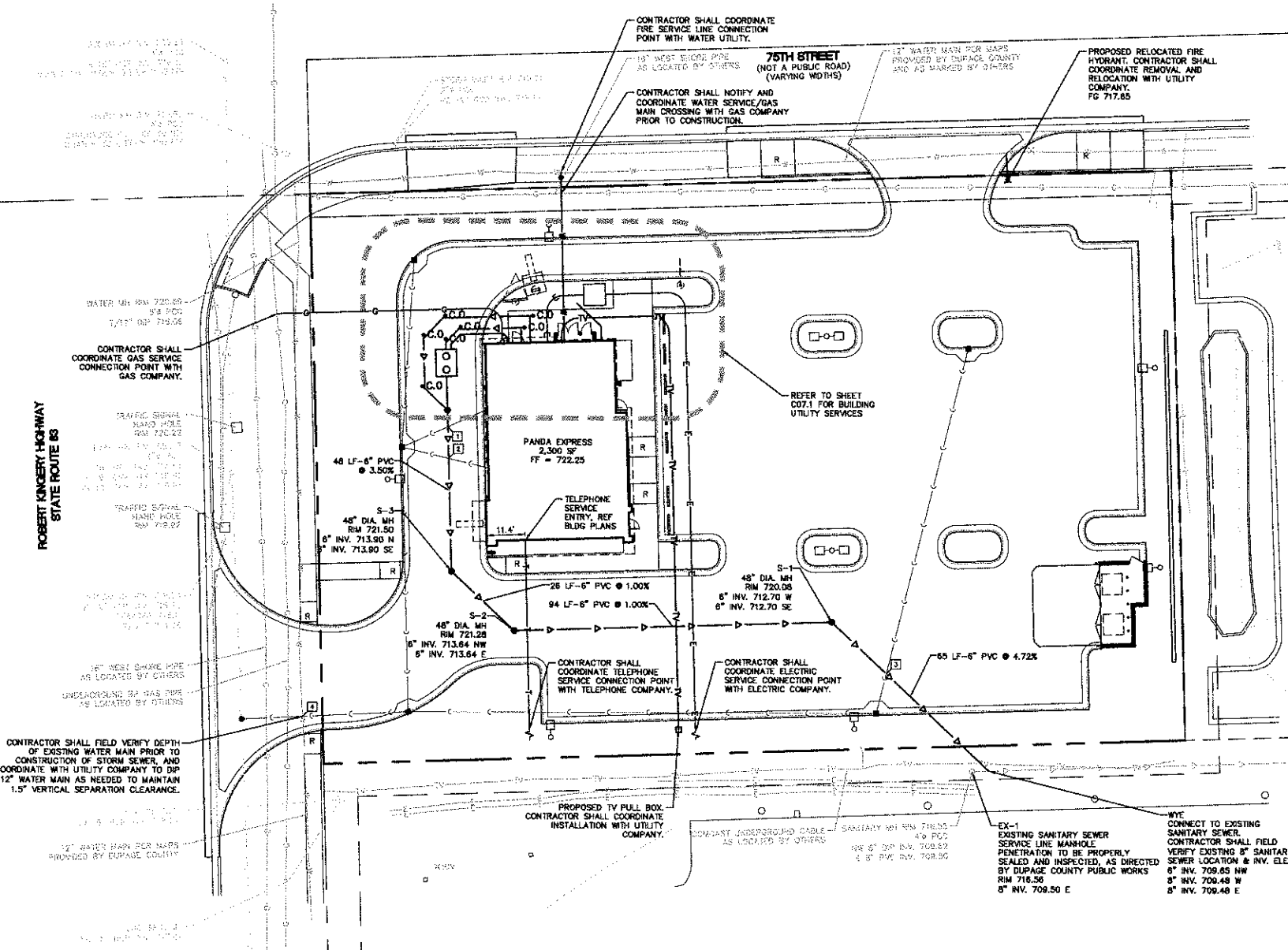


CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION WITH APPROPRIATE UTILITY COMPANIES TO ENSURE THAT THE REQUIRED VERTICAL AND HORIZONTAL CLEARANCES AT ALL LOCATIONS WHERE ANY UTILITY LINE CROSSES AN EXISTING OR PROPOSED GAS, TELEPHONE OR ELECTRIC LINE ARE MET.

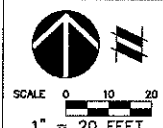
- NOTES:**
- SEE SHEET C01.1 FOR GENERAL NOTES.
 - SEE MEP PLANS FOR CONTINUATION OF ALL UTILITIES INTO BUILDING.
 - SANITARY LATERALS SHALL HAVE A MINIMUM FALL OF 1.00%.
 - CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITIES AND THEIR LOCATIONS AND ELEVATIONS PRIOR TO STARTING CONSTRUCTION.
 - THE FINAL LOCATION OF FIRE HYDRANTS, VALVES, WATER LINES, BACKFLOW PREVENTERS, ETC. SHALL BE DETERMINED DURING CONSTRUCTION. NOTIFY THE ENGINEER OF ANY CHANGES TO LOCATION OR CONFIGURATION. NFPA CODES SHALL BE ADHERED TO.
 - THE CONTRACTOR SHALL CONTACT PUBLIC UTILITIES INSPECTIONS AT LEAST 72 HOURS PRIOR TO ANY CONSTRUCTION ACTIVITY.
 - ALL WORK TO BE DONE IN STRICT ACCORDANCE WITH LOCAL GOVERNING CODES.
 - UTILITY CONDUIT MATERIAL FOR ELECTRIC, TELEPHONE, AND CABLE SHALL BE INSTALLED PER UTILITY PROVIDER SPECIFICATIONS.
 - CONTRACTOR TO BUILD CONCRETE TRANSFORMER PAD AND COORDINATE THE INSTALLATION OF THE CONDUITS WITH THE UTILITY COMPANY.
 - CONTROLLED BACK FILL TO BE PLACED IN 8" LOOSE LIFT AND COMPACTED PER GEOTECHNICAL REPORT RECOMMENDATIONS PRIOR TO STORM AND SANITARY SEWER CONSTRUCTION. BACK FILL SHALL BE PLACED TO A MINIMUM OF $\pm 2'$ ABOVE THE CROWN ELEVATION OF THE PIPES.
 - STORM SEWER AND SANITARY SEWER LENGTHS ARE MEASURED FROM CENTER LINE OF STRUCTURE TO CENTERLINE OF STRUCTURE OR FACE OF HEADWALL.
 - ALL PIPE LENGTHS SHOWN ARE ROUNDED TO THE NEAREST FOOT.
 - ALL SANITARY SEWER PIPING SHALL BE TRENCHED, BEDDED AND BACK FILLED ACCORDING TO DETAIL ON SHEET C08.3 UNLESS SPECIFICALLY NOTED OTHERWISE.
 - UNFORESEEN SUBSURFACE CONDITIONS SHALL BE BROUGHT TO THE OWNER'S AND ENGINEER'S ATTENTION IMMEDIATELY UPON IMPLEMENTATION OF CORRECTIVE BEDDING MEASURES WITHOUT THE OWNER'S APPROVAL. SHALL BE AT THE CONTRACTOR'S OWN RISK AND AT NO ADDITIONAL COMPENSATION.
 - EXISTING GRADES SHOWN ARE APPROXIMATE AND DO NOT REFLECT TOP SOIL REMOVAL, CLEARING, AND GRUBBING OPERATIONS. THE CONTRACTOR SHALL ASCERTAIN FOR HIMSELF THE EXTENT OF DISTURBANCE FOR THESE ACTIVITIES.
 - THE CONTRACTOR SHALL REFERENCE THE GEOTECHNICAL REPORT PREPARED FOR THE OWNER FOR SUBSURFACE CONDITIONS. THE GEOTECHNICAL REPORT IS NOT A PART OF THE CONTRACT DOCUMENTS.
 - EXCAVATIONS FOR STRUCTURES SHALL BE TAKEN AS A TRENCHING EXCAVATION WITHOUT FURTHER COMPENSATION.

UTILITY CROSSING CHART						
CROSSING NUMBER	FINISHED GROUND	TOP UTILITY	BIP ELEVATION	BOTTOM UTILITY	TYP ELEVATION	CLEARANCE
1	721.54	6" STORM	717.32	6" SANITARY	715.73	1.59
2	721.51	6" STORM	717.39	6" SANITARY	715.40	1.99
3	719.53	12" STORM	715.65	6" SANITARY	712.17	3.95
4	719.78	12" STORM	714.91	12" WATERMAIN	713.41	1.50

FIELD VERIFY DEPTH OF EXISTING WATER MAIN PRIOR TO CONSTRUCTION OF STORM SEWER. DIP 12" WATER MAIN AS NEEDED TO MAINTAIN 1.5' VERTICAL SEPARATION CLEARANCE.



24-HOUR CONTACT:
PANDA FM
HAKIM YALA
847-477-5125



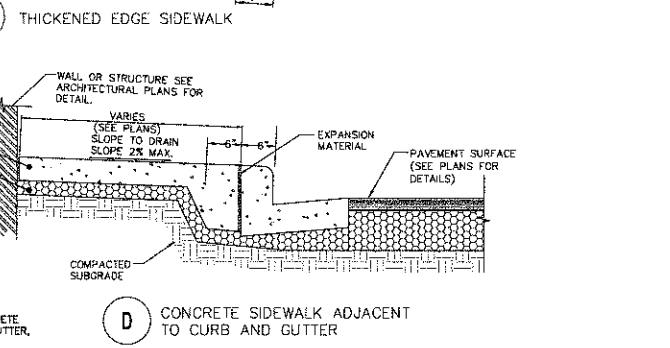
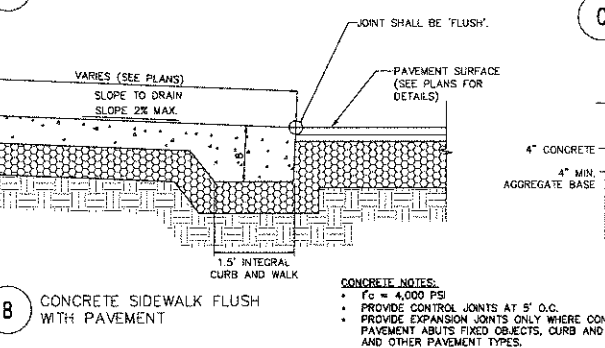
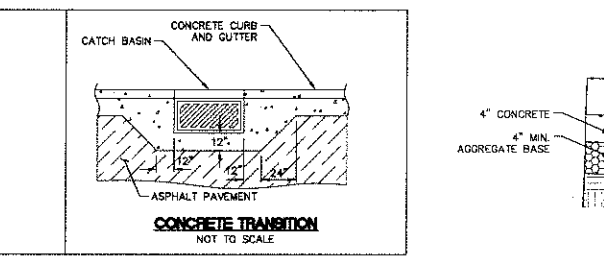
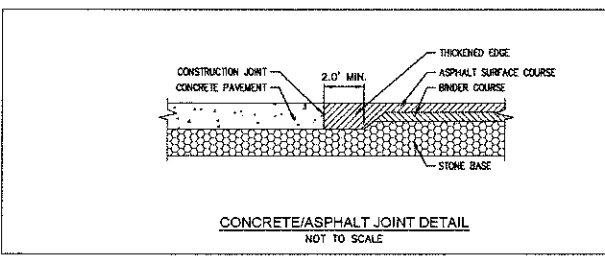
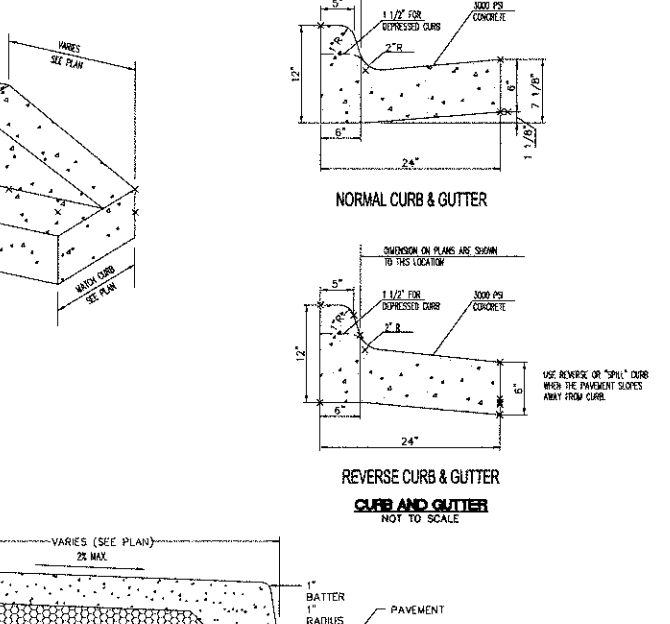
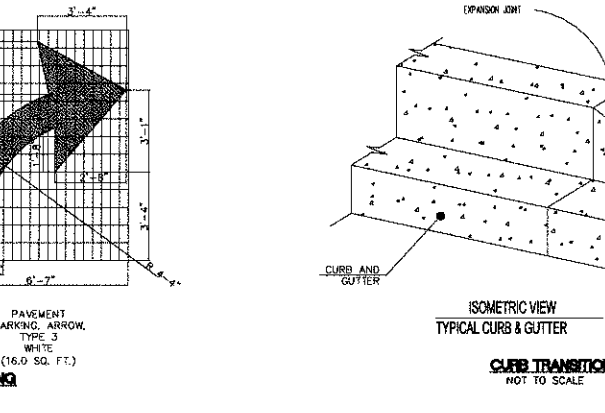
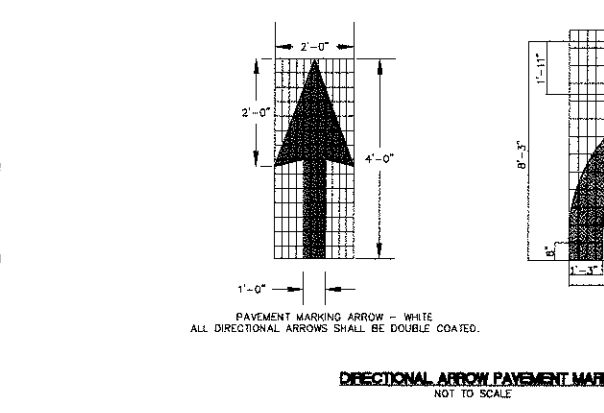
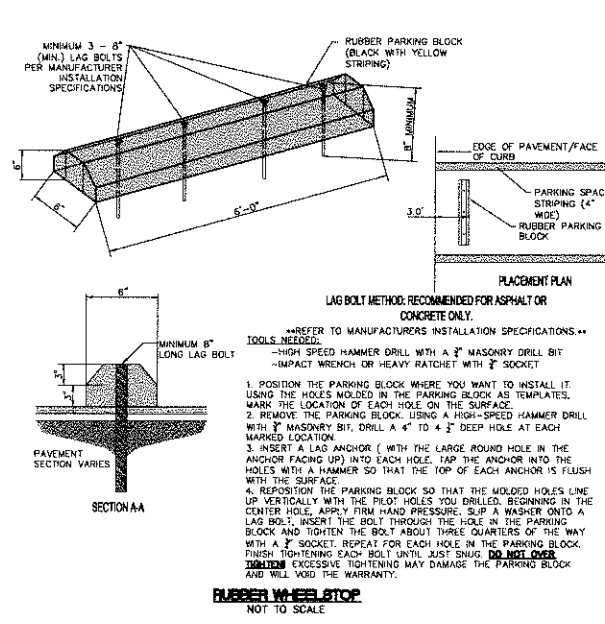
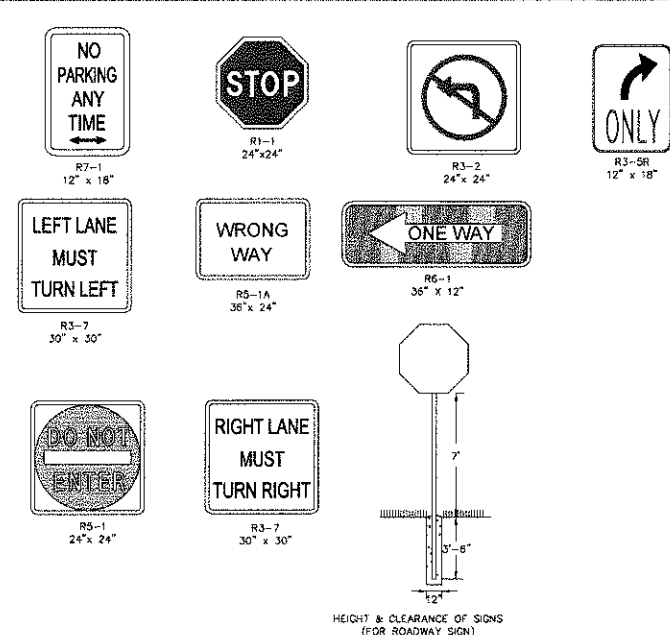
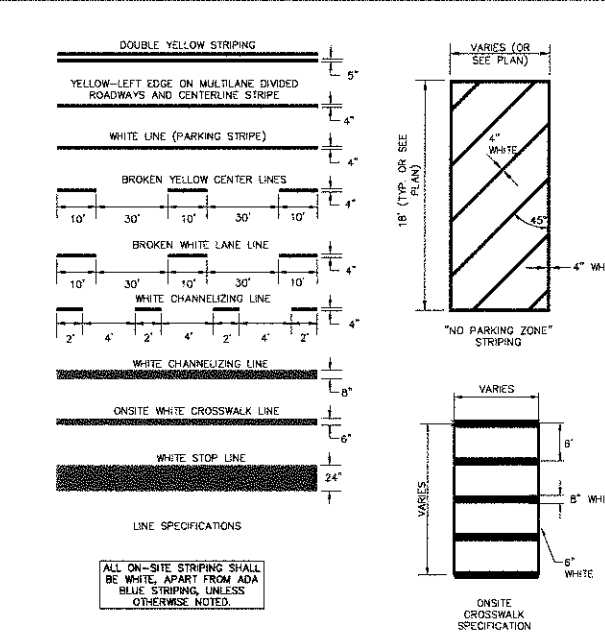
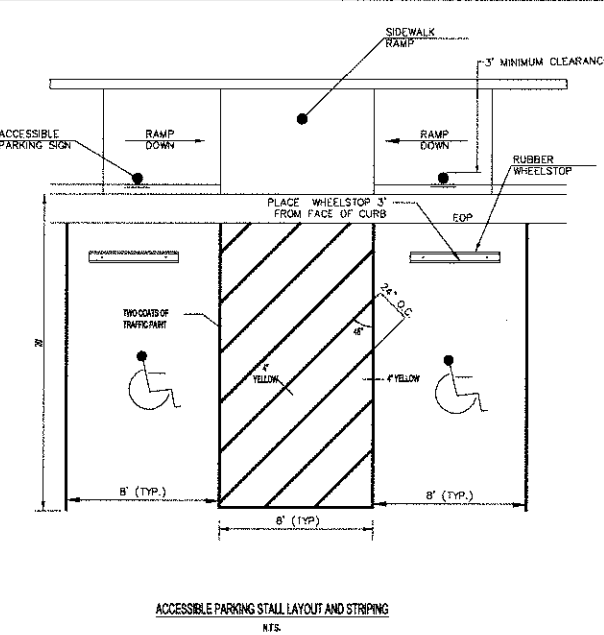
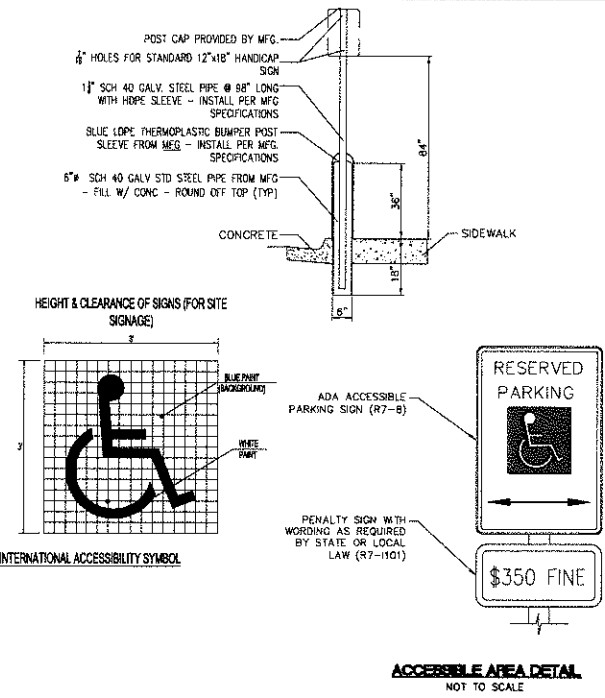
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EXP: 11/30/2021
PROFESSIONAL ENGINEER
ILLINOIS



THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN AS APPROXIMATE. THEY MAY NOT BE EXACTLY LOCATED. THE CONTRACTOR SHALL FIELD VERIFY THE EXISTENCE AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MAY BE INCURRED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PREPARE ANY AND ALL UNDERGROUND UTILITIES.

NOTICE: CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE OWNER SHALL NOT BE RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MAY BE INCURRED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PREPARE ANY AND ALL UNDERGROUND UTILITIES.

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24-HOUR CONTACT:
PANDA PM
HAKIM YALA
847-477-5125

BOHNE & REAR
082-081201
REGISTERED
PROFESSIONAL
ENGINEER
OR
ILLINOIS

DATE: 1/19/2021
EXP: 11/30/2021

PANDA EXPRESS, INC.
1683 Walnut Grove Ave.
Rosemead, California
91770
Telephone: 626.799.9998
Facsimile: 626.372.6288

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ISSUE DATE:

PC SUBMITTAL	11/20/2020
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DRAWN BY: LEH

PANDA PROJECT #: D7058
ATWELL PROJECT #: 18003769

NORR
ARCHITECTURAL CONSULTANTS

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NAPERVILLE, IL 60563
DESIGN FIRM #164-000878

PANDA EXPRESS
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7505 KINGERY HIGHWAY
WILLOWBROOK, IL 60527

CONSTRUCTION DETAILS

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Rosemead, California
91770
Telephone: 626.799.9898
Facsimile: 626.372.8288

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REVISIONS:

VILLAGE PLAN REV. #1 01/19/2021

ISSUE DATE:

PC SUBMITTAL 11/20/2020

DRAWN BY: LEH

PANDA PROJECT # D7058
ATWELL PROJECT #: 18003769



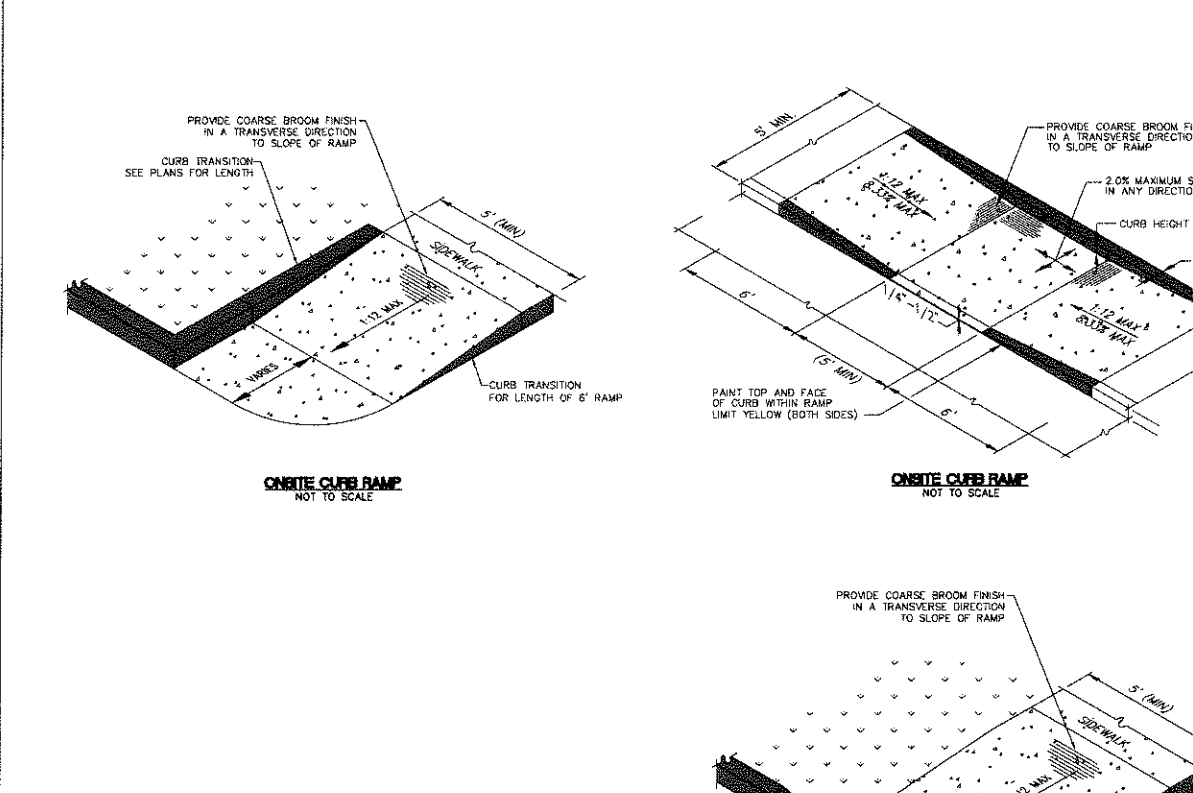
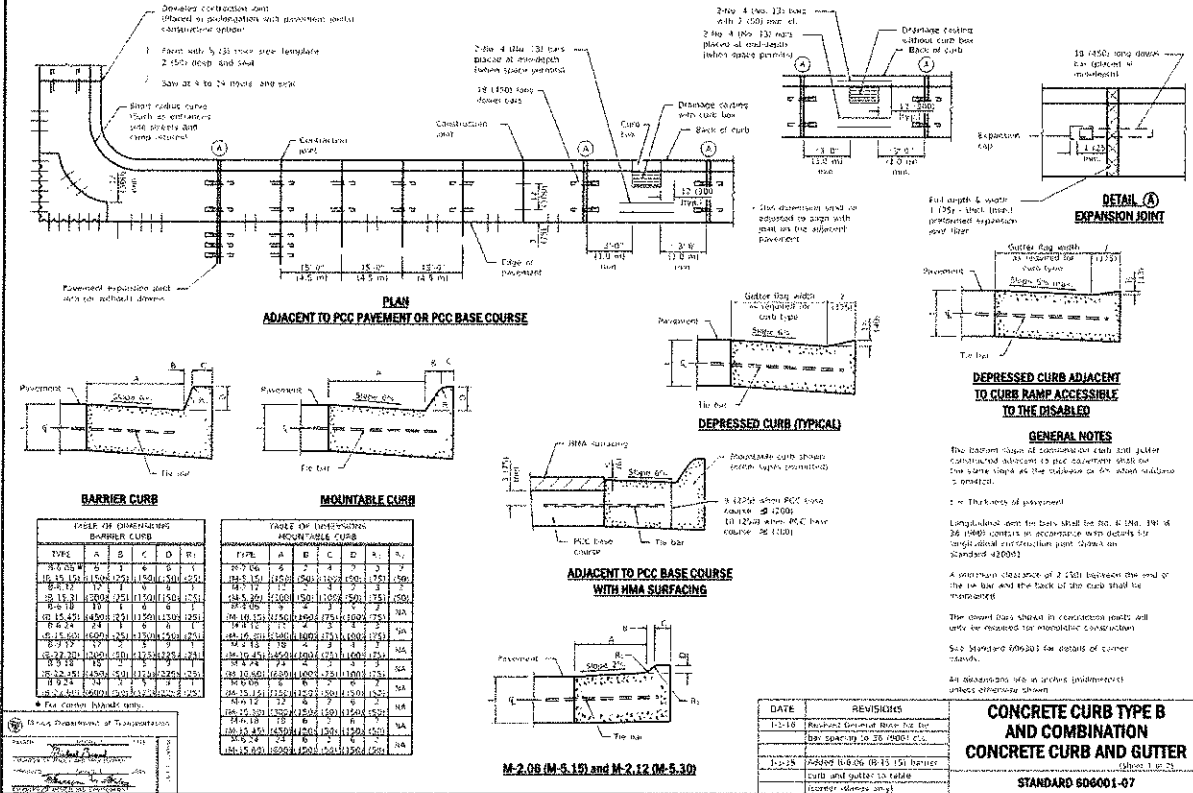
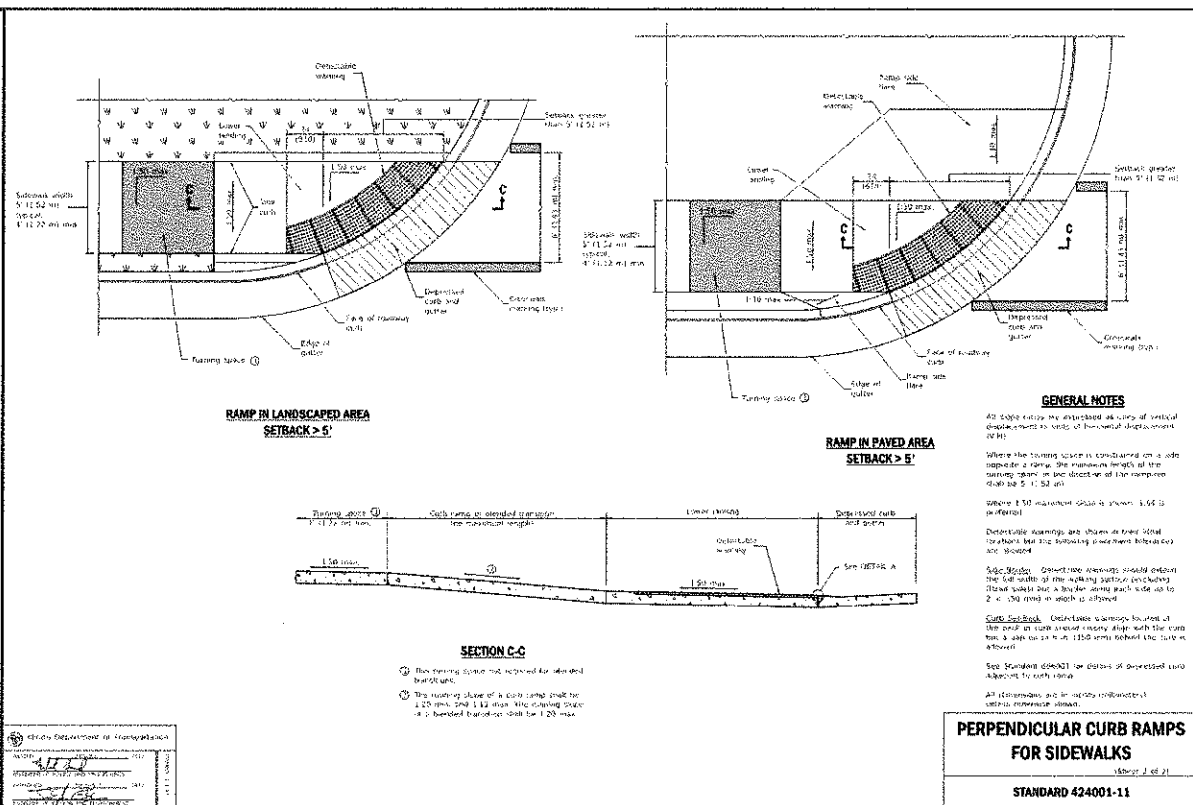
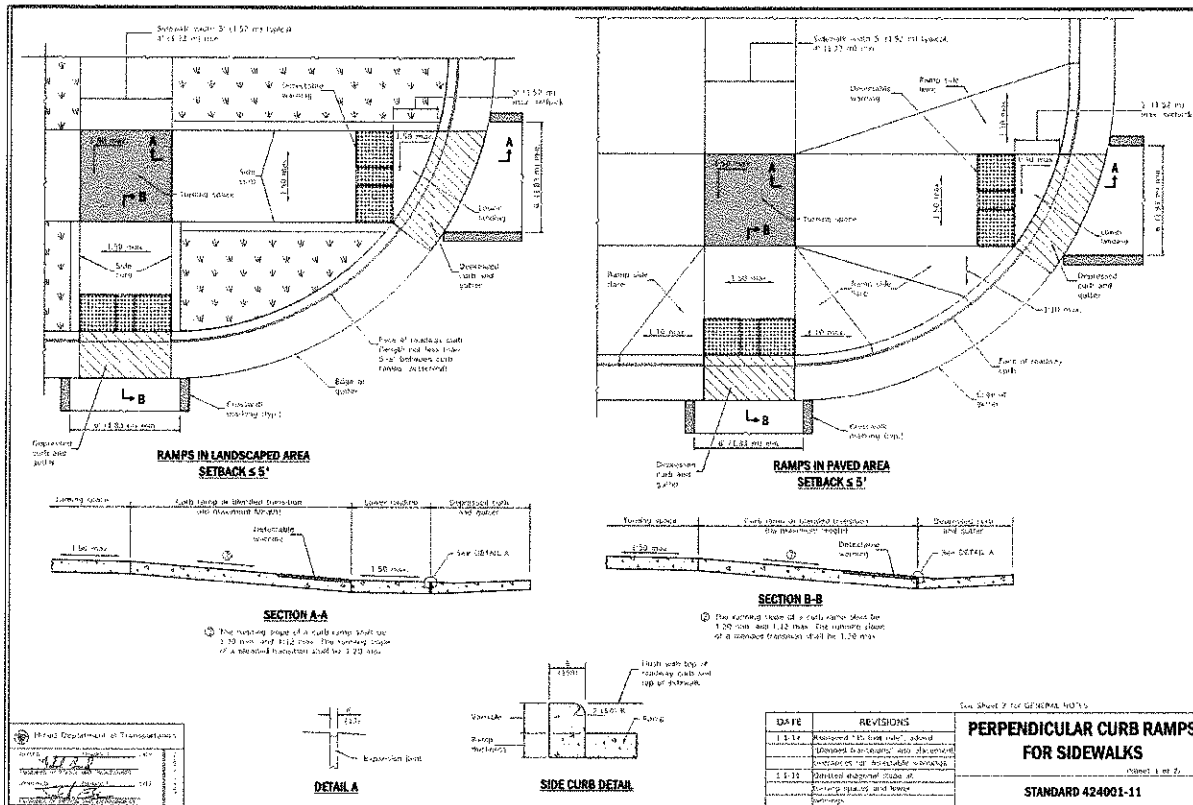
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NOTICE:
CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. NEITHER THE OWNER NOR THE ENGINEER SHALL BE DEEMED TO ASSUME ANY RESPONSIBILITY FOR SAFETY OF THE WORK, OR PERSONS ENGAGED IN THE WORK, OR ANY OTHER PERSONS, OR OF ANY OTHER PERSONS.

24-HOUR CONTACT:
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HAKIM YALA
847-477-5125

ROSE E. HARRIS
085-081201
REGISTERED
PROFESSIONAL
ENGINEER
ILLINOIS

DATE: 1/19/2021
EXP: 11/30/2021

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NAPERVILLE, IL 60563
DESIGN PM: #184-033876



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REVISIONS

VILLAGE PLAN REV. #1 01/19/2021

ISSUE DATE:

PC SUBMITTAL 11/20/2020

DRAWN BY: LEH

PANDA PROJECT #: D7058
ATWELL PROJECT #: 18003769

NORR
ARCHITECTS INTERIORS PLANNERS

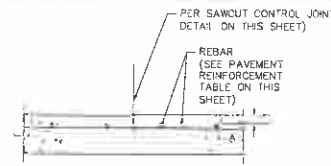
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1250 EAST DEWE ROAD, SUITE 300
NAPERVILLE, IL 60563
DESIGN TEAM #164-000576

PANDA EXPRESS
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7505 KINGSLEY HIGHWAY
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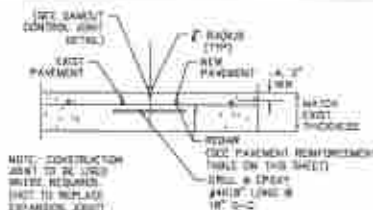
**CONSTRUCTION
DETAILS**

C08.2

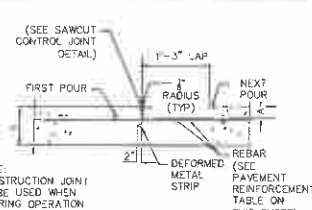
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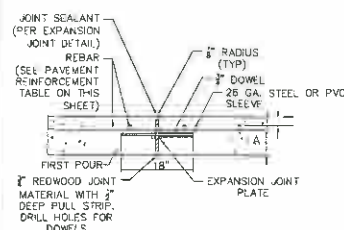
SAWCUT JOINT
N.T.S.



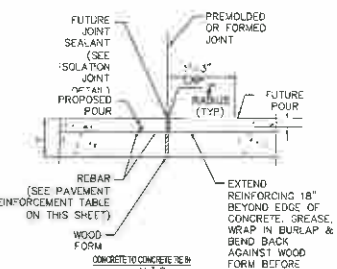
CONCRETE TO CONCRETE JOINT
N.T.S.



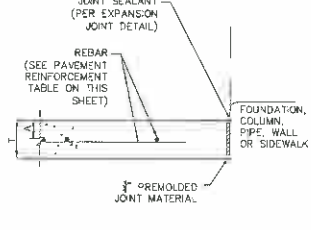
CONSTRUCTION JOINT
N.T.S.



SAWCUT JOINT
N.T.S.



CONCRETE TO CONCRETE JOINT
N.T.S.

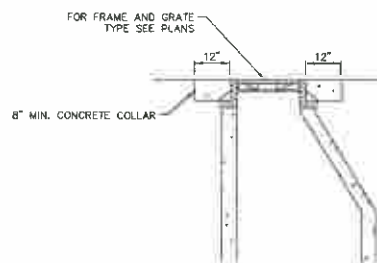


CONSTRUCTION JOINT
N.T.S.

NOTES:
1. REINFORCING STEEL BAR SIZE/SPACING SPECIFICATIONS IN SECTION REPORT SHALL SUPERSEDE JOINT TABLE.
2. REINFORCING STEEL BAR/SPACING IS BASED ON MIN. 60,000 PSI TENSILE STRENGTH REINFORCING STEEL AS SHOWN.
3. CONCRETE PAVING MIX DESIGN SHALL HAVE MINIMUM 4000 PSI COMPRESSIVE STRENGTH AT 28 DAYS. GEOTECHNICAL REPORT CONCRETE PAVING MIX DESIGN SHALL SUPERSEDE VALUES HEREIN.
4. BASEMENT JOINT SPACING SHALL BE PER JOINT LAYOUT PLAN (IF PROVIDED) BUT SHALL NOT EXCEED VALUES IN TABLE.
5. MAXIMUM JOINT SPACING IN GEOTECHNICAL REPORT SHALL SUPERSEDE VALUES IN JOINT TABLE.
6. USE STATE DOT SURFACE DRAINAGE (OTHERWISE SPECIFIED BY GEOTECHNICAL REPORT).
7. ALL JOINTS IN PAVING SHALL BE REFLECTED IN CURBS AND SHALL HAVE ALL THEIR RESPECTIVE JOINTED MATERIALS PROVIDED (I.E. EXPANSION JOINTS SHALL HAVE THEIR RESPECTIVE FILLER BOARD AND CATCH REPLACES).
8. CURB EXPANSION JOINTS - IF THERE IS AN EXPANSION JOINT IN THE PAVING, THE EXPANSION JOINT MUST ALSO BE CUT AT THE EXPANSION JOINT AND NOT ALLOWED TO RUN THROUGH THE CURB. THE EXPANSION JOINTS MUST ALSO BE CUT AT THE EXPANSION JOINTS. A SAW CUT EXPANSION JOINT IS NOT ACCEPTABLE. BECAUSE NORMAL EXPANSION AND CONTRACTION WILL CAUSE THE CONCRETE TO PUSH AGAINST THE CURB AND CAUSE THE CURB TO EVENTUALLY FAIL. IF AN EXPANSION JOINT IS LEFT OUT AND MUST BE SAW CUT IN, THE CURB SHOULD BE CUT TWICE AND A 2" PIECE OF CONCRETE IS REMOVED. IN ALL CASES THE JOINT SHOULD BE CAULKED WITH NPI.
9. CONCRETE TOUCHING THE BACK OF CURBS - ANY CONCRETE THAT TOUCHES THE BACK OF A CURB INCLUDING SIDEWALKS, ISLAND NOSINGS AND PARKING PADS SHALL BE ISOLATED FROM THE CURB USING A BLACK EXTERIOR APPROPRIATE POLYURETHANE. CONTRACTOR SHALL USE A REMOVABLE STRIP ON A 2" STRIP AND SEAL THE JOINT WITH ALL THE JOINT EXCEPTED IS 2" THE ISLAND NOSINGS ARE POURED MONOLITHICALLY WITH THE CURB AND PAVED SIDE.
10. CURBS AT THE BUILDING FOUNDATION - IF A CURB TOUCHES THE BUILDING FOUNDATION, IT NEEDS TO BE ISOLATED WITH EXPANSION JOINT MATERIAL JUST LIKE THE PAVING. IF AN EXPANSION JOINT IS LEFT OUT AND MUST BE SAW CUT IN, A 2" PIECE OF CONCRETE SHOULD BE REMOVED. THE JOINT SHOULD BE CAULKED WITH NPI.
11. EXPANSION JOINTS AT ISLAND NOSINGS - IF THE ISLAND NOSINGS ARE POURED MONOLITHICALLY WITH THE CURB AND PARKING LOT, THEN PAVING EXPANSION JOINTS SHOULD CONTINUE THROUGH THE NOSINGS.

REINFORCEMENT TABLE				
CONCRETE SECTION DESIGNATION	SLAB THICKNESS (IN)	COVER (IN) MIN	MAX EXPANSION JOINT SPACING (FT)	60,000 PSI STEEL REINFORCING STEEL BAR SIZE & SPACING
PARKING STALLS	5	2	15	#5 @ 24" CC
DRIVE THRU	6	2	15	#5 @ 24" CC
TRUCK COMPACTOR	7	2	15	#5 @ 24" CC

CONCRETE PAVEMENT SECTION
1/11 TO SCALE



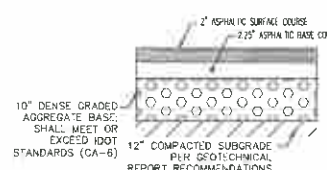
CONCRETE COLLAR DETAIL
NOT TO SCALE

NOTES:
1. CONCRETE COLLARS SHALL BE INSTALLED AROUND ALL UTILITY STRUCTURE TOPS (CATCH BASINS, MANHOLES, ETC.)
2. CONCRETE COLLAR SHALL BE PAINTED BLACK IF STRUCTURE IS WITHIN ASPHALT PAVEMENT.

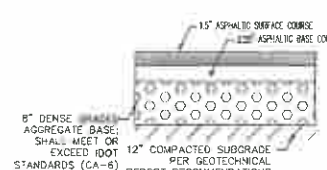
Design Specific Soil Information

	Soil Region			
	Foundation	Drainage	Drainage	Drainage
Drainage	GP	GP	GP	GP
Foundation	Angular (1/2" clear stone (10' thick))	Angular (1/2" clear stone (10' thick))	Angular (1/2" clear stone (10' thick))	Angular (1/2" clear stone (10' thick))
Drainage	GP	GP	GP	GP
Foundation	GP	GP	GP	GP
Drainage	GP	GP	GP	GP
Foundation	GP	GP	GP	GP
Drainage	GP	GP	GP	GP
Foundation	GP	GP	GP	GP

NOTES:
1. This design was prepared in accordance with the minimum design criteria required by the Illinois Department of Transportation (IDOT) and the National Concrete Drainage Association (NCDRA) for the design of concrete drainage systems.
2. No analysis of global stability, local or differential settlement, or seismic effects has been performed.
3. This design is for a typical drainage system and does not take into account any specific site conditions or special requirements.
4. The design is based on the assumption that the drainage system will be installed and maintained in accordance with the design criteria.
5. The design is based on the assumption that the drainage system will be installed and maintained in accordance with the design criteria.
6. The design is based on the assumption that the drainage system will be installed and maintained in accordance with the design criteria.



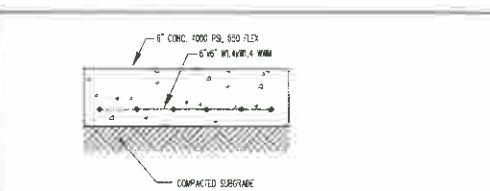
HEAVY-DUTY ASPHALT



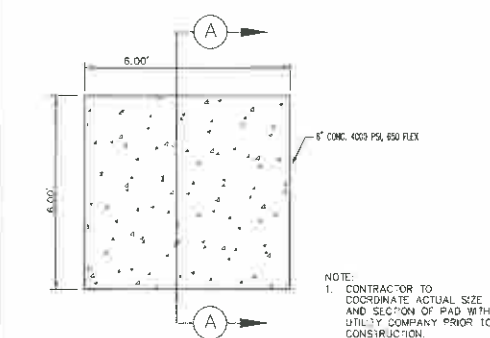
LIGHT-DUTY ASPHALT

ASPHALT NOTES:
• THE ASPHALT SURFACE COURSE SHOULD CONFORM TO THE MOST RECENT EDITION OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) STANDARD SPECIFICATION FOR ROAD AND BRIDGE CONSTRUCTION, FOR HOT MIX ASPHALTIC CONCRETE SURFACE COURSE.
• THE BASE COURSE SHOULD CONFORM TO THE IDOT STANDARDS FOR BASE COURSE.
• TACK COAT SHOULD BE PROVIDED ATOP EACH PAVEMENT SECTION.

ASPHALT PAVEMENT SECTION
NOT TO SCALE

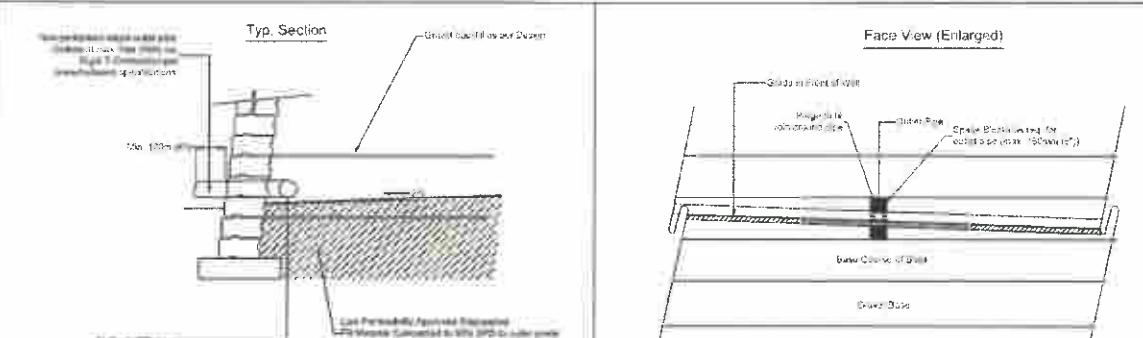
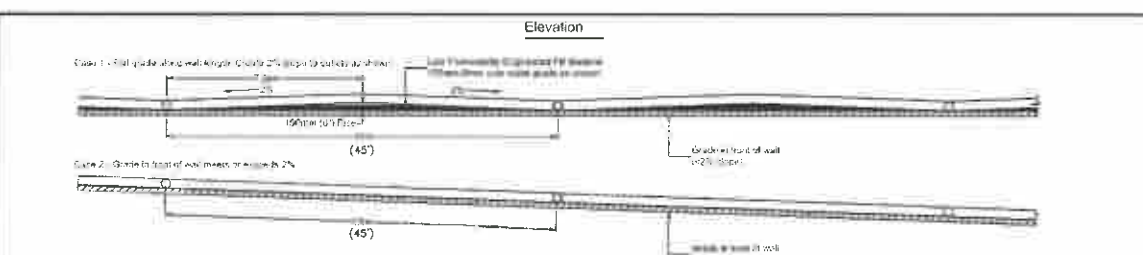


SECTION A-A



TRANSFORMER PAD
NOT TO SCALE

NOTE:
1. CONTRACTOR TO COORDINATE ACTUAL SIZE AND SECTION OF PAD WITH UTILITY COMPANY PRIOR TO CONSTRUCTION.



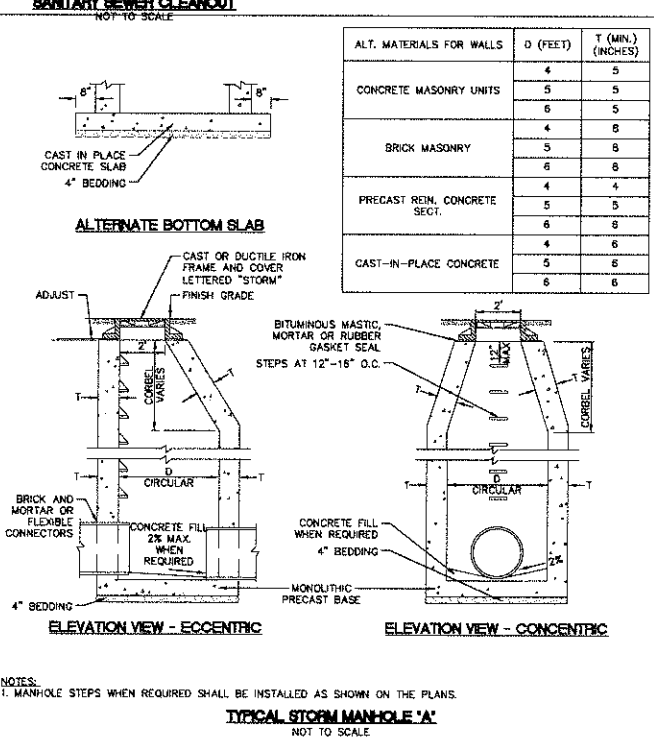
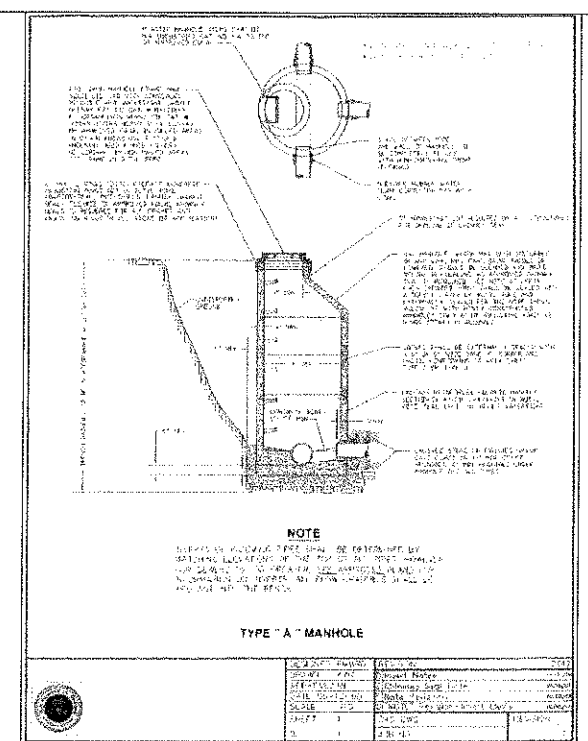
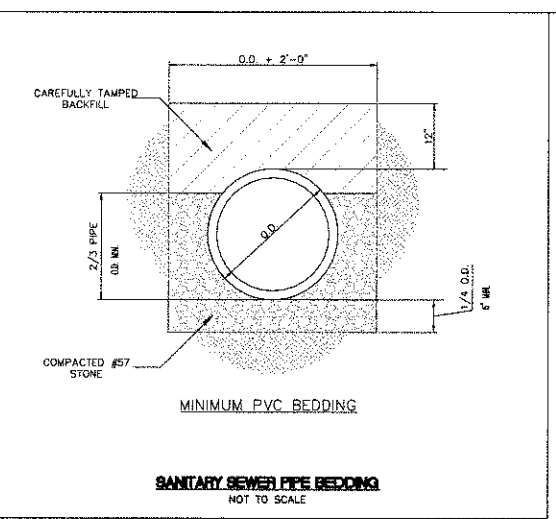
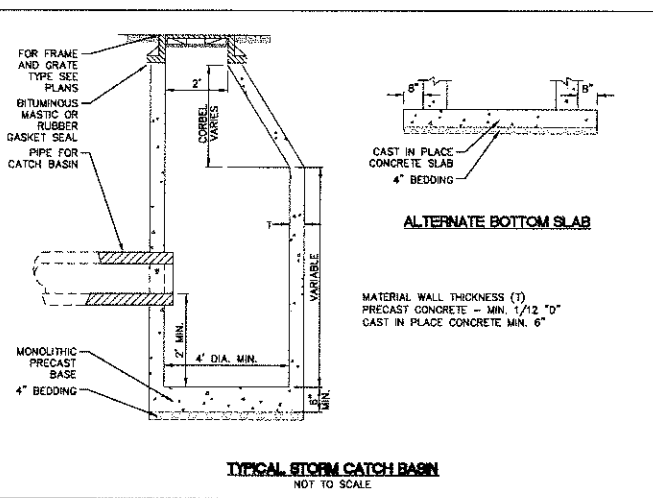
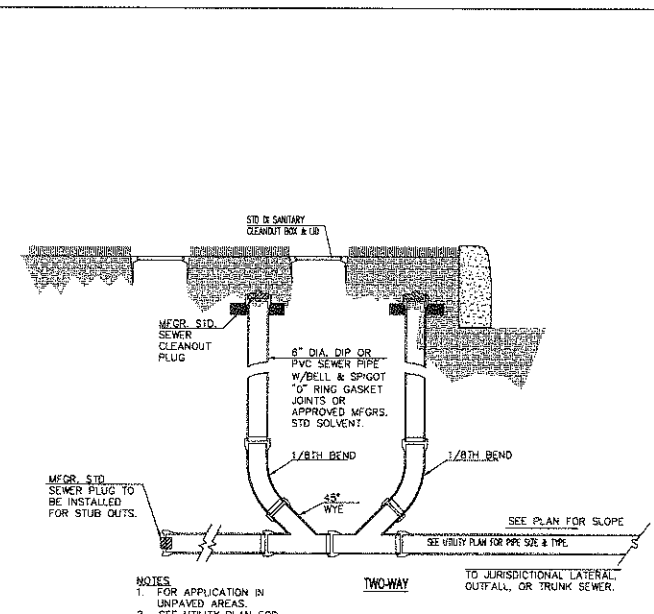
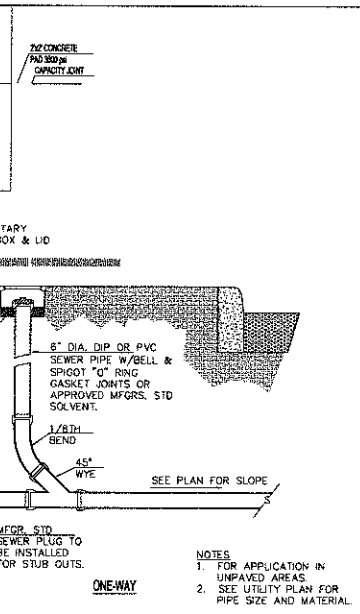
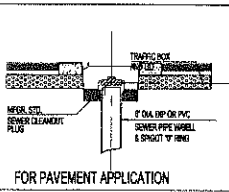
DESIGN SPECIFIC GEOTECHNICAL INFORMATION			
Drainage System	GP	Drainage System	GP
Foundation	GP	Foundation	GP
Drainage System	GP	Drainage System	GP
Foundation	GP	Foundation	GP
Drainage System	GP	Drainage System	GP
Foundation	GP	Foundation	GP
Drainage System	GP	Drainage System	GP
Foundation	GP	Foundation	GP

Pisa® / Roman®
Drain Outlet Through Face of Wall
RisiStone®
retaining wall systems
P21DET003.DWG

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NOTES:
1. THE LOCATION OF UTILITIES SHALL BE DETERMINED BY THE UTILITY LOCATOR OR BY THE UTILITY LOCATOR. THE UTILITY LOCATOR SHALL BE RESPONSIBLE FOR THE LOCATION OF UTILITIES. THE UTILITY LOCATOR SHALL BE RESPONSIBLE FOR THE LOCATION OF UTILITIES. THE UTILITY LOCATOR SHALL BE RESPONSIBLE FOR THE LOCATION OF UTILITIES.
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24-HOUR CONTACT:
PANDA PM
HAKIM YALA
847-477-5125





ALT. MATERIALS FOR WALLS	D (FEET)	T (MIN.) (INCHES)
CONCRETE MASONRY UNITS	4	5
BRICK MASONRY	5	8
PRECAST REIN. CONCRETE SECT.	4	4
CAST-IN-PLACE CONCRETE	5	6

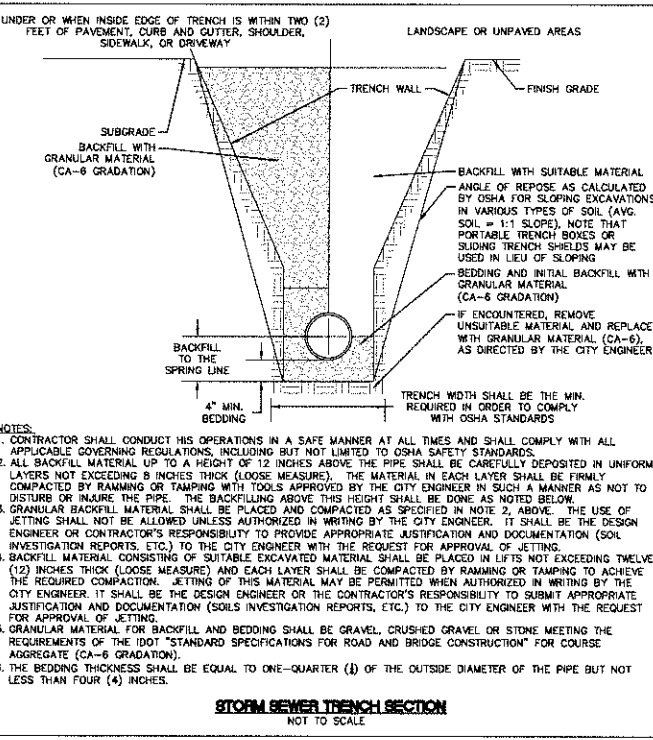
PLAN VIEW


ELEVATION VIEW

NOTES:

1. DETAIL OF M.H. TOP TO BE USED WHERE RESTRICTED HEAD ROOM WILL NOT PERMIT TAPERED WALLS.

2. MANHOLE STEPS WHEN REQUIRED SHALL BE INSTALLED AS SHOWN ON THE PLANS.






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HAKIM YALA
847-477-5125



DATE: 1/19/2021
EXP: 11/30/2021



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1683 Walnut Grove Ave.
Rosemead, California 91770
Telephone: 626.799.9898
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REVISIONS:


VILLAGE PLAN REV. #1	01/19/2021
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ISSUE DATE:


PC SUBMITTAL	11/20/2020
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DRAWN BY: LEH

PANDA PROJECT # D7058
ATWELL PROJECT # 18003769



REINFORCED TRENCH SHIELDS



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NAPERVILLE, IL 60563
DESIGN FIRM #184-305519

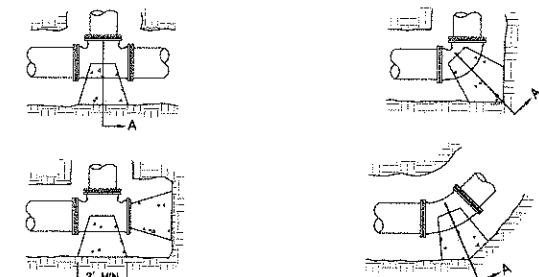
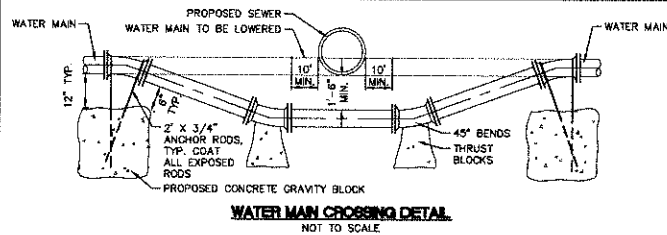
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WILLOWBROOK, IL 60527

CONSTRUCTION
DETAILS

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1/19/2021 11:20 AM HAKIM YALA

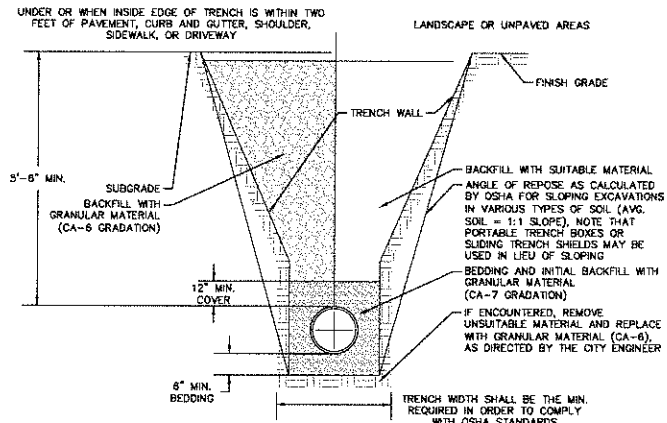


MINIMUM SQ. FT. AREA OF VERTICAL CROSS SECTION AGAINST 2000 PSF UNDISTURBED EARTH

	6"	8"	10"	12"
90° BEND	5.5	9.4	15.4	21.8
45° BEND	3.0	5.1	8.4	11.9
22.5° BEND	1.5	2.8	4.3	6.1
11.25° BEND	1.5	2.6	4.3	6.1
TEE	3.9	6.7	10.9	15.5

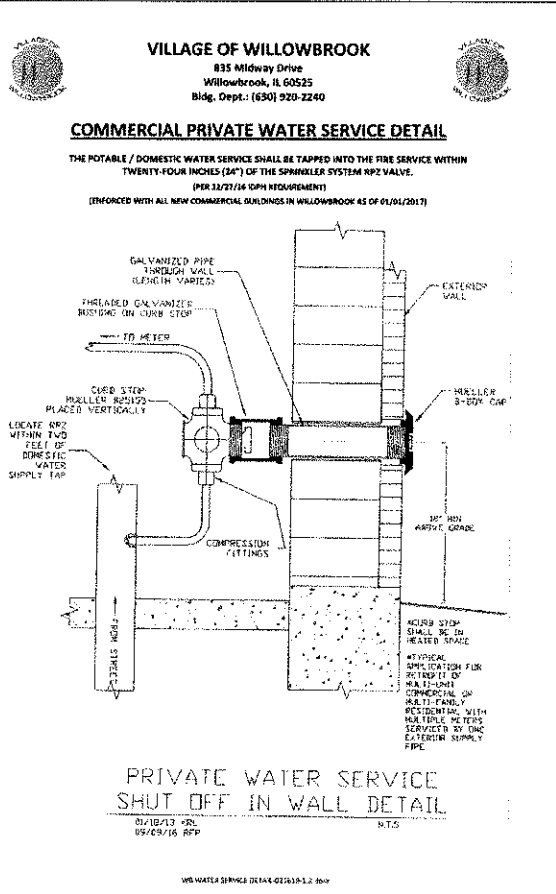
NOTES:
 1. THRUST BLOCKING TO PREVENT MOVEMENT OF LINES UNDER PRESSURE AT BENDS, TEES, CAPS, VALVES, HYDRANTS AND AT POINTS SPECIFIED BY THE ENGINEER SHALL BE CLASS "SI" CONCRETE, A MINIMUM OF 12" THICK, PLACED BETWEEN SOLID GROUND AND FITTING, AND SHALL BE ANCHORED IN SUCH A MANNER THAT PIPE AND FITTING WILL BE ACCESSIBLE FOR REPAIRS. THRUST BLOCKS SHALL BE PLACED AT BENDS OF 15-1/4 DEGREES OR MORE. RETAINER GLANDS MAY BE USED IN PLACE OF THRUST BLOCKS. THE COST OF THRUST BLOCKS OR RETAINER GLANDS SHALL BE INCLUDED IN THE COST OF THE FITTING.

THRUST BLOCKS
NOT TO SCALE



- NOTES:**
- CONTRACTOR SHALL CONDUCT HIS OPERATIONS IN A SAFE MANNER AT ALL TIMES AND SHALL COMPLY WITH ALL APPLICABLE COVERING REGULATIONS INCLUDING BUT NOT LIMITED TO OSHA SAFETY STANDARDS.
 - ALL BACKFILL MATERIAL UP TO A HEIGHT OF 12 INCHES ABOVE THE PIPE SHALL BE CAREFULLY DEPOSITED IN UNIFORM LAYERS NOT EXCEEDING 8 INCHES THICK (LOOSE MEASURE). THE MATERIAL IN EACH LAYER SHALL BE FIRMLY COMPACTED BY RAMMING OR TAMPING WITH TOOLS APPROVED BY THE CITY ENGINEER IN SUCH A MANNER AS NOT TO DISTURB OR INJURE THE PIPE. THE BACKFILLING ABOVE THIS HEIGHT SHALL BE DONE AS NOTED BELOW.
 - GRANULAR BACKFILL MATERIAL SHALL BE PLACED AND COMPACTED AS SPECIFIED IN NOTE 2, ABOVE. THE USE OF JETTING SHALL NOT BE ALLOWED UNLESS AUTHORIZED IN WRITING BY THE CITY ENGINEER. IT SHALL BE THE DESIGN ENGINEER OR CONTRACTOR'S RESPONSIBILITY TO PROVIDE APPROPRIATE JUSTIFICATION AND DOCUMENTATION (SOIL INVESTIGATION REPORTS, ETC.) TO THE CITY ENGINEER WITH THE REQUEST FOR APPROVAL OF JETTING. NO RECYCLED CONCRETE SHALL BE ALLOWED.
 - BACKFILL MATERIAL CONSISTING OF SUITABLE EXCAVATED MATERIAL SHALL BE PLACED IN LIFTS NOT EXCEEDING TWELVE (12) INCHES THICK (LOOSE MEASURE) AND EACH LAYER SHALL BE COMPACTED BY RAMMING OR TAMPING TO ACHIEVE THE REQUIRED COMPACTION. JETTING OF THIS MATERIAL MAY BE PERMITTED WHEN AUTHORIZED IN WRITING BY THE CITY ENGINEER. IT SHALL BE THE DESIGN ENGINEER OR THE CONTRACTOR'S RESPONSIBILITY TO SUBMIT APPROPRIATE JUSTIFICATION AND DOCUMENTATION (SOIL INVESTIGATION REPORTS, ETC.) TO THE CITY ENGINEER WITH THE REQUEST FOR APPROVAL OF JETTING.
 - GRANULAR MATERIAL FOR BACKFILL AND BEDDING SHALL BE GRAVEL, CRUSHED GRAVEL OR STONE MEETING THE REQUIREMENTS OF THE DOT "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" FOR COURSE AGGREGATE.

WATER MAIN TRENCH SECTION
NOT TO SCALE



PRIVATE WATER SERVICE SHUT OFF IN WALL DETAIL
N.T.S.

WATER SERVICE DETAIL 4-4-02 (10/13/12) RPL

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24-HOUR CONTACT:
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REVISIONS:

NO.	DESCRIPTION	DATE
1	VILLAGE PLAN REV. #1	01/19/2021

ISSUE DATE:

NO.	DESCRIPTION	DATE
1	PC SUBMITTAL	11/20/2020

DRAWN BY: LEH

PANDA PROJECT # D7058
 ATWELL PROJECT # 18003769



PANDA EXPRESS
 WARM & WELCOME 2300
 7505 KINGSLEY HIGHWAY
 WILLOWBROOK, IL 60527

CONSTRUCTION DETAILS

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TRUE WARM & WELCOME 2300



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REVISIONS:

VILLAGE PLAN REV. #1 01/19/2021

ISSUE DATE:

PC SUBMITTAL 11/20/2020

DRAWN BY: LEH

PANDA PROJECT #: D7058

ATWELL PROJECT #: 18003769

NORR
ARCHITECTS PLANNERS ENGINEERS



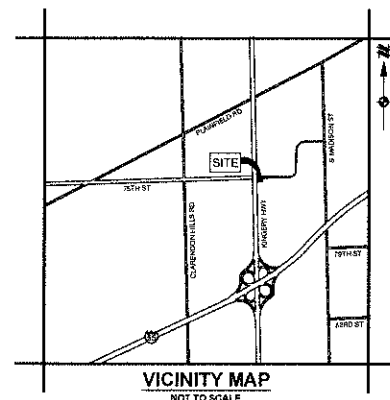
PANDA EXPRESS

WARM & WELCOME 2300
7505 KINGSWAY HIGHWAY
WILLOWBROOK, IL 60527

LANDSCAPE PLAN

L01.0

TRUE WARM & WELCOME 2300



VICINITY MAP
NOT TO SCALE

LANDSCAPE LEGEND

---	BOUNDARY LINE
---	EXISTING CONTOUR
---	PROPOSED CONTOUR
---	EXISTING ROW
---	EXISTING EASEMENT LINE
---	EXISTING SANITARY SEWER
---	PROPOSED SANITARY SEWER
---	EXISTING STORM SEWER
---	PROPOSED STORM SEWER
---	EXISTING WATER LINE
---	PROPOSED WATER LINE
---	EXISTING UNDERGROUND GAS
---	PROPOSED UNDERGROUND GAS
---	EXISTING UNDERGROUND TELEPHONE
---	EXISTING UNDERGROUND ELECTRIC
---	PROPOSED UNDERGROUND ELECTRIC
---	EXISTING UNDERGROUND CABLE
---	PROPOSED UNDERGROUND CABLE
---	PROPOSED IRRIGATION CONDUIT
---	PROPOSED ALUMINUM EDGING
---	EXISTING LIGHT POLE
---	PROPOSED LIGHT POLE/WALL PACK LIGHT
---	PROPOSED SHRUBS
---	PROPOSED EVERGREEN TREES
---	PROPOSED ORNAMENTAL TREES
---	PROPOSED DECIDUOUS TREES
---	PROPOSED KENTUCKY BLUE GRASS SOD
---	PROPOSED STONE MULCH

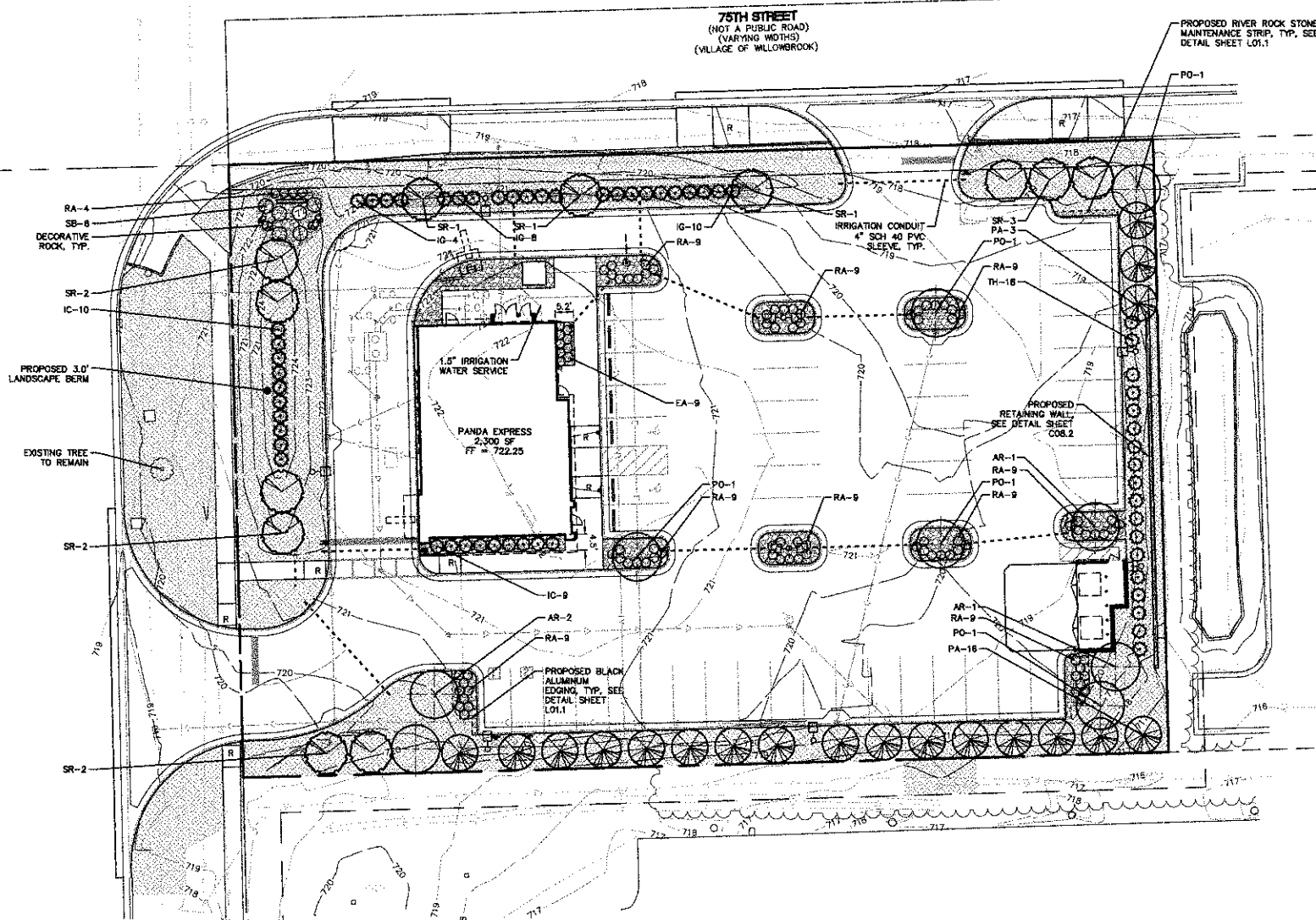
PLANT SCHEDULE

KEY	QTY.	BOTANICAL NAME	COMMON NAME	SIZE	REMARKS/SPEC.
AR	4	Acer rubrum	Red Maple	2 1/2" Cal.	B&B
PO	5	Platanus x acerifolia 'Bloodgood'	Bloodgood London Planetree	2 1/2" Cal.	B&B
PA	19	Picea abies	Norway Spruce	5'-6' Ht.	B&B
SR	12	Syringa reticulata 'Ivory Silk'	Ivory Silk Japanese Lilac	1 1/2" Cal. (6' Ht. min)	B&B, Multi-stem, 3 stems
EA	9	Eucalyptus alata 'Compacta'	Dwarf Burningbush	36" Ht., No. 5 Cont.	Trim to hedge
RA	85	Rhus aromatica 'Gro-Low'	Gro-Low Fragrant Sumac	No. 3 Cont.	
SB	5	Spiraea x bumalda 'Gold Mound'	Gold Mound Spirea	No. 3 Cont.	
IC	19	Ilex crenata 'Green Lustre'	Green Lustre Japanese Holly	24"-36" Ht., No. 5 Cont.	
IG	22	Ilex glabra 'Shamrock'	Shamrock Highberry	48" Ht., No. 5 Cont.	
TH	18	Taxus x media 'Hicksii'	Hicks Yew	No. 3 Cont.	Trim to hedge

CONTRACTOR SHALL PROTECT ALL ITEMS OUTSIDE LIMITS OF CONSTRUCTION UNLESS OTHERWISE NOTED IN THE CONSTRUCTION PLANS OR SPECIFICATIONS.

CONTRACTOR RESPONSIBLE FOR MAINTENANCE OF LANDSCAPE FOR 90 DAYS FOLLOWING INSTALLATION.

CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITIES (LOCATIONS AND ELEVATIONS) PRIOR TO STARTING CONSTRUCTION AND ALERT ENGINEER TO ANY DISCREPANCIES IMMEDIATELY.



PLANT CALCULATION

ORDINANCE COMPLIANCE MATRIX			
Zoning: B-2 COMMUNITY SHOPPING	Acres: 1.00	Acres	
Parking: 46	spaces	spaces	
Street Frontage (Route 83): 170.00	feet	feet	
Street Frontage (75th St): 284.00	feet	feet	
Sign Surface Area: 174.00	square feet	square feet	
USDA Zone: 5 (-20 degrees to -10 degrees)			

Section 9-4-1, BUSINESS DISTRICT LANDSCAPING REQUIREMENTS

Section 9-4-1(D) - FRONT AND EXTERIOR SIDE YARD LANDSCAPE PLANTING REQUIREMENTS

requirement	required	provided
1. Landscape plantings shall be provided within any yard adjoining a street within a business district, in conformance with the standards contained within section 9-14-2.23 of the zoning ordinance. The quantity of plantings required within all yards adjacent to a street, in addition to required parking trees, shall be based upon a point system ratio according to five (5) times the linear length of the specific yard frontage, measured at the property line.	850 Points (Route 83) 1280 Points (75th St)	960 Points 1280 Points
Route 83		
Shade Tree	100	200
Conifer Tree	50	60
Ornamental Tree	50	300
Evergreen Shrub	15	14
Deciduous Shrub	10	19
Total	960	
75th street		
Shade Tree	100	200
Conifer Tree	50	3
Ornamental Tree	50	6
Evergreen Shrub	15	22
Deciduous Shrub	10	27
Total	1280	

3. Earth berms constructed in a continuous and/or undulating configuration which perform a screening function and are within a minimum height range of two and one-half feet (2 1/2') to three feet (3'), may be credited up to a maximum of fifty percent (50%) of the total landscape planting points required within a yard. The maximum percentage of points credited for berms shall be equal to ten (10) times the average berm height where said berm extends across a minimum of seventy five percent (75%) of the yard frontage.

2.5 to 3 Feet Min Berm Ht. (Route 83)
3 Feet Min Berm Ht. (Route 83)
48 % Berm Length (Route 83)
75 % Min Berm Length (Route 83)
No point adjustment applied as Berm length is <75% of the street frontage.
No berm provided along 75th Street

Section 9-10-04 and 9-10-5, OFF STREET LOADING & PARKING LANDSCAPING REQUIREMENTS

Section 9-10-5(D) - DESIGN AND MAINTENANCE

requirement	required	provided
1. Front Yards and Exterior Side Yards: Permanent peripheral screening at least four (4) feet high shall be constructed and maintained in interior side and rear yards adjacent to parking areas. This screen shall consist of a planted earth berm, densely planted.	4' berm/screen	4' evergreen screen
2. Interior Side and Rear Yards: Permanent peripheral screening at least five feet (5') high shall be constructed and maintained in interior side and rear yards adjacent to parking areas. This screen shall consist of a planted earth berm, densely planted.	5' berm/screen	5' evergreen screen
3. Interior Parking Lot Landscaping: Landscaped areas shall occupy not less than ten percent (10%) of the remaining site area excluding areas occupied by buildings or other structures, the required front yard and all street rights of way. End parking islands of a minimum one foot (1') width shall be provided at each side end of all parking bays. Intermediate islands of a minimum six foot (6') width shall be provided between every twenty (20) parking spaces. Center dividing islands of a minimum ten foot (10') width shall be provided between every three (3) parking bays. Each island shall be effectively landscaped with appropriately sized plantings at a minimum ratio of one shade tree and nine (9) dwarf variety shrubs per island and/or fifty (50) linear feet of parking area. Shade tree distribution shall provide a minimum of one tree in the area occupied by each twenty (20) parking spaces.	10 % 9 trees 81 shrubs	25 % 9 trees 81 shrubs

*Two of the interior landscape island trees have been provided at the ends of the southern parking bay, and one in the exterior side yard due to layout constraints.

Section 9-11-14, MONUMENT SIGN LANDSCAPING REQUIREMENTS

Section 9-11-14(A)

requirement	required	provided
1. For every square foot of sign surface area, there shall be provided one and one-half (1 1/2) plants.	261 SF	261 SF

Section 9-14-2.23 GENERAL LANDSCAPING REQUIREMENTS

Section 9-14-2.23(B) - DESIGN AND MAINTENANCE

requirement	required	provided
1. Minimum Plantings: The applicant shall minimally provide one approved planting or each seven hundred twenty five (725) square feet of gross lot area. For each required planting, the applicant shall provide a minimum of one (1) approved planting. The 199 plantings are provided as a result of separate requirements (i.e. front and side yard screening, off street parking, monument sign, and foundation requirements) and satisfies the minimum requirement of 90 plantings.	80 Plantings 600 SF ground cover	199 Plantings 2625 SF ground
2. Approved Plantings and Ground Cover		
Not less than 2 1/2" in caliper and comprise a Shade Tree: minimum of 2% of the required plantings, excluding pathway trees.	5% Min	15%
Shall not be less than 5 feet in height and Conifers comprise a minimum of 10% of the required plantings.	10% Min	32%
Shall be at least 1 1/2" caliper not less than 6' in height, and comprise at minimum 20% of the landscape plantings.	20% Min	20%
3. Border Plantings And Foundation Plantings: A landscaped area having a minimum width of seven feet (7') shall be provided around all buildings except for building entrances and the front of retail business facilities to permit appropriate foundation plantings. Border plantings and foundation plantings shall be provided and shall comprise the remainder of the required landscape plantings, of which a minimum of one-half (1/2) shall be deciduous shrubs at a minimum height of three feet (3') and the remaining one-half (1/2) shall be evergreen shrubs at a minimum height of two feet (2').	7 Foot foundation planting width 50% Deciduous Shrubs 50% Evergreen Shrubs	4.5 "Foot foundation planting width 50% Deciduous Shrubs 50% Evergreen Shrubs

*A variance is requested on the required foundation planting width.

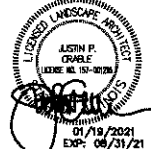
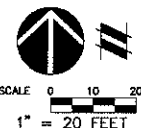


Know what's below.
Call before you dig.

THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN BY AN APPROXIMATE WAY ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVE. THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MAY BE OCCURRED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

NOTICE: CONSTRUCTION SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SAFETY OF THE WORK AREA AND SHALL BE RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MAY BE OCCURRED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

24-HOUR CONTACT:
PANDA PM
HAKIM YALA
847-477-5125



GENERAL LANDSCAPE NOTES

- LANDSCAPE CONTRACTOR (CONTRACTOR) SHALL VISIT SITE, INSPECT EXISTING CONDITIONS AND REVIEW PROPOSED PLANTINGS AND RELATED WORK. LANDSCAPE CONTRACTOR TO VERIFY ALL UTILITY LOCATIONS ON PROPERTY WITH THE GENERAL CONTRACTOR AND BY CALLING 811 PRIOR TO STARTING ANY CONSTRUCTION. IN CASE OF DISCREPANCY BETWEEN PLAN AND PLANT LIST, PLAN SHALL GOVERN QUANTITIES CONTACT LANDSCAPE ARCHITECT AND/OR OWNER'S REPRESENTATIVE WITH ANY CONCERNS. SIZES SPECIFIED IN THE PLANT LIST ARE MINIMUM SIZES TO WHICH THE PLANTS ARE TO BE INSTALLED.
- PRIOR TO ANY LAND CLEARING OR CONSTRUCTION, TREE PROTECTION FENCING IS TO BE INSTALLED BY THE CONTRACTOR. THIS FENCING SHALL BE INSTALLED AT THE DRIP LINE OF ALL TREES AND SHRUBS AND MUST BE MAINTAINED AS APPROVED FOR THE DURATION OF THE PROJECT. NO CUTTING, FILING OR TRESPASSING SHALL OCCUR INSIDE THE FENCED AREAS.
- LANDSCAPE CONTRACTOR SHALL COORDINATE THE PHASES OF CONSTRUCTION AND PLANTING INSTALLATIONS WITH OTHER CONTRACTORS WORKING ON SITE.
- WHERE EXISTING TREES AND/OR SIGNIFICANT SHRUB MASSINGS ARE FOUND ON SITE, WHETHER SHOWN ON THE DRAWING OR NOT, THEY SHALL BE PROTECTED AND SAVED UNLESS NOTED TO BE REMOVED AND/OR ARE IN AN AREA TO BE GRADED. ANY QUESTION REGARDING WHETHER PLANT MATERIAL SHOULD REMAIN OR NOT SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT AND/OR OWNER'S REPRESENTATIVE PRIOR TO REMOVAL.
- ALL EXISTING TREES TO REMAIN TO BE FERTILIZED AND PRUNED TO REMOVE DEAD WOOD AND DAMAGED OR RUBBING BRANCHES.
- NO PLANT MATERIAL SUBSTITUTIONS WILL BE ACCEPTED UNLESS APPROVAL IS REQUESTED OF THE LANDSCAPE ARCHITECT, OWNER, AND THE VILLAGE ZONING ADMINISTRATOR BY THE LANDSCAPE CONTRACTOR PRIOR TO INSTALLATION.
- ALL PLANT MATERIAL SHALL COMPLY WITH THE LATEST EDITION OF THE AMERICAN STANDARD FOR NURSERY STOCK, AMERICAN NURSERY AND LANDSCAPE ASSOCIATION. ALL LANDSCAPING SHALL BE INSTALLED AND MAINTAINED IN ACCORDANCE WITH THE STANDARDS OF THE AUTHORITY HAVING JURISDICTION AND IN ACCORDANCE WITH CURRENT INDUSTRY STANDARDS IN A NEAT, HEALTHY AND WEED FREE CONDITION.
- CONTRACTOR WILL SUPPLY FINISHED GRADE AND EXCAVATE AS NECESSARY TO SUPPLY 4" TOPSOIL DEPTH IN ALL PLANTING BEDS AND 4" TOPSOIL DEPTH IN ALL LAWN AREAS. BACKFILL AND CROWN PARKING LOT ISLANDS 6" ABOVE ADJACENT CURBS WITH TOPSOIL BACKFILL DIRECTLY BEHIND ALL CURBS AND ALONG SIDEWALKS AND COMPACT TO TOP OF CURB OR WALK TO SUPPORT VEHICLE AND PEDESTRIAN WEIGHT WITHOUT SETTLING.
- ACCEPTANCE OF GRADING AND SOO/SEED SHALL BE BY LANDSCAPE ARCHITECT AND/OR PROJECT REPRESENTATIVE. THE LANDSCAPE CONTRACTOR SHALL ASSUME MAINTENANCE RESPONSIBILITY UNTIL FINAL ACCEPTANCE HAS BEEN RECEIVED. MAINTENANCE SHALL INCLUDE WATERING, WEEDING, REPLACEMENT OF WASH-OUTS AND OTHER OPERATIONS NECESSARY TO KEEP SOO/SEED IN A THRIVING CONDITION UPON FINAL ACCEPTANCE BY LANDSCAPE ARCHITECT AND/OR OWNER'S REPRESENTATIVE. THE OWNER WILL ASSUME ALL MAINTENANCE RESPONSIBILITIES.
- PLANT MATERIAL LOCATIONS SHOWN ARE DIAGRAMMATIC AND MAY BE SUBJECT TO CHANGE IN THE FIELD AS REQUIRED.
- REPAIR ALL DAMAGE TO PROPERTY FROM PLANTING OPERATIONS AT NO COST TO THE OWNER.
- OWNER OR OWNER'S REPRESENTATIVE SHALL INSPECT LANDSCAPE INSTALLATION AND HAVE THE RIGHT TO REJECT AND WITHHOLD PAYMENT ON ANY PLANT MATERIAL(S) OF DAMAGED OR POOR QUALITY OR NOT MEETING SPECIFICATIONS.
- LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEAN-UP OF SITE AT THE COMPLETION OF LANDSCAPING EACH DAY. AT ALL TIMES THE SIDEWALKS SHALL BE MAINTAINED CLEAN AND FREE OF DEBRIS. REMOVE SURPLUS SOIL AND WASTE MATERIAL, TRASH AND DEBRIS FROM THE SITE AND LEGALLY DISPOSE OF SAME IN ACCORDANCE WITH FEDERAL, STATE AND LOCAL CODES AND REGULATIONS.
- LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR SOIL EROSION AND DUST CONTROL MEASURES PRIOR TO AND DURING CONSTRUCTION. THE LANDSCAPE CONTRACTOR SHALL PREVENT EROSION OF SOIL AND ENTRY OF SOIL-BEARING WATER AND AIRBORNE DUST ON ADJACENT PROPERTIES AND INTO THE PUBLIC STORMWATER FACILITIES. REFER TO EROSION CONTROL PLANS FOR DETAILS.

IRRIGATION SYSTEM NOTES (DESIGN BUILD)

- THE CONTRACTOR SHALL DESIGN AND INSTALL AN IRRIGATION SYSTEM FOR ALL LANDSCAPING ON-SITE. CONTRACTOR SHALL PROVIDE SHOP DRAWINGS TO LANDSCAPE ARCHITECT AND OWNER FOR REVIEW AND APPROVAL PRIOR TO CONSTRUCTION.
- RAIN BIRD SYSTEM OR EQUIVALENT SHALL BE USED.
- SEE KITCHEN ELEVATION FOR LOCATION OF THE IRRIGATION CONTROL BOX.
- DRIP IRRIGATION IS REQUIRED IN BUILDING PLANTERS.
- RAIN BIRD RSD SERIES RAIN SENSOR, OR EQUIVALENT, SHALL BE INCLUDED IN THE DESIGN AND INSTALLATION. CONSULT PANDA EXPRESS PROJECT MANAGER FOR PLACEMENT.
- LOCATE ALL UTILITIES BEFORE DIGGING. PROTECT ALL UTILITIES FROM DAMAGE DURING CONSTRUCTION.
- PLACE IRRIGATION LINES WITHIN PLANTING AND TURF AREAS UNLESS SLEEVING IS INDICATED. ROUTE PIPING TO AVOID TREE LOCATIONS. LOCATE MAINLINE AND VALVE BOXES WITHIN PROPERTY LINES.
- PROCURE ALL NECESSARY PERMITS TO COMPLETE INSTALLATION OF IRRIGATION SYSTEM PER STATE AND LOCAL CODES.
- PROVIDE SLEEVING UNDER PAVEMENT AND SIDEWALKS TO ACCOMMODATE LATERAL LINES, MAIN LINES, AND/OR CONTROL WIRE.
- PROVIDE A MINIMUM 12" COVER OVER LATERAL LINES AND A MINIMUM OF 18" COVER OVER MAINLINES.
- INSTALL ALL ELECTRICAL SERVICE TO MEET LOCAL CODES.
- PROVIDE POP-UP SPRINKLERS ONLY. MOUNT SPRINKLERS ON POLY PIPE WITH BARBED FASTENERS UNLESS OTHERWISE SPECIFIED IN JURISDICTIONAL REGULATIONS.
- MOUNT HEADS FLUSH WITH FINISHED GRADE. INSTALL HEADS 6" OFF ALL SIDEWALKS AND 1" FROM BUILDING WALLS.
- INSTALL VALVES IN 15" BOXES WITH TAMPER RESISTANT LIDS.
- ALL IRRIGATION CONTROL WIRE TO BE 14-GAUGE WIRE. PROVIDE WHITE FOR COMMON, RED FOR HOT, AND A SPARE BLUE WIRE PARALLEL TO COMMON THROUGHOUT THE ENTIRE SYSTEM.
- MAKE ALL WIRE SPLICES WITH WATERPROOF SPLICE KITS RATED FOR DIRECT BURIAL. NEATLY COIL 30' OF SLACK WIRE IN VALVE BOX.
- ADJUST HEADS TO MAXIMIZE COVERAGE AND MINIMIZE OVERSPRAY.

GENERAL PLANTING NOTES

- NO PLANTING TO BE INSTALLED UNTIL GRADING AND CONSTRUCTION HAS BEEN COMPLETED IN THE IMMEDIATE AREA.
- SEE CIVIL/SITE PLAN FOR ALL SITE DIMENSIONS, SQUARE FOOTAGES, PARKING CALCULATIONS, AND DETAILS OF ALL SITE IMPROVEMENTS.
- IF THE LANDSCAPE CONTRACTOR PERCEIVES ANY DEFICIENCIES IN THE PLANT SELECTIONS, SOIL CONDITIONS, OR ANY OTHER SITE CONDITION WHICH MIGHT NEGATIVELY AFFECT PLANT MATERIAL ESTABLISHMENT, SURVIVAL, OR GUARANTEE, THEY SHALL BRING THESE DEFICIENCIES TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- PRIOR TO ANY LAND CLEARING OR CONSTRUCTION, TREE PROTECTION FENCING IS TO BE INSTALLED BY THE CONTRACTOR. THIS FENCING SHALL BE INSTALLED AT THE DRIP LINE OF ALL TREES AND SHRUBS (TO BE PROTECTED) ACCORDING TO THE TREE PROTECTION DETAIL AND MUST BE MAINTAINED FOR THE DURATION OF THE PROJECT. NO CUTTING, FILING OR TRESPASSING SHALL OCCUR INSIDE THE FENCED AREAS WITHOUT APPROVAL.
- ALL PLANTS TO BE INSTALLED AS PER PLANTING DETAILS. PLANT MATERIALS ARE TO BE PLANTED IN THE SAME RELATIONSHIP TO GRADE AS WAS GROWN IN NURSERY CONDITIONS. IF WET, CLAY SOILS OR POOR DRAINING SOILS ARE EVIDENT, PLANT HIGHER, REMOVE ALL TWINE, WIRE AND BURLAP FROM TOP 1/3 OF ROOT BALL AND FROM TREE TRUNKS.
- ONE SHRUB PER TYPE AND SIZE IN EACH PLANTING BED AND EVERY TREE SHALL BE CLEARLY IDENTIFIED (COMMON OR LATIN NOMENCLATURE) WITH A PLASTIC TAG WHICH SHALL NOT BE REMOVED PRIOR TO OWNER ACCEPTANCE.
- SEED AND/OR SOO ALL AREAS DISTURBED DUE TO GRADING AND CONSTRUCTION ACTIVITIES. WHERE SOO/SEED ADJUTS PAVED SURFACES, FINISHED GRADE OF SOO/SEED SHALL BE HELD 1" BELOW SURFACE ELEVATION OF TRAIL, SLAB, CURB, ETC. SOO SHALL BE Laid PARALLEL TO THE CONTOUR AND SHALL HAVE STAGGERED JOINTS ON SLOPES STEEPER THAN 3:1 OR IN DRAINAGE SWALES, THE SOO SHALL BE STAKED TO THE GROUND. REFER TO PLAN FOR SOO/SEED LOCATIONS.
- PRUNE, THIN AND SHAPE TREES AND SHRUBS ACCORDING TO STANDARD HORTICULTURAL PRACTICES. APPLY MINIMUM 4" MULCH CUP AT ALL TREES NOT PLANTED IN PLANTING BEDS.
- EXISTING LAWN AREAS TO BE SAVED AND AREAS THAT ARE DAMAGED DURING CONSTRUCTION MUST BE INSPECTED TO DETERMINE VIABILITY. IF THE EXISTING LAWN IS FOUND TO BE LEVEL, HEALTHY, DENSE & FREE FROM WEEDS, LAWN MAY NOT REQUIRE REPLACEMENT OR RENOVATION. IF RENOVATION IS REQUIRED OR IS PART OF THE APPROVED PLAN, THEN THE FOLLOWING REQUIREMENTS WILL BE APPLIED.

EXISTING LAWN FOUND TO BE IN POOR CONDITION MUST FIRST BE SPRAYED WITH ROUND-UP (OR EQUAL) TO KILL THE EXISTING LAWN AND WEEDS. AREAS, WAIT A MIN. OF (10) DAYS FOR THE HERBICIDE TO TAKE EFFECT, THEN REMOVE ALL DEAD SOO & WEEDS TO A MIN. DEPTH OF (2) INCHES. ADD A MIN. OF 6 INCHES OF NEW TOPSOIL TO ALL LAWN AREAS. BACKFILL AND COMPACT TOPSOIL TO THE TOP OF ALL CURBS & WALKS PRIOR TO SODDING. REGRADE TO ELIMINATE ALL BUMPS & DEPRESSIONS AND RESO ALL AREAS.

EXISTING LAWN FOUND TO BE IN GOOD CONDITION, BUT WITH BARE, SPARSE OR WEEDY AREAS MUST BE RENOVATED BY FILLING IN LOW AREAS, RAKING, OVERSEEDING AND TOP DRESSING ALL SPARSE AND BARE SPOTS AND BY INITIATING A WEED AND FEED PROGRAM.

CONVERSION OF ALL ASPHALT AND GRAVEL AREAS TO LANDSCAPE SHALL BE DONE IN THE FOLLOWING MANNER:

A. REMOVE ALL ASPHALT, GRAVEL AND COMPACTED EARTH TO A DEPTH OF 24"-30" DEPENDING ON THE DEPTH OF SUB BASE AND DISPOSE OF OFF SITE.

B. REPLACE EXCAVATED MATERIAL W/ GOOD, MEDIUM TEXTURED PLANTING SOIL (LOAM OR LIGHT YELLOW CLAY) TO A MIN. OF 2" ABOVE TOP OF CURB AND SIDEWALK, ADD 4"-6" OF TOPSOIL AND CROWN TO A MIN. OF 6" ABOVE ADJACENT CURB AND WALK AFTER EARTH SETTLING, UNLESS NOTED OTHERWISE ON THE PLANS.

IF CONVERSION TO LANDSCAPE OCCURS IN AN EXISTING (OR BETWEEN) LANDSCAPE AREAS, REPLACE EXCAVATED MATERIAL TO 4"-6" BELOW ADJACENT EXISTING GRADE W/ GOOD MEDIUM TEXTURED PLANTING SOIL (LOAM OR LIGHT YELLOW CLAY) AND ADD 4"-6" OF TOPSOIL TO MEET EXISTING GRADES AFTER EARTH SETTLING.

ALL TREE PITS MUST BE TESTED FOR PROPER DRAINAGE PRIOR TO PLANTING TREES. A DRAINAGE SYSTEM MUST BE INSTALLED IF PLANTING PIT DOES NOT DRAIN SUFFICIENTLY. (REQUIRED IN HEAVY CLAY SOILS)

ALL LANDSCAPE AREAS SHALL HAVE PROPER DRAINAGE THAT PREVENTS EXCESS WATER FROM STANDING ON LAWN AREAS OR AROUND TREES & SHRUBS.

ALL MULCH RINGS AND SHRUB BEDS IN LAWN AREAS SHALL BE EDGED WITH A MANICURED EDGE OR WITH MANUFACTURED EDGING AS INDICATED.

MULCHING AND WATERING OF ALL PLANTS & TREES SHALL BE IMMEDIATELY OR WITHIN 16 HOURS AFTER INSTALLATION.

LANDSCAPE PLANTING SPECIFICATIONS

- PLANTING BED PREPARATION: ALL MASS PLANTING BEDS SHALL BE TILED TO A MINIMUM DEPTH OF 10". AMENDMENTS (LEAF MOLD OR SPAGNUM PEAT MOSS) SHALL BE APPLIED DURING CULTIVATION. ALL BEDS ARE TO BE GRADED SMOOTH BEFORE PLANTING. PLANT GROUNDCOVER TO WITHIN 12" OF TREES OR SHRUBS. REMOVE ALL STONES LARGER THAN 2". ALL STICKS, ROOTS, RUBBISH AND ANY OTHER EXTRANEIOUS MATERIALS. LANDSCAPE AREAS, PARKING LOT ISLANDS AND LANDSCAPE BEDS NEXT TO BUILDINGS SHALL BE EXCAVATED OF ALL BUILDING MATERIALS AND POOR SOILS TO A DEPTH OF 18" AND BACKFILLED WITH GOOD, MEDIUM TEXTURED PLANTING SOIL (LOAM OR LIGHT YELLOW CLAY). ADD 4" OF TOPSOIL OVER FILL MATERIAL AND CROWN A MINIMUM OF 6" ABOVE TOP OF CURBS AND/OR WALKS AFTER EARTH SETTLING UNLESS OTHERWISE NOTED ON THE GRADING PLAN.
- BACKFILL SOIL: USE SOIL EXCAVATED FROM PLANTING HOLES AND PROVIDE AMENDMENTS (1 PART LEAF MOLD OR SPAGNUM PEAT MOSS AND 3 PARTS EXCAVATED SOIL). ALL NEW TOPSOIL USED SHALL BE FREE OF WEEDS AND OTHER FOREIGN VEGETATION, AS WELL AS, STONES LARGER THAN 2", ALL STICKS, ROOTS, RUBBISH AND ANY OTHER EXTRANEIOUS MATERIALS.
- FERTILIZATION: ALL PLANT MATERIALS SHALL BE FERTILIZED UPON INSTALLATION WITH DRIED BONE MEAL OR OTHER SPECIFIED FERTILIZER MIXED IN WITH THE PLANTING SOIL PER THE MANUFACTURER'S INSTRUCTIONS UNLESS NOTED OTHERWISE.
- MULCH MATERIAL: STONE SHALL BE RIVER ROCK AS SPECIFIED ON THE LANDSCAPE PLANS. MASS MULCH ALL PLANTING BEDS TO 3" DEPTH. ALL SHRUB PLANTING BEDS TO RECEIVE 3" DEEP STONE RIVER ROCK MULCH. ALL EVERGREEN AND DECIDUOUS TREES (F USED) TO RECEIVE STONE RIVER ROCK MULCH AS SHOWN IN DETAILS.
- TREE STAKING: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO STAKE AND/OR CUY THE TREES ACCORDING TO THE DETAILS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO TAKE EVERY STEP NECESSARY TO MAINTAIN THE TREES AND SHRUBS IN AN UPRIGHT AND PLUMB CONDITION AT ALL TIMES UNTIL THE END OF THE PLANT GUARANTEE PERIOD. ESPECIALLY WHERE VANDALISM, SOIL OR WIND CONDITIONS ARE A PROBLEM. AT END OF GUARANTEE PERIOD ALL STAKES SHALL BE REMOVED BY LANDSCAPE CONTRACTOR. ALL STAKES USED FOR TREE SUPPORTS SHALL POINT AWAY FROM ANY AND ALL CIRCULATION ROUTES.
- TREE WRAPPING: WRAPPING MATERIAL SHALL BE QUALITY, HEAVY WATERPROOF CREPE PAPER MANUFACTURED FOR THIS PURPOSE. WRAP ALL DECIDUOUS TREES PLANTED IN THE FALL PRIOR TO 12-1 AND REMOVE ALL WRAPPING AFTER 5-1.
- EDGING: EDGING SHALL BE SPADE EDGED.
- FERTILIZER: JUMP-SHOT ROOT STIMULATOR AS MANUFACTURED BY AGRIE, OR APPROVED EQUAL, SHALL BE APPLIED TO THE SOIL BACKFILL OF EACH PLANT DURING INSTALLATION.
- PLANT SIZING: MEASURE TREES AND SHRUBS ACCORDING TO ANSI Z60.1 STANDARDS. TAKE CALIPER MEASUREMENTS 8 INCHES ABOVE GROUND FOR TREES 12" CALIPER AND 12 INCHES ABOVE GROUND FOR LARGER TREES. ALWAYS HANDLE BALLED AND BURLAPPED MATERIAL BY THE ROOT BALL. PLANT MATERIAL SHALL BE DELIVERED TO THE SITE AND PLANTED THE SAME DAY.
- PLANTING PLAN: ALL PROPOSED PLANTS SHALL BE LOCATED CAREFULLY AS SHOWN ON THE PLANS. PLAN TAKES PRECEDENCE OVER PLANT SCHEDULE IF DISCREPANCIES IN QUANTITIES EXIST. SPECIFICATIONS TAKE PRECEDENCE OVER NOTES. RESPECT STATED DIMENSIONS. DO NOT SCALE DRAWINGS.
- WEED BARRIER: CONTRACTOR SHALL APPLY 4.2 OZ. NON-WOVEN POLYPROPYLENE, NEEDLE-PUNCHED FABRIC WEED BARRIER AT ALL INTERIOR LANDSCAPE ISLANDS AND STONE MULCH STRIPS.

PLANT MATERIAL NOTES

- PROVIDE PLANTS OF QUANTITY, SIZE, GENUS, SPECIES, AND VARIETY SHOWN AND SCHEDULED AND IN CONFORMANCE WITH THE REQUIREMENTS OF ANSI Z60.1 "AMERICAN STANDARD FOR NURSERY STOCK". PLANTS SHALL HAVE BEEN GROWN IN A RECOGNIZED NURSERY IN ACCORDANCE WITH GOOD HORTICULTURAL PRACTICE.
- ALL PLANTS SHALL BE FULL, WELL-BRANCHED PLANTS CHARACTERISTIC OF THE SPECIES. PLANTS SHALL BE FREE OF DISEASE, INSECTS, EGGS, LARVAE, AND DEFECTS SUCH AS KNOTS, SUN-SCALD, INJURIES, ABRASIONS, OR DISFIGUREMENT.
- PLANT STOCK SHALL HAVE BEEN GROWN UNDER CLIMATIC CONDITIONS SIMILAR TO CONDITIONS IN THE LOCALITY OF THE PROJECT.
- LABEL AT LEAST ONE PLANT OF EACH KIND WITH A SECURELY ATTACHED WATERPROOF TAG BEARING LEGIBLE DESIGNATION OF BOTANICAL AND COMMON NAME.
- PROVIDE FRESHLY DUG BALLED & BURLAPPED PLANT MATERIALS. DO NOT DROP BALLED & BURLAPPED STOCK DURING DELIVERY.
- DO NOT REMOVE CONTAINER-GROWN STOCK FROM CONTAINERS UNTIL PLANTING TIME.

INSTALLATION OF PLANT MATERIAL NOTES

- INSTALL TREES AND SHRUBS ACCORDING TO STANDARD DETAILS SHOWN ON THE PLAN.
- ALL TREE SAUCERS SHALL BE SOAKED WITH WATER AND MULCHED IMMEDIATELY FOLLOWING PLANTING.
- ALL TREE SAUCERS AND SHRUB BEDS SHALL BE MULCHED WITH A 3-INCH LAYER OF ORGANIC TRIPLE SHREDDED HARDWOOD BARK MULCH. NON-ORGANIC MULCHES SUCH AS GRAVEL, CRUSHED BRICK, LAVA ROCK, ETC. ARE UNACCEPTABLE.
- TREE GUYING SHALL BE REMOVED AFTER ONE FULL GROWING SEASON.
- APPLY 12 CUBIC FEET OF PEAT MOSS PER 100 SQUARE FEET AND 20 POUNDS OF 8-8-8 FERTILIZER PER 100 SQUARE FEET OF GROUND COVER PLANTING BEDS. ROTOTILL THE BEDS TO A DEPTH OF 8 INCHES AND SMOOTH TO AN EVEN AND UNIFORM SURFACE. PLANT GROUND COVER MATERIALS, APPLY 2 INCHES OF ORGANIC MULCH, AND WATER.

LAWN NOTES

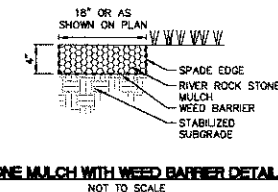
- KENTUCKY BLUE GRASS SOO SHALL BE PROMOTED IN AREAS WHERE SEEDING IS NORMALLY UNSUCCESSFUL OR WILL BE UNSUCCESSFUL DUE TO CLIMATE, SEASON, OR OTHER TEMPORARY CONSTRAINT. SOO SHALL BE STRONGLY ROOTED, FREE OF WEEDS, AND OF UNIFORM THICKNESS WITH NO MORE THAN 1.5 INCHES OR LESS THAN 1 INCH OF SOIL.
- SOO SHALL BE TIGHTLY-FITTED TOGETHER. ENDS AND EDGES SHALL MEET WITHOUT OVERLAP AND JOINTS SHALL BE STAGGERED WITH ADJACENT ROWS. AFTER INSTALLATION, SOO SHALL BE THOROUGHLY WATERED. ON SLOPES STEEPER THAN 2:1, SOO SHALL BE HELD IN PLACE WITH WOODEN STAKES MEASURING 1 INCH SQUARE BY 6 INCHES LONG. STAKES SHALL BE DRIVEN FLUSH WITH THE TOP OF THE SOO'S SOIL LAYER.

PLANT MATERIAL MAINTENANCE AND WARRANTY NOTES

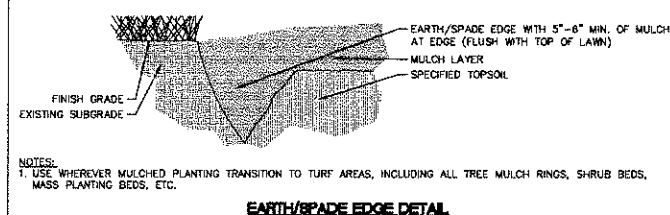
- MAINTENANCE OF PLANT MATERIALS AND LAWN AREAS SHALL BEGIN IMMEDIATELY AFTER INSTALLATION AND SHALL CONTINUE UNTIL FINAL ACCEPTANCE, BUT IN NO CASE, LESS THAN THE FOLLOWING STATED PERIODS:
PLANT MATERIALS: 90 DAYS AFTER SUBSTANTIAL COMPLETION
LAWN AREAS: 60 DAYS AFTER SUBSTANTIAL COMPLETION

- AFTER REQUIRED MAINTENANCE PERIOD, THE OWNER, UPON REQUEST, WILL MAKE AN INSPECTION TO DETERMINE ACCEPTABILITY. UNACCEPTABLE WORK SHALL BE REPAIRED OR REPLACED AND REINSPECTED BEFORE FINAL ACCEPTANCE IS GRANTED.
- A WRITTEN WARRANTY SHALL BE PROVIDED TO THE OWNER GUARANTEEING THAT ALL PLANT MATERIALS, SOO, AND/OR SEEDS ARE WILL BE THRIVING FOR THE FOLLOWING STATED PERIODS:
TREES, SHRUBS, AND GROUND COVERS: ONE YEAR AFTER FINAL ACCEPTANCE
SOO AND SEEDS AREAS: 90 DAYS AFTER FINAL ACCEPTANCE
PERENNIALS: 90 DAYS AFTER FINAL ACCEPTANCE

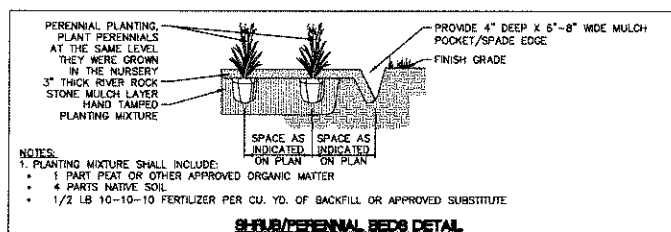
- THE CONTRACTOR SHALL PROVIDE THE OWNER WITH WRITTEN INSTRUCTIONS REGARDING MAINTENANCE OF EACH TYPE OF VEGETATION. THE OWNER IS RESPONSIBLE FOR PROPER MAINTENANCE OF THE MATERIALS DURING THE WARRANTY PERIOD AS OUTLINED IN THE MAINTENANCE INSTRUCTIONS. THE CONTRACTOR SHALL MAKE PERIODIC INSPECTIONS OF THE SITE AND WILL INFORM THE OWNER OF ANY LACK OF PROPER MAINTENANCE IN WRITING. OWNER'S FAILURE TO COMPLY WITH THE MAINTENANCE PROGRAM SHALL RENDER THE WARRANTY NULL AND VOID.
- THE CONTRACTOR IS NOT RESPONSIBLE FOR ACTS OF NATURE INCLUDING ABNORMAL WEATHER CONDITIONS, EROSION, VANDALISM, NOR DAMAGES BY OTHERS. IF ANY CONDITIONS BEYOND THE CONTROL OF THE CONTRACTOR SHOULD OCCUR, THE MATERIALS AFFECTED WILL NO LONGER BE COVERED BY THE WARRANTY.



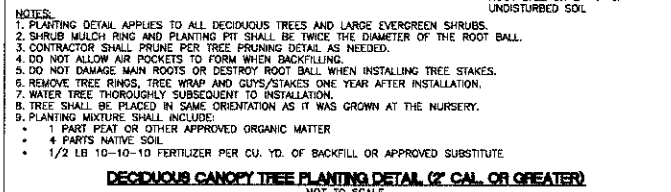
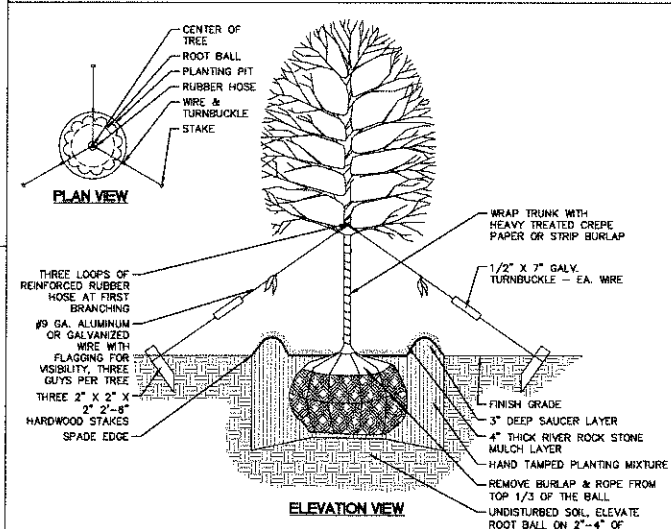
STONE MULCH WITH WEED BARRIER DETAIL
NOT TO SCALE



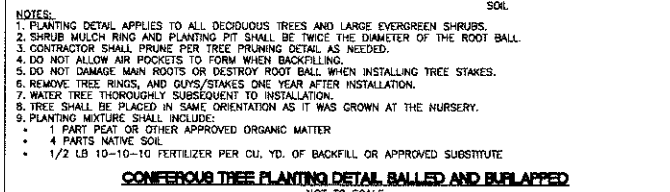
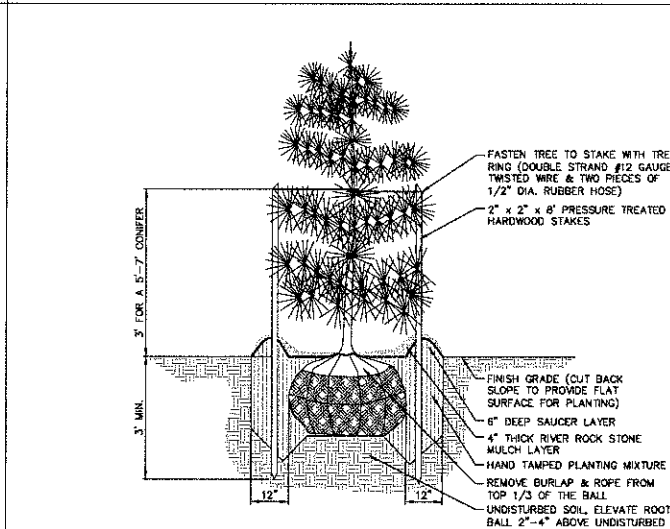
EARTH/SPADE EDGE DETAIL
NOT TO SCALE



SHRUB/PERENNIAL BEDS DETAIL
NOT TO SCALE



DECIDUOUS CANOPY TREE PLANTING DETAIL, 2\"/>



CONIFEROUS TREE PLANTING DETAIL, BALLED AND BURLAPPED
NOT TO SCALE

811
Know what's below.
Call before you dig.

THE LOCATION OF EXISTING UNDERGROUND UTILITIES ARE SHOWN ON AN APPROXIMATE WAY ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE CONTRACTOR. THE OWNER SHALL BE RESPONSIBLE FOR THE LOCATION OF ALL EXISTING UTILITIES BEFORE CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MUST BE OCCURRED BY THE CONTRACTOR'S FAILURE TO EXCAVATE, LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

NOTICE: CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. NOTHING ON THIS DRAWING OR THE OWNER'S REPRESENTATIVE SHALL BE RESPONSIBLE FOR THE SAFETY OF THE WORK OR ANY INJURY TO PERSONS OR PROPERTY OR ANY OTHER PERSONS.

24-HOUR CONTACT:
PANDA PM
HAKIM YALA
847-477-5125

DESIGN LANDSCAPE ARCHITECT
JUSTIN P. CHAMBERLAIN
01/19/2021
EXP: 06/31/21

DO NOT REMOVE LEADER(S) PRUNE BRANCHES IF BROKEN OR DAMAGED OR IF SEE TREE PRUNING DETAIL. PLANT SHRUBS AT SAME GROUND LEVEL THEY WERE GROWN IN THE NURSERY. 3" THICK RIVER ROCK STONE MULCH LAYER. 2" DEEP SPADE EDGE. FINISH GRADE. UNDISTURBED SUBGRADE.

ON CENTER DIMENSION VARIES SEE PLAN

NOTES:
1. PLANTING MIXTURE SHALL INCLUDE:
• 1 PART PEAT OR OTHER APPROVED ORGANIC MATTER
• 4 PARTS NATIVE SOIL
• 1/2 LB 10-10-10 FERTILIZER PER CU. YD. OF BACKFILL OR APPROVED SUBSTITUTE

CONTAINER SHRUB PLANTING DETAIL
NOT TO SCALE



PANDA EXPRESS, INC.
1683 Walnut Grove Ave.
Rosemead, California
91770
Telephone: 626.799.8888
Facsimile: 626.372.8288

All ideas, designs, arrangements and plans indicated or represented by this drawing are the property of Panda Express Inc. and were created for use on this specific project. None of these ideas, designs, arrangements or plans may be used by or disclosed to any person, firm, or corporation without the written permission of Panda Express Inc.

REVISIONS:
VILLAGE PLAN REV. #1 01/19/2021

ISSUE DATE:
PC SUBMITTAL 11/20/2020

DRAWN BY: LEH
PANDA PROJECT # D7058
ATWELL PROJECT # 18003769

NORR
SCAPE DESIGN, PLANNING

ATWELL
866.850.4200 www.atwell-group.com
1250 EAST DEER ROAD, SUITE 300
NAPERVILLE, IL 60563
DESIGN #164-000818

PANDA EXPRESS
WARM & WELCOME 2300
7505 KINGERY HIGHWAY
WILLOWBROOK, IL 60527

**LANDSCAPE NOTES
AND DETAILS**

L01.1

TRUE WARM & WELCOME 2300



Project: Willowbrook, IL - Panda Express
Location: 7505 S. Kingery Hwy.
Willowbrook, IL 60527

Date: 11/20/2020

1250 East Diehl Road, Suite 300

Naperville, IL 60563

Ph: 630-577-0800

Engineer's Opinion of Probable Construction Cost

This EOPC was prepared in accordance with Final Site Plans for Willowbrook, IL Panda Express, prepared by Atwell dated 11/20/2020.

	Quantity	Unit	Unit Cost	Total
SITEWORK				
Erosion Control				
Construction Entrance	1	EACH	\$5,000.00	\$5,000.00
Concrete Washout	1	EACH	\$500.00	\$500.00
Tree Protection Fence	12	LF	\$4.00	\$48.00
Silt Fence (Wire Back)	880	LF	\$2.50	\$2,200.00
Inlet Filter	7	EACH	\$200.00	\$1,400.00
Erosion Control Sub-Total				\$9,148.00
Demolition and Earthwork				
Clear and Grub	0.08	ACRES	\$5,000.00	\$400.00
Remove Existing Sanitary Sewer Pipe	154	LF	\$10.00	\$1,540.00
Remove Existing Water Service Pipe	138	LF	\$10.00	\$1,380.00
Relocate Existing Hydrant	1	EACH	\$1,750.00	\$1,750.00
Remove Existing Wood Wall	99	LF	\$2.00	\$198.00
Remove Existing Light Pole	11	EACH	\$225.00	\$2,475.00
Remove Existing Bollard	2	EACH	\$100.00	\$200.00
Remove Existing Fence	346	LF	\$3.00	\$1,038.00
Remove Existing Concrete	17	SF	\$1.50	\$25.50
Topsoil Stripping and Stockpiling	199	CY	\$2.25	\$447.75
Topsoil Respread (4")	1772	SY	\$1.50	\$2,658.00
Excavation to Stockpile	211	CY	\$3.00	\$633.00
Fill from Stockpile	211	CY	\$3.00	\$633.00
Fill from Import	396	CY	\$18.00	\$7,128.00
Fine Grade Pad	256	SY	\$2.50	\$640.00
Subgrade Preparation - Rough Grade	2910	SY	\$1.00	\$2,910.00
Remove Existing Pavement	4437	SY	\$5.25	\$23,294.25
Sawcut Pavement, Full Depth	341	LF	\$3.50	\$1,193.50
Remove Existing Curb and Gutter	1195	LF	\$5.00	\$5,975.00
Demolition & Earthwork Sub-Total				\$54,519.00
Utilities				
Sanitary Sewer				
6" PVC Service	348	LF	\$32.00	\$11,136.00
6" Clean Out	10	EACH	\$500.00	\$5,000.00
4'-0" Diameter Manhole	1	EACH	\$4,500.00	\$4,500.00
Connect to Existing Sanitary Sewer Manhole	1	EACH	\$800.00	\$800.00
Grease Trap (1,000 gal - precast concrete)	1	EACH	\$5,500.00	\$5,500.00
Testing & Inspections	1	EACH	\$1,000.00	\$1,000.00
Sanitary Sewer Sub-Total				\$27,936.00

	Quantity	Unit	Unit Cost	Total
Water				
1.5" Irrigation Water Service	10	LF	\$18.00	\$180.00
6" DIP Fire Water Service	73	LF	\$30.00	\$2,190.00
Dip Exiting Water Main	1	EACH	\$4,000.00	\$4,000.00
Connection to Existing Water Main	1	EACH	\$1,000.00	\$1,000.00
Water Sub-Total				\$7,370.00
Storm Sewer				
6" HDPE Storm Sewer Pipe	54	LF	\$20.00	\$1,080.00
12" HDPE Storm Sewer Pipe	436	LF	\$38.00	\$16,568.00
4'-0" Diameter Type "A" Catch Basin	5	EACH	\$3,800.00	\$19,000.00
4'-0" Diameter Manhole	1	EACH	\$4,500.00	\$4,500.00
Storm Sewer Sub-Total				\$41,148.00
Other Utilities				
Area Lights	8	EACH	\$1,500.00	\$12,000.00
Other Utilities Sub-Total				\$12,000.00
Paving				
Subgrade Preparation - Fine Grading	2959	SY	\$1.50	\$4,438.50
Aggregate Base Course (CA-6), 4" (Curb & Gutter)	24	SY	\$5.50	\$132.00
Aggregate Base Course (CA-6), 5" (Drive Thru)	285	SY	\$6.00	\$1,710.00
Aggregate Base Course (CA-6), 5" (PCC Parking)	121	SY	\$6.00	\$726.00
Aggregate Base Course (CA-6), 5" (Dumpster)	83	SY	\$6.00	\$498.00
Aggregate Base Course (CA-6), 8" (Asphalt Parking)	680	SY	\$7.75	\$5,270.00
Aggregate Base Course (CA-6), 10" (Asphalt drive Aisles)	1766	SY	\$8.50	\$15,011.00
Hot Mix Asphalt Binder Course, N50, 2-1/2" (Parking)	680	SY	\$11.75	\$7,990.00
Hot Mix Asphalt Binder Course, N50, 2-1/2" (Drive Aisles)	1766	SY	\$11.75	\$20,750.50
Hot Mix Asphalt Surface Course, 1-1/2" (Parking)	680	SY	\$9.00	\$6,120.00
Hot Mix Asphalt Surface Course, 2" (Drive Aisles)	1766	SY	\$11.25	\$19,867.50
PCC Pavement, 5" (Parking)	121	SY	\$28.00	\$3,388.00
PCC Pavement, 6" (Drive thru)	285	SY	\$32.00	\$9,120.00
PCC Pavement, 7" (Dumpster)	83	SY	\$36.00	\$2,988.00
Concrete Curb and Gutter Type B-6:12	1512	LF	\$12.00	\$18,144.00
PCC Sidewalk with Subbase, 4"	294	SY	\$45.00	\$13,230.00
Pavement Marking, ADA Parking Symbol	2	EACH	\$250.00	\$500.00
Pavement Marking, ADA Striping	1	EACH	\$350.00	\$350.00
Pavement Marking, Crosswalk	1	EACH	\$750.00	\$750.00
Pavement Marking, Drive Thru Parking Space	2	EACH	\$350.00	\$700.00
Pavement Marking, 4" Solid White	913	LF	\$3.00	\$2,739.00
Pavement Marking, 24" Solid White	54	LF	\$9.50	\$513.00
Rubber Wheel Stops	7	EACH	\$325.00	\$2,275.00
Signage	5	EACH	\$200.00	\$1,000.00
Paving Sub-Total				\$138,210.50
Landscaping				
Retaining Wall	248	SF	\$27.50	\$6,820.00
Sod	1472	SY	\$5.00	\$7,360.00
Mulch	36	CY	\$50.00	\$1,800.00
Tree	46	EACH	\$350.00	\$16,100.00
Shrub	203	EACH	\$40.00	\$8,120.00
Landscaping Sub-Total				\$40,200.00
TOTAL				\$330,531.50

Quantity	Unit	Unit Cost	Total
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This Engineer's Opinion of Probable Cost is made on the basis of Engineer's experience and qualifications using estimated quantities and represents Engineer's best judgment as an experienced and qualified professional Engineer generally familiar with the construction industry. However, since Engineer has no control over the cost of labor, materials, equipment, or services furnished by others, or over the Contractor's methods of determining prices, or over competitive bidding or market conditions, or over quantities of work actually performed, Engineer cannot and does not guarantee that proposals, bids, or actual construction cost will not vary from Opinions of Probable Construction Cost prepared by Engineer. This Opinion of Probable Construction Cost is limited to those items stated herein.

Notes:

1. The EOCP includes site work only and does not include the building, monument sign, or drive-thru equipment.
2. Demolition of the existing building, underground storage tank, fuel pumps, fuel piping, canopy and monument sign are to be completed by the Seller and are not included in this EOCP.



CONSULTING. ENGINEERING. CONSTRUCTION.

To: Hakim Yala
Panda Express

From: Brian A. Styck, P.E.
Project Manager

Michael Keith, P.E.
Project Manager

Date: January 5, 2021

Subject: Willowbrook, IL Panda Express
Site Traffic Study
Special Use Permit

This memorandum report summarizes the findings and recommendations of the Site Traffic Study performed by Atwell, LLC for the Site Plan Review, Special Use Permit and Variations requested for a proposed Panda Express located at 7505 Kingery Highway (IL Route 83), Willowbrook, IL. The proposed project includes demolition of an existing 2,131 SF Shell Gasoline and Service Station with Convenience Mart and construction of a 2,300 SF Panda Express Restaurant. A Special Use Permit is requested to allow for a proposed drive-thru within the Village of Willowbrook B-2 Community Shopping Zoning District.

EXISTING CONDITIONS

Location

The subject property is a ±1.0 acre parcel located at the southeast corner of the Kingery Highway and 75th Street intersection. The site address is 7505 Kingery Highway, Willowbrook, IL 60527.



Figure 1 – Site Location Aerial View

Site Accessibility

The subject property is currently accessed from both Kingery Highway (IL Route 83) and 75th Street (see Figure 1).

Kingery Highway (IL Route 83) is a north-south principle arterial roadway under the jurisdiction of the Illinois Department of Transportation (IDOT). Adjacent to the site, Kingery Highway is a four-lane divided roadway section. At the signalized intersection with 75th Street, there are two north bound left turn lanes, one north bound right turn lane, one south bound left turn lane and one south bound right turn lane. Kingery Highway has a median consequently requiring right in/right out access at the site driveway. Based on IDOT's Traffic Data Map (year: 2019), Kingery Highway carries approximately 50,000 average daily trips.

75th Street is an east-west major collector roadway under the jurisdiction of the Village of Willowbrook. East of the project site, 75th Street is a two-lane undivided roadway section. West of the project site (west of the Kingery Highway intersection), 75th Street is a four-lane undivided roadway section. At the signalized intersection with Kingery Highway, there is an eastbound left and right turn lane and a westbound left and right turn lane. The current site does allow a full movement access on 75th Street. Based on IDOT's Traffic Data Map (year: 2019), 75th Street carries approximately 3,000 average daily trips (east of the Kingery Highway intersection).

Existing Land Use Traffic Generation

The existing site land use is a developed Shell Gasoline and Service Station with Convenience Mart (ITE Code 945). In accordance with the ITE Trip Generation Manual 10th Edition, the existing site generates the following traffic counts:

- Average Daily Trips = 2,131 SF x 1,440.02 Trips/KSF = 3,069 Trips

- AM Peak Hour Total Trips = 2,131 SF x 75.99 Trips/KSF = 162 Trips
- PM Peak Hour Total Trips = 2,131 SF x 88.35 Trips/KSF = 188 Trips

PROPOSED CONDITIONS

Site Accessibility

The Panda Express site layout proposes constructing new access driveways in the approximate location of the existing access driveways, with the exception of the westernmost right-in only site access drive on 75th Street which will be closed off and curbed. The access on 75th Street will be a full movement for inbound traffic and limited to a right-out only. The access on Kingery Highway will remain to be a right-in/right-out access.

75th Street – The proposed site access driveway on 75th Street meets the Village requirement of 70' from the nearest lot line, however, it does not meet the minimum spacing from the ROW intersection of 500' (or outside the functional area), therefore a variation will be required. This variation was previously granted for the existing site access driveway.

Kingery Highway – The proposed site access driveway on Kingery Highway neither meets the Village requirement of 70' from the nearest lot line, nor the minimum spacing from the ROW intersection of 500' (or outside the functional area), therefore variations will be required for both requirements. These variations were previously granted for the existing site access driveway.

Proposed Land Use Traffic Generation

The proposed site land use is a Fast Food Restaurant with Drive-Thru (ITE Code 934). In accordance with the ITE Trip Generation Manual 10th Edition, the proposed site will generate the following traffic counts:

- Average Daily Trips = 2,300 SF x 470.95 Trips/KSF = 1,083 Trips
- AM Peak Hour Total Trips = 2,300 SF x 40.19 Trips/KSF = 92 Trips*
 - *NOTE: Panda Express typical hours of operation are 10 AM to 10 PM, therefore, the AM peak hour total trips listed above are not anticipated for this site. The AM Peak Hour Total Trips information for a Fast Food Restaurant with Drive-Thru is provided for reference only.
- PM Peak Hour Total Trips = 2,300 SF x 32.67 Trips/KSF = 75 Trips

Based on trip calculations from the ITE Trip Generation Manual 10th Edition, the proposed site land use will generate approximately 60%-65% less traffic (trips) than the existing site land use.

	Existing Land Use Shell Gasoline/Service Station with Convenience Mart (ITE Code 945)	Proposed Land Use Panda Express Fast Food Restaurant with Drive-Thru (ITE Code 934)	Total Trip Decrease	% Decrease
Average Daily Trips	3,069	1,083	-1,986	-64.71%
AM Peak Hour Trips	162	92*	-70	-43.21%
PM Peak Hour Trips	188	75	-113	-60.11%

*NOTE: Panda Express typical hours of operation are 10 AM to 10 PM, therefore, the AM peak hour total trips listed above are not anticipated for this site. The AM Peak Hour Total Trips information for a Fast Food Restaurant with Drive-Thru is provided for reference only.

Drive-Thru Queueing

Data was collected from three (3) similar nearby Panda Express drive-thru restaurants for the purpose of estimating peak drive-thru queueing for the proposed Willowbrook, IL Panda Express. The three similar nearby Panda Express drive-thru restaurants include the following locations:

Panda Express #1054
185 Countryside Plaza
Countryside, IL 60525

Panda Express #2685
6501 S Cicero Ave
Chicago, IL 60638

Panda Express #2969
1010 Ogden Ave
Downers Grove, IL 60515



Figure 2 – Similar Nearby Panda Express Drive-Thru Restaurants

The data collected from three (3) similar nearby Panda Express drive-thru restaurants included seven (7) consecutive days of drive-thru transaction counts, divided into 30-minute time spans. Copies of the drive-thru transaction information for each nearby location is included in the attachments. At the request of Panda Express, the net sales information has been removed from the drive-thru transaction tables. An average service time of 5 minutes (information provided by Panda Express) starting with the food order at the menu board and ending with food pickup at the drive-thru window was used for the drive-thru queueing analysis.

	Panda #1054	Panda #2685	Panda #2969
Avg. Daily Total (10 AM - 10 PM)	345	327	174
Max. One Day Total (10 AM - 10 PM)	397	386	201
Peak 7 Day Avg./30 Min.	22	18	12
Peak Total/30 Min.	25	23	15
Estimated Max Queueing	5	4	3

Panda Express #1054 had the highest drive-thru traffic counts in all categories. Both the peak 7-day average transactions/30 minutes and the peak total transactions/30-minutes occurred during lunch hours (11 AM – 2 PM). The peak total transactions/30-minute span (25 cars) occurred on three (3) separate occasions during

the one-week data collection. The maximum drive-thru queueing estimate for the proposed Willowbrook, IL Panda Express, based on counts from Panda Express #1054, is not expected to exceed five (5) cars during peak business hours.

The Willowbrook, IL Panda Express proposed site layout is designed to provide a total storage length of seven (7) vehicles for the drive-thru window queue, with placement of the menu/order board at the fifth car. Furthermore, two (2) drive-thru parking pick up spaces are proposed which will help reduce drive-thru queueing as needed during peak business hours. A copy of the Traffic Plan showing the proposed drive-thru queueing and drive-thru parking pick up spaces is provided in the attachments.

Parking Evaluation

The Village of Willowbrook Ordinance 9-10-5(K) requires 1 parking space for each 100 square feet of floor area which equals 23 parking spaces (2,300 SF x 1 parking space/100 SF). For comparison, the nearby similar Panda Express Drive-Thru Restaurant #2969 also requires 23 parking spaces (2,300 SF x 10 parking spaces/1,000 SF) in accordance with the Village of Downers Grove Municipal Code (Sec 28.7.030). The proposed Panda Express site layout provides a total of 46 parking spaces which is double the Village of Willowbrook parking requirement and should be more than adequate to meet current and future parking needs. The 46 total parking spaces are broken down as follows:

- Regular = 42 Spaces
- Drive-Thru Pickup = 2 Spaces
- ADA (Van Accessible) = 2 Spaces

Truck Turn Analysis

A truck turn analysis was completed for the proposed Panda Express site layout with the following vehicles:

- Fire Truck: 40' Overall Length (22' wheel base)
- Front Load Garbage Truck: 33.75' Overall Length (22.167' wheel base)
- Delivery Truck: 45.5' Overall Length (40' wheel base)

No issues are anticipated with truck access in and out of the site. A copy of the Truck Turn Analysis Exhibit is provided in the Attachments.

CONCLUSION

Conclusion

Based on the proposed Panda Express Drive-Thru Restaurant development plans and the preceding findings, the following conclusions and recommendations are made:

- The proposed site access is more than adequate to serve the site and will provide for safer traffic flow by eliminating the westernmost driveway on 75th Street and requiring a right turn only onto 75th Street.
- The proposed Panda Express Drive-Thru Restaurant is estimated to generate less traffic than the existing Shell Gasoline and Service Station with Convenience Mart; therefore, the proposed development will have no additional impact on adjacent roadways and/or land uses.
- The proposed drive-thru is designed to provide a total storage length that exceeds the estimated peak drive-thru queueing. Furthermore, two drive-thru parking pick up spaces are proposed which will help to reduce drive-thru queueing as needed during peak business hours.
- The proposed site layout is designed to provide sufficient parking spaces that exceed the Village of Willowbrook off street parking requirements.

ATTACHMENTS

Attachments

1. Traffic Plan
2. Nearby Panda Express Drive-Thru Transaction Counts:
 - a. Panda #1054 (Countryside)
 - b. Panda #2685 (Chicago)
 - c. Panda #2969 (Downers Grove)
3. Truck Turn Analysis

Panda Express #1054 (Countryside, IL)

Day of Week		Sun		Mon		Tue		Wed		Thu		Fri		Sat		Average Daily	
Store	Daypart Detail	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)
1054	Breakfast 10:00 - 10:29		0		0		1		1		1		0		0		0
1054	Breakfast 10:30 - 10:59		4		5		9		6		8		7		3		6
1054	Lunch 11:00 - 11:29		10		14		18		17		14		14		12		14
1054	Lunch 11:30 - 11:59		13		17		21		19		21		20		15		18
1054	Lunch 12:00 - 12:29		18		25		22		25		22		23		17		21
1054	Lunch 12:30 - 12:59		16		21		22		22		20		25		19		21
1054	Lunch 13:00 - 13:29		16		22		18		20		19		23		19		20
1054	Lunch 13:30 - 13:59		15		18		16		19		17		23		20		18
1054	Afternoon 14:00 - 14:29		16		18		15		18		16		20		21		18
1054	Afternoon 14:30 - 14:59		16		15		11		14		14		19		19		16
1054	Afternoon 15:00 - 15:29		18		16		14		14		12		16		20		16
1054	Afternoon 15:30 - 15:59		20		13		17		16		17		16		17		16
1054	Afternoon 16:00 - 16:29		18		14		12		15		14		17		20		16
1054	Afternoon 16:30 - 16:59		18		13		17		16		17		17		18		17
1054	Dinner 17:00 - 17:29		21		21		17		19		19		21		18		19
1054	Dinner 17:30 - 17:59		17		18		22		21		19		20		20		20
1054	Dinner 18:00 - 18:29		19		17		18		22		17		20		19		19
1054	Dinner 18:30 - 18:59		16		18		15		18		22		20		20		18
1054	Dinner 19:00 - 19:29		12		15		14		19		18		20		16		16
1054	Dinner 19:30 - 19:59		8		11		14		13		16		18		12		13
1054	Evening 20:00 - 20:29		7		11		12		15		13		16		14		12
1054	Evening 20:30 - 20:59		4		6		5		9		6		11		7		7
1054	Evening 21:00 - 21:29		1		3		3		2		2		8		6		3
1054	Evening 21:30 - 21:59		1		1		1		1		1		3		1		1
1054	Evening 22:00 - 22:29		0		0		0		0		0		1		0		0
1054	Evening 22:30 - 22:59		0		0		0		0		0		0		0		0
1054	Evening 23:00 - 23:29		0		0		0		0		0		0		0		0
1054	Evening 23:30 - 23:59		0		0		0		0		0		0		0		0
		300		330		332		361		341		397		351		345	

Panda Express #2685 (Chicago, IL)

Day of Week		Sun		Mon		Tue		Wed		Thu		Fri		Sat		Average Daily	
Store	Daypart Detail	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)
2685	Breakfast		1		2		6		3				5		2		3
2685	Breakfast		7		6		9		6				9		5		7
2685	Lunch		9		8		10		7				13		10		10
2685	Lunch		13		10		15		12				15		13		13
2685	Lunch		15		13		17		14				21		13		15
2685	Lunch		15		11		18		15				20		17		16
2685	Lunch		15		11		13		18				21		18		16
2685	Lunch		20		14		9		16				19		15		15
2685	Afternoon		16		13		14		14				23		11		15
2685	Afternoon		15		16		10		14				16		20		15
2685	Afternoon		13		13		13		16				12		18		14
2685	Afternoon		11		14		10		18				18		20		15
2685	Afternoon		12		14		12		18				16		16		14
2685	Afternoon		10		15		13		14				16		17		14
2685	Dinner		13		17		16		20				19		20		17
2685	Dinner		15		20		15		21				22		17		18
2685	Dinner		15		15		14		20				19		16		17
2685	Dinner		13		17		11		20				14		12		15
2685	Dinner		12		17		12		17				15		11		16
2685	Dinner		12		19		14		18				16		13		16
2685	Evening		10		16		12		18				15		13		14
2685	Evening		12		12		9		16				15		11		13
2685	Evening		12		11		11		12				18		11		12
2685	Evening		12		11		3		3				10		8		6
2685	Evening		6		4		0		0				2		3		1
2685	Evening		0		0		1		0				0		0		0
2685	Evening		0		0		0		0				0		0		0
			289		304		283		347		350		386		329		327

Panda Express #2969 (Downers Grove, IL)

Day of Week		Sun		Mon		Tue		Wed		Thu		Fri		Sat		Average Daily	
Store	Daypart Detail	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)	DT Net Sales	DT Total Cars (Good Data)
2969	Breakfast 10:00 - 10:29		1		0		0		1		1		0		1		0
2969	Breakfast 10:30 - 10:59		3		3		5		5		5		6		4		4
2969	Lunch 11:00 - 11:29		4		7		10		12		10		10		6		8
2969	Lunch 11:30 - 11:59		7		10		11		11		12		15		8		10
2969	Lunch 12:00 - 12:29		8		13		14		14		12		14		8		12
2969	Lunch 12:30 - 12:59		8		11		13		11		12		13		6		11
2969	Lunch 13:00 - 13:29		7		12		12		11		12		13		9		11
2969	Lunch 13:30 - 13:59		7		10		11		10		11		11		7		9
2969	Afternoon 14:00 - 14:29		7		7		9		8		8		9		8		8
2969	Afternoon 14:30 - 14:59		8		7		6		7		7		8		7		7
2969	Afternoon 15:00 - 15:29		7		10		8		7		9		9		7		8
2969	Afternoon 15:30 - 15:59		6		8		7		4		7		6		6		7
2969	Afternoon 16:00 - 16:29		6		7		7		6		6		10		7		7
2969	Afternoon 16:30 - 16:59		7		7		7		10		10		7		7		7
2969	Dinner 17:00 - 17:29		9		10		10		7		11		11		8		9
2969	Dinner 17:30 - 17:59		8		9		9		10		11		12		10		10
2969	Dinner 18:00 - 18:29		10		10		8		12		12		6		6		9
2969	Dinner 18:30 - 18:59		9		9		10		7		13		10		8		9
2969	Dinner 19:00 - 19:29		10		8		8		10		9		9		6		9
2969	Dinner 19:30 - 19:59		5		5		8		7		7		9		6		7
2969	Dinner 20:00 - 20:29		4		7		6		6		5		6		6		6
2969	Evening 20:30 - 20:59		4		3		4		4		4		4		4		4
2969	Evening 21:00 - 21:29		0		3		1		3		1		2		1		2
2969	Evening 21:30 - 21:59		1		1		1		1		0		0		1		0
2969	Evening 22:00 - 22:29		0		0		0		0		0		0		0		0
2969	Evening 22:30 - 22:59		0		0		0		0		0		0		0		0
2969	Evening 23:00 - 23:29		0		0		0		0		0		0		0		0
2969	Evening 23:30 - 23:59		0		0		0		0		0		0		0		0
		145		175		183		183		193		201		141		174	



PANDA EXPRESS, INC.
1853 Walnut Grove Ave.
Rosemead, CA 91768
Telephone: 626.795.8888
Facsimile: 626.272.8888

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REVISIONS:

NO.	DATE	DESCRIPTION
1	11/02/2009	FOR SUBMITTAL

DRAWN BY: LEH

PANDA PROJECT # 07058
ATWELL PROJECT # 18003788

NORR

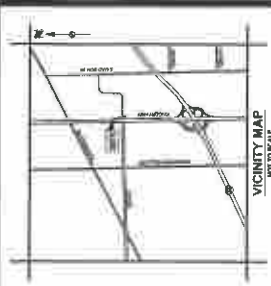


PANDA EXPRESS
WALN & WILSON RD
7405 KIMBERLY HIGHWAY
WILLOWBROOK, IL 60077

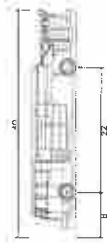
TRUCK TURN ANALYSIS

EX-01

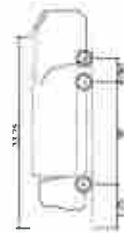
TRUE NORTH & WELCOME DRG



VICINITY MAP
NOT TO SCALE



Pumper Fire Truck
Overall Length 40.000 ft
Overall Width 8.165 ft
Overall Body Height 7.745 ft
Front Body Ground Clearance 8.655 ft
Lock-to-Lock Time 5.065 s
Max Wheel Angle 45.00°



Front-Load Garbage Truck
Overall Length 33.750 ft
Overall Width 8.375 ft
Overall Body Height 10.565 ft
Front Body Ground Clearance 9.500 ft
Lock-to-Lock Time 6.005 s
Curb to Curb Turning Radius 29.300 ft



WB-40 - Intermediate Semi-Trailer
Overall Length 45.499 ft
Overall Width 8.500 ft
Overall Body Height 11.344 ft
Front Body Ground Clearance 9.000 ft
Lock-to-Lock Time 20.39 s
Door Sliding Range (Virtual)



LEGEND
PUMPER FIRE TRUCK
FRONT-LOAD GARBAGE TRUCK
WB-40 DELIVERY TRUCK



24-HOUR CONTACT:
PANDA, INC.
PANDA TALA
847-477-5885

NOTES:
1. THE DESIGN OF THE TRUCK TURN ANALYSIS IS BASED ON THE ASSUMPTION THAT THE TRUCKS WILL BE DRIVING AT A SPEED OF 10 MPH. THE DESIGNER IS NOT RESPONSIBLE FOR THE RESULTS OF THE ANALYSIS IF THE TRUCKS ARE DRIVING AT A HIGHER SPEED.
2. THE DESIGNER IS NOT RESPONSIBLE FOR THE RESULTS OF THE ANALYSIS IF THE TRUCKS ARE DRIVING AT A HIGHER SPEED.
3. THE DESIGNER IS NOT RESPONSIBLE FOR THE RESULTS OF THE ANALYSIS IF THE TRUCKS ARE DRIVING AT A HIGHER SPEED.





PANDA EXPRESS, INC.
1683 Walnut Grove Ave.
Rosemead, California
91768
Telephone: 626 799 8888
Facsimile: 626 372 8828

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REVISIONS:

NO. DATE	BY	REVISION
1	1/18/2020	1/18/2020

DRAWN BY: LKH

PANDA PROJECT # D7068
ATWELL PROJECT # 1003769

NORR



PANDA EXPRESS
1683 WALNUT GROVE AVE.
ROSEMEAD, CA 91768

TRUCK TURN
ANALYSIS

EX-01

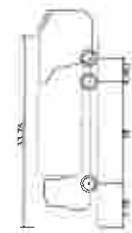
TRUE NORTH & WELCOME 2000



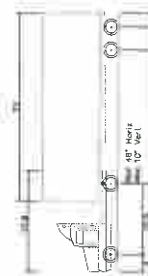
VICINITY MAP
NOT TO SCALE



Pumper Fire Truck
Overall Length
Overall Width
Overall Height
Min. Body Height
Min. Body Ground Clearance
Lock-to-Lock Time
Max Wheel Angle

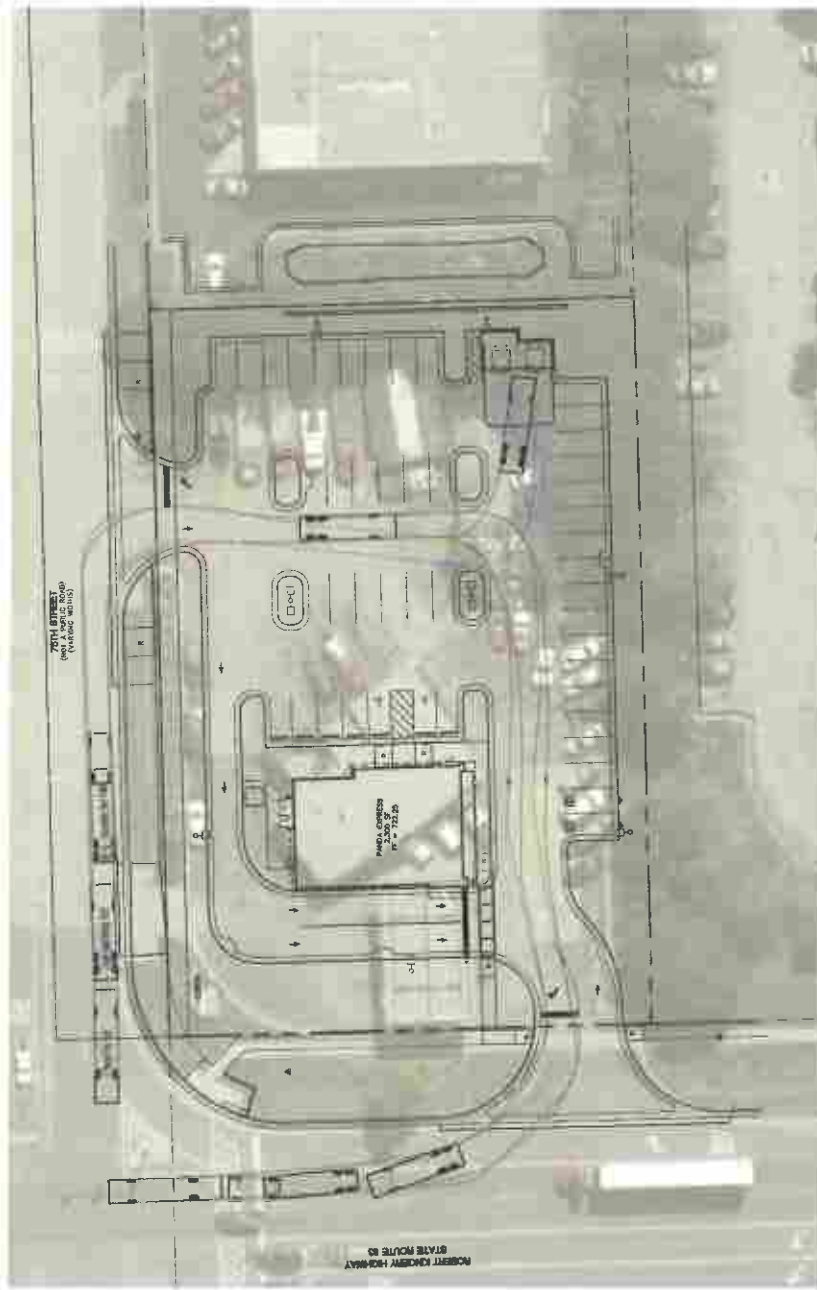


Front-Load Garbage Truck
Overall Length
Overall Width
Overall Height
Min. Body Height
Min. Body Ground Clearance
Lock-to-Lock Time
Max Wheel Angle



WS-40 Intermediate Semi-Trailer
Overall Length
Overall Width
Overall Height
Min. Body Height
Min. Body Ground Clearance
Lock-to-Lock Time
Max Wheel Angle

LEGEND:
PUMPER FIRE TRUCK
FRONT-LOAD GARBAGE TRUCK
WS-40 DELIVERY TRUCK



STATE ROUTE 88
ROBERT KENNEDY HIGHWAY



24-HOUR CONTACT
HARM YALA
847-477-5225

NOTES:
1. THE TRUCK TURN ANALYSIS WAS PERFORMED USING THE FOLLOWING ASSUMPTIONS:
a. THE TRUCKS WERE ASSUMED TO BE APPROXIMATELY 10 FEET LONG.
b. THE TRUCKS WERE ASSUMED TO BE APPROXIMATELY 8 FEET WIDE.
c. THE TRUCKS WERE ASSUMED TO BE APPROXIMATELY 13 FEET HIGH.
d. THE TRUCKS WERE ASSUMED TO BE APPROXIMATELY 9 FEET MINIMUM BODY HEIGHT.
e. THE TRUCKS WERE ASSUMED TO BE APPROXIMATELY 9 FEET MINIMUM BODY GROUND CLEARANCE.
f. THE TRUCKS WERE ASSUMED TO BE APPROXIMATELY 45 FEET LOCK-TO-LOCK TIME.
g. THE TRUCKS WERE ASSUMED TO BE APPROXIMATELY 45 DEGREE MAX WHEEL ANGLE.

2. THE TRUCK TURN ANALYSIS WAS PERFORMED USING THE FOLLOWING ASSUMPTIONS:
a. THE TRUCKS WERE ASSUMED TO BE APPROXIMATELY 10 FEET LONG.
b. THE TRUCKS WERE ASSUMED TO BE APPROXIMATELY 8 FEET WIDE.
c. THE TRUCKS WERE ASSUMED TO BE APPROXIMATELY 13 FEET HIGH.
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e. THE TRUCKS WERE ASSUMED TO BE APPROXIMATELY 9 FEET MINIMUM BODY GROUND CLEARANCE.
f. THE TRUCKS WERE ASSUMED TO BE APPROXIMATELY 45 FEET LOCK-TO-LOCK TIME.
g. THE TRUCKS WERE ASSUMED TO BE APPROXIMATELY 45 DEGREE MAX WHEEL ANGLE.

3. THE TRUCK TURN ANALYSIS WAS PERFORMED USING THE FOLLOWING ASSUMPTIONS:
a. THE TRUCKS WERE ASSUMED TO BE APPROXIMATELY 10 FEET LONG.
b. THE TRUCKS WERE ASSUMED TO BE APPROXIMATELY 8 FEET WIDE.
c. THE TRUCKS WERE ASSUMED TO BE APPROXIMATELY 13 FEET HIGH.
d. THE TRUCKS WERE ASSUMED TO BE APPROXIMATELY 9 FEET MINIMUM BODY HEIGHT.
e. THE TRUCKS WERE ASSUMED TO BE APPROXIMATELY 9 FEET MINIMUM BODY GROUND CLEARANCE.
f. THE TRUCKS WERE ASSUMED TO BE APPROXIMATELY 45 FEET LOCK-TO-LOCK TIME.
g. THE TRUCKS WERE ASSUMED TO BE APPROXIMATELY 45 DEGREE MAX WHEEL ANGLE.

EX-02

INDICED	REGULAR (W x H)	45 SPACES
	2 SPACES	
	1 SPACE	
	ADDITIONAL	40 SPACES
	TOTAL PREPARED SPACES	40 SPACES

31.15 PER HOUR PM (1-4) PARKING (STANDARD) - 30 SPACES

[illegible]



Designer
 R. ZINSELMAYER
 Date
 10/29/2020
 Scale
 NOTED
 Drawing No.
 Summary

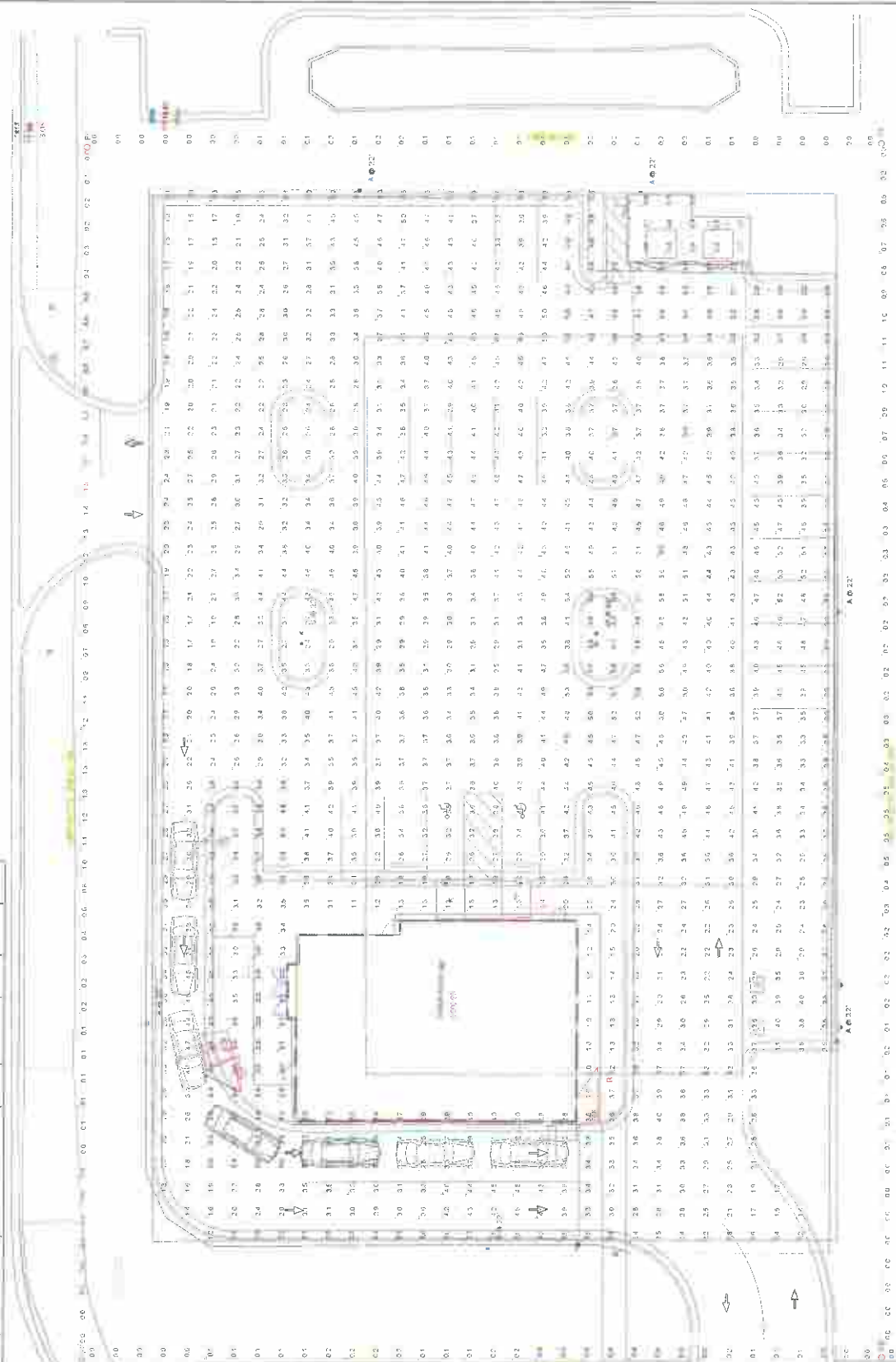
Note

1. MOUNTING HEIGHT OF 22' (20' POLE)
2. CALCULATIONS TAKEN AT GROUND LEVEL
3. CANTON VILLA LIGHTING - RYAN ZINSELMER

RYAN ZINSELMER@VILLALIGHTING.COM 314-531-2600

[illegible]

Case No.	Case Name	Case Type	Case Status	Case Date	Case Time	Case Location	Case Description	Case Details	Case Notes
6	SIM-LED-18L-SIL-FT-40	CONTACT RYAN ZINSELMER-314-531-2500	1	SIM-LED-18L-SIL-FT-40-70CEN-IL-68	12046	1	148.3		
6	SIM-LED-18L-SIL-3-40-70CEN-IL	CONTACT RYAN ZINSELMER-314-531-2500	1	SIM-LED-18L-SIL-3-40-70CEN-IL-68	12046	1	148.3		
2	SIM-LED-18L-SIL-FT-40-70CEN-IL	CONTACT RYAN ZINSELMER-314-531-2500	1	SIM-LED-18L-SIL-FT-40-70CEN-IL-68	12046	1	297		



Plan View
 $2\frac{1}{2} \times 16\frac{1}{2} = 16\frac{1}{2}$

1429



WILLOWBROOK, IL PANDA EXPRESS – PROPOSED CONSTRUCTION SCHEDULE

Item	Description	2021									
		Jun	Jul	Aug	Sep	Oct	Nov	Dec			
1	Mobilization / Begin Construction		X								
2	Demolition		X								
3	Earthwork		X								
4	Building - Foundation/Slab		X								
5	Site Utilities			X	X						
6	Building - Vertical Construction			X	X	X					
7	Site Concrete/Paving				X	X					
8	Landscaping					X					
9	Site Signage / Parking Lot Striping					X					
10	Final Completion / Store Opening						X				



Attachment 3
Findings of Fact
Standards for Special Use Permits (2 pages)

9-14-5(B): Standards for Special Use

The Plan Commission shall not recommend and the Board of Trustees shall not grant a Special Use Permit from the regulation of this title unless affirmative findings of fact shall be made as to all of the standards hereinafter set forth, which findings of fact shall be based upon evidence adduced upon the hearing held thereon, that:

- (A) That the establishment, maintenance, or operation of the special use will not be detrimental to or endanger the public health, safety, morals, comfort or general welfare.**

Applicant Finding: Panda Express is a nationwide, experienced restaurateur that has developed many sites prior to this one. Establishment, maintenance, or operation of the proposed Panda Express Drive-Thru Restaurant will not be detrimental to or endanger the public health, safety, morals, comfort or general welfare.

- (B) That the special use will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish and impair property values within the neighborhood.**

Applicant Finding: The proposed Panda Express Drive-Thru Restaurant will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish and impair property values within the neighborhood. The proposed improvements made to an old site will be a major upgrade to what exists there now.

- (C) That the establishment of the special use will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.**

Applicant Finding: Establishment of the proposed Panda Express Drive-Thru Restaurant will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district. The entire surrounding area is already developed.

- (D) That adequate utilities, access roads, drainage and/or other necessary facilities have been or are being provided.**

Applicant Finding: Adequate utilities, access (drives), drainage, and/or other necessary facilities will be engineered, permitted and constructed in accordance with local requirements for the proposed Panda Express Drive-Thru Restaurant.

- (E) That adequate measures have been or will be taken to provide ingress and egress so designed as to minimize traffic congestion in the public streets.**

Applicant Finding: Site layout, access drives, parking, signage, pavement markings and estimated drive-thru queueing were analyzed to provide ingress and egress so designed as to minimize traffic congestion in the public streets for the proposed Panda Express Drive-Thru Restaurant. Furthermore, in accordance with the ITE Trip Generation 10th Edition, traffic generated from the proposed land use will be substantially lower than existing traffic generated from the current land use.



- (F) That the special use shall in all other respects conform to the applicable regulations of the district in which it is isolated, except as such regulations may in each instance be modified by the Village Board pursuant to the recommendation of the Plan Commission.**

Applicant Finding: The proposed Panda Express Drive-Thru Restaurant shall conform to the applicable regulations of the "B-2" Zoning District with the exception of Variations approved by the Village Board pursuant to the recommendation of the Plan Commission.

- (G) Conditions in the area have substantially changed, and at least one year has elapsed since any denial by the Village Board of any prior application for a special use permit that would have authorized substantially the same use of all or part of the site. (Ord. 97-O-05, 1-27-1997).**

Applicant Finding: The proposed Panda Express Drive-Thru Restaurant has not previously been submitted for a Special Use Permit.



Attachment 4
Findings of Fact
Standards for Variations (2 pages)

9-14-4(E): Standards for Variations

The Plan Commission shall not recommend and the Board of Trustees shall not grant variations from the regulation of this title unless affirmative findings of fact shall be made as to all of the standards hereinafter set forth, which findings of fact shall be based upon evidence adduced upon the hearing held thereon, that:

- (A) The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located.**

Applicant Finding: The subject property for the proposed Panda Express Drive-Thru Restaurant is a substandard size lot for the B-2 district and cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. The requested variations are needed to provide a site which has economic viability and can be operated safely with good access and parking.

- (B) The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district.**

Applicant Finding: The requested variations will not merely serve as a convenience for the proposed Panda Express Drive-Thru Restaurant, but rather are necessary for any business development on the existing substandard lot. The small size of this lot creates a hardship that can only be remedied with the variations requested so that a safe efficient operation can be maintained.

- (C) The alleged hardship has not been created by any person presently having a proprietary interest in the premises.**

Applicant Finding: The alleged hardships were not created by any person presently having a proprietary interest in the premises and proposed Panda Express Drive-Thru Restaurant. Previous Variations approved for the site under Ordinance 94-O-16 are similar to the variations being requested herein so that the site could yield a reasonable return. The size and dimensions of the lot were preexisting.

- (D) The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.**

Applicant Finding: Proposed Variations for the proposed Panda Express Drive-Thru Restaurant will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood as they were previously granted for the existing uses on the property with no negative impacts on surrounding property.



- (E) The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, increase the danger of fire, or endanger the public safety.**

Applicant Finding: Proposed Variations for the proposed Panda Express Drive-Thru Restaurant will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.

- (F) The proposed variation will not alter the essential character of the locality. (Ord. 77-O-4, 2-14-1977)**

Applicant Finding: Proposed Variations for the proposed Panda Express Drive-Thru Restaurant will not alter the essential character of the locality as the restaurant will conform to the existing surrounding area uses. As they were previously granted for this site so the new development is consistent with was previously existing as far as variations.

- (G) The proposed variation is in harmony with the spirit and intent of this title. (Ord. 97-O-05, 1-27-1997)**

Applicant Finding: Proposed Variations for the proposed Panda Express Drive-Thru Restaurant are in harmony with the spirit and intent of Title 9 of this Code. The subject property cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. Previous variations approved for the subject site under Ordinance 94-O-16 changed the driveways and also noted that a portion of the lot was taken by the widening of Route 83. Variations are necessary for development on the existing substandard lot.



WILLOWBROOK, IL PANDA EXPRESS – VARIATIONS REQUESTED

#	Village Code	Description	Requirement	Existing Conditions (Shell Gas/Service Station; Ordinance 94-O-16)	Requested Variation
1	9-3-3(C)	Minimum Lot Area, Two Uses	2 Acres	1 Acre	1 Acre
2	9-3-7(A)1	Route 83 Setback	100'	58' (Service Station Building) 5'-4" (Overhead Canopy)	50' (Building) 62.4' (Order Canopy) 56.6' (Menu Board)
3	9-6-1(A)	Minimum Lot Area, Two Uses	2 Acres	1 Acre	1 Acre
4	9-6B-3(A)	Minimum Lot Area, (B-2 Bulk Regulations)	2 Acres	1 Acre	1 Acre
5	9-6B-3(C)1	Minimum Lot Width	200'	170'	170'
6	9-6B-3(E)1	Minimum Front Yard Setback (Route 83)	60'	58' (Service Station Building) 5'-4" (Overhead Canopy) 15' (Gas Pump)	50' (Building) 62.4' (Order Canopy) 56.6' (Menu Board)
7	9-6B-3(E)3	Minimum Exterior Side Yard Setback (75th St.)	60'	43'	43.4' (Building)
8	9-10-5(L)2(b)(2)A	Minimum Driveway Radius Connecting to 75th Street	35'	16'	25.0' (Order Canopy) 32.7' (Menu Board)
9	9-10-5(L)2(d)	Driveway Location to Nearest Lot Line	70'	17'-6"	25'
10	9-10-5(L)2(e)	Minimum Spacing Between Separate Driveways	400'	±171' (75th St.) ±306' (Route 83)	±169' (75th St.) ±313' (Route 83)
11	9-10-5(L)2(f)	Minimum Spacing Between Driveway & ROW Intersection	500' or Outside Functional Area (whichever is less)	154.3' (75th St.) 101.4' (Route 83)	146.2' (75th St.) 94.3' (Route 83)
12	9-10-5(L)2(g)	Access Driveway Concrete Median & Driveway Storage	Concrete Median & 100' Storage Area	No Concrete Median, 0' Storage Area	No Concrete Median, 0' Storage Area
13	9-14-2(D)2(c)(4)	Minimum Landscape Foundation Plantings Width	7'	0' (none provided)	4.5' at South Façade 5.2' at Northern East Façade 0' at Western Façade & Remaining East Façade

EXHIBIT A



Attachment 5
Public Hearing Notice (3 pages)

NOTICE OF PUBLIC HEARING
ZONING HEARING CASE NO. 21-01

NOTICE IS HEREBY GIVEN that the Plan Commission of the Village of Willowbrook, DuPage County, Illinois, will conduct a public hearing at a regular meeting of the Plan Commission on the 13th of January, 2021 at the hour of 7:00 P.M. This meeting would typically take place in the Willowbrook Police Department Training Room, 7760 S. Quincy St, Willowbrook, IL 60527. However, due to the current circumstances concerning Covid-19, this meeting will be held virtually. Internet address and access instructions will be provided on the Village of Willowbrook's Plan Commission website once available:

<https://www.willowbrookil.org/Archive.aspx?AMID=44>

The purpose of this public hearing shall be to consider a petition requesting approval of a special use permit for a fast food establishment and a special use permit for a drive-through in the B-2 Community Shopping District, including certain variations from Title 9 of the Village Code. The applicant seeks to demolish the existing gas/service station and construct a one-story building and drive-through with associated site improvements on the property legally described as follows:

THE NORTH 170.0 FEET OF THAT PART OF THE SOUTHEAST QUARTER OF SECTION 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING EAST OF THE EAST RIGHT OF WAY OF STATE ROUTE 83, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT 102.65 FEET EAST OF THE NORTHWEST CORNER OF SAID SOUTHEAST QUARTER; THENCE EAST, ALONG THE NORTH LINE OF SAID SOUTHEAST QUARTER, A DISTANCE OF 256.24 FEET; THENCE SOUTH, ALONG A LINE PARALLEL WITH THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170 FEET; THENCE WEST, ALONG A LINE PARALLEL WITH THE NORTH LINE OF SAID SOUTHEAST QUARTER, A DISTANCE OF 256.24 FEET TO THE INTERSECTION OF SAID PARALLEL LINE WITH THE EAST RIGHT OF WAY OF SAID STATE ROUTE 83, WHICH POINT IS 102.45 FEET EAST OF THE WEST LINE OF SAID SOUTHEAST QUARTER; THENCE NORTH, ALONG THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170.0 FEET TO THE POINT OF BEGINNING, IN DUPAGE COUNTY, ILLINOIS.

PIN: 09-26-400-013

ADDRESS: 7505 S. Kingery Highway, Willowbrook IL 60527.

The applicant for this petition is Hakim Yala of Panda Express, Inc., 1683 Walnut Grove Avenue, Rosemead CA 91770. The property owner is True North Energy, LLC, 10346 Brecksville Road, Brecksville OH 44141.

A copy of the Village's petition is on file in the Office of the Village Planner, Village of Willowbrook, 835 Midway, Willowbrook, Illinois, and is available for public inspection. Any individual with a disability requiring a reasonable accommodation in order to participate in any public meeting held under the authority of the Village of Willowbrook should contact Ann Choi, Village of Willowbrook, 835 Midway, Willowbrook, IL 60527, or call (630) 920-2233, Monday through Friday, between 8:30 A.M. and 4:30 P.M. All persons desiring to be heard in support or opposition to the application shall be afforded an opportunity and may submit their statements orally, in written form, or both. Written comments may be submitted up to the hour of 6:00pm on January 13, 2021 to planner@willowbrook.il.us. This hearing may be recessed to another date if not concluded on the evening scheduled.

/s/ Brian Pabst
Village Administrator
(630) 920-2261

Published in the December 24, 2020 edition of *The Doings* Newspaper.

Call: 866-399-05



Notice of Sign on Illinois Route 83/Kingery Highway frontage (posted on December 30, 2020)



Notice of Sign on 75th Street frontage (posted on December 30, 2020)





Attachment 6
Traffic Regulation Agreement (11 pages)

Return Recorded Document

to:

Brian Pabst
Village Administrator
Village of Willowbrook
835 Midway Drive
Willowbrook, IL 60527

TRAFFIC REGULATION AGREEMENT

THIS TRAFFIC REGULATION AND ENFORCEMENT AGREEMENT (the "Agreement") is made as of this _____ day of _____, 2019, by and between True North Energy, LLC, a Delaware limited liability company authorized to conduct business in the State of Illinois (the "Owner"), and the Village of Willowbrook, an Illinois home-rule municipal corporation (the "Village"), within which the "Complex" (as defined in Section 1.1 below) is located, for the regulation of traffic and other matters within the Complex and for the enforcement of said regulations by the assigned traffic law enforcement personnel of the Village.

WITNESSETH:

ARTICLE 1. DEFINITIONS: As used in this Agreement, the following definitions apply:

- 1.1 Complex: The land, buildings and other improvements commonly known as 7505 S. Kingery Highway, situated in the Village of Willowbrook, DuPage County, Illinois, and legally described in the attached Exhibit "A".
- 1.2 Permanent Index Numbers (PINs): 09-26-400-013.

- 1.3 Manager: Those persons or entity employed or retained by Owner from time to time with authority to administer, manage and operate the Complex for the purposes of this Agreement.
- 1.4 Owner: True North Energy, LLC.
- 1.5 Village: Village of Willowbrook, DuPage County, Illinois.

ARTICLE 2. RECITAL OF FACTS: The following recitals of fact are an integral part of this Agreement.

- 2.1 Owner holds record title to the Complex.
- 2.2 The Complex is located within the corporate jurisdiction of the Village.
- 2.3 It is the mutual desire of the parties hereto that the Village shall have the authority, but not the obligation, to regulate the parking of vehicles, traffic, roller skating, bicycle riding, skateboarding and/or other recreational activities within the Complex, and to enforce said regulations by the assigned traffic law enforcement personnel of the Village.
- 2.4 The Illinois Vehicle Code (625 ILCS 5/11-209), the Illinois Municipal Code (65 ILCS 5/1-1-7) and the Village's home rule authority provide authority for such agreement between the Village and the Owner and said statutory authorization enumerated on those matters which may be included in such agreement. Further, additional matters may be included in such agreements pursuant to Article VII, Section 6 and Section 10 of the Illinois Constitution.
- 2.5 Manager, in its capacity with Owner, is empowered to enter into this Agreement.

ARTICLE 3. COVENANTS: In consideration of the foregoing premises and of the covenants and conditions hereinafter contained, the adequacy and sufficiency of which the parties hereto hereby stipulate, each of the respective parties hereto covenants and agrees as follows:

- 3.1 The Owner will cause the Manager or its designated representative to cooperate with the Chief of Police of the Village or his designated representative in inspecting the parking area of the Complex to determine what, if any, stop signs, yield signs, person with disabilities parking area signs, or any other traffic markers are to be erected, and to determine what, if any, areas are to be marked as stop intersections, yield intersections, person with disabilities parking areas or pedestrian crossings, in order to provide for the safe and efficient trafficking of the parking area of the Complex. Further, such determination may include the regulation and/or prohibition of roller skating, the riding of bicycles, the riding or

operation of skateboards and/or other recreational activities in and upon the Complex, and the posting of the signs with the respect thereto.

- 3.2 If it be determined, pursuant to Section 3.1 of this Agreement, that stop signs, yield signs, person with disabilities parking area signs, or any other markers are to be erected or that specified intersections are to be marked as stop intersections, yield intersections, or pedestrian crossings, the Owner agrees to erect such signs and markers at the indicated places on the Complex, in accordance with all applicable regulations and specifications promulgated by the State of Illinois. The Owner shall bear the costs and expenses of obtaining, erecting and maintaining any and all such signs and markers necessary for enforcement of the regulations agreed to by the parties herein.
- 3.3 The Owner shall cause the Manager to mark such fire lanes as the local Fire Protection District Chief or his designated representatives shall recommend as necessary for effective movement of Fire Department and other emergency vehicles.
- 3.4 Signs or other devices providing for the regulation of traffic and parking, or the regulation or prohibition of roller skating, bicycle riding, skateboarding and/or other recreational activities, within the Complex, as well as a designation of the exact regulations to be imposed thereon, shall be installed as shown in the Traffic Regulation Plan for (Project Name) Willowbrook, IL Panda Express as prepared by (Consultant) Atwell, LLC, (Address) 1250 East Diehl Road, Suite 300, Naperville, IL 60563, referenced as Project No. 18003769.01, consisting of 1 sheet, dated November 16, 2020 and revised through N/A, _____ a copy of which is attached hereto as Exhibit "B" and by this reference incorporated herein.
- 3.5 The Village has the authority, but not the obligation, to enforce all regulations in the parking areas of the Complex by use of assigned traffic enforcement personnel of the Village; to issue citations to any and all violators of such regulations; and to adopt and enforce any additional reasonable rules and regulations with respect to traffic and parking in the parking area as local conditions may require or the safety and convenience of the public or the users of the parking area.
- 3.6 The Owner hereby agrees to provide for the removal of vehicles that are abandoned or parked in areas where stopping, standing or parking is prohibited. Such removal shall be done pursuant to a towing agreement and in accordance with the requirements of 625 ILCS 5/4-203, including, but not limited to, posting of the notice required therein.
- 3.7 Neither the Owner nor the Manager shall permit any person to park a vehicle at any location in the Complex for the purpose of displaying such vehicle for sale, lease or ride-share. The owner of any such vehicle may be ticketed by the Village.

- 3.8 The Owner has named the Village as an additional insured on a primary non-contributory basis on its Comprehensive General Liability Insurance Policy and on their Excess Liability Insurance Policy and a copy of said Certificate of Insurance is attached hereto as Exhibit "C" and incorporated herein. Such liability insurance shall provide that the Village, its officers, agents, agencies, employees, and departments shall be additional insured under such insurance. Said insurance shall be in the minimum amount of one million dollars (\$1,000,000) combined single limit or in such amounts acceptable to the Village and shall be in such form and with such Company as shall be approved by the corporate authorities.

The Owner hereby agrees to keep said policies in full force and effect throughout the terms of this Agreement. A mandatory written notice must be provided upon the cancellation of any policy as outlined under the Certificate of Insurance evidencing the coverage provided for herein.

- 3.9 In the event the Owner changes Managers, the Owner shall notify in writing the Village within ten (10) days of such change.

ARTICLE 4. TERM:

- 4.1 This Agreement shall be in full force and effect from and after the date of its execution for a period of twenty (20) years of the date thereof, and may, by further agreement of the parties, be continued for additional periods of like duration.

Notwithstanding any provision contained herein to the contrary, after this Agreement has been in effect for a term of one (1) year, this Agreement may be canceled upon the giving of thirty (30) days prior written notice by either party hereto, except to the extent that the Owner may be required to maintain this Agreement pursuant to any zoning relief granted by the Village.

- 4.2 The sole remedy available to the Owner, upon any breach of this Agreement by the Village, shall be the cancellation of the Agreement under its terms. It is of the essence of this Agreement that the Village shall not be liable in money damages for any breach of this Agreement.

ARTICLE 5. SUCCESSORS: This Agreement shall be binding upon and inure to benefit the respective assigns, successors and personal representatives of each of the parties hereto, and this Agreement shall run with title to the Complex and the obligations of the Owner shall be and are binding on future owners of any portion of the Complex.

- ARTICLE 6. GOVERNING LAW: This Agreement shall be governed by the laws of the State of Illinois.
- ARTICLE 7. EXECUTION AND RECORDING: This Agreement shall be executed by the parties' prior to the commencement of operations of the Complex. A fully executed copy of the Agreement shall be recorded in the Office of the Recorder of Deeds of the County of DuPage of the State of Illinois against title to the Complex, and it is agreed, pursuant to the statutes set forth above, that no regulation made pursuant to this Agreement shall be effective or enforceable until three (3) days after this Agreement is recorded.
- ARTICLE 8. NOTICES: All notices hereunder shall be in writing and sent by Certified Mail, addressed to the Manager at EC Developments II, LLC, 1120 North Town Center Drive, Suite 150, Las Vegas, NV 89144 and, if to the Village, at the Office of the Village Administrator of Willowbrook, 7760 Quincy Street, Willowbrook, Illinois 60527.

IN WITNESS WHEREOF, the parties hereto have caused this Agreement to be duly executed, as of the date and year first above mentioned.

OWNER True North Energy, LLC
By: Mark E. Lyden

BY:

MARK E. LYDEN, President/CEO
(Managing Agent or Owner)

VILLAGE OF WILLOWBROOK

BY:

President

ATTEST:

Village Clerk

EXHIBIT "A"

LEGAL DESCRIPTION OF COMPLEX

THE NORTH 170.0 FEET OF THAT PART OF THE SOUTHEAST 1/4 OF SECTION 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING EAST OF THE EAST RIGHT OF WAY OF STATE ROUTE 83, MORE PARTICULARLY DESCRIBED AS FOLLOWS: BEGINNING AT A POINT 102.65 FEET EAST OF THE NORTHWEST CORNER OF SAID SOUTHEAST 1/4; THENCE EAST, ALONG THE NORTH LINE OF SAID SOUTHEAST 1/4, A DISTANCE OF 256.24 FEET; THENCE SOUTH, ALONG A LINE PARALLEL WITH THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170 FEET; THENCE WEST, ALONG A LINE PARALLEL WITH THE NORTH LINE OF SAID SOUTHEAST 1/4, A DISTANCE OF 256.24 FEET TO THE INTERSECTION OF SAID PARALLEL LINE WITH THE EAST RIGHT OF WAY OF SAID STATE ROUTE 83, WHICH POINT IS 102.45 FEET EAST OF THE WEST LINE OF SAID SOUTHEAST 1/4; THENCE NORTH, ALONG THE EAST RIGHT OF WAY LINE OF SAID STATE ROUTE 83, A DISTANCE OF 170.0 FEET TO THE POINT OF BEGINNING, IN DUPAGE COUNTY, ILLINOIS.

EXHIBIT "B"

TRAFFIC REGULATION PLAN

Pursuant to the attached agreement, the Willowbrook Police Department has the authority, but not the obligation, to enforce the following areas as designated on the traffic control plan attached hereto.

1. Traffic control signs including posted speed limit signs, stop signs, yield signs, and one-way signs.
2. No parking within 15 feet of hydrant locations as marked.
3. Handicap parking areas as marked with an official sign.
4. Prohibition of parking along all streets between 2:00 a.m. and 6:00 a.m.
5. Enforcement of yellow curb markings with adjacent "No Parking" signs indicating no parking areas.
6. Enforcement of posted, "No Trespassing" signs.
7. Posted "No Parking" zones.
8. Parking of vehicles for the purpose of being displayed for sale.
9. Enforcement of Village ordinance violations.
10. Prohibition of roller skating, bicycle riding, skateboarding and/or other recreational activities within the complex as posted.

Any future signs, crosswalks, and so forth may be agreed upon at a later date.

Tamara K. Evans, LLC
By: [Signature]

Owners' Representative

Chief of Police

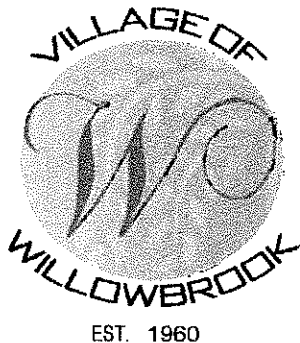
[ATTACH PLAN]

EXHIBIT "C"

CERTIFICATE OF INSURANCE



Attachment 7
Village Review Letters (20 pages)



Willowbrook

835 Midway Drive
Willowbrook, IL 60527-5549

Phone: (630) 323-8215 Fax: (630) 323-0787 www.willowbrookil.org

Mayor

Frank A. Trilla

Village Clerk

Deborah Hahn

Village Trustees

Sue Berglund

Umberto Davi

Michael Mistele

Gayle Neal

Paul Oggerino

Gregory Ruffolo

Village Administrator

Brian Pabst

Chief of Police

Robert Schaller

Director of Finance

Carrie Dittman



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Illinois Route 66 Scenic Byway

January 13, 2021

Atten: Hakim Yala
Panda Express, Inc.
1683 Walnut Grove Avenue
Rosemead, CA 91770

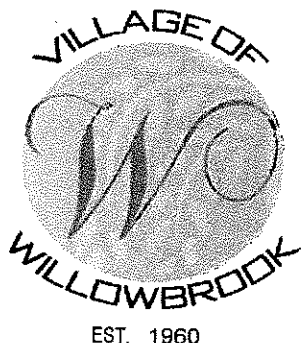
Brian A. Styck
Atwell, LLC

Re: **7505 Kingery (Panda Express & Drive-Through) Planning Review #1**

Mr. Yala and Styck,

Planning staff has reviewed the submitted application, submitted on November 25, 2020, for the Panda Express Fast-Food Restaurant and Drive-Through development on the lot currently occupied by Shell Oil gas station on 7505 Kingery Highway in Willowbrook, IL 60527. The following plan has been reviewed per the requirements of the Village of Willowbrook Zoning Ordinance:

1. "Legal Description" – 1 page, undated and prepared by Atwell Group.
2. "ALTA-NSPS Land Title Survey" – 2 sheets (Job. No. 18003769), dated 8/10/2020 and bearing the latest revision date of 9/23/2020, and prepared by NORR LLC.
3. "Site Layout Plan" – 1 sheet, Sheet C04.0, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, and prepared by NORR Architects Engineers Planners.
4. "Architectural Drawings" – 5 sheets, Sheets A-101, A-200, A-201, A-202, A-300, Panda Project #: D7058, Arch. Project #: JCDT180453, bearing an issue date of 11-04-2020, and prepared by NORR Architects Engineers Planners.
5. "Proposed Sign package" – 16 pages, Project #: C59897, dated 12.19.2018, bearing the latest revision date of 11.02.2020, and prepared by Priority Sign.
6. "Final Engineering Plans" – 21 sheets, Panda Project #: D7058, Arch. Project #: JCDT180453, bearing an issue date of 11-20-2020, and prepared by NORR Architects Engineers Planners.
7. "Engineer's Opinion of Probable Construction Cost" – 3 pages, dated 11/20/2020, and prepared by Atwell Group.
8. "Traffic Turn Analysis" – 1 sheet, Sheet EX-01, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, and prepared by NORR Architects Engineers Planners.
9. "Traffic Plan" – 1 sheet, Sheet EX-02, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, and prepared by NORR Architects Engineers Planners.
10. "Traffic Regulation Agreement" – 9 pages, dated 11/20/2020, and prepared by Atwell Group.
11. "Landscape Plan" – 1 sheet, Sheet L01.0, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, and prepared by NORR Architects Engineers Planners.
12. "Photometric Plan" – 1 sheet, Sheet L01.0, dated 10/29/2020, and prepared by Villa Lighting.
13. "Lighting Cut Sheets" – 1 sheet, Sheet A-400, Panda Project #: D7058, Arch. Project #: JCDT180453, dated 10/29/2020, and prepared by NORR Architects Engineers Planners.



Willowbrook

835 Midway Drive
Willowbrook, IL 60527-5549

Phone: (630) 323-8215 Fax: (630) 323-0787 www.willowbrookil.org

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14. "Construction Schedule" – 1 page, dated 11/17/2020, and prepared by Atwell Group.
15. "Standards for Special Use Permits" – 2 pages, prepared by Atwell Group.
16. "Standards for Variations" – 2 pages, prepared by Atwell Group.
17. "Variations Requested" – 1 page, dated 11/22/2020, and prepared by Atwell Group.
18. "Kane-DuPage Soil and Water Conservation District (SWCD) Land Use Opinion" – 1 page, dated December 14, 2020.
19. "Illinois Department of Conservation, Endangered Species Consultation" – 3 pages, dated 11/17/2020.

Staff offers the following comments:

General Comments

1. Revise Standards for Variations according to the new list of variations noted at the end of this review letter.

ALTA-NSPS Land Title Survey

2. The survey should be signed by a registered Illinois Professional Land Surveyor (missing signature).

Site Layout Plan

3. Include the distances from the center of the driveway on Route 83 to the center of the driveway on the adjacent property to the south.
4. Include the distances from the center of the driveway on 75th Street to the center of the driveway on the adjacent property to the east.
5. Label the width of all parking islands.
6. Identify and label the land uses adjacent to the subject property to the north, south, east, and west. Include the name of development and the zoning district.
7. Label the three shapes indicated in dashed and solid lines located north of the proposed building. (Are these menu boards?) Add dimensions from the north and west lot lines.

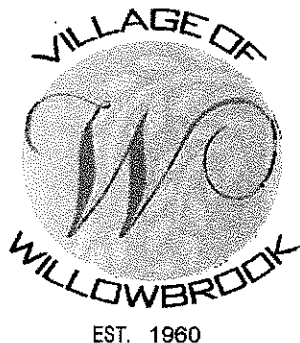
Traffic

Per your request, Gewalt Hamilton Associates, Inc. (GHA) has reviewed the following material provided for the above captioned project pertaining to traffic, parking, and on-site circulation:

- A. Site Traffic Study, prepared by Atwell dated January 5, 2021, which includes Traffic Plan, Sheet EX-02.
- B. Response to Comments Letter, prepared by Atwell dated January 5, 2021.
- C. Truck Turn Analysis, Sheet EX-01, prepared by Atwell dated November 20, 2020.

Conclusions of this effort and recommendations for consideration are presented below:

8. The TIS prepared by Atwell is generally in conformance with the guidelines published by the Institute of Transportation Engineers (ITE).
9. GHA has found the documentation and/or responses provided, in general, to adequately address the comments made and concerns raised in our previous Project Traffic Review dated September 30, 2020. An updated Site Traffic Study is not required.



Willowbrook

835 Midway Drive
Willowbrook, IL 60527-5549

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10. Employees should be encouraged to park in remote spaces (furthest from the site entrance) to provide convenient parking for customers.
11. The Site Traffic Study references ITE Land Use Code (LUC) 945, Gasoline/Service Station with Convenience Market using the building size to estimate traffic generated by the existing site use. Based on the existing site parameters (8 vehicle fueling positions, a convenience store less than 2,000 SF and ancillary vehicle service center), LUC 944, Gasoline/Service Station would be a more appropriate use. However, the trip generation estimates for the existing site use based on multiple independent variables (building size and number of vehicle fueling positions) would still result in a traffic estimate that is greater than the proposed Panda Express, fast-food restaurant with drive thru. Accordingly, no modifications to the study are required.
12. 75th Street to the west of Kingery Highway is under the jurisdiction of the DuPage County Division of Transportation.
13. Kingery Highway (IL Route 83) is also designated as a Strategic Regional Arterial (SRA) route. SRA routes are designed to carry higher traffic volumes at higher travel speeds through access control and increased access and traffic signal installation/spacing requirements.
14. A Certificate of Insurance will be required as an attachment to the Traffic Regulation Agreement.

Bulk Regulations & Landscaping

15. The Landscape Plan, Sheet L01.0, incorrectly labels the nine shrubs in the parking island immediately to the southeast of the buildings as "HR-9" when it should be labeled "RA-9". Revise to correct the typo.
16. Revise the "Ordinance Compliance Matrix" under the "Plant Calculation" table in the Landscape Plan to reflect that the Route 83 frontage is 170 feet and the 75th Street frontage is 256 feet.
17. Revise the "Section 9-6-1(D)" requirement 1. table in the Landscape Plan to reflect that 1,280 points is required for 75th Street and that 850 points is required for Route 83.
18. Differentiate between the evergreen and the deciduous shrubs under the Plant Schedule.
19. Section 9-6-1(D) of the Village Code provides that the front yard (Route 83) requires a minimum of 850 points. The Landscape Plan indicates 2 shade trees = 200 pts, 5 conifer trees = 300 pts, 6 ornamental trees = 300 pts, 19 evergreen shrubs = 285 pts and 76 deciduous shrubs = 760 pts, for a total of 1,845 pts. The total of 1845 pts. Exceeds the minimum requirement of 850 points.
20. Section 9-6-1(D) of the Village Code provides that the exterior side yard (75th Street) provides a minimum of 1,280 points. The Landscape Plan indicates 1 shade trees = 100 pts, 4 conifer trees = 240 pts, 6 ornamental trees = 300 pts, 16 evergreen shrubs = 240 pts, 24 deciduous shrubs = 240 pts, for a total of 1,120 points. The exterior side yard setback is deficient 160 points. **A variation from this requirement will be required unless you can provide more plantings.** A berm may be credited up to a maximum of fifty percent (50%) of the total landscape planting points required within a yard pursuant to Section 9-6-1(D)3.
21. The existing conditions plan (Sheet C02.0) calls out a 16-inch diameter underground gas pipe within a 7-foot easement along the north lot line. Typically, when a gas main is this large, Nicor Gas would send Watch and Protect when anyone is digging in close proximity to the main. Since it is such a large main, coordination with the gas company should be



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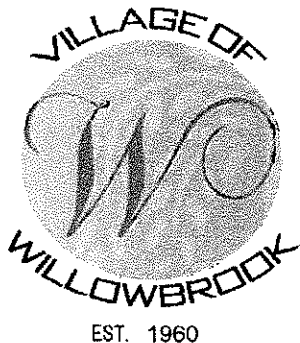
initiated during the design phase to understand how close any trees or landscape plantings with large root balls should be planted.

Photometric Plan

22. The photometric plans, consisting of two sheets and prepared by Villa Lighting and dated October 29, 2020 are in general compliance with Village Code and standard engineering methods. Please note that the Village reserves the right to require glare shields to be installed, should it deem appropriate.

Loading & Parking

23. Section 9-10-5(G) of the Village code requires that a landscaped earth berm of a minimum three-foot (3') height shall be required and appropriately landscaped with densely planted evergreen trees or shrubs to an effective minimum height of four feet (4') upon installation. **Staff requests that the Site Plan (Sheet C04.0) and Landscape Plan (Sheet L01.0) be revised to reflect this requirement.**
24. In addition, Section 9-10-5(G) requires that in all non-residential districts, parking shall be permitted in required front, side and/or rear yards, provided that the following screening and landscaping standards are met:
1. Front Yards and Exterior Side Yards: Permanent peripheral screening at least four feet (4') high shall be constructed and maintained in front yard and exterior side yard areas adjacent to parking areas. This screening shall consist of a planted earth berm, densely planted evergreen shrubs or trees or a combination of both. **Staff requests that the Site Plan (Sheet C04.0) and Landscape Plan (Sheet L01.0) be revised to reflect a berm within the front yard and exterior side yard to comply with this requirement and Section 9-6-1(D)3 (since the exterior side yard area was deficient by 160 points).**
 2. Interior Side and Rear Yards: Permanent peripheral screening at least five feet (5') high shall be constructed and maintained in interior side and rear yards adjacent to parking areas. This screening shall consist of a planted earth berm, densely planted evergreen shrubs or trees, or a combination of both. In interior side and rear yards, the screening shall be so located and constructed as not to interfere or conflict with the use of any utility easement or utility installations existing or planned to be installed in these easement areas. **The proposed Chadwick Yews in the rear yard only grow to a maximum height of 3' to 4'. Staff requests that these be replaced to comply with Section 9-10-5(G)2.**
 3. Interior Parking Lot Landscaping: Section 9-10-5(G)3 requires that each island shall be effectively landscaped with appropriately sized plantings at a minimum ratio of one shade tree and nine (9) dwarf variety shrubs per island and/or fifty (50) linear feet of parking area. Shade tree distribution shall provide a minimum of one tree in the area occupied by each twenty (20) parking spaces. **Add landscape plantings to the end island located to the northeast of the proposed building.**
25. Section 9-10-5(L)2(e) provides for spacing between separate driveway entrances requirements. **Staff requests to include these on the revised site plan to verify. A variation from this code section will be required.**



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26. Section 9-10-5(L)2(f) provides for spacing between driveway entrance and right-of-way of an adjacent intersecting street requirements. **Staff requests to include these on the revised site plan to verify or to indicate the functional area of the intersection. A variation from this code section may be required.**

Signage

27. Section 9-11-12(B)1(a) permits 1.25 square feet of sign surface area for each 1' of business site frontage up to a maximum of 350 square feet of sign surface area. In the case of a corner lot, "Business Site Frontage" is defined under Section 9-2-2 as the length of the exterior side lot line, measured in feet. The permitted sign surface area is 320 square feet.
28. Section 9-11-12(B)1(b)1 permits a total of four (4) wall signs.
29. Section 9-11-12(B)1(d)6 further reduces the sign surface area by seven and one-half (7 1/2) square feet per each foot of sign height for any freestanding or ground sign constructed in excess of eight feet (8') above average surrounding grade. The proposed ground sign S9 is twelve feet (12') tall and therefore in excess of 4 feet (4') above average surrounding grade. Therefore, the allowable sign surface area is reduced to 290 square feet.
30. Private traffic direction signs directing traffic movement onto a premises or within a premises, not exceeding four (4) square feet in area and four feet (4') in height for each sign shall be exempt under Section 9-11-6(L). Illumination of these signs shall be permitted in accordance with the regulations contained in this chapter. Sign D8 has two faces and the sum of these two faces is 4.33 square feet which exceeds the 4 square foot maximum. Please include a table that indicates compliance with the minimum area requirement for private directional signs.
31. Signs must contain brick or other similar material in keeping with the design of the principal structure under Section 9-11-12(B)1(d)1.

Fences

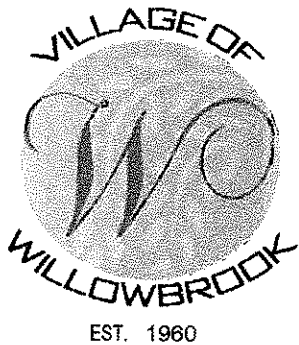
32. Section 9-12-4(D)2 requires that fences not greater than 3' in height and at least eighty percent (80%) open may be located anywhere on a lot. Based on Sheet C0.82, Staff calculates that the retaining wall located within the rear yard has a maximum height of 1.68' up to a height of 33". **Staff requests that the applicant confirm this dimension by providing a note to this effect on the Site Plan.**

Trash Enclosure

33. No comments.

Zoning Certificates and Occupancy Certificates

34. Section 9-14-2(D)2(a) requires that all areas of the development not proposed to be improved with structures, paved areas, walks, retention or detention areas, lighting or other approved landscaping shall be improved with Kentucky blue grass sod or seed. Sheet L01.1, Lawn Note No.1 indicates locally grown sod. **Please specify Kentucky blue grass sod or seed on the Landscape Plan (Sheet L01.0).**
35. Section 9-14-2(D)2(c)4 requires that a landscaped area having a minimum width of seven feet (7') shall be provided around all buildings except for building entrances and the front of retail business facilities to permit appropriate foundation plantings. Border plantings and



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foundation plantings shall be provided and shall comprise the remainder of the required landscape plantings, of which Please revise the sizes of the evergreen and deciduous shrubs in compliance with 9-14-2(D)2(c)4 which requires a minimum of one-half of the required landscape plantings shall be deciduous shrubs at a minimum height of three feet (3') and the remaining one-half (1/2) shall be evergreen shrubs at a minimum height of two feet (2').

36. Consider adding foundation planting along the north portion of the east façade to comply with Section 9-14-2(D)2(c)4. A variation for a reduced foundation planting width will be required along the west façade.

37. Add the dimension of a 4.5' wide foundation planting with on the Landscape Plan.

Engineering

38. Sheet C01.0: No comments.

39. Sheet C01.1:

- In Storm Sewer Note 4, add that the grates must have the "Dump No Waste - Drains to Creek" emblem.
- Watermain Note 11 should be revised to state that the contractor shall verify with the Public Services Department for specific make and model of fire hydrants, however there are no proposed hydrants on the plans.
- Add to the Watermain Notes "The contractor shall coordinate with the Public Services Department at least two working days prior to making the service tap to coordinate schedule, verify materials, and schedule required inspections."

40. Sheet C01.2: No comments.

41. Sheet C02.0: No comments.

42. Sheet C02.1

- Add to the note regarding the sanitary service note "as directed by Du Page County Public Works". Additional comments may be generated as more information is provided.

43. Sheet C03.0, L03.1: No comments.

44. Sheet C04.0, 4.1:

- The path along 75th Street is an asphalt path with concrete curb ramps at the driveways. Modify how the new pavement is shown at the proposed 75th Street driveway to only show the curb ramps to be concrete, with the remainder to be asphalt (up to first joint shown).
- We presume that the signs shown at the two handicapped accessible stalls are the required signs although not matching the legend.

45. Sheet C05.0, 5.1: No comments.

46. Sheet C06.0, 7.0, 7.1:

- The proposed water service should be as close as possible to a direct route from the main to the point of entry into the building.
- The Building Department will review the details of the water service, as it is considered plumbing under the Illinois Plumbing Code. Hydrocarbon resistant gaskets may be required since the service will cross a petroleum pipeline.
- Modify the note for the connection of the sanitary service to the existing manhole to refer to Du Page County requirements, not Village requirements.
- Show where the retaining wall underdrain will discharge.

47. Sheet C08.0 - 8.4;



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- a. For the handicapped sign detail, specify that the fine is \$350. The signage must be in compliance with the Illinois Accessibility Code standards.
48. Sheet L01.0, 1.1: Refer to Bulk Regulations & Landscaping, Loading & Parking and Zoning Certificates and Occupancy Certificates sections above.
49. General Comments:
 - a. An abbreviated Stormwater Submittal is required. We foresee this document being one to two pages with a project narrative and statements that the site does not contain floodplain or wetlands, and the development does not reach the net new impervious thresholds to trigger Best Management Practices or Detention.
 - b. A permit is required from IDOT prior to any work in the Route 83 right of way.
 - c. A permit is required from the Du Page County Public Works Department for the sanitary sewer connection.
 - d. The applicant must file a Notice of Intent with the IEPA prior to the start of construction.

Tri-State Fire Protection District

50. The Bureau of Fire Prevention has received a resubmitted copy of the proposed site, traffic, and auto turn plans for the above listed project. After review, the plans are found to be in apparent compliance with applicable standards relative to fire prevention and life safety.
51. Necessary inspections are to be performed along with any outstanding fees paid before occupancy is granted.
52. The Bureau of Fire Prevention has been asked to review the plans, specifications or other documents submitted to see if compliance has been made with the Fire Prevention Codes and Ordinances of the Tri-State Fire Protection District. Errors or omissions by representatives of the Bureau of Fire Prevention do not constitute permission to cancel, set aside or waive any provision of any applicable Code or Ordinance of the Tri-State Fire Prevention District. Approvals by the Bureau of Fire Prevention will be in writing only.

Illinois Department of Conservation, Endangered Species Consultation

53. The Illinois Natural Heritage Database contains no record of State-listed threatened or endangered species, Illinois Natural Area Inventory sites, dedicated Illinois Nature Preserves, or registered Land and Water Reserves in the vicinity of the project location.

Zoning Requests

Approval of a Special Use Permit for the construction of a one-story restaurant in the B-2 zoning district including a drive-through lane and associated site improvements. The approval of the special use permit will include, but is not limited to, the following variations:

1. A variation from Section 9-3-3(C) to decrease the minimum lot area for two (2) or more special uses from two acres (2 AC) to one acre (1 AC).
2. A variation from Section 9-3-7(A)1 to decrease the minimum special setback on Illinois Route 83 from one hundred feet (100') to fifty feet (50') for the proposed building.
3. A variation from Section 9-6-1(A) to decrease the minimum lot area for two (2) or more special uses (fast food establishment and drive-through) from two acres (2 AC) to one acre (1 AC).
4. A variation from Section 9-6-1(D)1 to decrease the required exterior side yard landscape planting requirements from 1,280 points to 1,120 points. **STAFF RECOMMENDS REVISING THE LANDSCAPE PLAN TO ELIMINATE THIS VARIATION**



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5. A variation from Section 9-6B-3(A) to decrease the minimum lot area for all uses from two acres (2 AC) to one acre (1 AC).
6. A variation from Section 9-6B-3(C)1 to decrease the minimum lot width of 200 feet for restaurants from two hundred feet (200') to one hundred seventy feet (170'). *(Existing Condition)*
7. A variation from Section 9-6B-3(E)1 to decrease the minimum front yard setback on Illinois Route 83 from sixty feet (60') to fifty feet (50').
8. A variation from Section 9-6B-3(E)3 to decrease the minimum exterior side yard setback on 75th Street from sixty feet (60') to forty-three point four feet (43.4') for the proposed building.
9. *A variation from Section 9-10-5(G) to eliminate the requirement of a 3' tall berm within the front yard adjacent to Illinois Route 83. (May not need if plan is revised)*
10. *A variation from Section 9-10-5(G)2 to eliminate the permanent peripheral screening at least five feet (5') high to be constructed and maintained in interior side and rear yards adjacent to parking areas. (May not need if plan is revised)*
11. A variation from Section 9-10-5(L)2(b)(2)a to decrease the radius connecting street pavement edge and driveway edge on 75th street from 35' to 25'. *(Existing Condition)*
12. A variation from Section 9-10-5(L)2(d) to decrease the driveway location on Route 83 from seventy feet (70') to twenty-five point seven feet (25.7'). *(Existing Condition)*
13. A variation from Section 9-10-5(L)2(e) to decrease the minimum spacing between separate driveway entrances from four hundred feet (400') to approximately three hundred thirteen feet (+/- 313') on Illinois Route 83 and to approximately one hundred sixty-nine feet (+/- 169') on 75th Street. *(Existing Condition)* **APPLICANT TO PROVIDE DIMENSIONS ON SITE PLAN**
14. A variation from Section 9-10-5(L)2(f) to decrease the minimum spacing between driveway entrance and right-of-way of an adjacent intersecting street from five hundred feet (500') to ninety-four point three feet (94.3') on Illinois Route 83 and to one hundred forty-six point two feet (146.2') on 75th Street. *(Existing Condition)*
15. A variation from Section 9-10-5(L)2(g) to eliminate the requirement of a concrete median separation and to reduce the driveway storage from one hundred feet (100') to zero feet on Illinois Route 83 and 75th Street.
16. A variation from Section 9-14-2(D)2(c)4 to decrease the minimum foundation landscape area from seven feet (7') to four point five feet (4.5') along the southern façade, and from seven feet (7') to zero feet along the western building façade and the northern portion of the eastern building façade.



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If you have any questions, you may contact me at planner@willowbrook.il.us or 630.920.2233.

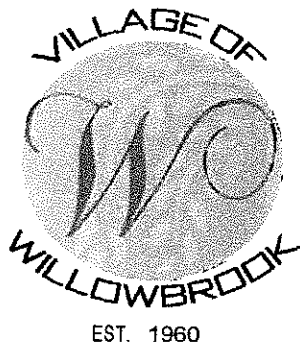
Sincerely,

Ann Choi
Village Planning Consultant
WBK Engineering, LLC

Cc: Brian Pabst
Michael S. Mertens
Dan Lynch
Roy Giuntoli



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January 22, 2021

Atten: Hakim Yala
Panda Express, Inc.
1683 Walnut Grove Avenue
Rosemead, CA 91770

Brian A. Styck
Atwell, LLC

Re: **7505 Kingery (Panda Express & Drive-Through) Planning Review #2**

Mr. Yala and Styck,

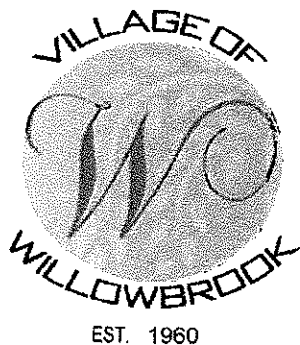
Planning staff has reviewed the revised application materials submitted on January 19, 2021, for the Panda Express Fast-Food Restaurant and Drive-Through development on the lot currently occupied by Shell Oil gas station on 7505 Kingery Highway in Willowbrook, IL 60527. The following plan has been reviewed per the requirements of the Village of Willowbrook Zoning Ordinance:

1. **"ALTA-NSPS Land Title Survey"** – 2 sheets (Job. No. 18003769), dated 8/10/2020, bearing the latest revision date of 9/23/2020, resubmitted on January 19, 2021 and prepared by NORR LLC.
2. **"Site Layout Plan"** – 1 sheet, Sheet C04.0, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, bearing the latest revision date of 01/19/2021 and prepared by NORR Architects Engineers Planners.
3. **"Final Engineering Plans"** – 18 sheets, Panda Project #: D7058, Atwell Project #: 18003769, bearing an issue date of 11/20/2020, bearing the latest revision date of 01/19/2021 and prepared by NORR Architects Engineers Planners.
4. **"Stormwater Management Summary"** – 3 pages, bearing an issue date of January 19, 2021, and prepared by Atwell, LLC.
5. **"Traffic Regulation Agreement"** – 11 pages, resubmitted by Atwell Group on 01/19/2021.
6. **"Landscape Plan"** – 2 sheets, Sheets L01.0 and L01.1, Panda Project #: D7058, Atwell Project No. 18003769, bearing an issue date of 11/20/2020, bearing the latest revision date of 01/19/2021 and prepared by NORR Architects Engineers Planners.
7. **"Proposed Sign Package"** – 17 pages, Project #: C59897, dated 12.19.2018, bearing the latest revision date of 01.19.2020, and prepared by Priority Sign.
8. **"Standards for Variations"** – 3 pages, submitted by Atwell Group on 01/21/2021.

Staff offers the following comments:

Site Layout Plan

1. Label the width of all parking islands. End parking islands closest to proposed building are missing dimensions.



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Traffic

GHA has reviewed the response letter and supporting documentation prepared by Atwell dated January 19, 2021 and found it to adequately address our comments, questions, and concerns. No further response is needed.

Loading & Parking

2. In addition, Section 9-10-5(G) requires that in all non-residential districts, parking shall be permitted in required front, side and/or rear yards, provided that the following screening and landscaping standards are met:
 1. Front Yards and Exterior Side Yards: Permanent peripheral screening at least four feet (4') high shall be constructed and maintained in front yard and exterior side yard areas adjacent to parking areas. This screening shall consist of a planted earth berm, densely planted evergreen shrubs or trees or a combination of both. **To comply with Section 9-10-5(G)1, add a row of Shamrock Inkberry shrubs on the east side of the 75th Street driveway within the exterior side yard.**
 2. Interior Parking Lot Landscaping: Section 9-10-5(G)3 requires that each island shall be effectively landscaped with appropriately sized plantings at a minimum ratio of one shade tree and nine (9) dwarf variety shrubs per island and/or fifty (50) linear feet of parking area. Shade tree distribution shall provide a minimum of one tree in the area occupied by each twenty (20) parking spaces. **Add one shade tree to each of the two (2) interior parking lot islands.**

Engineering

Our previous comments have been addressed, and we have no objection to the project moving forward subject to the following items:

- a. A permit is required from IDOT prior to any work in the Route 83 right of way.
- b. A permit is required from the Du Page County Public Works Department for the sanitary sewer connection.
- c. The applicant must file a Notice of Intent with the IEPA prior to the start of construction.
- d. It is our understanding that the property owner will provide the Village with a letter of No Further Remediation (NFR) from the IEPA prior to a building permit being issued.
- e. Prior to building permit, the Village will require a Plat of Easement to grant access to the B-Box at the building. This can be a 10 foot wide non-exclusive easement over the water service from 75th Street to the building.

Zoning Requests

Approval of a Special Use Permit for the construction of a one-story restaurant in the B-2 zoning district including a drive-through lane and associated site improvements. The approval of the special use permit will include, but is not limited to, the following variations:

1. A variation from Section 9-3-3(C) to decrease the minimum lot area for two (2) or more special uses from two acres (2 AC) to one acre (1 AC).



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2. A variation from Section 9-3-7(A)1 to decrease the minimum special setback on Illinois Route 83 from one hundred feet (100') to fifty feet (50') for the proposed building, to fifty-six and six tenths feet (56.6') for the proposed menu board, and to sixty-two and four tenths feet (62.4') to the order canopy.
3. A variation from Section 9-6-1(A) to decrease the minimum lot area for two (2) or more special uses (fast food establishment and drive-through) from two acres (2 AC) to one acre (1 AC).
4. A variation from Section 9-6B-3(A) to decrease the minimum lot area for all uses from two acres (2 AC) to one acre (1 AC).
5. A variation from Section 9-6B-3(C)1 to decrease the minimum lot width of 200 feet for restaurants from two hundred feet (200') to one hundred seventy feet (170').
6. A variation from Section 9-6B-3(E)1 to decrease the minimum front yard setback on Illinois Route 83 from sixty feet (60') to fifty feet (50') for the proposed building and to fifty-six and six tenths feet (56.6') for the proposed menu board.
7. A variation from Section 9-6B-3(E)3 to decrease the minimum exterior side yard setback on 75th Street from sixty feet (60') to forty-three and four tenths feet (43.4') for the proposed building, to thirty-two and seven tenths feet (32.7') for the proposed menu board, and to twenty-five feet (25') to the order canopy.
8. A variation from Section 9-10-5(L)2(b)2a to decrease the radius connecting street pavement edge and driveway edge on 75th street from thirty-five (35') to twenty-five (25').
9. A variation from Section 9-10-5(L)2(d) to decrease the driveway location on Route 83 from seventy feet (70') to twenty-five and seven tenths feet (25.7').
10. A variation from Section 9-10-5(L)2(e) to decrease the minimum spacing between separate driveway entrances from four hundred feet (400') to approximately three hundred thirteen feet (+/- 313') on Illinois Route 83 and to approximately one hundred sixty-nine feet (+/- 169') on 75th Street.
11. A variation from Section 9-10-5(L)2(f) to decrease the minimum spacing between driveway entrance and right-of-way of an adjacent intersecting street from five hundred feet (500') to ninety-four and three tenths feet (94.3') on Illinois Route 83 and to one hundred forty-six and two tenths feet (146.2') on 75th Street.
12. A variation from Section 9-10-5(L)2(g) to eliminate the requirement of a concrete median separation and to reduce the driveway storage from one hundred feet (100') to zero feet on Illinois Route 83 and 75th Street.
13. A variation from Section 9-14-2(D)2(c)4 to decrease the minimum foundation landscape area from seven feet (7') to four and five tenths feet (4.5') along the southern façade, to five and two tenths feet (5.2') along the northern portion of the eastern building façade and from seven feet (7') to zero feet along the western building façade and remaining portion of the eastern building façade.



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If you have any questions, you may contact me at annchoi@willowbrookil.org or 630.920.2233.

Sincerely,

Ann Choi
Village Planning Consultant
WBK Engineering, LLC

Cc: Brian Pabst
Michael S. Mertens
Dan Lynch
Roy Giuntoli



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TRI-STATE
FIRE PROTECTION DISTRICT

419 PLAINFIELD ROAD • DARIEN, ILLINOIS 60561 • (630) 323-6445

December 8, 2020

Village of Willowbrook
Attn: Ann Choi
835 Midway Drive
Willowbrook, IL 60527
630-323-8215

planner@willowbrook.il-us

RE: Site Plan Review #2
7505 Kingery Highway
Willowbrook, IL 60527

To whom it may concern,

We have received a resubmitted copy of the proposed site, traffic, and auto turn plans for the above listed project. After review, we find the plans to be in apparent compliance with applicable standards relative to fire prevention and life safety.

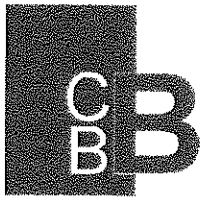
Necessary inspections are to be performed along with any outstanding fees paid before occupancy is granted.

The Bureau of Fire Prevention has been asked to review the plans, specifications or other documents submitted to see if compliance has been made with the Fire Prevention Codes and Ordinances of the Tri-State Fire Protection District. Errors or omissions by representatives of the Bureau of Fire Prevention do not constitute permission to cancel, set aside or waive any provision of any applicable Code or Ordinance of the Tri-State Fire Prevention District. Approvals by the Bureau of Fire Prevention will be in writing only.

Sincerely,

A handwritten signature in black ink, appearing to read "Chris Drews".

Chris Drews
Plan Review Specialist
Bureau of Fire Prevention
cdrews@tristatefd.com
630-654-6284



CHRISTOPHER B. BURKE ENGINEERING, LTD.

9575 W Higgins Road, Suite 600 Rosemont, Illinois 60018-4920 Tel (847) 823-0500 Fax (847) 823-0520

December 21, 2020

Village of Willowbrook
835 Midway Drive
Willowbrook, IL 60527

Attention: Ann Choi

Subject: 7505 Kingery – Final Engineering Plans
First Review
(CBBEL Project No. 900144.H212)

Dear Ann:

As requested on December 4, 2020, we have reviewed the Final Engineering Plans and supporting documents for the above property prepared by Atwell, and dated November 20, 2020. The following comments must be addressed before we can recommend approval:

Engineering Plans

Sheet C01.0

1. No Comments

Sheet C01.0

1. In Storm Sewer Note 4, add that the grates must have the "Dump No Waste – Drains to Creek" emblem.
2. Watermain Note 11 should be revised to state that the contractor shall verify with the Public Services Department for specific make and model of fire hydrants, however there are no proposed hydrants on the plans.
3. Add to the Watermain Notes "The contractor shall coordinate with the Public Services Department at least two working days prior to making the service tap to coordinate schedule, verify materials, and schedule required inspections."

Sheet C01.2

1. No Comments

Sheet C02.0

1. No Comments

Sheet C02.1

1. Add to the note regarding the sanitary service note "as directed by Du Page County Public Works".

Sheet C03.0, L03.1

1. No Comments

Sheet C04.0, 4.1

1. The path along 75th Street is an asphalt path with concrete curb ramps at the driveways. Modify how the new pavement is shown at the proposed 75th Street driveway to only show the curb ramps to be concrete, with the remainder to be asphalt (up to first joint shown).
2. We presume that the signs shown at the two handicapped accessible stalls are the required signs although not matching the legend.

Sheet C05.0, 5.1

1. No comments

Sheet C06.0, 7.0, 7.1

1. The proposed water service should be as close as possible to a direct route from the main to the point of entry into the building.
2. The Building Department will review the details of the water service, as it is considered plumbing under the Illinois Plumbing Code. Hydrocarbon resistant gaskets may be required since the service will cross a petroleum pipeline.
3. Modify the note for the connection of the sanitary service to the existing manhole to refer to Du Page County requirements, not Village requirements.
4. Show where the retaining wall underdrain will discharge.

Sheet C08.0 – 8.4

1. For the handicapped sign detail, specify that the fine is \$350. The signage must be in compliance with the Illinois Accessibility Code standards.

Sheet L01.0, 1.1

1. No Comments

Photometric Plans

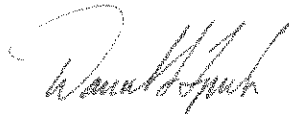
The photometric plans, consisting of two sheets and prepared by Villa Lighting and dated October 29, 2020 are in general compliance with Village Code and standard engineering methods. Please note that the Village reserves the right to require glare shields to be installed, should it deem appropriate.

General Comments

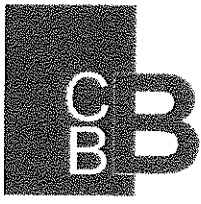
1. An abbreviated Stormwater Submittal is required. We foresee this document being one to two pages with a project narrative and statements that the site does not contain floodplain or wetlands, and the development does not reach the net new impervious thresholds to trigger Best Management Practices or Detention.
2. A permit is required from IDOT prior to any work in the Route 83 right of way.
3. A permit is required from the Du Page County Public Works Department for the sanitary sewer connection.
4. The applicant must file a Notice of Intent with the IEPA prior to the start of construction.

If there are any questions, please do not hesitate to contact me.

Sincerely,

A handwritten signature in black ink, appearing to read "Daniel Lynch", written in a cursive style.

Daniel L. Lynch, PE, CFM
Head, Municipal Engineering Department



CHRISTOPHER B. BURKE ENGINEERING, LTD.

9575 W Higgins Road, Suite 600 Rosemont, Illinois 60018-4920 Tel (847) 823-0500 Fax (847) 823-0520

January 20, 2021

Village of Willowbrook
835 Midway Drive
Willowbrook, IL 60527

Attention: Ann Choi

Subject: 7505 Kingery – Final Engineering Plans
Second Review
(CBBEL Project No. 900144.H212)

Dear Ann:

As requested on January 19, 2021, we have reviewed the Final Engineering Plans and supporting documents for the above property prepared by Atwell, and dated January 19, 2021. Our previous comments have been addressed, and we have no objection to the project moving forward subject to the following items:

1. A permit is required from IDOT prior to any work in the Route 83 right of way.
2. A permit is required from the Du Page County Public Works Department for the sanitary sewer connection.
3. The applicant must file a Notice of Intent with the IEPA prior to the start of construction.
4. It is our understanding that the property owner will provide the Village with a letter of No Further Remediation (NFR) from the IEPA prior to a building permit being issued.
5. Prior to building permit, the Village will require a Plat of Easement to grant access to the B-Box at the building. This can be a 10 foot wide non-exclusive easement over the water service from 75th Street to the building.

If there are any questions, please do not hesitate to contact me.

Sincerely,

Daniel L. Lynch, PE, CFM
Head, Municipal Engineering Department

Project Traffic Review #2



To: Ms. Ann Choi
Planning Consultant, Village of Willowbrook

625 Forest Edge Drive, Vernon Hills, IL 60061

TEL 847.478.9700 ■ FAX 847.478.9701

From: Lynn M. Means, P.E., PTOE
Senior Transportation Engineer

www.gha-engineers.com

Date: January 13, 2021

Subject: Panda Express Restaurant with Drive-Thru
7505 Kingery Highway (IL Route 83)
Willowbrook, Illinois

Per your request, Gewalt Hamilton Associates, Inc. (GHA) has reviewed the following material provided for the above captioned project pertaining to traffic, parking and on-site circulation:

1. Site Traffic Study, prepared by Atwell dated January 5, 2021, which includes Traffic Plan, Sheet EX-02.
2. Response to Comments Letter, prepared by Atwell dated January 5, 2021.
3. Truck Turn Analysis, Sheet EX-01, prepared by Atwell dated November 20, 2020.

Conclusions of this effort and **recommendations** for consideration are presented below:

1. The TIS prepared by Atwell is generally in conformance with the guidelines published by the Institute of Transportation Engineers (ITE).
2. GHA has found the documentation and/or responses provided, in general, to adequately address the comments made and concerns raised in our previous Project Traffic Review dated September 30, 2020. An updated Site Traffic Study is not required.
3. Employees should be encouraged to park in remote spaces (furthest from the site entrance) to provide convenient parking for customers.
4. The Site Traffic Study references ITE Land Use Code (LUC) 945, Gasoline/Service Station with Convenience Market using the building size to estimate traffic generated by the existing site use. Based on the existing site parameters (8 vehicle fueling positions, a convenience store less than 2,000 SF and ancillary vehicle service center), LUC 944, Gasoline/Service Station would be a more appropriate use. However, the trip generation estimates for the existing site use based on multiple independent variables (building size and number of vehicle fueling positions) would still result in a traffic estimate that is greater than the proposed Panda Express, fast-food restaurant with drive thru. Accordingly, no modifications to the study are required.
5. 75th Street to the west of Kingery Highway is under the jurisdiction of the DuPage County Division of Transportation.
6. Kingery Highway (IL Route 83) is also designated as a Strategic Regional Arterial (SRA) route. SRA routes are designed to carry higher traffic volumes at higher travel speeds through access control and increased access and traffic signal installation/spacing requirements.

* * * * *

We hope you find these comments helpful in your assessment of the proposed development. Should you have any questions please do not hesitate to contact GHA at 847-478-9700.

5503.900_Panda Express_7505 Kingery_Traffic Review 2_01.13.21

Project Traffic Review #3



CONSULTING ENGINEERS

625 Forest Edge Drive, Vernon Hills, IL 60061

TEL 847.478.9700 ■ FAX 847.478.9701

www.gha-engineers.com

To: Ms. Ann Choi
Planning Consultant, Village of Willowbrook

From: Lynn M. Means, P.E., PTOE
Senior Transportation Engineer

Date: January 22, 2021

Subject: Panda Express Restaurant with Drive-Thru
7505 Kingery Highway (IL Route 83)
Willowbrook, Illinois

GHA has reviewed the response letter and supporting documentation prepared by Atwell dated January 19, 2021 and found it to adequately address our comments, questions and concerns. No further response is needed.

Should you have any questions please do not hesitate to contact GHA at 847-478-9700.

5503.900_Panda Express_7505 Kingery_Traffic Review 3_1.22.21



Village of Willowbrook
Staff Report to the Plan Commission

Plan Commission Date:	February 3, 2021		
Prepared By:	Ann Choi, Village Planning Consultant		
Case Title:	Zoning Hearing Case No. 21-03: "735 Plainfield Road PUD" – Planned Unit Development for a car wash, bank with drive-through, and fast food restaurant with drive-through.		
Applicant:	GW Property Group, LLC		
Property Owner:	Viren-Gill Ltd. (Louis Viren)		
Action Requested:	Consideration of a petition requesting approval of a special use permit for a planned unit development, including a financial institution with drive through, fast-food establishment with drive through, and an automobile washing and cleaning facility, including certain relief, exceptions and variations from Title 9 and Title 10 of the Village Code; approval of a Preliminary Plat of Subdivision; and approval of a Preliminary Plat of PUD.		
Applicable Regulations:	Comprehensive Plan, Zoning Ordinance, Subdivision Regulations		
Location:	735 Plainfield Road, Willowbrook IL 60527		
PINs:	09-23-406-003		
Existing Zoning:	B-2 Community Shopping		
Proposed Zoning:	B-2 Community Shopping with a Special Use for a PUD		
Existing Land Use:	Willowbrook Bowl (not currently operational)		
Property Size:	2.79 Acres		
Surrounding Land Use:	Use	Zoning	
	North	Chase Bank/Office Buildings	LOP
	South	WB Town Center/Lock-Up Storage	B-2
	East	TCF Bank & WB Orthodontics	OR
	West	WB Town Center	B-2

Necessary Action by Plan Commission:	Conduct public hearing. Continue hearing, if necessary, to continue discussion and vote to a future meeting to be determined as plans are completed, or complete and close hearing, and forward a recommendation , if ready. Sample motions are provided on pages 25-27.
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Documents Attached:

Attachment 1: Legal Description

Attachment 2: Submitted Plans

- **"ALTA-NSPS Land Title Survey"** as prepared by Compass Surveying Group Ltd., Project No. 20-0006-01, Sheet 1 of 1, and dated 2/28/20.
- **"Preliminary Plat of PUD"** as prepared by Kimley-Horn and Associates, Inc., KHA Project No. 168726009, Sheet No. C1.0, bearing an original issue date of 12/15/2020, and bearing the latest revision date of 01/21/21.
- **"Car Wash First Floor Plan"** as prepared by Mark Shively Architecture, Sheet No. A2.1, and bearing a revision date of 01.14.2021.
- **"Car Wash Exterior Elevations"** as prepared by Mark Shively Architecture, Sheet No. A4.0, and bearing a revision date of 12.01.2020.
- **"Vacuum Canopy Plans, Elevations, Sections"** as prepared by Mark Shively Architecture, Sheet No. A1.1, and bearing a plot date of 01.25.2021.
- **"Vacuum Canopy Plan Diagram"** as prepared by Mark Shively Architecture, Sheet No. A1.2, and bearing a plot date of 01.25.2021.
- **"Pay Station & Trash Enclosure Elevations for Car Wash"** as prepared by Mark Shively Architecture, Sheet No. A1.2, and bearing a revision date of 01.14.2021.
- **"Preliminary Floor Plan" for Fast Food Establishment** as prepared by Interplan LLC, Project No. 2020.0919, 1 page, Sheet PFP, and dated 01.20.21.
- **"Architectural Elevations with Signs" for Fast Food Establishment** as prepared by Interplan LLC, 2 pages, and dated 01.20.21.
- **"Master Sign Key Plan" for Fast Food Establishment** – 1 page, undated and provided by GW Properties on January 24, 2021.
- **"Freestanding Sign" for Fast Food Establishment** – 1 page, Sheet ST1.1, bearing the latest revision date of 09.28.20, and provided by GW Properties on January 21, 2021.
- **"Exterior Perspective" for Fast Food Establishment** as prepared by Interplan LLC, 1 page, and dated 01.20.21.
- **"Floor Plan" for Chase Bank** as prepared by The Architects Partnership, TAP Project No. 17073, 1 page, and dated 01.22.2021.
- **"Architectural Elevations" for Chase Bank** as prepared by The Architects Partnership, TAP Project No. 19108, 2 pages, and dated 1.18.2021.
- **"Sign Package" for Chase Bank** as prepared by Signtech, Drawing No. 18-01613, Project No. CHASE_730_1, 41 pages, bearing an original issue date of 09/14/18, and bearing the latest revision date of 01.19.2021.
- **"Car Wash Rendering"** – 1 page, untitled, undated, and provided by GW Properties.



- **"PUD Plans"** as prepared by Kimley-Horn and Associates, Inc., KHA Project No. 168726009, 9 Sheets, bearing an original issue date of 12/15/2020, and bearing the latest revision date of 01/21/21. **(Includes Landscape Plan)**
- **"Photometric Plan"** as prepared by Kimley-Horn and Associates, Inc., KHA Project No. 168726009, Sheet No. E1.0, and bearing an original issue date of 12/15/2020.
- **"Traffic Control Plan"** as prepared by Kimley-Horn and Associates, Inc., KHA Project No. 168726009, 9 Sheets, Sheet No. 1 of 1, and bearing an original issue date of 12/15/2020.
- **"Traffic Impact Study"** as prepared by KLOA, 130 pages, and dated January 20, 2021.
- **"Traffic Regulation Agreement"** – 9 pages.
- **"Preliminary Plat of Subdivision"** as prepared by Compass Surveying Group Ltd., Project No. 20.0006, Sheet 1 of 1, and bearing the latest revision date of 01/19/21.
- **"Draft Declaration of Covenants"** – 20 pages (still undergoing review by Village Attorney).

Attachment 3: Standards for Special Use Permits

Attachment 4: Standards for Variations

Attachment 5: Standards for Planned Unit Developments and Findings of Fact for Planned Unit Developments

Attachment 6: Public Hearing Notice

Attachment 7: Village of Willowbrook Review Letter(s)



Development Proposal

Location & Zoning Request

The subject property is located on the south side of Plainfield Road, between Illinois Route 83 to the west and Adams Street to the east. The site contains about 2.79 acres and is currently improved with one vacant building, formerly occupied by the Willowbrook Bowling Alley. Adjacent developments include Pete's Fresh Market/Willows Shopping Center to the northwest, the existing Chase Bank to the north, Willowbrook Kindercare to the northeast, TCF Bank to the east, and the Willowbrook Town Center ("Town Center") to the west and south. The existing bowling alley building on the subject property has non-conforming setbacks as it is situated at the far southeast end of the lot, proximate to the south and east property lines.

Existing access to the subject property includes one driveway on Plainfield Road and one cross-access driveway along the western property line that connects to the Town Center. Nearly all of the lot is paved with a parking lot. Immediately east of subject property line is TCF Bank and currently, there is no direct access from the subject property to TCF Bank or to Adams Street, which has a higher elevation than the subject property.

GW Properties, LLC, (the "Applicant") proposes to demolish the existing building and is requesting approval of a special use for the proposed Planned Unit Development (PUD) that includes a car wash, restaurant with drive-through, and a bank. The PUD proposal also requests approval of certain relief, exceptions and variations from Title 9 and Title 10 of the Village Code; a Preliminary and Final Plat of Subdivision; and a Preliminary Plat of PUD. The Applicant intends to redevelop the subject property into a three-tenant mixed use development that will include a 5,850 square foot automobile washing and cleaning facility (Encore Car Wash) on the southern portion of the property, a 6,660 square foot financial institution (Chase Bank) with drive-through on the northwest portion of the property, and a 2,830 square foot fast food establishment with drive-through (Guzman Y Gomez) on the northeast portion of the property.

Exhibit 2: Location Map





Exhibit 3: Street View of Subject Property



Exhibit 4: Zoning Map (B-2 Community Shopping)



Summary of Requests:

1. Special Use Permit for a Planned Unit Development with an automobile washing and cleaning facility, fast food establishment with drive-through, and a financial institution with drive-through including certain relief, exceptions and waivers from Title 9 and Title 10 of the Village Code.
2. Subdivision of the subject property into three lots.

History/Background

The subject property was developed in 1963 by Louis Viren's Father, Lou Senior, as a bowling alley and restaurant. In 1973, ten years later, eight (8) additional bowling lanes were added towards the west. The property owner, Lou Viren, took over the ownership and operational management in 2003 and has run the business since. The Applicant, GW Properties, has been working with the property owner and the Village on potential redevelopment concepts for the property. The Plan Commission first saw concepts for the subject property in September 2018 for the reuse and repurposing of the existing bowling alley building (now vacant) for an entertainment venue consisting of boutique bowling, laser tag, video arcade and a restaurant bar. The previous concept also included a Chase Bank as an outlet.



Business Narrative

Car Wash

GW Property Group, LLC will be selling Lot 1, the 1.20-acre lot located on the southern end of the subject property, to Encore Car Wash who will be developing the property and operating the car wash. Encore Car Wash is marketed as a high-end, self-service automatic car wash. The owners of Encore Car Wash have experience primarily in the landscape architecture industry, predominantly for single-family homes throughout the country, but are currently developing two other car wash facilities in the Midwest (Plainfield, Illinois and Hammond, Indiana) since they first ventured into the car wash industry approximately two years ago.



Rendering of Encore Car Wash (left) and Photo of Guzman Y Gomez in Naperville, IL (right)

Restaurant with Drive-through

On the northeast corner of the subject property, GW Property Group will be developing a one-story, approximately 2,830 square-foot, free-standing restaurant with one drive-through lane on the 0.795-acre northeast lot (Lot 2). The fast casual restaurant operator is Guzman Y Gomez, an Australian-licensed, casual-dining restaurant chain that also operates internationally. The franchise specializes in Mexican cuisine dishes such as burritos, fires, nachos, tacos, and other specialty items. The franchise operates internationally in Singapore, Japan, and in January of 2020, opened their first location in the United States in Naperville, Illinois (1519 North Naper Boulevard). An aggressive expansion is currently taking place in the United States, especially in Illinois. Currently there are 135 locations globally.

The typical hours of operation at the Naperville location are Mondays – Thursdays from 6am to 10pm, and Fridays – Sundays from 6am to 11pm.

Financial Institution with Drive-Through

Chase Bank wishes to purchase Lot 3, the 0.792-acre parcel located at the northwest portion of the subject property. Chase Bank will be relocating from across the street into a brand new 6,660 square-foot property with three drive-through lanes. Chase Bank will be purchasing their portion of the property from GW Property Group, LLC and self-developing their new branch. The Chase Bank located across the street is currently open on Mondays – Fridays from 9am to 5pm, on Saturdays from 9am to 2pm, and are closed on Sundays.

Staff Analysis

Comprehensive Plan/Appropriateness of Use

Adjacent to one of the major commercial intersections in the Village, the Comprehensive Plan designates this property as "Community Commercial". The Comprehensive Plan includes several pages of development objectives and design guidelines that have been incorporated into the proposed project including the attention to landscaping, signage, pedestrian connection, and façade materials. Conformance with relevant



development and design policies and parameters are listed below. Community Commercial areas are intended to provide a full range of retail and service uses in the community with emphasis on community convenience related goods and services. The proposed development presents some challenges as the site will need to accommodate three high intensity uses on less than three acres. A traffic study has been provided and is discussed in more detail in the Traffic and Circulation section of this staff report. The subject property is zoned B-2 Community Shopping and is within the Route 83/Plainfield Road Business District. The proposed B-2 zoning complies with the Comprehensive Plan. The financial institution is a permitted use, the fast food establishment and automobile washing and cleaning facility are permitted as special uses in the B-2 Community Shopping District. There are also two drive-throughs proposed which are also allowed as special uses for permitted uses, in the B-2 Community Shopping District. Although the B-2 Community Shopping District is typically geared more towards retail uses and two of the three proposed uses will not generate retail sales tax revenue for the Village, the Village will benefit from a places of eating tax from the proposed fast food establishment with drive-through. The proposed uses will also occupy a key vacancy along Plainfield Road. The bank and the car wash may bring additional foot traffic and consumers to the adjacent shopping center and surrounding area.

Commercial Development Policies

The proposed redevelopment of the Willowbrook Bowling Alley into a planned unit development is consistent with the development policies set forth in the Comprehensive Plan. The existing site will be improved and upgraded with a bank, a fast food establishment and a car wash. Improvements to overall access, parking and landscaping are proposed through the relocation of the existing driveway long Plainfield Road, the proposed relocation of the shared access driveway between the subject property and the Town Center, and the incorporation of a dedicated left-turn lane into the subject property within the existing the Town Center drive. A future shared access drive is also proposed between the subject property and the lot to the east. These measures have been taken to reduce the negative impact on adjacent land uses. The proposed uses have also been sized and located to serve specific needs within the community and are clustered in small groupings with some shared parking areas, and an internal access drive serving all three proposed lots. The overall image and appearance of the now vacant lot will be upgraded and improved through new commercial area lighting, landscaping, signage, and new pedestrian connections.

Route 83/Plainfield Road Business District

The Route 83/Plainfield Road Business District Plan is generally bounded by 69th Street to the north, 72nd Court to the south, Illinois Route 83 to the west, and Adams Street and Willow Way Lane to the east. The Route 83/Plainfield Road Business District Plan was created in 2016 to serve as a funding mechanism for required off-site improvements, including necessary traffic improvements, for commercial properties located near Illinois Route 83 and Plainfield Road. These commercial properties were faced with challenges in finalizing their re-development project plans given the high cost of these off-site improvements. It was agreed that a public subsidy was warranted and resulted in the creation of a new Business District Sales Tax to fund these subsidies. The sales tax rate within the boundaries of the Route 83/Plainfield Road Business District was established at a 1% increase over those properties located beyond the boundaries of the Business District Plan. This 1% sales tax increment is collected from the state, forwarded to the Village, and deposited into a separate Village fund. Eligible project expenses are reimbursed to the developers from this fund to offset extraordinary development costs. The development may be subject to





the 1% Business District Tax of 1%. Village staff will defer to a Tax Impact Analysis, which was not included in the planning application, but one is required and has been included as a condition of approval. It should also be noted that the Applicant has not requested any assistance to offset their development costs.

Relief and Waivers Requested

PUDs may depart from the strict conformance with the required density, dimension, area, height, bulk and other regulations for the underlying zoning district and other provisions of the Zoning Ordinance to the extent specified in the preliminary plat and document authorizing the PUD so long as it will not be detrimental to or endanger the public health, safety, and general welfare, except that there are no exceptions for complying with seven (7) of the twelve (12) PUD Standards. The departures are identified as "waivers" and outlined in any approving ordinance. In order to maintain compliance with the Village Zoning Code, this PUD amendment and plat of subdivision require certain relief, exceptions, and waivers as outlined in the "Certain Relief Waivers Requested" in the last section of the staff report. The project as proposed includes nearly 60 waivers, which may be an indication that the site may not accommodate a combination of uses of this intensity.

Bulk Regulations

The subject property is zoned B-2; however, PUDs can allow for flexibility to the bulk requirements if authorized by the Village. A detailed discussion of important bulk exceptions and variations is provided below.

B-2 Zoning District Bulk Regulations						
Item	Code Section	Code Requirement	Proposed Lot 1 Car Wash	Proposed Lot 2 Restaurant	Proposed Lot 3 Bank	Departure
Min. Lot Area	9-6B-3(A)	Lot 1 – 1 acre Lot 2 – 2 acres Lot 3 – 2 acres	1.2 acres	0.795 acres	0.792 acres	Yes – Lots 2 and 3
Min. Lot Width	9-6B-3(C)2	Restaurants: 200' (Lot 2) Other uses: 150' (Lots 1 & 3)	289.86'	122.7'	161.3'	Yes – Lot 2
Min. Lot Depth	9-6B-3(D)	200'	180.3'	279.8'	212.7'	Yes – Lot 1
Min. Front Yard Setback (Plainfield Road)	9-6B-3(E)1	60'	N/A	116.6'	71.8'	Yes
Min. Interior Side Yard Setback	9-6B-3(E)2	30'	54.4' (west) 70.5' (east)	55.9' (west) 15.5' (east)	74.5' (west) 7' (east)	Yes – Lot 2 and 3
Min. Rear Yard Setback (south lot line)	9-6B-3(E)4	40'	58.9'	82.4'	62.7'	None
Max. Lot Coverage	9-6B-3(F)	50%	14.3%	9.2%	20.9%	None
Max. Height	9-6B-3(G)	30'	40'	28'	21'-6"	Yes – Lot 1
Max. Floor Area Ratio	9-6B-3(H)	0.30	0.112	0.082	0.193	None



- Height.** Maximum height in a B-2 district is thirty feet (30'). The proposed car wash building on Lot 1 has a maximum height of forty feet (40'), which is to the peak of the proposed water tower architectural feature. The proposed fast food establishment on Lot 2 has a maximum height of twenty-eight feet (28') and the proposed bank on Lot 3 has a maximum height of twenty-one feet six inches (21'-6").
- Yards.** Per Section 9-13-6(F) Standards for a PUD, Yards, building setbacks are required to be equal to the required setback of the adjoining zoning district, and not less than the height of the building (shown in the 4th column in the table below). The required and proposed minimum setbacks are shown in the table below. The number of building proposed on the overall site limits the placements of buildings on the subject property, so setback relief from the interior side yard requirements are being requested for Lots 2 and 3.

Yard	Description	Zoning Ordinance	Based on Height	Minimum Provided*			Departure
				Lot 1	Lot 2	Lot 3	
Front	Varies	60' front yard setback (and 100' specific setback)	Lot 1 is 40' Lot 2 is 28' Lot 3 is 21'-6"	85.4'	116.6'	73.9	None
Interior Side (east)	OR district to the east	20'		70.5'	15.5'	7'	Yes – Lots 2 & 3
Interior Side (west)	B-2 district to the west	30'		54.4'	59.9'	74.5'	None
Rear	B-2 district to the west	30'		58.9'	82.4'	62.7'	None

* Numbers represent minimums based on proposed design, not "build to" setbacks.

- Parking Area/Pavement Setbacks.** Parking area/pavement setbacks are regulated in the Off-Street Parking Section of the Zoning Ordinance under Section 9-10-5(G). The required and proposed minimum setbacks are shown in the table below.

Yard	Description	Zoning Ordinance	Minimum Provided*			Departure
			Lot 1	Lot 2	Lot 3	
Front	From Plainfield Road	25'	NA	24'	24'	Yes – Lots 2 & 3
Front	Between Lot 1 and Lots 2 & 3	15'	Zero feet	NA	NA	Yes – Lot 1
Interior Side	Varies	10'	4.4' (west) 5' (east)	Zero feet (west) 2.4' (east)	8.5' (west) Zero feet (east)	Yes
Rear	Varies	10'	5'	Zero feet	Zero feet	Yes

- Foundation Setbacks.** Section 9-14-2(D)2(c)4 of the Zoning Ordinance requires a minimum foundation landscape width of seven feet (7'). A few areas meet this requirement around the buildings, excluding areas along the north building facade of the car wash where a detached canopy is located close to the building's north wall and would therefore hinder the growth of any foundation plantings in that area; along the south and west building facades of the proposed fast food



establishment; and along the west, north and east building facades of the proposed bank. **Waivers from the code section are being requested as part of the PUD.**

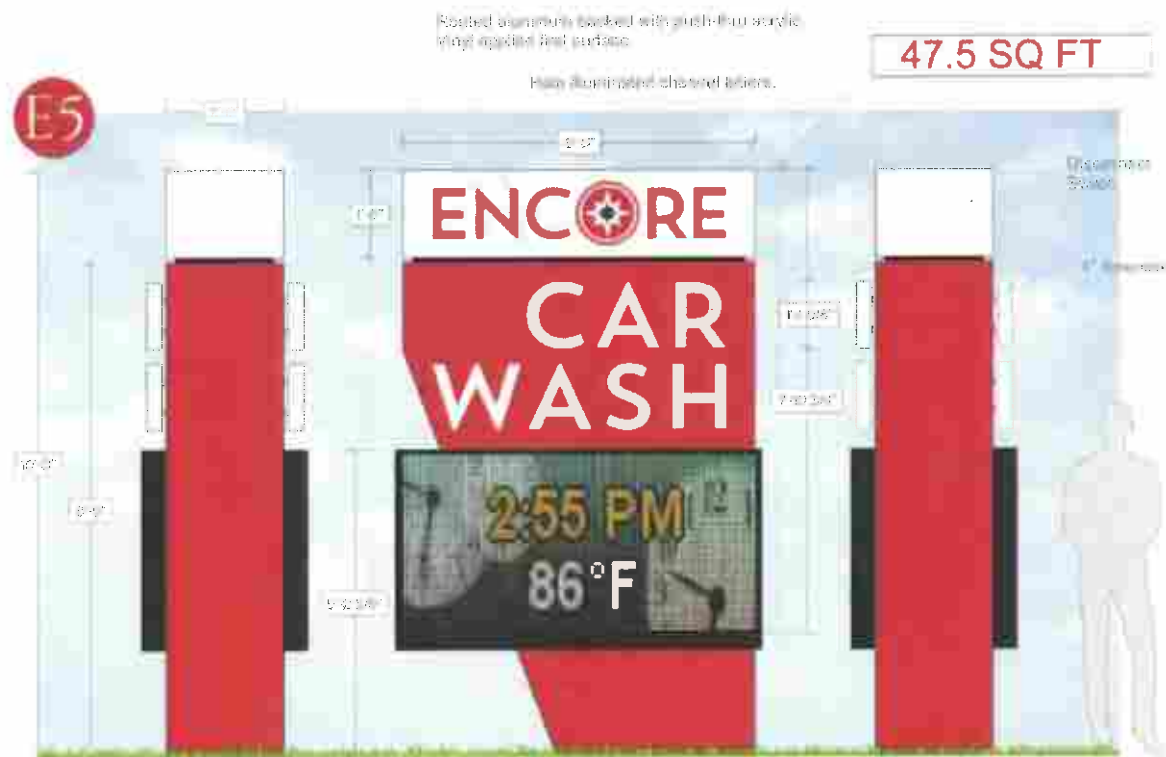
Signage

Signage variations are not allowed under the existing B-2 zoning district, and although relief is possible through a PUD, it is generally supported only to the extent needed to adequately promote the proposed business. On-going discussions about signage on this property have resulted in improvements, however, it is staff's opinion that additional modifications should be required before final consideration before the Village Board. The requested waivers to allow the prohibited roof signs, digital sign, painted signs, as well as the number of waivers from the total permitted sign surface area, location, and number of signs, should be carefully evaluated by the Plan Commission.

Freestanding Signs

Three freestanding signs are proposed for the overall development. Each lot will be required to accommodate a freestanding sign promoting only the business on that lot. The proposed freestanding signs that staff last reviewed are shown below and are included as **Attachment 2** of the staff report.

Proposed Freestanding Sign with Digital Display for Lot 1 (Encore Car Wash)



The freestanding sign on Lot 1 is located on Lot 3. Staff has recommended that this sign be relocated to Lot 1 and has included this as a condition of approval. All freestanding or ground signs are required to contain only the advertisement related to the identity of the business. The proposed freestanding sign has a maximum height of ten feet (10') and a maximum width of six feet (6'). The total sign surface area of the freestanding



sign is ninety-five square feet (95 SF) including both faces. Because the proposed ground sign is in excess of eight feet (8') in height, the total sign surface area for Lot 1 is reduced by fifteen square feet (15 SF). Therefore, the total sign surface area for Lot 1 is a maximum of three hundred thirty-five square feet (335 SF).

The proposed freestanding sign incorporates digital technology, which is currently not allowed by the Sign Ordinance. The applicant would like to incorporate this technology as part of their PUD consideration, much like the shopping center approved for a digital sign on 63rd Street west of Route 83 and for Pete's Fresh Market. Similar to these approvals, language will be incorporated into the ordinance that will restrict brightness, how messages are displayed, and the length of time required between changing messages. These include restrictions that permit only static messages (no animation of any kind), and dwell time that will be consistent with whatever provision is in place at the time a recommendation is forwarded to the Village Board. It is recommended that all digital technology be treated the same unless and until a comprehensive amendment is made to the code to regulate them village wide, as opposed to regulating them as part of PUD approvals.

Please note that the proposed freestanding sign also does not contain brick in keeping with the design of the principal structure. A number of waivers from Section 9-11-4(R), Prohibited Signs, and Section 9-11-12(B)1(d)1, Sign Surface Area, Number, Sign Types and Height Limitations, are included in the list of waivers for the proposed ground sign on Lot 1.

Proposed Freestanding Sign for Lot 2 (Guzman Y Gomez)

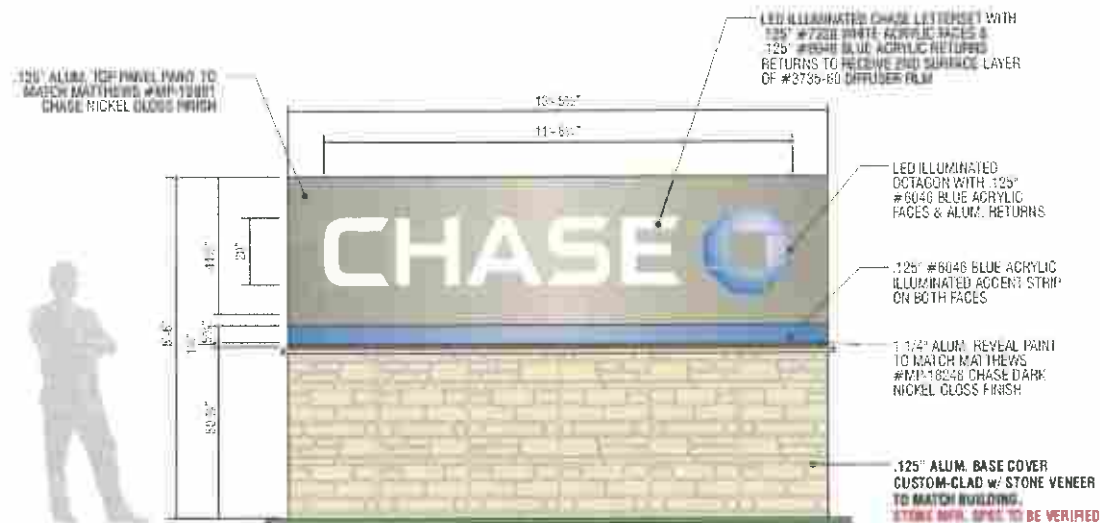


The freestanding ground sign for Lot 2 is located on Lot 3. For the same reason stated above, staff has recommended that this sign be relocated on its own lot, with a suggested location closer to the Plainfield Road access drive, and Staff has included this as a condition of approval. The proposed freestanding sign has a maximum height of ten feet (10') and a maximum width of five feet ten inches (5'-10"). The total sign surface area of the freestanding sign is sixty-eight square feet (68 SF) including both faces. Because the



proposed ground sign is in excess of eight feet (8') in height, the total sign surface area for Lot 2 is reduced by fifteen square feet (15 SF). Therefore, the total sign surface area for Lot 2 is a maximum of one hundred thirty-eight and four tenths square feet (138.4 SF).

Proposed Freestanding Sign with Digital Display for Lot 3 (Chase Bank)



The freestanding ground sign for Lot 3 is located at the northeast corner of the lot. The proposed freestanding sign has a maximum height of eight feet six inches (8'-6") and a maximum width of thirteen feet five and a half inches (13'-5 1/2"). The total sign surface area of the freestanding sign is two hundred thirty-three and three tenths square feet (233.3 SF). Because the proposed ground sign is in excess of eight feet (8') in height, the total sign surface area for Lot 2 is reduced by three and seventy-five hundredths square feet (3.75 SF). Therefore, the total sign surface area for Lot 3 is a maximum of one hundred ninety-seven and eighty-five hundredths square feet (197.85 SF).

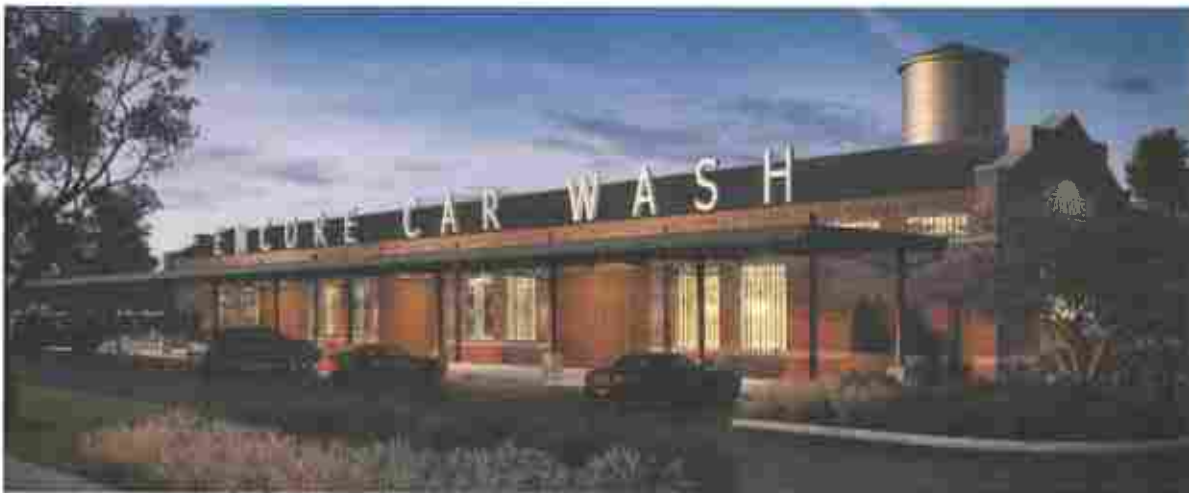
Please note that the latest sign package for Lot 2 submitted for this review, has proposed a multi-tenant sign located just north of the shared access drive with the Town Center (technically on Lot 3) and that has not been discussed with staff. Staff does not support this multi-tenant for several reasons. First, staff has not received any drawings to review the sign surface area, height, location, etc., and has not had sufficient time to evaluate what waivers would be required. Second, each lot already proposes the maximum number of ground signs permitted. Third, there is a concentration of signs located in that general vicinity: the proposed freestanding sign with digital display for the car wash and the existing freestanding sign for the Lock-Up self-storage facility are both located south of the shared access drive with the Town Center. Staff recommends eliminating the multi-tenant sign as proposed in the sign package for Lot 2 and has included this as a condition of approval.

Wall Signs

Wall signs are generally limited by the sum of all gross square foot measurements; meaning the height and width dimensions used are to the farthest edges of the sign, which includes "dead" space. The use of capital letters in signs or graphics (such as the Encore Car Wash roof signs) that extend beyond letters can limit overall signage based on the "gross square footage" calculation. As part of a PUD, however, signage can be



evaluated as to the quality of its design and whether it is effectively incorporated into the building's architecture. Staff has evaluated wall signage as can be seen in the elevation drawings and generally finds the appearance to be acceptable; however, a number of waivers are requested because the sign surface area of the proposed signs do not meet the code requirements. Staff has also found inconsistencies in the wall sign drawings and have noted these in the planning review letter that are included as **Attachment 7** of the staff report. Staff will require the provision of correct sign surface area information on revised plans, with future approvals referencing these revised numbers. A description of the requested signs and recommended modifications is provided in the table below.



Proposed Signs for Lot 1 (Encore Car Wash)				
Sign	Description	Location	Size	Comments
Lot 1 Wall Sign (1)	Sign E.1 Individual illuminated channel letters flush mounted to wall.	Along the lower pitch of roof on the north building façade.	[[[?]]]	<ul style="list-style-type: none"> Higher than allowed by code. Waiver reasonable given location – rear of the site and not highly visible from Plainfield Rd. Sign surface area needs to be confirmed.
Lot 1 Painted Signs (2)	Signs E.7 Non-illuminated painted sign on brick.	On east and west building facades.	[[[?]]]	<ul style="list-style-type: none"> Prohibited by code. Waiver is reasonable given location – rear of the site and not highly visible from Plainfield Rd. Sign surface area needs to be confirmed.
Lot 1 Roof Signs (3)	Signs E.2, E.3, E.4	On detached canopies over the vacuum stations and pay canopy.	E.2: [[[?]]] E.3: 20.6 SF E.4: 15.1 SF	<ul style="list-style-type: none"> Prohibited by code. Waiver is reasonable given location – rear of the site and not highly visible from Plainfield Rd.
Lot 1 Freestanding Sign (1)	Sign E.5	On Lot 3	95 SF	<ul style="list-style-type: none"> Needs to relocate to Lot 1 Reduces total SSA by 15 SF
Total Sign Surface Area Requested		[[[?]]]		
Total Sign Surface Area Allowed		335 SF		
Excess Wall Signage		[[[?]]]		



Waivers to permit an increase in the maximum total sign surface area for private traffic direction signs are also included for Lots 1 and 2.

Proposed Signs for Lot 2 (Guzman Y Gomez)				
Sign	Description	Location	Size	Comments
Lot 2 Wall Sign (3)	Signs A, D and F UL Listed channel letters.	On north, south, and west building facades.	A: 46.7 SF D: 46.7 SF F: 75 SF	<ul style="list-style-type: none"> Higher than allowed by code. Waiver reasonable given they are located on the taller "tower" elements. Min. separation distance lower than permitted by code.
Lot 2 Wall Signs (3)	Signs B, C and E Illuminated logo plex face/LED internal illumination	On north, east, and west building facades.	B: 49 SF C: 49 SF E: 49 SF	<ul style="list-style-type: none"> Higher than allowed by code. Waiver reasonable given they are located on the taller "tower" elements. Min. separation distance lower than permitted by code.
Lot 2 Freestanding Sign (1)	Sign G	Proposed on southwest corner of Lot 3	G: 68 SF	<ul style="list-style-type: none"> Needs to relocate to Lot 2 as it conflicts with Sign E.1 on Lot 3 Reduces total SSA by 15 SF
Total Sign Surface Area Requested		383.4 SF		
Total Sign Surface Area Allowed		335 SF		
Excess Wall Signage		48.4 SF		
Number of Wall Signs Requested		6		
Number of Wall Signs Allowed		3		

Proposed Signs for Lot 3 (Chase Bank)				
Sign	Description	Location	Size	Comments
Lot 3 Wall Sign (3)	Signs E.2, E.3 and E.4 24" White Channel Letters and Logo	On north, east, and west building facades.	E.2: 36.9 SF E.3: 36.9 SF E.4: 36.9 SF	<ul style="list-style-type: none"> Higher than allowed by code. Waiver reasonable given they are located on the taller "tower" elements. Min. separation distance lower than permitted by code.
Lot 3 Window Signs (2)	Signs I.30 and I.45 Universal Thin-Profile ATM Surround and Illuminated Interior Blue Octagon Ceiling- Hung	In vestibule of west building façade and in window of east building façade.	I.30: 8.34 SF I.45: 4 SF	<ul style="list-style-type: none"> Waiver for Sign I.30 reasonable given they are located within the building and serves as an ATM sign.
Lot 3 Freestanding Sign (1)	Sign E.1	Proposed on northwest corner of Lot 3	E.1 233.3 SF	<ul style="list-style-type: none"> Sign surface area needs to be confirmed. Reduces total SSA by 3.75 SF
Total Sign Surface Area Requested		344 SF		
Total Sign Surface Area Allowed		197.9 SF		
Excess Wall Signage		146.1 SF		
Number of Wall Signs Requested		3		
Number of Wall Signs Allowed		4		



Parking, Site Access & Circulation

The subject property is currently served by an existing eastbound right-in, right-out only access along Plainfield Road. The proposed development proposes constructing a new access driveway in the approximate location of the existing access driveway and to maintain this right-in/right-out access. This access has been shifted approximately twenty feet to the east to better align with the flow of traffic from the fast food establishment's drive-through.

A two-way internal driveway runs between each of the three proposed lots and provides the main circulation through the subject property. There is also a shared driveway between the subject property and the Town Center along the western portion of the subject property. The proposed site plan requires the relocation of this existing driveway between the two properties to be shifted slightly to the south more than twenty feet (20'). The proposed site plan also incorporates a dedicated left turn lane from the existing Town Center drive aisle for vehicles turning into the subject property from Plainfield Road. The proposed site plan as included in **Attachment 2** indicates sufficient parking with a total of one hundred five (105) parking spaces, including five (5) parallel employee parking spaces and six (6) accessible spaces. The parking requirement is fifty-four (54) parking spaces.

Bank

The bank is located on Lot 3 towards the northwest portion of the site. Three drive-through lanes are proposed along the south wall of the bank building and includes an additional bypass lane. The bank provides a total of thirty (30) parking spaces (including two handicap accessible spaces) and is accessible via the right-in/right-out driveway on Plainfield Road, where vehicles would travel in a clockwise rotation around the bank building. The bank is also accessible via the existing shared driveway between the Town Center and the subject property. The Traffic Impact Study (TIS) has indicated that approximately three to four vehicles will be accommodated within each of the drive-through lanes without blocking the access drives or internal circulation, and that this should be sufficient to accommodate the peak demand of the proposed bank. The TIS also included a recommendation that exiting movements from the drive-throughs should be under stop sign control, which has been included as a condition of approval.

Fast Food Establishment

The proposed restaurant and drive-through is located on Lot 2 and the drive-through facility for the proposed quick-serve restaurant will extend along the west side of the building. Vehicles will enter through the Plainfield Road driveway and make an immediate right turn and then an immediate left turn to either park the car in the restaurant's parking lot or drive through the parking lot to enter the single-lane drive-through. A separate by-pass lane is not proposed. A secondary path of travel is provided by the existing cross access driveway between the Town Center and the subject property, and via the internal drive aisle between all three uses. Vehicles will approach the menu/order board to place their order and then pick up items before exiting the drive-through. After exiting the drive-through, the vehicles will resume straight out to the Plainfield Road driveway. The restaurant provides a total of thirty (30) parking spaces including, two (2) handicap accessible spaces. Approximately eleven (11) vehicles can be accommodated within the drive-through lane without blocking the access drives or internal circulations. The TIS has determined that the stacking area will be sufficient to accommodate the peak demand of the proposed fast food establishment.

Car Wash

The single-lane automatic car wash tunnel will be an exterior-only car wash system and will have a clockwise rotation around Lot 3. The entrance to the car wash drive-through system is located at the southeast portion of Lot 1. Patrons to the car wash will enter from the Town Center driveway and vehicles will proceed east through the internal driveway that runs between each of the three proposed lots. Vehicles will then make two right turns (south) to enter the car wash drive aisle that leads patrons directly to the vacuum stations/parking lot that provide a total of thirty-seven (37) parking spaces, or vehicles can proceed forward



to three queue lanes that lead to the entrance of the car wash tunnel. At the entrance to the car wash tunnel, the driver will remain in the vehicle and the car wash will automatically pull the vehicle through the tunnel.

After exiting the tunnel, the vehicles will turn west and then either proceed straight to the vacuum positions and then exit the car wash by making a left-turn back to the Town Center driveway. If patrons of the car wash wish to use the bank or the restaurant, vehicles can make a right-turn out into the internal driveway.

There is stacking for approximately thirty (30) vehicles to queue before cars exit onto the internal drive aisle. Based on the experience with similar car washes, the TIS has determined that the proposed site plan provides for efficient circulation and adequate stacking for the proposed car wash. The Village Traffic Engineer, however, has indicated that the supporting car wash queue data provided is insufficient to verify the projected queue lengths since the TIS references a study that was performed in Minnesota in 2012, where 5 of the 6 carwash locations surveyed were at gasoline station facilities. The referenced study also did not include average daily traffic for a measure of comparison. The Village has therefore requested that the Applicant provide supporting operational characteristics (e.g., transaction information), as available, pertaining to peak operations and/or supporting operations / documentation from similar Chicagoland area existing sites during average, typical conditions. This has been noted in the Project Traffic Review #2 dated January 26, 2021. Please note that the Village Traffic Engineer does not believe the comments noted above will materially impact the findings / recommendations of the TIS.

Willowbrook Town Center with Internal Access Drive

The TIS has revealed that when a southbound exclusive left-turn lane is not provided at the access drive, the results of a simulation that was conducted indicate that vehicles are queueing back toward Plainfield Road, especially during the weekday evening and Saturday midday peak hours. Therefore, the TIS recommends the provision of an exclusive southbound left-turn lane to keep the southbound through lane clear and minimize the queueing back toward Plainfield Road. In addition, "Do Not Block Intersection" signage was recommended to prevent vehicles from blocking the access drive. The proposed site plan incorporates both of these recommendations.

Cross Access Agreements and Amendment to the Town Center PUD

The relocation of the existing driveway between the subject property and the Town center will require an amendment of the recorded plat for the Town Center PUD. The Applicant would be required to seek authorization from the Town Center to amend the Town Center PUD and the existing development agreement between the Town Center and the Village of Willowbrook as a condition of approval. Representatives of GW Properties indicated they had met with the Harlem Irving Group to discuss the amendment to the Town Center PUD. The response from Harlem Irving appears to have been positive and it appears as if full cooperation will be achieved.

The subject property also has an existing cross access agreement in place with the Town Center along the western portion of the lot. The proposed development will further require an additional cross access agreement since interior vehicular circulation is dependent upon shared access between all three lots. The proposed development also proposes an access drive between Lot 2 and the lot to the east of the property. The topography needs further study to see if this cross access is feasible and has included a condition of approval that this be further studied, and if feasible, to be incorporated as part of the Final PUD and Final Plat of Subdivision processes. Staff notes that Lot 3 will be landlocked due to the subdivision of the three lots and will not have frontage along a public street. Lot 3 will only be accessible via the existing drive aisle that is part of the Town Center. A waiver from the subdivision regulations that require lots to front on a public street will be required.



Pedestrian Access

Sidewalks along Plainfield are being added and/or reconstructed and additional sidewalks within the subject property are proposed to increase pedestrian connectivity between the proposed lots.

Parking Requirements

Parking for the proposed uses is regulated by the Zoning Ordinance as the sum of the individual uses planned. The following table summarizes the proposed uses and the associated parking requirements. Note that there appears to be a parking surplus of forty-nine (49) parking spaces and can be attributed to the number of parking spaces in Lot 1. Lot 1 also does not provide the minimum required number of stacking spaces and this is noted as a waiver as part of the PUD.

Item	Code Section	Code Requirement	Proposed			Departure
			Lot 1	Lot 2	Lot 3	
Required Spaces	9-10-5(K)	Car Wash: 1/1.5 employees = 2 spaces Stacking for 50 vehicles required Restaurant: 1/100 SF = 29 spaces Bank: 1/300 SF = 22 spaces	2 spaces Stacking for 30 vehicles	31 spaces (include 2 accessible spaces) Shared parking of 3 spaces from bank	30 spaces (include 2 accessible spaces)	Yes – Lot 1

Loading Requirements

No loading berths are proposed within the planned unit development. Since only businesses with a gross floor area of over 5,000 square feet are required to provide one loading berth, only the proposed car wash and bank are each required to provide one loading berth. However, since the automated car wash operations and the proposed bank do not anticipate any loading requirements to operate, Village staff considers the waivers from Section 9-10-4(H) to reduce the loading requirement to zero berths as reasonable requests.

Trash Enclosure

To comply with the current zoning ordinance, the Applicant has proposed two areas for trash enclosures on Lots 2 and 3. One trash enclosure would serve the car wash exclusively, and the other trash enclosure would be shared between the bank and the restaurant. Staff raised concerns that the location of the enclosure located on Lot 3 could be problematic as trash vehicles might block access to the restaurant's drive-through or to the drive aisle leading up to the car wash. The Applicant explained that the trash bins would be on rollers and trash pickup would be scheduled off hours to avoid conflicts in circulation.

Stormwater Management/Engineering

Stormwater control on redevelopment sites is not required if the net new impervious area is less than 25,000 square feet. The threshold for Best Management Practices (BMPs) is 2,500 square feet of net new impervious surface. As the subject property is nearly one hundred percent impervious, and the proposed redevelopment would provide more pervious area, stormwater control is not anticipated.

Tri-State Fire Protection District Preliminary Comments

The Fire Protection District offered the following comments:

1. Drive width of 20' clear aisles. This includes the drive adjacent to the car wash. The curb radius and drive space is less than 20' and our fire apparatus will have a difficult time accessing the building.



2. Fire truck dimensions are not an inner city bus and shall be for a vehicle that is 47.68' long with a clear height of 13'6" and a width of 10'4".
3. The trash enclosures located in that area make it impossible for a fire vehicle to access the drive that is adjacent to the building. Prove that the turn radius is available.
4. The canopies for the vacuum stations may not be an issue if they don't extend into the drive area. This needs to maintain a 20' clear path in the drive with a 13'6" clear height.
5. The exit from the car wash is less than 20' and the fire vehicles will not be able to exit without hitting the curbs and causing damage.
6. All the other canopies will not be an issue. There needs to be a verification with the car wash.

Staff recommends that the comments provided by Tri-State Fire Protection District be addressed prior to the consideration by the Village Board.

Police Department Comments

Police Chief Robert Schaller reviewed the proposed site plan and identified three main areas of concern that could impact police/fire.

1. To the west of the development is the entrance/exit into the shared Town Center drive which is heavily congested at times and will lead to traffic backing up in all directions. Currently at the Chase bank location across the street, the teller lanes often are 5-6 vehicles deep during peak times. Similar queues at the proposed bank location may lead to vehicles stacking up impacting the west entrance/exit and creating an even greater traffic bottleneck.
2. To the east at the restaurant drive through, Chief Schaller agrees with the concerns raised at the November 4th planning commission concept review. If the drive-through restaurant is in any way close to the volume of traffic Chick-fil-A produces, this will no doubt impact the ability to access the car wash.
3. Lastly, the northeast entrance/exit onto Plainfield could present an issue. During peak rush hour times there are occurrences where westbound Plainfield traffic is backed up past the northeast entrance/exit. Regardless, if there is right turn only lane, it is inevitable that traffic will turn left from that lane.

Engineering Comments

As requested by email on January 21, 2021, the Village Engineer has reviewed the PUD Plans and supporting documents. Based on this discussion, this is intended to be the Preliminary PUD submittal and this review is for compliance with Section 9-13-5.B of the Village Code.

1. On the Preliminary Plat of Subdivision, all of the certificates required on the Final Plat will be removed.
2. The Preliminary Plat will provide clarification that Public Utility and Drainage Easements will be provided as required. It remains to be determined if that will be specific easements for each utility, or a blanket easement covering all of the property except for building envelopes.
3. Section 9-13-5.B.4 provides that a statement of ownership be included listing the existing ownership and certification that there are no delinquent taxes on the property. We presume that this has been provided separately or will be added to the Preliminary PUD.
4. Section 9-13-5.B.7 provides for covenants providing various information be submitted. We presume that this will be reviewed by others.
5. Section 9-13-5.B.12 provides for preliminary landscape plans. We will defer review of these drawings to the Village Planner.
6. Section 9-13-5.B.13 provides for preliminary engineering information to be shown. These comments are provided in the next section of this letter.



7. Sections 9-13-5.B.14 – 17 provides for various studies to be provided. We presume these are being reviewed by others.

Preliminary Engineering Plans

Cover Sheet – C0.0

8. Under Utility and Governing Agencies, change the Sanitary Sewer Service to Du Page County Public Works.

Utility Plan – C4.0

9. The alignment for the proposed watermain has been discussed with the design engineer and will be revised. The connection to the existing main in the town center will be made at the driveway connection between the two sites, rather than south near the proposed car wash building. Also, the proposed watermain will be an 8" main. This will result in less new public watermain and is expected to be a lower cost for the applicant. A 20 foot wide utility easement would be required over this main.
10. Each building service shall have an 8" x 6" Tee with valve. This will allow the public main to be constructed, and then each building service can be installed at the time of building construction without interrupting the public main.
11. The last segment of storm sewer west of the car wash, where the south end of the site connects to the off-site sewer, is shown to be 18". We presume this will be revised down to 12" on the final plans.
12. We presume the intent is for the sanitary sewer serving the bank and restaurant to be a public sewer. This will be owned by Du Page County Public Works and will be reviewed by that agency. We presume they will require a 20 foot wide easement over the main.

General Comments

13. A permit will be required from Du Page County Division of Transportation for work within the Plainfield Road right of way.
14. A permit will be required from Du Page County Public Works for the proposed sanitary sewer and connections.
15. The Construction Schedule and Engineer's Opinion of Cost will be reviewed with the Final PUD Plat.
16. The fire truck turning templates indicate that presumed truck can maneuver the site. We presume that prior to final approval the plans will be provided to the fire district for comment on both the geometrics, but also the location of hydrants and fire department connections on each building.

Traffic & Circulation Comments

The Village Traffic Engineer (Gewalt Hamilton Associates, Inc.) offers the following comments that must be addressed before final consideration before the Village Board:

1. GHA has found the documentation and/or responses provided, in general, to adequately address the comments.
2. The applicant should provide documentation of the DuDOT approval of TIS and Plainfield Road access, upon receipt.
3. The response letter adequately addressed the comment pertaining to the trip rates assumed in the traffic study for the proposed carwash component of the development during the weekday morning peak hour; however, the concern raised pertaining to the Saturday morning timeframe was not addressed. Please provide response and/or supporting documentation for use of the lower trip generation estimates based on the tunnel independent variable versus the square-footage of the use.



4. The updated study appropriately provides a queuing exhibit that illustrates the stacking capability of each of the proposed uses, as well as additional supporting documentation. However, the supporting carwash queue data provided is insufficient to verify the projected queue lengths. It references a study that was performed in Minnesota in 2012, where 5 of the 6 carwash locations surveyed were at gasoline station facilities, as well as the referenced study did not include average daily traffic for a measure of comparison. While we understand it may be difficult to obtain peak observations at similar facilities, as carwash operations tend to peak in the wintertime on a sunny day following a snowfall, please provide supporting operational characteristics (e.g., transaction information), as available, pertaining to peak operations and/or supporting operations / documentation from similar Chicagoland area existing sites during average, typical conditions.

In addition, the quick service restaurant use has been identified as Guzman Y Gomez. There is an existing drive-through location in Naperville. Again, please provide either data from observations and/or transaction data to support the capacity provided is adequate to support the peak queue.

The Village Traffic Engineer has indicated that the comments noted above will materially impact the findings and/or recommendations of the Traffic Impact Study.

Certain Relief/Waivers Requested

To maintain compliance with the Village Zoning Code, the proposed development will require certain relief, exceptions, and waivers. Staff recommends that the Applicant work with planning staff to minimize the number of waivers and variations, as feasible.

Zoning Ordinance

Pursuant to Section 9-13-6 of the Village Code, the following waivers from the provisions of the Zoning Ordinance include but are not limited to:

Overall Site

1. That Section 9-3-3(B), Division of Lots, be varied to permit the division of the SUBJECT REALTY into more than two (2) lots that do not conform with all the applicable bulk regulations of the B-2 zoning district.
2. That Section 9-3-3(C), Multiple Uses on a Lot, be varied to permit a reduction in the minimum lot area for two (2) or more special uses from five acres (5 AC) to two and 79/100ths acres (2.79 AC).
3. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required front yard parking area setback from twenty-five feet (25') to twenty-four feet (24') along Plainfield Road.
4. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required height of a landscaped earth berm along the Plainfield Road frontage from three feet (3') to two feet (2'). **STAFF RECOMMENDS REVISIONS TO THE LANDSCAPE PLAN TO ELIMINATE THIS WAIVER.**
5. That Section 9-10-5(L)2(d), Driveway Location On Lots For All Uses Other Than Single-Family Residential, be varied to permit a reduction in the minimum separation requirement between an access driveway entrance and an adjoining lot line from seventy feet (70') to fifty-five point four feet (55.4').
6. That Section 9-10-5(L)2(e), Spacing Between Separate Driveway Entrances On All Lots Other Than In Single-Family Attached Districts, be varied to permit a reduction in the minimum access driveway spacing for separate driveways from four hundred feet (400') to one hundred fifty-three and 8/10ths feet (153.8').
7. That Section 9-10-5(L)2(g), Access Driveways From Arterial Streets For All Uses Other Than Single-Family Residential, be varied to permit a reduction in the driveway storage area from one hundred feet (100') to thirty-eight feet (38').



LOT 1 (Automobile Washing and Cleaning Facility)

8. That Section 9-6-1(E)2, General Conditions, Building Façade Materials, be varied to permit metal siding and metal panels as building façade materials, as shown on the architectural elevations.
9. That Section 9-6B-3(D), Minimum Lot Depth, be varied to permit a reduction in the minimum required lot depth from two hundred feet (200') to one hundred eighty and three tenths feet (180.3').
10. That Section 9-6B-3(E)1, Required ~~Setbacks~~, Front Yard, be varied to permit a reduction in the minimum required front yard setback from sixty feet (60') to thirteen and five tenths feet (13.5') for the detached canopy along the north side of the proposed car wash building.
11. That Section 9-6B-3(E)4, Required Setbacks, Rear Yard, be varied to permit a reduction in the minimum required rear yard setback from forty feet (40') to eight tenths feet (0.8') for the detached canopy along the south side of the proposed car wash building.
12. That Section 9-6B-3(G), Maximum Height, be varied to permit an increase in the maximum building height from thirty feet (30') to forty feet (40') to the top of the proposed water tank.
13. That Section 9-10-4(H) Off-Street Loading, Required Berths, be varied to permit a reduction of the minimum number of required loading berths from one (1) loading berth to zero (0) loading berths.
14. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required interior side and rear yard parking area setbacks from ten feet (10') to four and four tenths feet (4.4') along the west lot line, to five feet (5') along the east and south lot lines.
15. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required front yard parking area setback from fifteen feet (15') to zero feet along the north lot line.
16. That Section 9-10-5(K), Off-Street Parking, Required Spaces, be varied to permit a reduction in the minimum number of stacking spaces for automobile laundries from fifty (50) spaces to thirty (30) spaces for the automobile washing and cleaning facility.
17. That Section 9-10-5(L)1, Off-Street Parking, Minimum Parking Space And Aisle Dimensions, be varied to permit five (5) parallel parking spaces for employee parking along the southern building façade.
18. That Section 9-11-4(C), Prohibited Signs, Roof Signs, be varied to permit the roof signs, as shown in the elevations.
19. That Section 9-11-4(K), Prohibited Signs, Painted Signs, be varied to permit painted signs on the east and west building facades.
20. That Section 9-11-4(R), Prohibited Signs, Multiple Message, Digital, Dynamic and/or Video Display Signs, be varied to permit the digital displays on the ground sign located near the shared access driveway with the Willowbrook Town Center, as shown in the elevations.
21. That Section 9-11-12(B)1(a), Sign Surface Area, Number and Height Limitations, Business Districts, be varied to permit an increase in the maximum total sign surface area from three hundred thirty-five square feet (335 SF) to [??] for the one (1) freestanding sign proposed at the northwest corner of the lot, for each wall sign proposed on the north, east and west building facades for a total of three (3) wall signs, and for the three (3) roof signs proposed on each of the three detached canopies, as shown in the sign package. **APPLICANT TO PROVIDE SIGN SURFACE AREA**
22. That Section 9-11-11(B)2 "Sign Location", be varied to allow building mounted sign to extend higher than one foot (1') below the top line of the face of the building and exceed a height of twenty feet (20') for the signage on the north elevation of the automobile washing and cleaning facility.
23. That Section 9-12-2, Permitted Accessory Buildings, Structures and Uses, Awning/Canopies/Marquees, be varied to permit an increase in the permitted encroachment from 1/3 bulk district standard to allow the detached canopy located to the south of the building to be set back eight tenths feet (0.8') from the south lot line, and to allow the northernmost detached canopy located to the north of the building to be set back thirteen and five tenths feet (13.5') from the north lot line.
24. That Section 9-12-4(C)3, Bulk Regulations, Detached Accessory Structures, be varied to permit a reduction in the minimum clearance above grade from fourteen feet (14') to twelve feet (12') for the detached pay canopy located to the south of the building and the detached canopies over the vacuum stations.



25. That Section 9-14-2(D)2(c)4, Border Plantings and Foundation Plantings, be varied to permit a reduction in the minimum foundation landscape area width from seven feet (7') to zero feet along the north, east and west building facades.

LOT 2 (Fast Food Establishment and Drive-Through)

26. That Section 9-3-3(C), Multiple Uses on a Lot, be varied to permit a reduction in the minimum lot area for two (2) or more special uses from two acres (2 AC) to seven hundred ninety-five thousandths acre (0.795 AC).
27. That Section 9-6-1(A), Minimum Lot Area; Two Or More Uses On A Lot, be varied to permit reduction in the minimum lot area for two (2) or more special uses (fast food establishment and drive-through) from two acres (2 AC) to seven hundred ninety-five thousandths acre (0.795 AC).
28. That Section 9-6B-3(A), Minimum Lot Area, be varied to permit a reduction in the minimum lot area for the fast food establishment and drive-through from two acres (2 AC) to seven hundred ninety-five thousandths acre (0.795 AC).
29. That Section 9-6B-3(C)1, Minimum Lot Width, be varied to permit a reduction in the minimum lot width for restaurants from two hundred feet (200') to one hundred twenty-two and seven tenths feet (122.7').
30. That Section 9-6B-3(E)2, Minimum Interior Side Yard Setback, be varied to permit a reduction in the minimum interior side yard setback along the east lot line from thirty feet (30') to fifteen and five tenths feet (15.5') for the proposed fast food establishment, to eleven and four tenths feet (11.4') for the proposed attached canopies, to sixteen and one tenths feet (16.1') and to twenty-six and two tenths feet (26.2') for the menu/order boards closest to the east lot line.
31. That Section 9-6B-3(E)4, Required Setbacks, Rear Yard, be varied to permit a reduction in the minimum required rear yard setback from forty feet (40') to twenty-two and nine tenths feet (22.9') and twenty-three feet (23') for the menu/order boards located closest to the south lot line.
32. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required interior side and rear yard parking area setbacks from ten feet (10') to two and four tenths feet (2.4') along the east lot line and to zero feet along the west and south lot lines.
33. That Section 9-10-5(G)3 Off-Street Parking, Interior Parking Lot Landscaping, be varied to permit a reduction in the minimum end parking island width from nine feet (9') to five feet (5') and six and eight tenths feet (6.8') for two end parking islands.
34. That Section 9-10-5(G)3 Off-Street Parking, Interior Parking Lot Landscaping, be varied to eliminate the requirement for one (1) shade tree and nine (9) dwarf variety shrubs required within the three parking islands.
35. That Section 9-11-6(L), Signs, Exemptions, be varied to permit in increase in the permitted sign surface area for private traffic direction signs from four (4) square feet to eight and one tenths square feet (8.1 SF) for each of the three private traffic direction signs and fifteen and six tenths square feet (15.6 SF) for the private traffic direction sign located underneath the freestanding sign.
36. That Section 9-11-12(B)1, Sign Surface Area, Number and Height Limitations, Business Districts, be varied to permit an increase in the total sign surface area from one hundred thirty-eight and four tenths square feet (138.4 SF) to three hundred eighty-three and four tenths square feet (383.4 SF), to permit an increase in the maximum number of wall signs from three (3) wall signs to six (6) wall signs, to permit building mounted signage to exceed a height of twenty feet (20') for the wall signs on the north and west building elevations, and to permit a reduction in the minimum separation distance between wall signs on the north and west building facades to less than 20% of the linear dimension of the business site frontage, in accordance with the building elevations.
37. That Section 9-12-2, Permitted Accessory Buildings, Structures and Uses, Awning/Canopies/Marquees, be varied to permit an increase in the permitted encroachment from 1/3 bulk district standard to eleven and four tenths feet (11.4') from the east lot line for the two attached canopies located along the east building façade.



38. That Section 9-14-2(D)2(c)4, Border Plantings and Foundation Plantings, be varied to permit a reduction in the minimum foundation landscape area width from seven feet (7') to zero feet along the north, west and south building facades, and to less than seven feet (7') along the east building façade.

LOT 3 (Financial Institution and Drive-Through)

39. That Section 9-3-3(C), Multiple Uses on a Lot, be varied to permit a reduction in the minimum lot area for two (2) or more special uses from two acres (2 AC) to seven hundred ninety-two thousandths acre (0.792 AC).
40. That Section 9-3-7(A)4, Specific Setbacks, Plainfield Road, be varied to permit a reduction in the minimum special setback from for one hundred feet (100') to seventy-one and 8/10ths feet (71.8') for the proposed building.
41. That Section 9-6-1(A), Minimum Lot Area; Two Or More Uses On A Lot, be varied to permit reduction in the minimum lot area for two (2) or more special uses (financial institution and drive-through) from two acres (2 AC) to seven hundred ninety-two thousandths acre (0.792 AC).
42. That Section 9-6-1(E)2, General Conditions, Building Façade Materials, be varied to allow aluminum composite panels as building façade materials, as shown on the architectural elevations.
43. That Section 9-6B-3(A), Minimum Lot Area, be varied to permit a reduction in the minimum lot area for the financial institution and drive-through from two acres (2 AC) to seven hundred ninety-two thousandths acre (0.792 AC).
44. That Section 9-6B-3(E)2, Minimum Interior Side Yard Setback, be varied to permit a reduction in the minimum interior side yard setback along the east lot line from thirty feet (30') to seven feet (7') for the proposed building.
45. That Section 9-6B-3(E)4, Required Setbacks, Rear Yard, be varied to permit a reduction in the minimum required rear yard setback from forty feet (40') to thirty-six and two tenths feet (36.2') for the attached canopy along the south side of the building.
46. That Section 9-10-4(H) Off-Street Loading, Required Berths, be varied to permit a reduction of the minimum number of required loading berths from one (1) loading berth to zero (0) loading berths.
47. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required interior side and rear yard parking area setbacks from ten feet (10') to eight and five tenths feet (8.5') along the west lot line and to zero feet along the east and south lot lines.
48. That Section 9-11-6(L), Signs, Exemptions, be varied to permit in increase in the permitted sign surface area for private traffic direction signs from four (4) square feet to four and six tenths square feet (4.6 SF) for the private traffic direction sign located to the southwest of the building.
49. That Section 9-11-5(E)2, Illuminated Window Signs, be varied to permit an increase in the maximum sign surface area for an illuminated window sign for the Chase ATM located in the vestibule of the west building facade from four square feet (4 SF) to eight and thirty-four hundredths square feet (8.34 SF).
50. That Section 9-11-6(M), Exemptions, ATM Signs, be varied to permit an increase in the maximum sign surface area from four square feet (4 SF) to eight and thirty-four hundredths square feet (8.34 SF).
51. That Section 9-11-12(B)1(a), Sign Surface Area, Number and Height Limitations, Business Districts, be varied to permit an increase in the maximum total sign surface area from one hundred ninety-seven and eighty-five hundredths square feet (197.85 SF) to three hundred forty-four square feet (344 SF) for the one (1) freestanding sign proposed at the northwest corner of the lot and for each wall sign proposed on the north, east and west building facades for a total of three wall signs, as shown in the sign package.
52. That Section 9-11-12(B), Sign Surface Area, Number and Height Limitations, Business Districts, be varied to permit the signs for the financial institution in accordance with the building elevations.
53. That Section 9-12-2, Permitted Accessory Buildings, Structures and Uses, Awning/Canopies/Marquees, be varied to permit an increase in the permitted encroachment from 1/3 bulk district standard to allow the attached canopy located to the south of the building to be set back eleven and five tenths feet (11.5') from the east lot line.



54. That Section 9-14-2(D)2(c)4, Border Plantings and Foundation Plantings, be varied to permit a reduction in the minimum foundation landscape area width from seven feet (7') to zero feet along the north, east, west and south building facades.

PUD Standards

Pursuant to Section 9-13-6 of the Village Code, the following variations from the provisions of the Planned Unit Development Regulations include but are not limited to:

55. That Section 9-13-6(B) PUD Standards, Size and Ownership, be varied to waive the requirement that the subject realty be under single ownership and/or unified control.
56. That Section 9-13-6(F) PUD Standards, Yards, be varied to waive the requirement that the required yards or setbacks along the periphery of the planned unit development be at least equal in width or depth to that of the applicable required yard within the adjacent zoning district.
57. That Section 9-13-6(L) PUD Standards, Other Standards, be varied to waive the requirement that the planned unit development must comply with the minimum standards set forth in subsections (A), (B), (C), (D), (H), (I) and (K) of this section.

Subdivision Ordinance

Pursuant to Section 10-8-7 of the Village Code, the following variations from the provisions of the Subdivision Regulations be and the same are requested:

58. That Section 10-4-3(A)2, Lots, Sizes and Shapes, be varied to waive the requirement that the lot areas and lot widths conform to at least the minimum requirements of the zoning ordinance for the district in which the subdivision is proposed for Lots 2 and 3.
59. That Section 10-4-3(A)3, Lots, Sizes and Shapes, be varied to waive the requirement that building setback lines conform to at least the minimum requirements of the zoning ordinance for the district.
60. That Section 10-4-3(B), Lots, Arrangement, be varied to waive the requirement that Lot 1 front on a public street.

Applicable Findings

Section 9-14-5(B) of the Willowbrook Zoning Ordinance establishes seven (7) standards for a Special Use Permit that must be evaluated by the Plan Commission and Village Board. The applicant must meet all standards and draft responses to these standards if they wish to proceed with a petition for special use approval.

Additionally, the Plan Commission and Village Board shall not recommend or grant variations from the regulations of the Village's Zoning Ordinance unless affirmative findings of fact are made as to all of the standards set forth in Section 9-14-4(E) of the Willowbrook Zoning Ordinance. The applicant is also applying for a Planned Unit Development, and therefore the applicant must also draft responses to each of the Standards for Planned Unit Developments.

Summary

The Plan Commission last saw concepts for a Planned Unit Development (PUD) proposal at 735 Plainfield Road at the February 5, 2020 meeting and then again at the November 4, 2020 meeting. The previous proposal included a ninety (90) room hotel and a coffee shop with drive-through. Due to the economic downturn caused by the Covid-19 pandemic, the hotel operator has since withdrawn, and the Applicant has since replaced the hotel use with a car wash and has found a fast food/drive-through operator for the



previous coffee shop use. These two uses are anticipated to bring additional foot traffic to the area and will support surrounding commercial uses.

While there are some minor changes that will still be required prior to forwarding to the Village Board for formal consideration, the bigger issues needing feedback from the Plan Commission before a recommendation should be considered is a review of a Market Study and Tax Impact Study as well as finalizing signage. Other questions or concerns may come out of the public hearing process and can also be incorporated into any final direction to the applicant to be used in finalizing their plans for a formal vote at a future meeting date that can be determined by the Plan Commission at the meeting. If the Plan Commission chooses to continue the public hearing, any motion to continue the hearing should include the date and location of the continued meeting.

Staff is generally supportive of the proposed permitted and special uses and the proposed design layout on the site. For a number of reasons, the subject property has been slow to develop over the years. Efforts so far have focused on making this product work physically on this site, and there are modifications in that regard that are required. Additional efforts, however, are required to ensure the intended uses on and within the property are fully understood and to finalize details on signage or identity as discussed in this report. This is the last remaining commercial lot within the Village in a very visible and well-traveled part of the community, and additional efforts from the Village and the developer are required to solidify details to ensure the end product will be successful for the end use while remaining compatible with the commercial environment it will share with its neighbors. Staff therefore recommends that the Applicant continue to work with the Village to address the outstanding comments provided in the review letters (Tri-State, Police, Engineering, Traffic and Planning) and to provide the required studies, prior to forwarding a recommendation to the Village Board for consideration.

If the Plan Commission wishes to continue the public hearing, staff recommends approval of the following sample motion:

Sample Motion to Continue the Public Hearing

Based on the submitted petition, the testimony provided by the Applicant, and the staff report for PC 21-03 at the February 3, 2021 Plan Commission meeting, I move that the Plan Commission continue the public hearing to March 3, 2021 (or to another date to be determined) to allow Village Staff and the Applicant time to address the various issues raised by the Plan Commission and Village staff.

If the Plan Commission wishes to support the project, staff recommends approval of the following sample motion:

Sample Motion to Recommend Approval

Based on the submitted petition, the testimony provided by the applicant, and the staff report prepared for PC 21-03 at the February 3, 2021 Plan Commission meeting, I move that the Plan Commission recommend and forward to the Village Board the Findings of Fact presented and discussed by the Plan Commission at the February 3, 2021 meeting, and further recommend that the Village Board approve the following:

1. A special use for a planned unit development associated with PC 21-03, including the "proposed waivers" outlined in the staff report.
2. Special uses for one 5,582 square foot automobile washing and cleaning facility, one 2,830 square foot fast food establishment with drive-through, and one 6,660 square foot financial institution with drive-through.



3. Approval of the Preliminary Plat of Subdivision and Preliminary Plat of PUD for "735 Plainfield Road Subdivision", except for revisions required by the Village Engineer to be revised prior to forwarding to the Village Board for consideration.

Subject to the following conditions:

Recommended Conditions of Approval

1. All plans and documents shall be revised and resubmitted as required by Village staff and the Plan Commission as indicated in the staff report or as discussed during the February 3, 2021 meeting, and approved by staff prior to being forwarded to the Village Board for final consideration.
2. Approval of Preliminary Plat of PUD is expressly conditioned on the approval of an amendment of the Town Center PUD with respect to changes in the ingress, egress and incorporation of a dedicated left-turn lane as shown on 735 Plainfield Road Plat of PUD. An exclusive southbound left-turn lane shall be provided at the proposed full movement access drive in order to keep the southbound through lane clear and minimize the queueing back toward Plainfield Road.
3. That as part of the approval of a Preliminary Plat of Subdivision and Preliminary Plat of PUD, the applicant shall submit a Market Study and Tax Impact Study or related studies to the satisfaction of the Village, prior to Village Board consideration.
4. That as part of the approval of a Preliminary Plat of Subdivision and Preliminary Plat of PUD, the applicant shall submit a Cross Access Agreement and indicate the required easements on their preliminary plats.
5. That as part of the approval of a Preliminary Plat of Subdivision and Preliminary Plat of PUD, a parking agreement shall be executed to allow three of the parking spaces located on Lot 3 to satisfy the parking requirement on Lot 2.
6. That as part of the Final PUD and Final Plat of Subdivision processes, the Applicant shall investigate the feasibility of proposing an Ingress and Egress Easement on the northeast end of the subject property to allow vehicular access between the subject realty and the TCF Bank property.
7. Prior to approval of a Final Plat of Subdivision and Final Plat of PUD, the applicant shall submit all required executed Traffic Regulation, Improvement and Redevelopment Agreements in a form acceptable to the Village Board, approved by Village staff and subject to review by the Village attorney.
8. That as part of the Final PUD and Final Plat of Subdivision processes, the applicant shall submit the Declaration of Covenants, which is subject to the approval by the Village.
9. The Applicant shall provide documentation of the DuDOT approval of Traffic Impact Study and Plainfield Road access, upon receipt.
10. The completion of all County of DuPage and Village traffic improvements shall be made prior to the issuance of the first permanent occupancy permit for the subject realty.
11. Outdoor dining and restaurant seating shall not be allowed on the subject realty except where identified as "Outdoor Dining" on the fast food establishment architectural plans.
12. Off-site improvements shall include a sidewalk to be constructed in accordance with the PUD plans. The sidewalk shall be installed along Plainfield Road and is subject to the County of DuPage permitting, inspection and approval.
13. That the digital signs on the SUBJECT REALTY shall be at all times subject to the following requirements:
 - a. Operational Limitations: Display shall contain static messages only, and shall not have movement of any kind, or the appearance or optical illusion of movement, of any part of the sign.
 - b. Minimum Display Time: Each message on the sign must be displayed for a minimum of 8 seconds or such longer duration as is hereafter enacted in the Village Sign Ordinance for comparable signs.



- c. Message Change Sequence: The change between static messages must be accomplished immediately, with no use of any transitions.
 - d. Illumination: The sign must include light sensors and dimmer controls that automatically adjust to outdoor lighting levels so that illumination levels are dimmer at night and on cloudy days than during sunny days; but in no instance shall illumination and lighting not be in compliance with Section 9-11-13 of the Willowbrook Zoning Ordinance.
 - e. Only one freestanding or ground sign shall be constructed or erected on Lot 1.
 - f. The sign shall not contain any other advertising other than the identity of the car wash, the address; and the promotion of related business products for Lot 1.
 - g. A separate sign permit shall be obtained pursuant to Village Code.
- 14. All freestanding or ground signs shall not contain any advertisement other than the identity of the business located therein; therefore, the freestanding sign proposed for each business shall be located on its own lot.
 - 15. The multi-tenant sign located north of the shared access drive with the Town Center shall be removed from all plans prior to consideration before the Village Board.
 - 16. No deliveries or other loading and unloading activities shall be allowed on the subject realty between the hours of 7:00 pm to 10:00 am.
 - 17. No trucks shall be permitted to sit idling on the subject realty.
 - 18. No outside loudspeakers shall be permitted other than businesses with approved drive-through windows and then only for the operation of the drive-through service.
 - 19. Outside refuse compactors shall only be operated between the hours of 9:00 a.m. and 6:00 p.m. and shall be screened by a masonry wall large enough to visually screen the compactor dumpster and buffer any noise created by the compactor unit.
 - 20. The earthen berms located along the Plainfield Road frontage shall be constructed prior to the issuance of the first temporary or permanent occupancy permit for the subject realty, or such earlier time as is reasonably practical. All exiting movements from the drive-throughs shall be under stop sign control.
 - 21. Exiting movements from the bank drive-through shall be under stop sign control.
 - 22. "Do Not Enter" signs shall be placed at the exit of each drive-through lane to deter opposing traffic from entering the drive-throughs from the one-way exit direction.
 - 23. A permit will shall be required from Du Page County Division of Transportation for work within the Plainfield Road right of way.
 - 24. A permit shall be required from Du Page County Public Works for the proposed sanitary sewer and connections.
 - 25. Prior to final approval, the plans will be provided to the fire district for comment on both the geometrics and the location of hydrants and fire department connections on each building.

Next Steps...

The Plan Commission and the Applicant were advised that the next steps include the Applicant's revision to the plans and submission of the required studies to evaluate the market and tax impacts, after which staff will complete reviews and inform the Applicant of additional modifications (if any) before the item is schedule for consideration by the Village Board for a formal vote.



Attachment 1
Legal Description

LOT 1 IN WILLOWBROOK CENTER UNIT NO. 1, OF PART OF LOT 7 IN OWNER'S SUBDIVISION IN SECTIONS 23 AND 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED OCTOBER 16, 1963, AS DOCUMENT R63-37895, IN DUPAGE COUNTY, ILLINOIS.

PIN: 09-23-406-003

ADDRESS: 735 Plainfield Road, Willowbrook IL 60527.



Attachment 2
Submitted Plans

GRAPHIC 20-42

Number of children in the household

Percentage of households

0 1 2 3 4 5 6 7 8 9 10

100% 0%

Source: U.S. Census Bureau, 2000



REGISTRATION

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LOCATION

THE UNIVERSITY OF CHICAGO

SETHURAMAN, S. S. H. AND CHANDRASEKHAR

Journal of Management Inquiry 20(4)

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0893-3200/00/\$12.00
DOI: 10.1037/0893-3200.14.1.100

Notes

1. The following are the main components of the system:
 - a. The system is designed to be used by a single user at a time.
 - b. The system is designed to be used by a single user at a time.
 - c. The system is designed to be used by a single user at a time.
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1 using System;
2 using System.Collections.Generic;
3 using System.Linq;
4 using System.Text;
5 namespace ConsoleApplication1
6 {
7 class Person
8 {
9 public string Name { get; set; }
10 public int GetAge()
11 {
12 return 25;
13 }
14 public double GetSalary()
15 {
16 return 10000;
17 }
18 }
19 }

```

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[illegible]

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[illegible]

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| ----- | 1981-1985 |
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| ----- | 2001-2005 |
| ----- | 2006-2010 |
| ----- | 2011-2015 |
| ----- | 2016-2020 |

## 5. THEORETICAL & EXPERIMENTAL

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**COMPASS**  
SURVEYING LTD



THE UNIVERSITY OF CHICAGO

**Kimley » Horn**

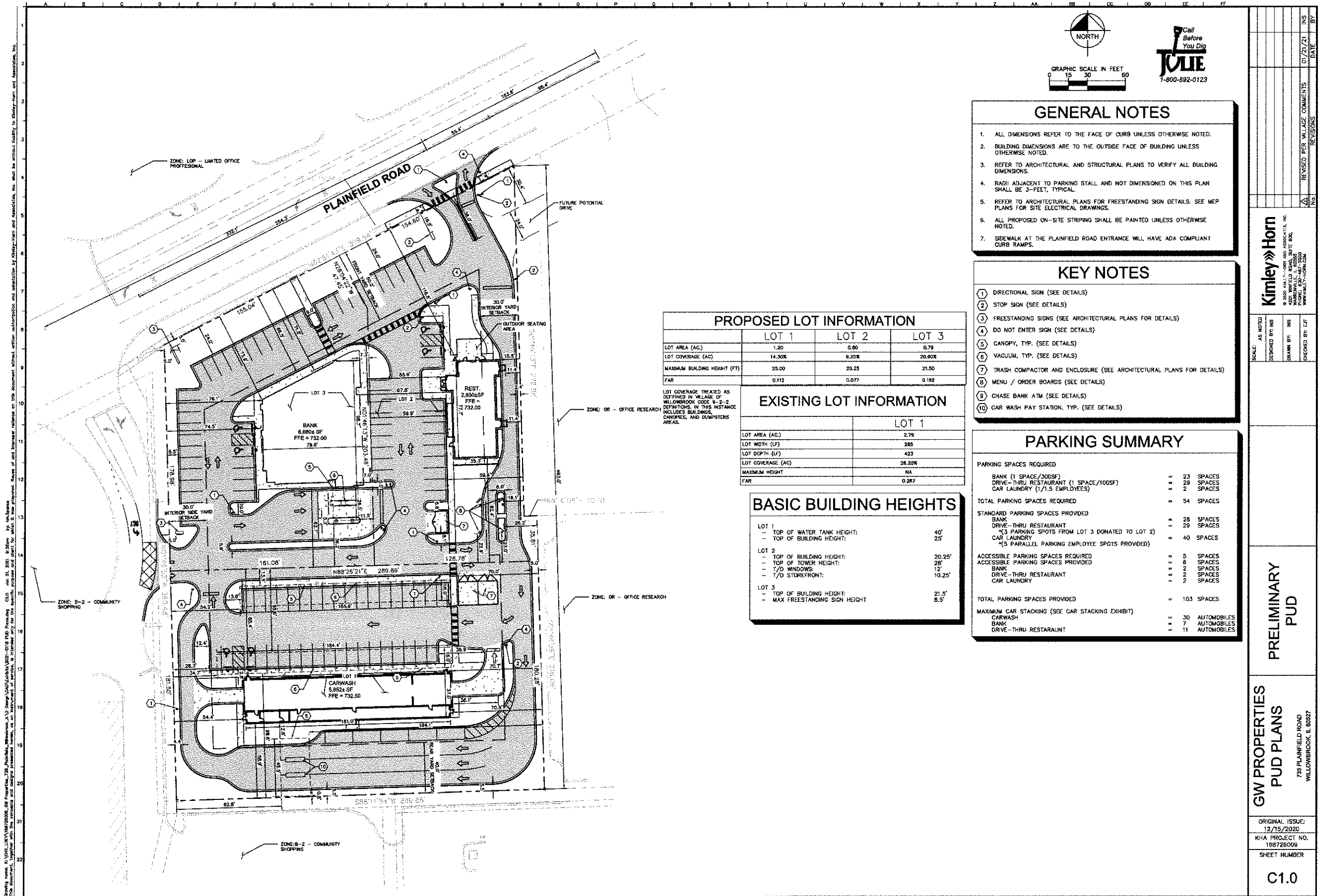
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Journal of Internal Medicine 258: 105–112

**CITIZENSHIP AND VOTING**

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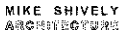




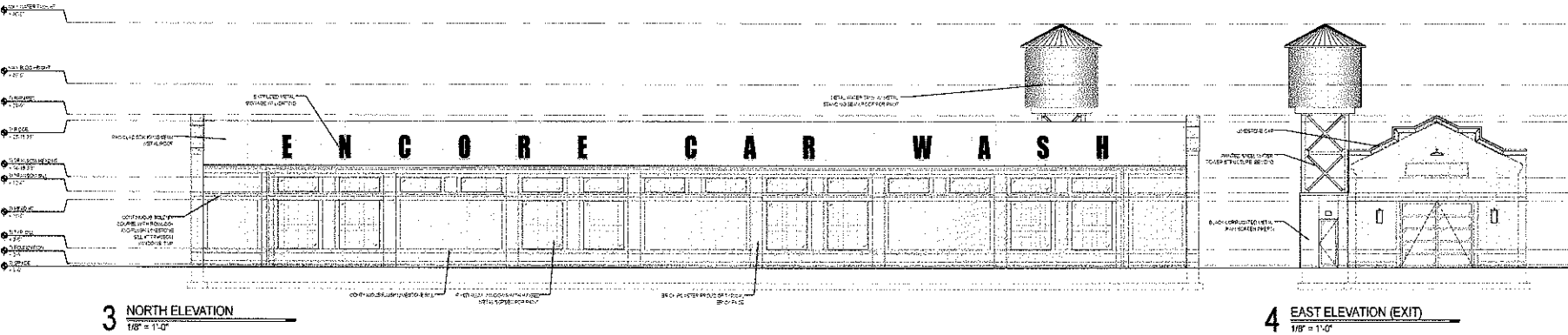
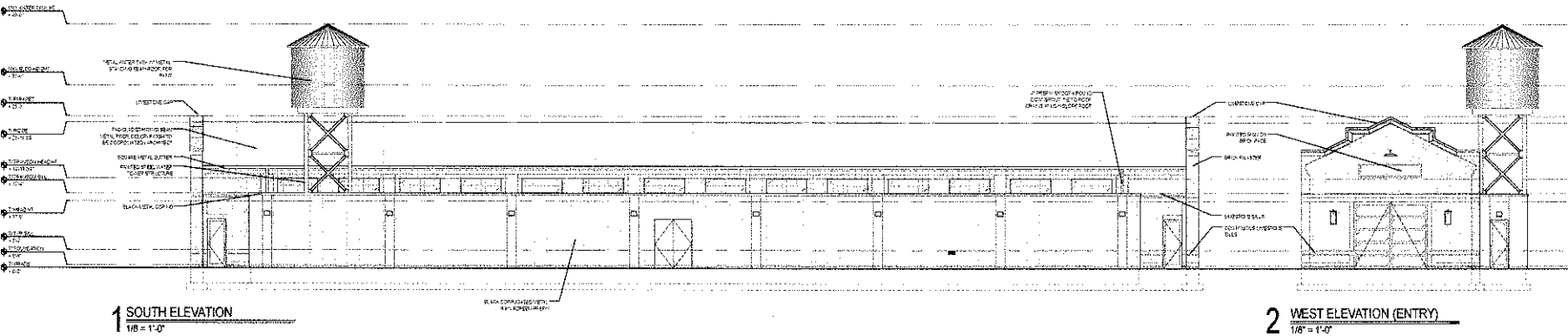








Pharmaceuticals: The Pharmaceutical Research and Manufacturers of America (PhRMA) is the leading industry group for pharmaceuticals. It represents the interests of pharmaceutical manufacturers and is a major lobbying force in Washington. PhRMA's website is <http://www.phrma.org>.



**Encore Car Wash**  
R&B Construction 2016 Reed Building Works  
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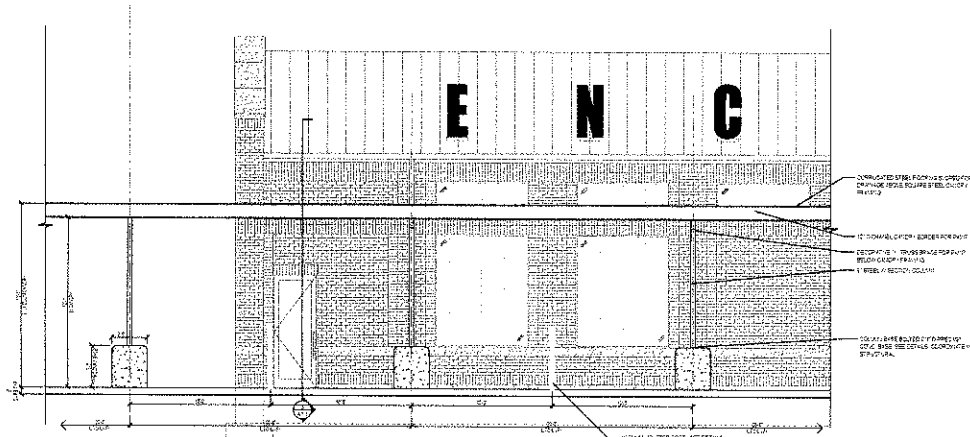
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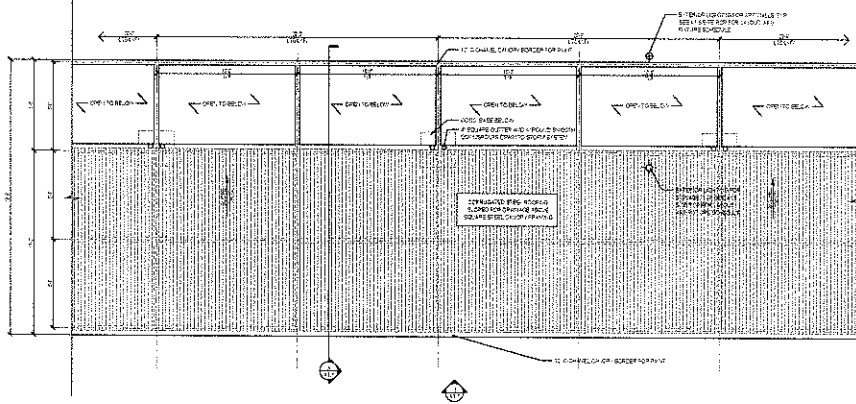
### Car Wash Exterior Elevations

| Revisions           |            |
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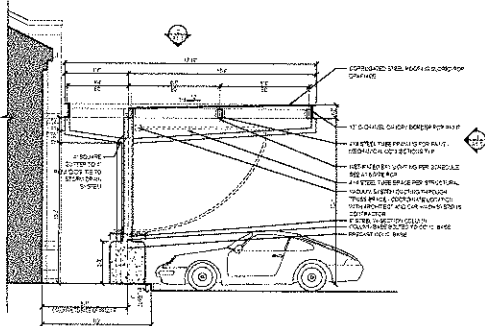




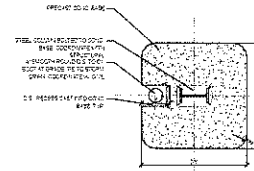
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1/4" = 1'-0"



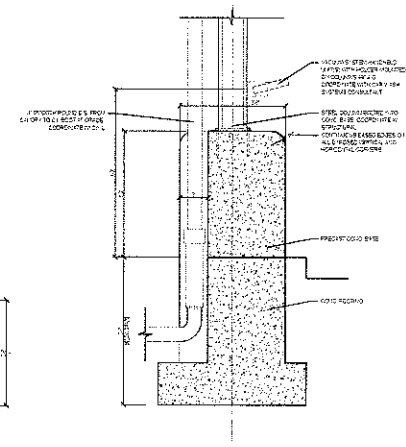
2 VACUUM CANOPY TYP. ROOF PLAN  
1/4" = 1'-0"



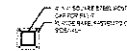
3 VACUUM CANOPY TYP. SECTION  
1/4" = 1'-0"



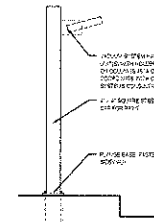
4 COLUMN BASE PLAN DTL  
3/4" = 1'-0"



5 COLUMN BASE SECTION DTL  
3/4" = 1'-0"



6 VACUUM HOLSTER POST PLAN  
3/4" = 1'-0"



7 VACUUM HOLSTER POST ELEVATION  
3/4" = 1'-0"



MIKE SHIVELY  
ARCHITECTURE

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Orem, UT 84401  
801.225.1111  
mshively.com

1. 1/2\"/>

Encore Car Wash  
1414 East 1000 N.  
Orem, UT 84401

A1.1  
Vacuum Canopy  
Plans, Elevations,  
Sections

| Revised | By             | Date       |
|---------|----------------|------------|
| 1       | Initial Design | 12.01.2019 |
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PKA Date: 01.25.2021





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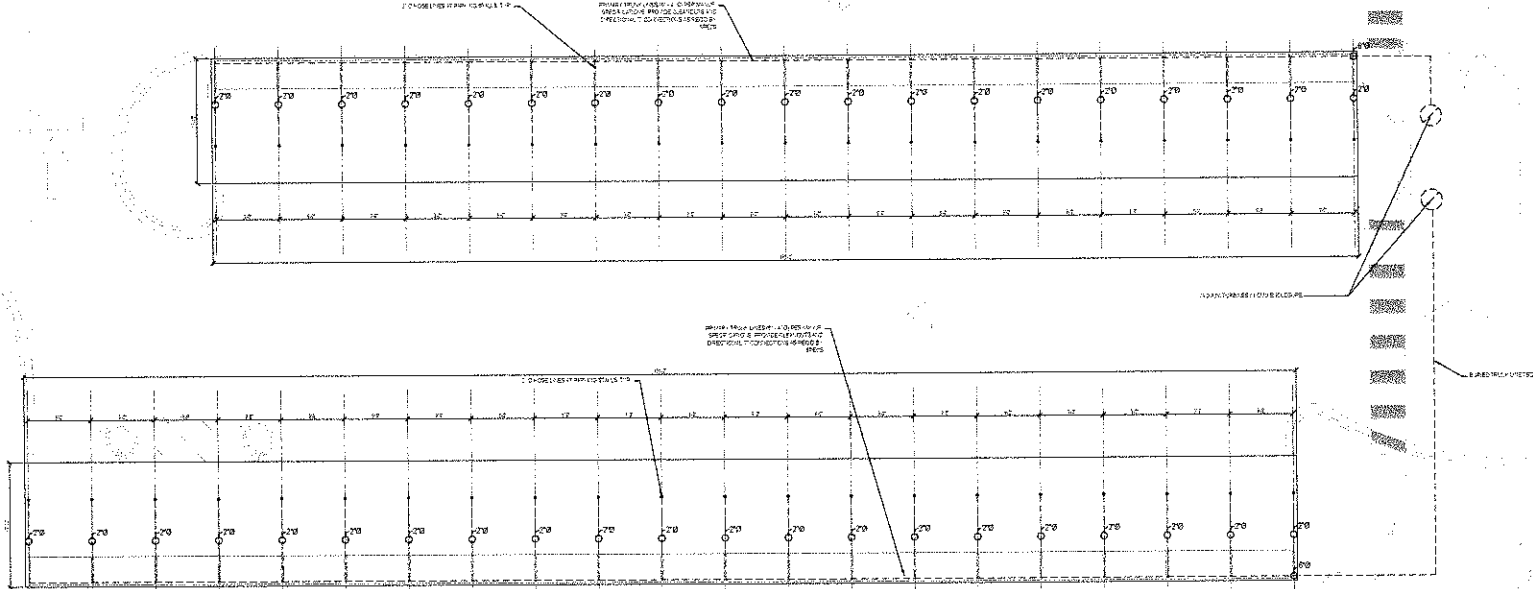
773.771.1122  
mshively@msa.com

A1.2

Vacuum Canopy  
Duct plan diagram

| Revisions |      |
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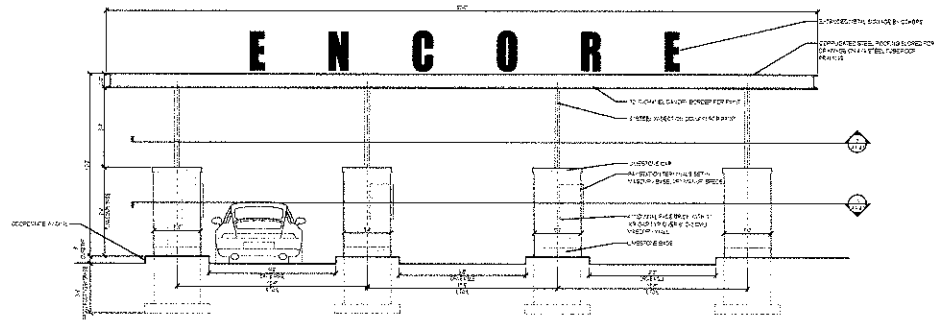
PM Date: 01.25.2021



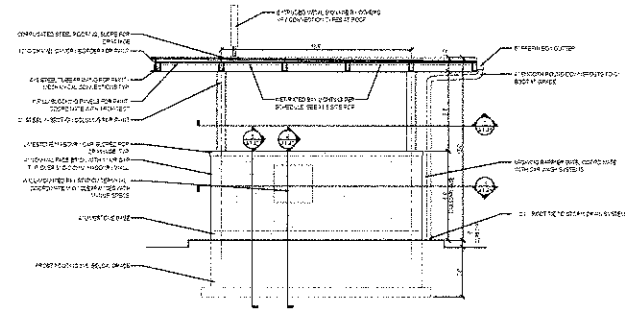
1 VACUUM CANOPY PLAN DIAGRAM  
1/8" = 1'-0"



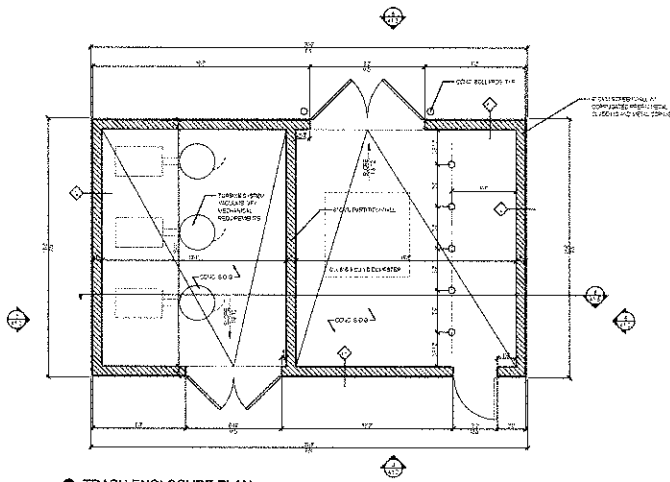




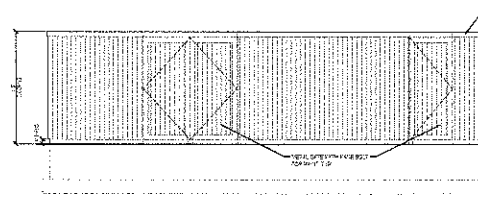
1 PAY CANOPY FRONT ELEVATION  
1/4" = 1'-0"



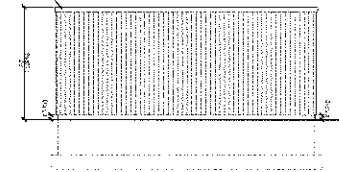
2 PAY CANOPY LATERAL SECTION  
1/4" = 1'-0"



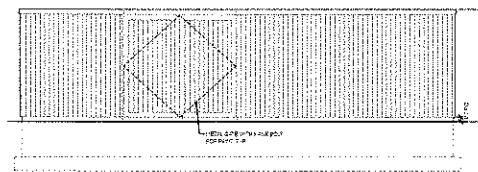
3 TRASH ENCLOSURE PLAN  
1/4" = 1'-0"



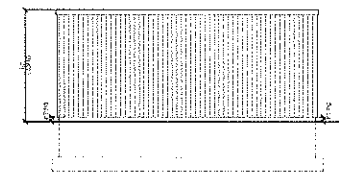
4 REFUSE ENCLOSURE REAR ELEVATION  
1/4" = 1'-0"



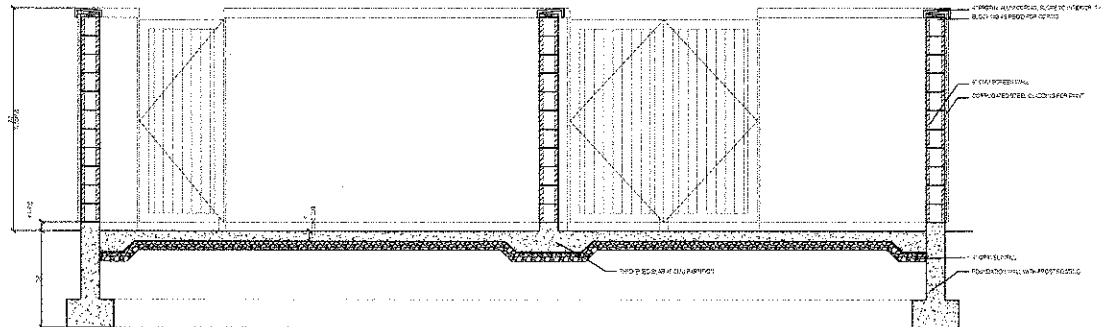
5 REFUSE ENCLOSURE RIGHT ELEVATION  
1/4" = 1'-0"



6 REFUSE ENCLOSURE FRONT ELEVATION  
1/4" = 1'-0"



7 REFUSE ENCLOSURE LEFT ELEVATION  
1/4" = 1'-0"



8 REFUSE ENCLOSURE SECTION  
1/2" = 1'-0"



MIKE SHIVELY  
ARCHITECTURE

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Project: Refuse Enclosure Design  
Prepared: Mike Shively, Architect  
Checked: Mike Shively, Architect  
Date: 01.14.2011

Client: Chicago Transit Authority  
Project: Refuse Enclosure Design  
Date: 01.14.2011

Encore Car Wash  
and a Commercial Wash Station  
Schematic Design 9

720 West Madison  
Chicago, IL 60607

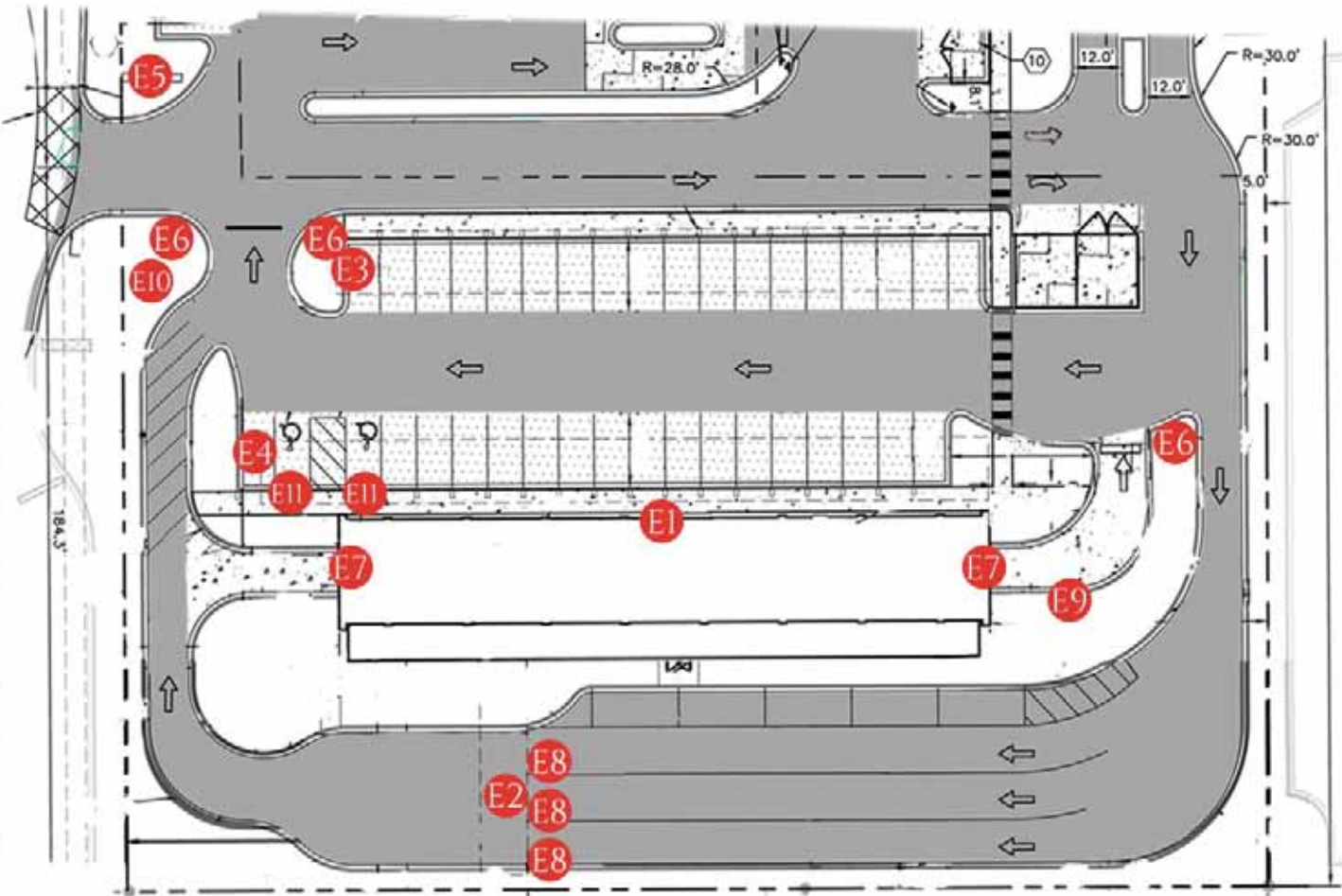
A1.2  
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Revisions  
1 Schematic Design 12.01.2010  
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| EXTERIOR SIGN LEGEND |          |                                                         |         |
|----------------------|----------|---------------------------------------------------------|---------|
| Elevation            | Sign No. | Description                                             | Sq. Ft. |
| North                | E.1      | Illuminated Channel Letters                             | 202.45  |
| East                 | E.2      | Illuminated Channel Letters                             | 47.69   |
| West                 | E.3      | Illuminated Channel Letters                             | 20.58   |
| West                 | E.4      | Illuminated Channel Letters                             | 15.08   |
| North                | E.5      | Free Standing Sign                                      | 47.5    |
|                      | E.6      | Non-Illuminated Directional Sign                        |         |
|                      | E.7      | Non-Illuminated Painted Sign on Brick                   |         |
| East                 | E.8      | Illuminated Menu Sign (24sqft per qty 3-1 per pay lane) | 72      |
| West                 | E.9      | Illuminated Exit Indicator Sign                         | 12.5    |
|                      | E.10     | Temporary Construction Sign - Skid Mount                |         |
|                      | E.11     | Post Mounted Handicap Stall Sign                        |         |
|                      |          |                                                         | 417.8   |

AERIAL IMAGE - NOT TO SCALE



SITE PLAN FOR LOCATION REFERENCE - NOT TO SCALE

735 PLAINFIELD RD  
WILLOWBROOK, IL  
LOT 1

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1100 Route 34  
Aurora, Illinois 60503  
JC 630 898 5900 office  
630 898 6091 fax

Prepared For: ENCORE CAR WASH  
Location Name:

Address: 735 PLAINFIELD RD  
City/State: WILLOWBROOK, IL

Draw: 210073 Sheet Map Design Date: 1/14/2021  
Rev 1:  
Rev 2:

LANDLORD APPROVAL SIGNATURE  
PRINT

TITLE:  
DATE:

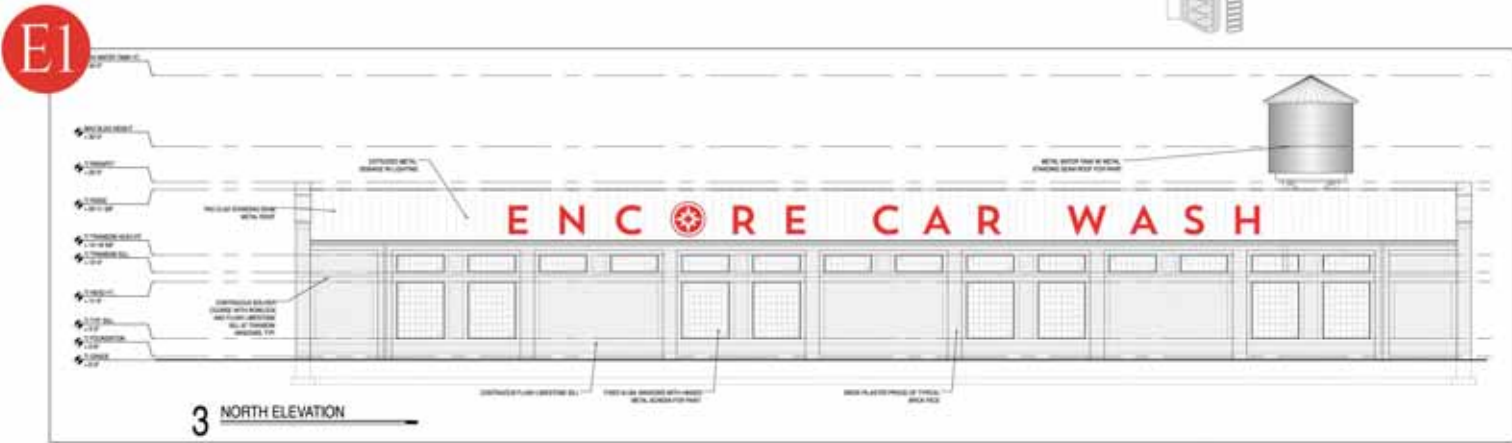
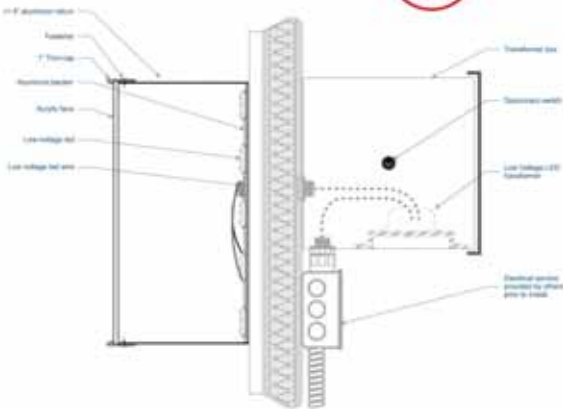




202.45 SQ FT

ILLUMINATED CHANNEL LETTERS FLUSH MOUNTED TO WALL  
FABRICATE AND INSTALL CHANNEL LETTERS OF ALUMINUM AND ACRYLIC.  
LETTER FACES TO BE ACRYLIC WITH VINYL APPLIED FIRST SURFACE.  
ALL EXPOSED METAL SURFACES TO BE COATED WITH ACRYLIC POLYURETHANE.  
INTERNAL ILLUMINATION TO BE WHITE LEDS.  
SCALE 1/8"=1'

INSTALLATION INSTRUCTIONS  
CENTER SIGN LEFT/RIGHT, TOP/BOTTOM ON WALL AS SHOWN.  
CONNECT TO ELECTRICAL SERVICE TO BE PROVIDED BY OTHERS PRIOR TO INSTALL.



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|                                                                                       |                                                 |                                                           |                                  |                                    |                                           |                     |
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| 1100 Route 34<br>Aurora, Illinois 60503<br>JC 630 898 5900 office<br>630 898 6091 fax | Prepared For: ENCORE CAR WASH<br>Location Name: | Address: 735 PLAINFIELD RD<br>City/State: WILLOWBROOK, IL | Draw: 210073<br>Rev 1:<br>Rev 2: | Sheet: 2<br>Design Date: 1/14/2021 | LANDLORD APPROVAL SIGNATURE:<br><br>PRINT | TITLE:<br><br>DATE: |
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SIGN TO  
BE UL  
LISTED



ILLUMINATED CHANNEL LETTERS

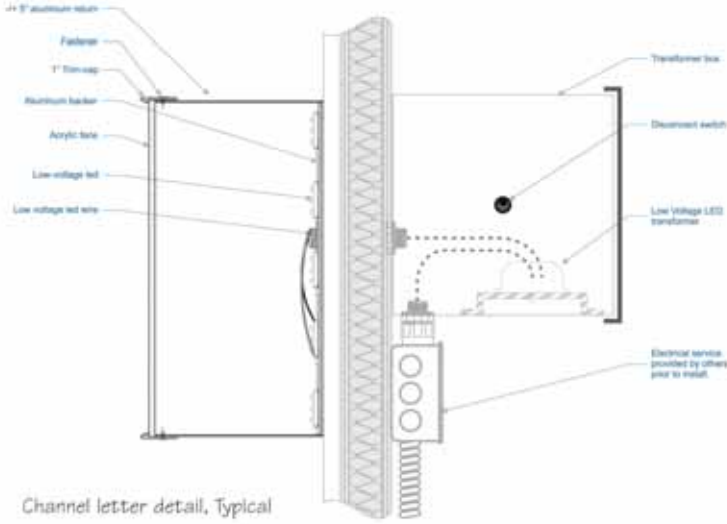
FABRICATE AND INSTALL CHANNEL LETTERS OF ALUMINUM AND ACRYLIC.  
LETTER FACES TO BE ACRYLIC WITH VINYL APPLIED FIRST SURFACE.  
ALL EXPOSED METAL SURFACES TO BE COATED WITH ACRYLIC POLYURETHANE.  
TRIM-CAP AND RETURNS TO BE BLACK.  
INTERNAL ILLUMINATION TO BE WHITE LEDs.  
SCALE 1/4"=1'

**47.69 SQ FT**

INSTALLATION INSTRUCTIONS

INSTALL SIGN ON AREA SHOWN.

WEST ELEVATION



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|                                                                                       |                                                 |                                                           |                                 |                                    |                                      |                 |
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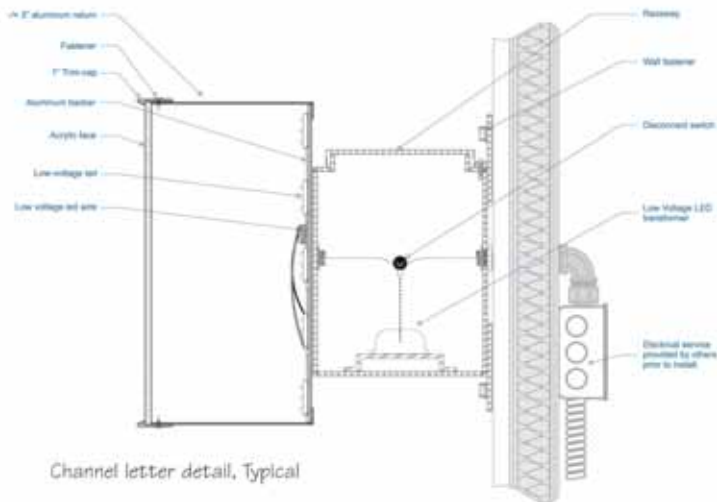
SIGN TO  
BE UL  
LISTED

ILLUMINATED CHANNEL LETTERS ON A RACEWAY  
FABRICATE AND INSTALL CHANNEL LETTERS OF ALUMINUM AND ACRYLIC.  
LETTER FACES TO BE ACRYLIC WITH VINYL APPLIED FIRST SURFACE.  
ALL EXPOSED METAL SURFACES TO BE COATED WITH ACRYLIC POLYURETHANE.  
TRIM-CAP AND RETURNS TO BE BLACK.  
INTERNAL ILLUMINATION TO BE WHITE LEDS.  
RACEWAY PAINTED TO MATCH CANOPY,  
SCALE 3/4"=1"

20.58 SQ FT

INSTALLATION INSTRUCTIONS  
INSTALL SIGN ON AREA SHOWN.  
CONNECT TO ELECTRICAL SERVICE TO  
BE PROVIDED BY OTHERS PRIOR TO INSTALL.

E3



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|                                                                                       |                                                 |                                                           |                                 |                                    |                                      |                 |
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13'-0"

1'-1 1/8"

# FREE VACUUMS

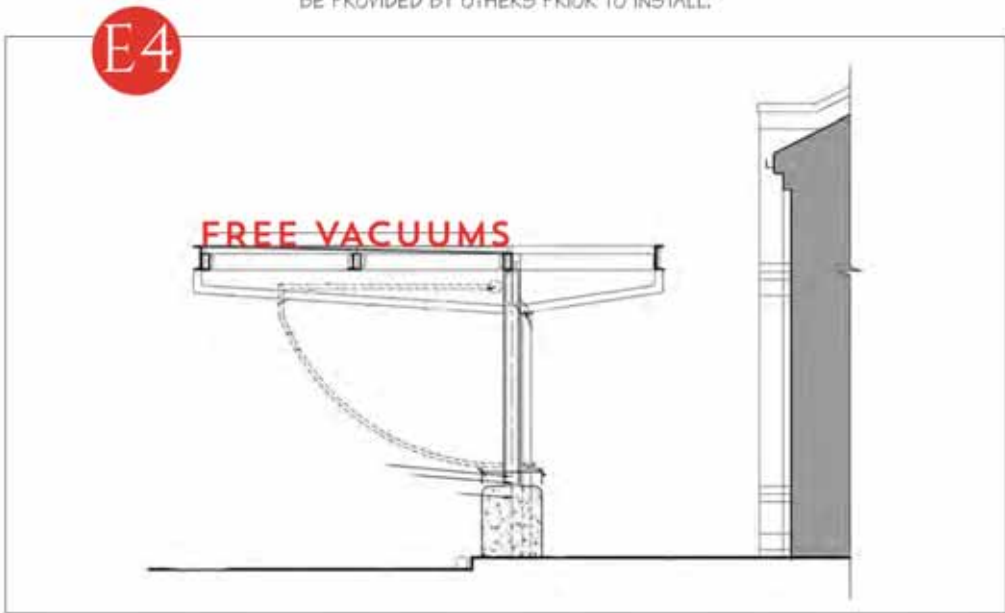
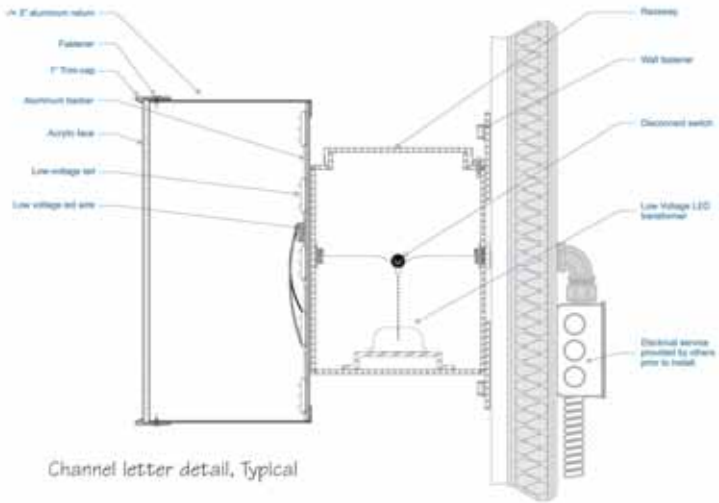
Disconnect  
Switch

SIGN TO  
BE UL  
LISTED

ILLUMINATED CHANNEL LETTERS ON A RACEWAY  
FABRICATE AND INSTALL CHANNEL LETTERS OF ALUMINUM AND ACRYLIC.  
LETTER FACES TO BE ACRYLIC WITH VINYL APPLIED FIRST SURFACE.  
ALL EXPOSED METAL SURFACES TO BE COATED WITH ACRYLIC POLYURETHANE.  
TRIM-CAP AND RETURNS TO BE BLACK.  
INTERNAL ILLUMINATION TO BE WHITE LEDS.  
RACEWAY PAINTED TO MATCH CANOPY,  
SCALE 3/4"=1'

15.08 SQ FT

INSTALLATION INSTRUCTIONS  
INSTALL SIGN ON AREA SHOWN.  
CONNECT TO ELECTRICAL SERVICE TO  
BE PROVIDED BY OTHERS PRIOR TO INSTALL.



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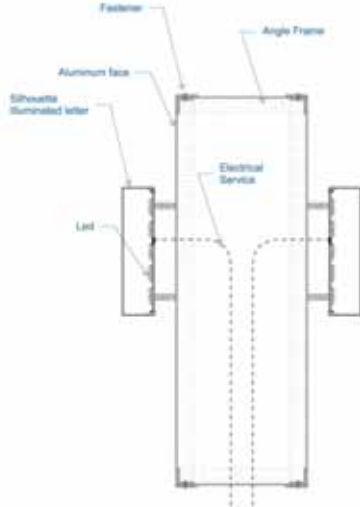
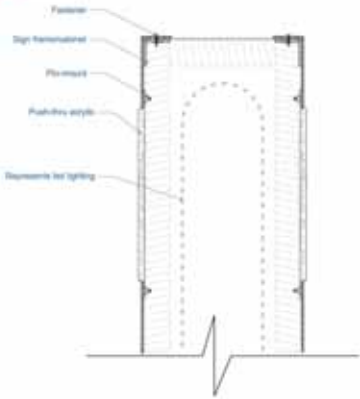
|                                                                                       |                |                 |             |                   |        |        |        |   |              |           |                             |  |        |
|---------------------------------------------------------------------------------------|----------------|-----------------|-------------|-------------------|--------|--------|--------|---|--------------|-----------|-----------------------------|--|--------|
| 1100 Route 34<br>Aurora, Illinois 60503<br>JC 630 898 5900 office<br>630 898 6091 fax | Prepared For:  | ENCORE CAR WASH | Address:    | 735 PLAINFIELD RD | Drawg: | 210073 | Sheet: | 6 | Design Date: | 1/14/2021 | LANDLORD APPROVAL SIGNATURE |  | TITLE: |
|                                                                                       | Location Name: |                 | City/State: | WILLOWBROOK, IL   | Rev 1: |        |        |   |              |           |                             |  | DATE:  |
|                                                                                       |                |                 |             |                   | Rev 2: |        |        |   |              |           | PRINT                       |  |        |

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LISTED



**INSTALLATION INSTRUCTIONS**  
SET 4" STEEL PIPE IN CONCRETE FOOTING TO BE 2' X 3' BY 4' DEEP. CONNECT TO ELECTRICAL SERVICE TO BE PROVIDED BY OTHERS PRIOR TO INSTALL.

**DOUBLE FACE ILLUMINATED SIGN**  
FABRICATE AND INSTALL SIGN OF ALUMINUM AND ACRYLIC.  
ALL EXPOSED METAL SURFACES TO BE COATED WITH ACRYLIC POLYURETHANE.  
INTERNAL ILLUMINATION TO BE WHITE LEDS.  
SEE SCHEDULE A FOR MESSAGE UNIT  
SCALE 1/2"=1'

**SCHEDULE A**

W6MM COLOR  
PIXEL MATRIX 36X126  
CABINET SIZE 41" H X 6' 3" L  
VIEWING ARE 36" H X 6' L  
DOUBLE FACE  
CHARACTER SIZE 4 LINES  
/25 CHARACTERS AT A 4" TYPE

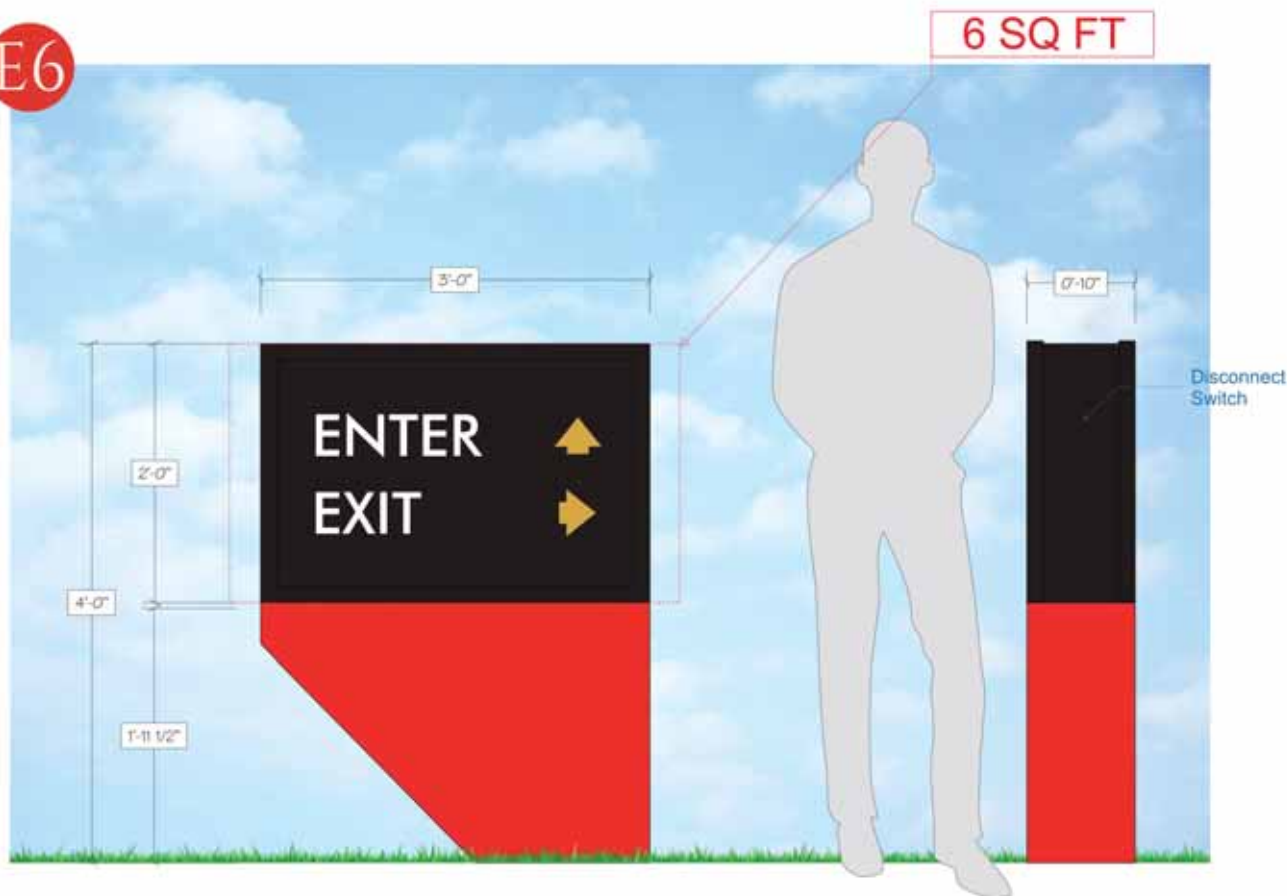
SEE SURVEY FOR SIGN LOCATION

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|                                                                                       |                                                 |                                                           |                                 |                                    |                                      |                 |
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E6

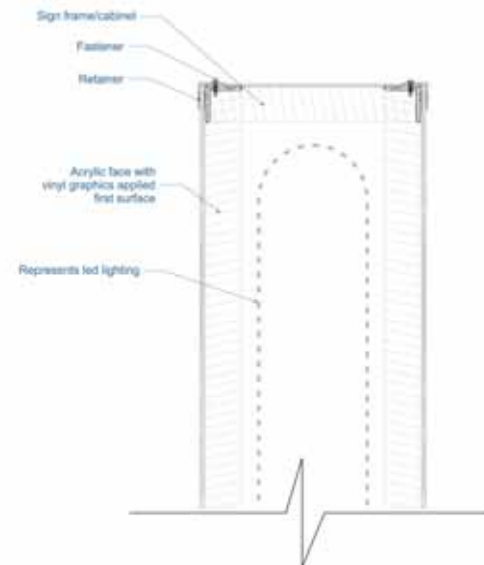


SIGN TO  
BE UL  
LISTED

**AURORA** SIGN CO.

All Signs Shall Be Installed in Accordance With N.E.C. Article 600  
Engineering Specifications  
All Signs Fabricated as per  
A.S. & Specifications & 2014 I.B.C.

Electrician Specifications  
All Signs Fabricated as per  
2015 N.E.C. Specifications



### INSTALLATION INSTRUCTIONS

SET 2" STEEL PIPE IN CONCRETE FOOTING TO BE  
10" ROUND AT 4' DEEP. CONNECT TO ELECTRICAL SERVICE  
TO BE PROVIDED BY OTHERS PRIOR TO INSTALL.

### DOUBLE FACE ILLUMINATED DIRECTIONAL SIGN

FABRICATE AND INSTALL SIGN OF ALUMINUM AND ACRYLIC.  
SIGN FACE TO BE WHITE ACRYLIC WITH VINYL APPLIED FIRST SURFACE.  
ALL EXPOSED METAL SURFACES TO BE COATED WITH ACRYLIC POLYURETHANE.  
INTERNAL ILLUMINATION TO BE WHITE LEDS.  
SCALE 1"=1'

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1100 Route 34  
Aurora, Illinois 60503  
JC 630 898 5900 office  
630 898 6091 fax

Prepared For: ENCORE CAR WASH

Address: 735 PLAINFIELD RD

Dwg: 210073

Sheet: B

Design Date: 1/14/2021

LANDLORD APPROVAL SIGNATURE

TITLE:

Location Name:

City/State: WILLOWBROOK, IL

Rev 1:

Rev 2:

PRINT

DATE:

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FREE STANDING SIGN  
X 1



WAY FINDING SIGN

ENCORE CAR WASH  
BUILDING SIGN X1

FREE VACUUMS  
VAC (SOUTH) CANOPOY SIGN X 1

PAINTED 'CAR WASH' - PER  
ARCHITECTUAL PLANS -BY OTHERS  
X 2 @ 12 SQFT EACH



MENU SIGN  
X3 @ 24 SQFT EACH  
BY OTHERS

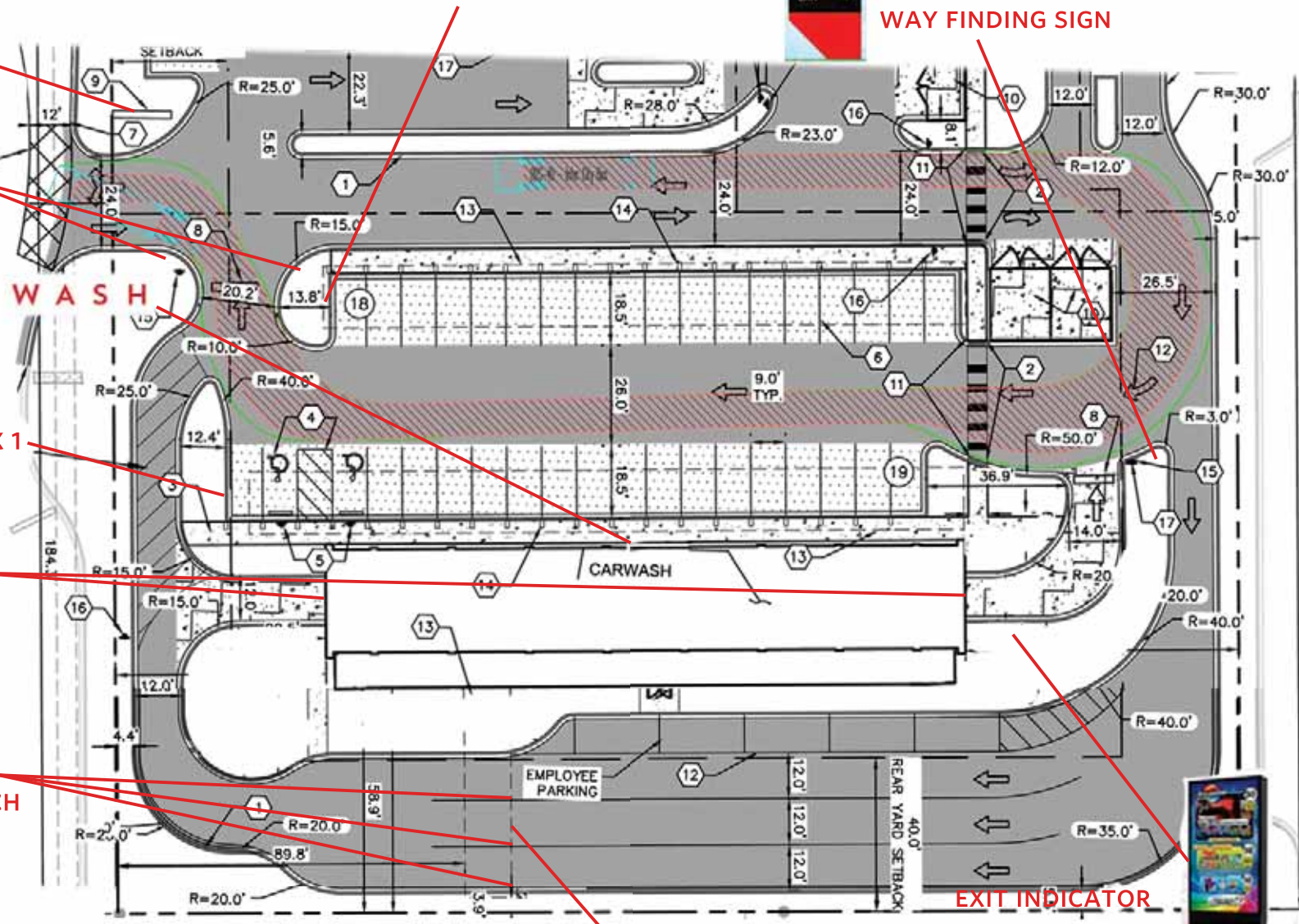
CAR WASH  
VAC (NORTH) CANOPOY SIGN X 1



WAY FINDING SIGN

EXIT INDICATOR  
SIGN BY OTHERS  
X1 @ 12.5 SQFT

ENCORE  
PAY CANOPY SIGN X 1



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|                                                                                       |                                                 |                                                           |                                  |                                      |                                      |                 |
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| 1100 Route 34<br>Aurora, Illinois 60503<br>JC 630 898 5900 office<br>630 898 6091 fax | Prepared For: ENCORE CAR WASH<br>Location Name: | Address: 735 PLAINFIELD RD<br>City/State: WILLOWBROOK, IL | Draw: 210073<br>Rev 1:<br>Rev 2: | Sheet: Map<br>Design Date: 1/14/2021 | LANDLORD APPROVAL SIGNATURE<br>PRINT | TITLE:<br>DATE: |
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# INTERPLANE

ARCHITECTURE  
ENGINEERING  
INTERIOR DESIGN  
PROJECT MANAGEMENT

DESIGN TEAM REGISTRATION  
#15403211-0054

ONE SOUTH 780 SUMMIT AVE  
SUITE D  
OVERBROOK TERRACE, IL 60187  
PH 630.932.2234  
FX 630.932.2339

DATE:

THIS DOCUMENT IS NOT  
FOR REGULATORY  
APPROVAL, PERMITTING,  
OR CONSTRUCTION.

REV 001  
REVISION



GUZMAN  
Y GOMEZ

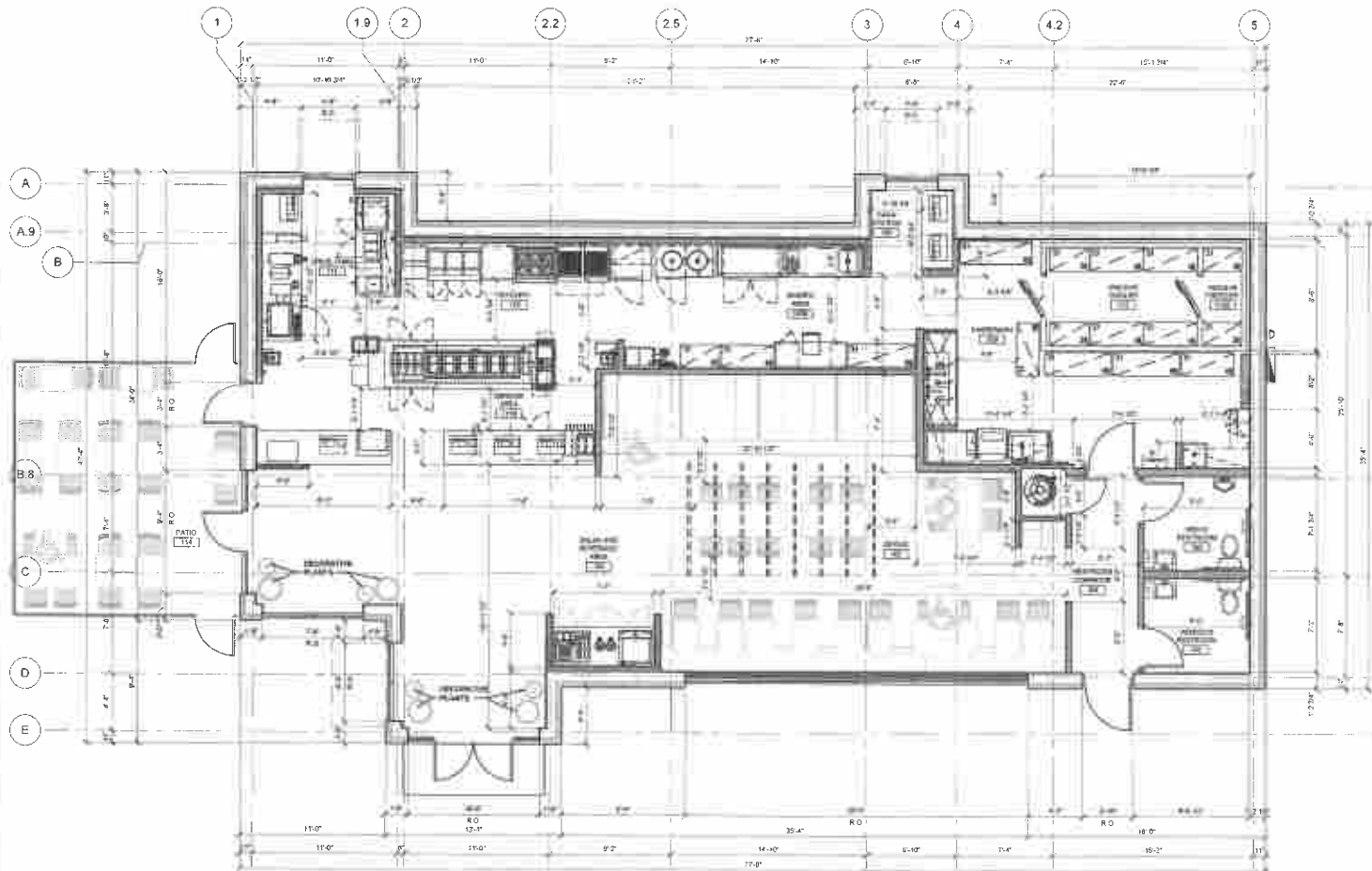
735 FLAMFIELD ROAD  
WILLOWBROOK, IL 60097

PROJECT NO: 2008.0919  
DATE: 01.20.21

PFP

INTERIOR DESIGN  
FLOOR

CHECKED: MP DRAWING: DC



1 FLOOR PLAN



0 1' 2' 4' 8'  
SCALE: 1/4"=1'-0"



## Sign A UL Listed channel Letters

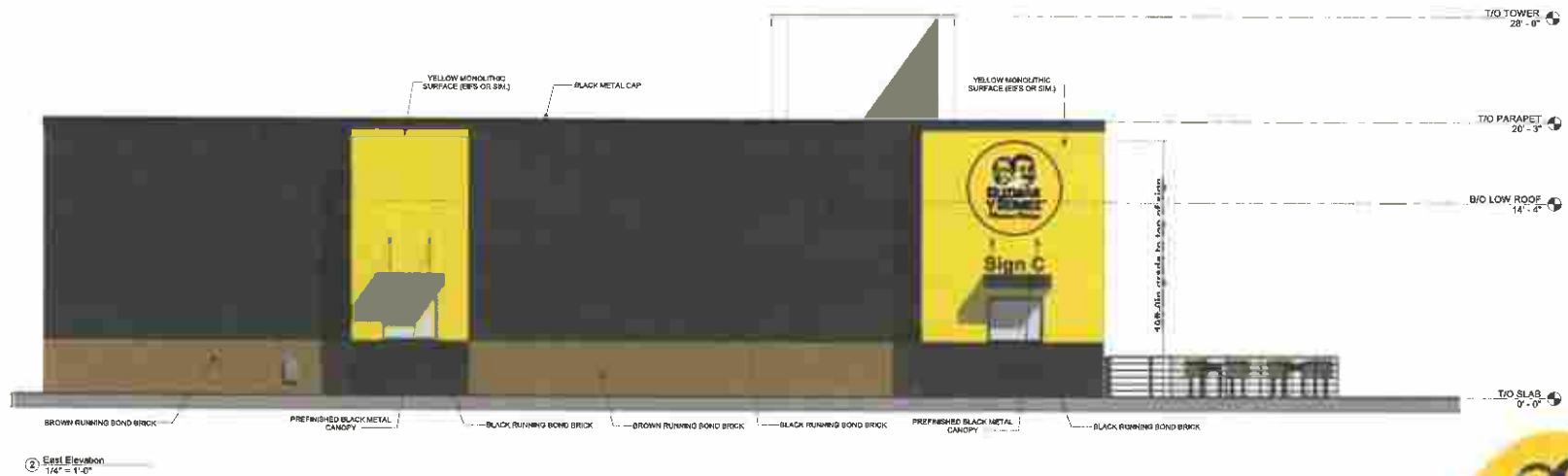


5in projection from wall

Illuminated LOGO  
plex face/LED internal illumination  
UL Listed

5in projection

## Sign B & C





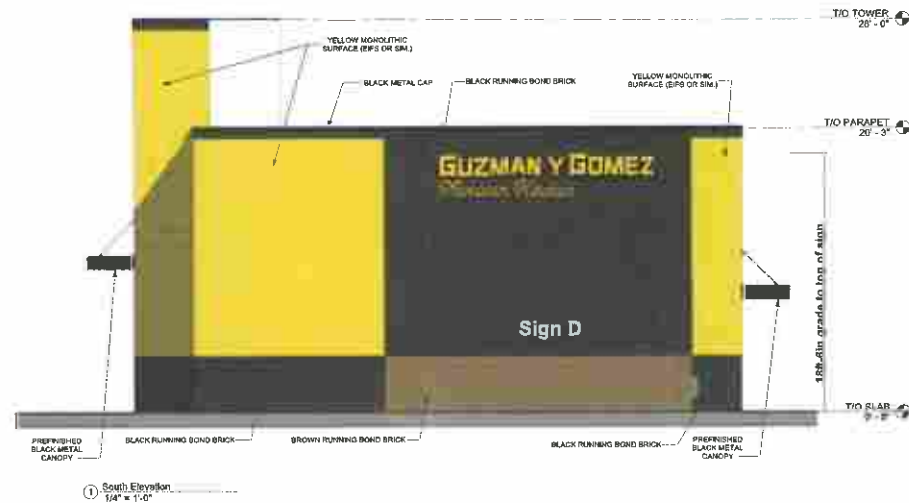
# Sign D UL Listed channel Letters



5in projection from wall

## Sign E

Illuminated LOGO  
plex face/LED internal illumination  
UL Listed  
5in projection



## Sign F



UL Listed channel Letters

5in projection from wall

735 PLAINFIELD RD.  
WILLOWBROOK, IL 60527  
01.20.21





735 PLAINFIELD RD.  
WILLOWBROOK, IL 60527

R4

Sign G



DF Monument Sign

Building Frontage  
43ft-4in X 77ft-8in

Max wall sign SSA 315.2 sqft  
Wall sign SSA proposed 315.2 sqft

GyG Willowbrook SSA Chart

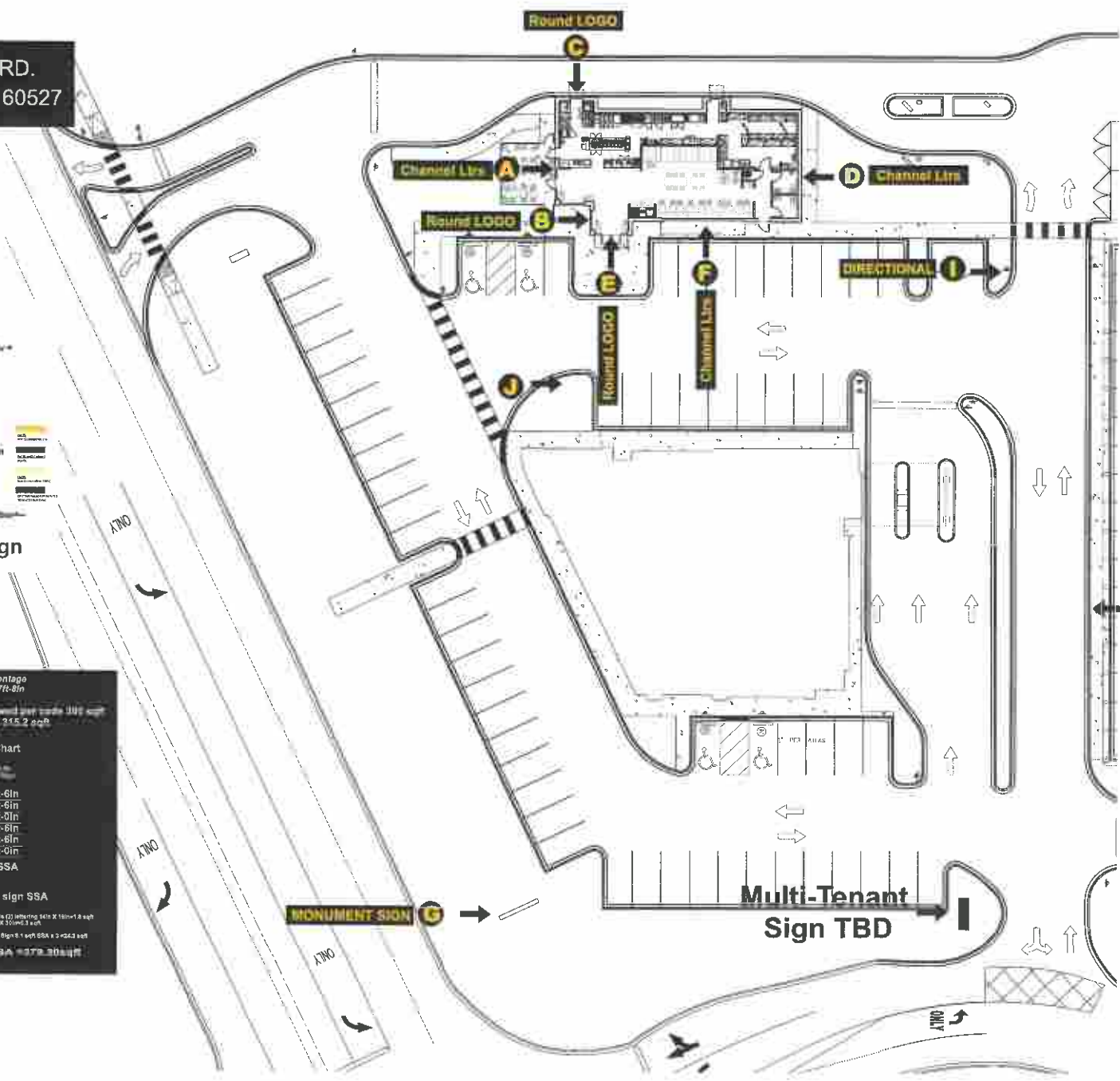
|        | SSA<br>sqft | Height<br>ft | Width<br>ft |
|--------|-------------|--------------|-------------|
| Sign A | 46.6        | 18ft-6in     | 18ft-6in    |
| Sign B | 49          | 26ft-6in     | 18ft-6in    |
| Sign C | 49          | 19ft-0in     | 18ft-6in    |
| Sign D | 46.6        | 18ft-6in     | 18ft-6in    |
| Sign E | 49          | 26ft-6in     | 18ft-6in    |
| Sign F | 75          | 19ft-0in     | 18ft-6in    |

315.2 total wall sign SSA

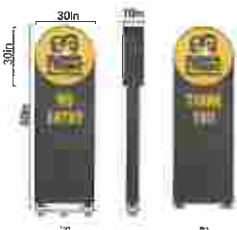
39.8 monument sign SSA

H, I, J Directionals: Directional (I) lettering 14in X 18in=1.8 sqft  
Logo 30in X 30in=6.3 sqft  
Directional Sign 8.1 sqft SSA X 3=24.3 sqft

Total Proposed SSA =379.30sqft



Signs H, I & J  
Directionals



lettering 14in X 18in=1.8 sqft  
Logo 30in X 30in=6.3 sqft  
Directional Sign 8.1 sqft SSA

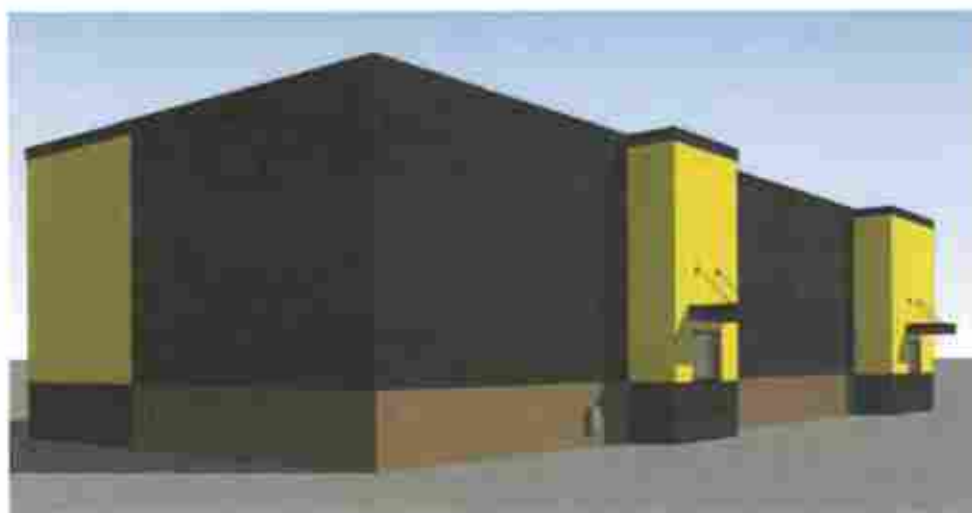




① Northwest Corner



② Southwest Corner

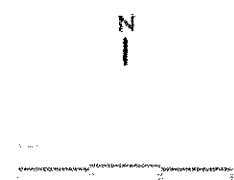


③ Southeast Corner



④ Northeast Corner





## FLOOR PLAN

01 22 2021





GLASS PANEL

GLASS PANEL

NORTH ELEVATION



WEST ELEVATION



**CHASE**   
**WILLOWBROOK RELO**  
 730 Plainfield Road  
 Willowbrook, IL 60527

**ARCHITECTURAL ELEVATIONS**  
 01.18.2021

Architect/Designer:  
 The Architects Partnership  
 200 South Michigan Avenue  
 Chicago, IL 60604  
 T: 312.583.8800  
 F: 312.583.8890  
 TAP Project Number: 10100





- 
 LIGHT GRAY  
 CONCRETE  
 EXTERIOR
- 
 DARK GRAY  
 CONCRETE  
 EXTERIOR
- 
 MEDIUM GRAY  
 CONCRETE  
 EXTERIOR

- 
 DARK GRAY  
 CONCRETE  
 EXTERIOR
- 
 LIGHT GRAY  
 CONCRETE  
 EXTERIOR
- 
 MEDIUM GRAY  
 CONCRETE  
 EXTERIOR
- 
 YELLOW  
 CONCRETE  
 EXTERIOR



SOUTH ELEVATION



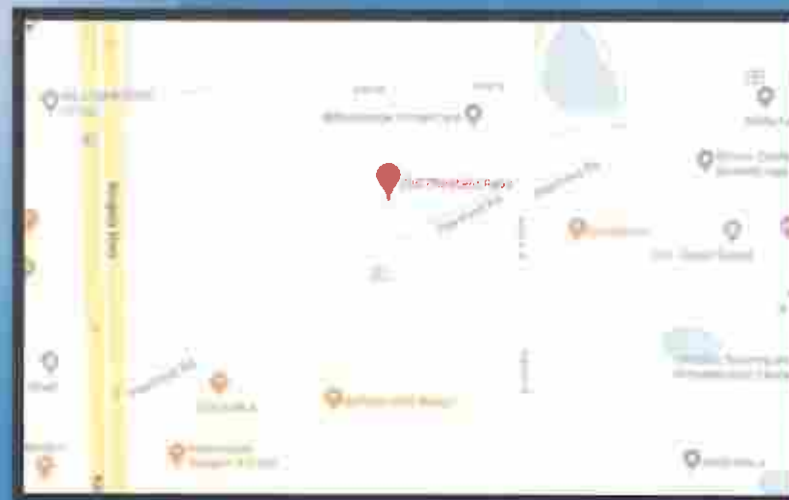
EAST ELEVATION





# CHASE

**#730**  
**Willowbrook**  
**730 Plainfield Rd.**  
**Willowbrook, IL 60527**



## REVISION NOTES:

R7: Detail Drawing - Added details - AS - 09/23/20  
 R8: Detail Drawing - Removed 138 from scope of work - AS - 05/19/20  
 R11: Detail Drawing - 138 changed to 24in. Removed 130 and 41 from scope and updated CSS artwork - AS - 09/30/20  
 R13: Detail Drawing - Lettersets updated to 24in added property and building dimensions to site plan. Elevations updated - AS - 01/19/21



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## JP Morgan Chase Bank #730

Willowbrook  
 730 Plainfield Rd.  
 Willowbrook, IL 60527

Initial Date: 09/14/18  
 Salesperson: Arthur Navarro  
 Coordinator: Tracey Pichetti  
 Designer: bjones  
 Scale: As noted

## CUSTOMER APPROVAL

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

**COPY, COLORS & SIZES**

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Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

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Drawing Number: 18-01613

Project ID: CHASE\_730\_1

Revision: R13 - 01.19.21

Page: 1

**DRC Page 1**



### EXTERIOR SIGN LEGEND - ALLOWED

| Elevation | Sign No. | Sign Type        | Description                                             | Sq. Ft. |
|-----------|----------|------------------|---------------------------------------------------------|---------|
|           | E.1      | CUSTOM M-50-RE   | Illuminated Monument Sign w/ Custom Stone-Clad Base     | 228.8   |
| West      | E.2      | LIF-WBO-24       | 24" White Channel Letters and Logo                      | 36.9    |
|           | E.3      | LIF-WBO-24       | 24" White Channel Letters and Logo                      | 36.9    |
| East      | E.4      | LIF-WBO-24       | 24" White Channel Letters and Logo                      | 36.9    |
|           | E.5      | NONE             | REMOVED FROM SCOPE OF WORK                              |         |
|           | E.6      | NONE             | REMOVED FROM SCOPE OF WORK                              |         |
|           | E.7      | NONE             | REMOVED FROM SCOPE OF WORK                              |         |
|           | E.8      | D-2-RE           | Non-Illuminated Directional Sign                        |         |
|           | E.9      | TC-P-ADA-IL-V-RE | Post Mount Handicap Stall Sign - Van Accessible         |         |
|           | E.10     | TC-P-ADA-IL-RE   | Post Mount Handicap Stall Sign                          |         |
|           | E.11     | TC-W-H-RE        | Wall Mount Do Not Enter Sign                            |         |
|           | E.12     | TC-W-H-RE        | Wall Mount Do Not Enter Sign                            |         |
| Northwest | E.13     | NONE             | REMOVED FROM SCOPE OF WORK                              |         |
|           | E.14     | DU-C             | ATM Lane Designator Sign                                |         |
|           | E.15     | DU-C             | ATM Lane Designator Sign                                |         |
|           | E.16     | TC-CL-W          | Clearance Sign                                          |         |
|           | E.17     | COMING SOON      | Temporary Construction Site Sign - Skid Mount Structure |         |
|           | E.18-20  | TC-W-A-RE        | Wall Mount Bank Parking Only Sign                       |         |
|           | E.21-28  | TC-P-A-RE        | Post Mount Bank Parking Only Sign                       |         |

Total Proposed Sq Ft 339.5



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**JP Morgan Chase Bank #730**

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Rayano  
Coordinator: Tracey Pichieri  
Designer: DDES  
Scale: AS NOTED

### CUSTOMER APPROVAL

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
COPY, COLORS & SIZES

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Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

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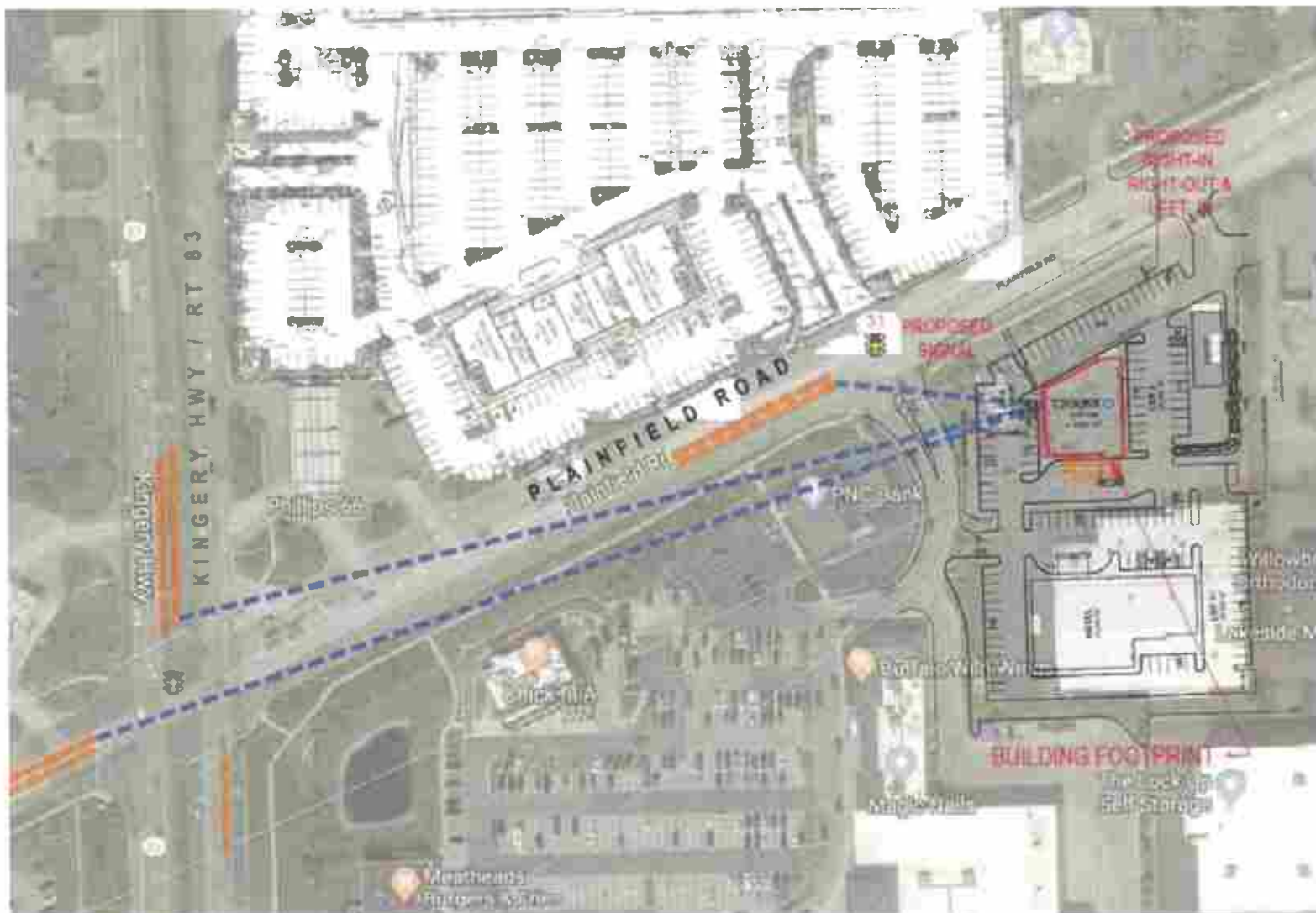
Project ID: CHASE\_730\_1

Revision: R13 - 01.19.21

Sign Legend / Site Plan

Page: 2 **DRC Page 2**





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**JP Morgan Chase Bank  
#730**

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tracey Pichierri  
Designer: [blank]  
Scale: As noted

**CUSTOMER APPROVAL**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

**COPY COLORS & SIZES**

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Enlarged Aerial Site

Page: 3 **DRC Page 3**



# INTERIOR SIGN LEGEND

| Elevation | Sign No.          | Sign Type                                              | Description | Sq.Ft. |
|-----------|-------------------|--------------------------------------------------------|-------------|--------|
| E.29      | SUR-TTW-U-4-TP    | Universal Thin-Profile ATM Surround                    |             |        |
| I.30      | SUR-TTW-U-4-TP    | Universal Thin-Profile ATM Surround                    |             | 32.95  |
| E.31      | ADA-EP            | ADA Accessible Entrance Plaque                         |             |        |
| E.32      | ADA-EP            | ADA Accessible Entrance Plaque                         |             |        |
| I.33      | ADA-EX            | ADA Exit Plaque                                        |             |        |
| I.34      | ADA-EX            | ADA Exit Plaque                                        |             |        |
| I.35      | ADA-EX            | ADA Exit Plaque                                        |             |        |
| I.36      | ADA-EX            | ADA Exit Plaque                                        |             |        |
| I.37      | ADA-TW            | ADA Accessible Teller Window Plaque                    |             |        |
| I.38      | OCT-21-INT        | Illuminated Bitro-Blue Interior Octagon                |             |        |
| I.39      | NONE              | REMOVED FROM SCOPE OF WORK                             |             |        |
| I.40      | NONE              | REMOVED FROM SCOPE OF WORK                             |             |        |
| I.41      | NONE              | REMOVED FROM SCOPE OF WORK                             |             |        |
| I.42      | ADA-RRW-A-G       | ADA Accessible Women's Restroom Plaque                 |             |        |
| I.43      | ADA-RRM-A-G       | ADA Accessible Men's Restroom Plaque                   |             |        |
| I.44      | ADA-EEX           | ADA Emergency Exit Plaque                              |             |        |
| I.45      | CUSTOM-OCT-24-INT | Illuminated Interior Blue Bitro Octagon - Ceiling-Hung |             |        |



FLOOR PLAN

SCALE: 1/16" = 1'-0"

| DATE     | DESIGNER |
|----------|----------|
| 10/21/19 | DG       |

| DESIGN STANDARDS         |    |
|--------------------------|----|
| CE 2020                  | 7B |
| OVERHEAD DOOR ACCEPTABLE |    |



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## JP Morgan Chase Bank #730

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tracey Pichart  
Designer: Jhona  
Scale: As noted

## CUSTOMER APPROVAL

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
COPY, COLORS & SIZES

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## Sign Legend / Floor Plan

Page: 4 **DRC Page 4**

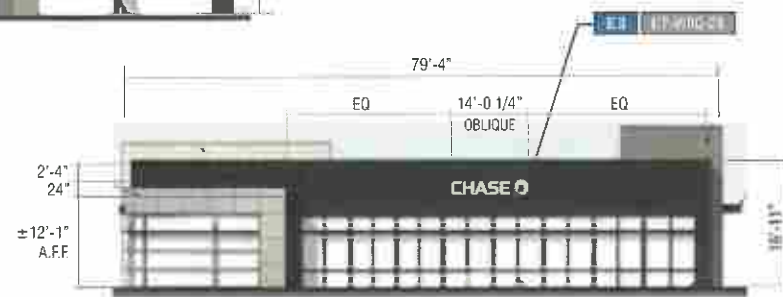


## SIGNAGE OVERVIEW - ALLOWED



WEST ELEVATION

SCALE: 1/16" = 1'-0"



NORTH ELEVATION

SCALE: 1/16" = 1'-0"



EAST ELEVATION

SCALE: 1/16" = 1'-0"



SOUTH ELEVATION

SCALE: 1/16" = 1'-0"



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### JP Morgan Chase Bank #730

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tanya Pichon  
Designer: bjones  
Scale: As noted

### CUSTOMER APPROVAL

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

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Project ID: CHASE\_730\_1

Revision: R13 - 01.19.21

### Exterior Elevations

Page: 5

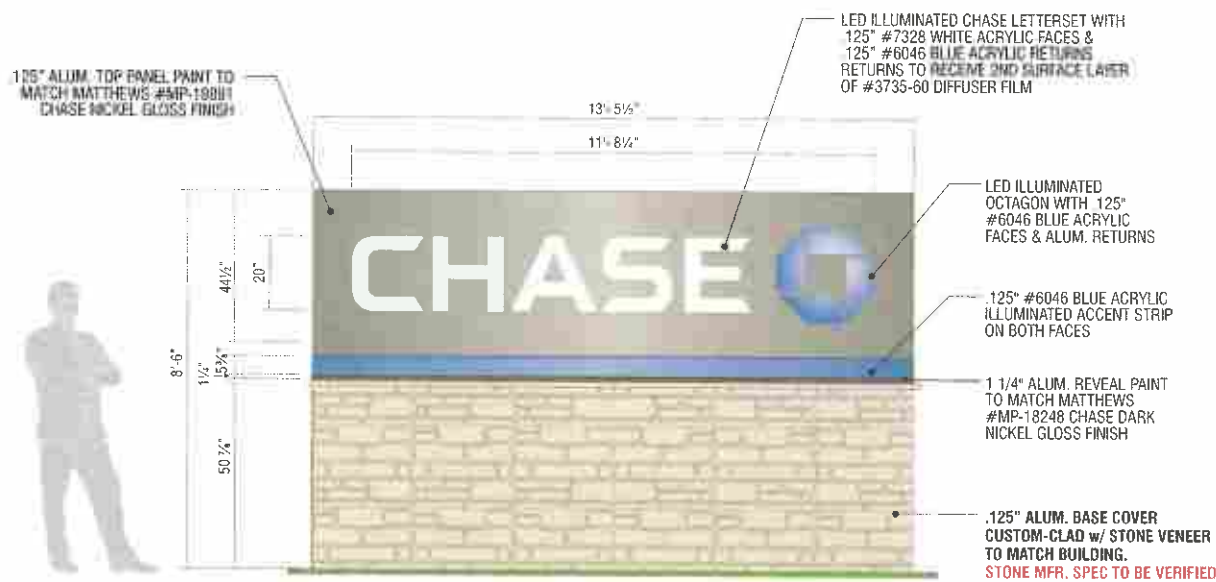
**DRC Page 5**



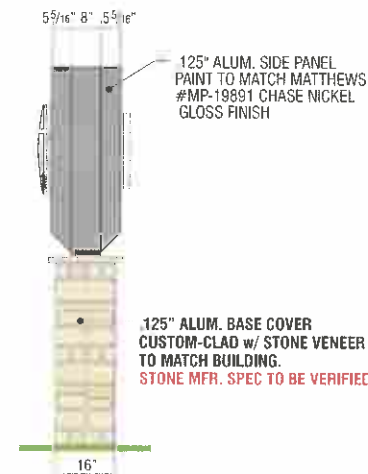
## SIGNAGE OVERVIEW



TOP VIEW SCALE: 3/8" = 1'-0"



FRONT VIEW SCALE: 3/8" = 1'-0"



END VIEW

**E.1** SIGN TYPE **FIELDSTONE-CLAD BASE TO MATCH BLDG. DIRECT BURIAL**  
**MANUFACTURE AND INSTALL ONE (1) INTERNALLY ILLUMINATED D/F MONUMENT SIGN**



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### JP Morgan Chase Bank #730

Willowbrook  
 730 Plainfield Rd.  
 Willowbrook, IL 60527

Initial Date: 09/14/18  
 Salesperson: Arturo Navarro  
 Coordinator: Tracey Eckhardt  
 Designer: Signtech  
 Scale: As noted

#### CUSTOMER APPROVAL

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
 COPY, COLORS & SIZES

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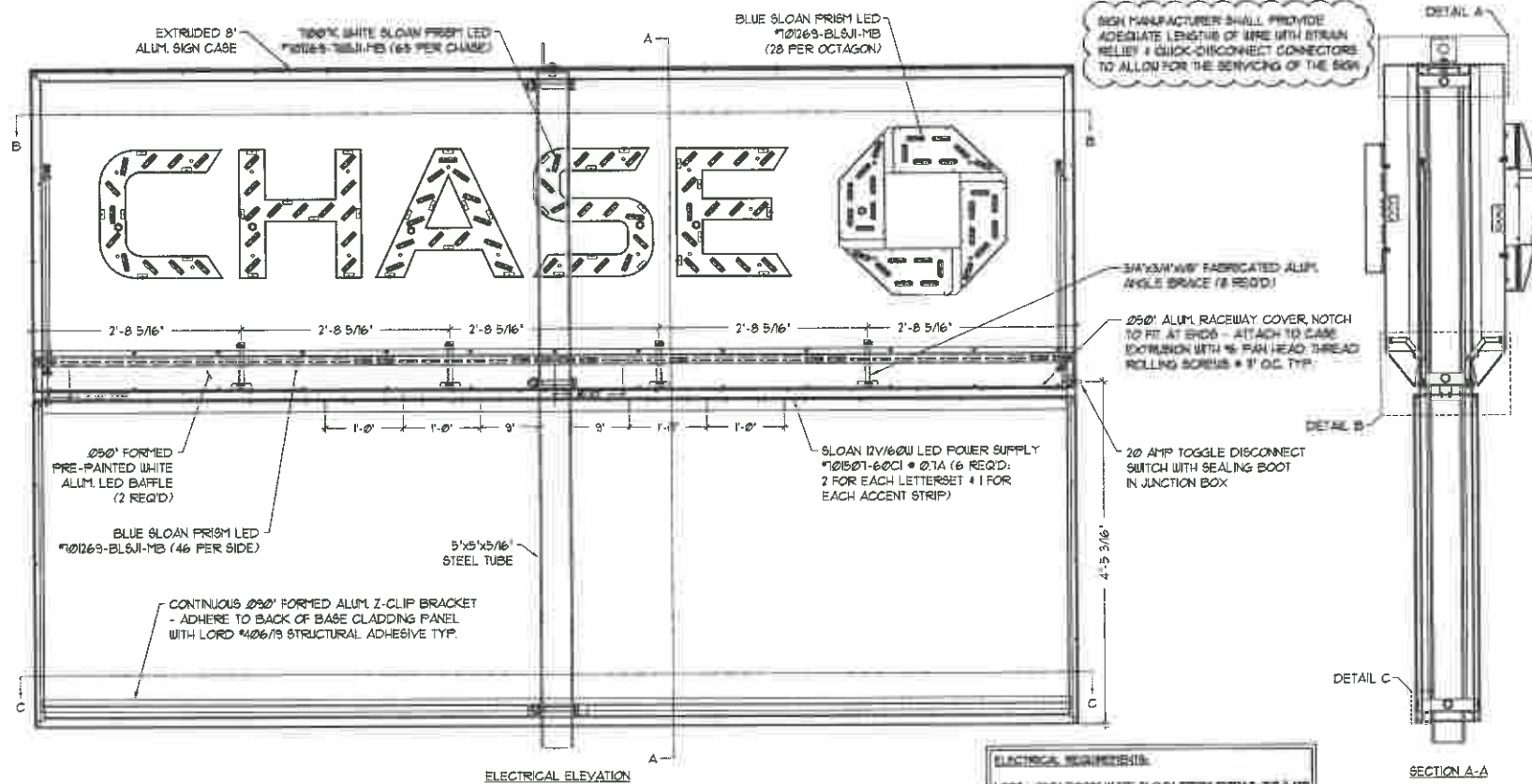
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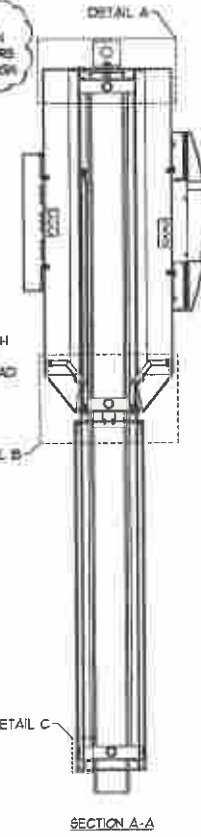
Project ID: CHASE\_730\_1

Revision: R13 - 01 19.21





**ELECTRICAL REQUIREMENTS:**  
 1. LEDS: (100%) 100% WHITE SLOAN PRISM 10269-BL6JI-MB  
 (100%) BLUE SLOAN PRISM 10269-BL6JI-MB  
 POWER SUPPLY: (167) SLOAN 12V/60W LEDSET-60C1 • 0.1A (6 REQ'D: 2 FOR EACH LETTERSET + 1 FOR EACH ACCENT STRIP)  
 TOTAL LOAD: 438 WATT  
 LENGTH: (1) 20 AMP REQ'D.



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**JP Morgan Chase Bank #730**

Willowbrook  
 730 Plainfield Rd.  
 Willowbrook, IL 60527

Initial Date: 09/14/18  
 Salesperson: Arthur Navarro  
 Coordinator: Tracey Pichiani  
 Designer: Bones  
 Signer: As noted

**CUSTOMER APPROVAL**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
 COPY, COLORS & SIZES

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Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

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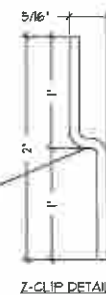
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Revision: R13 - 01.19.21

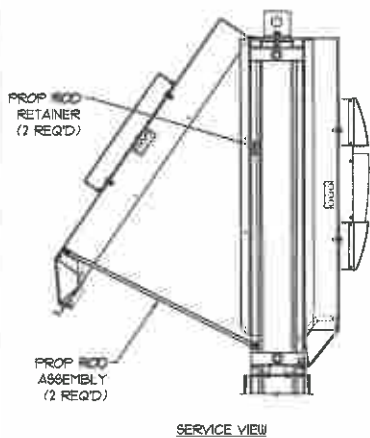
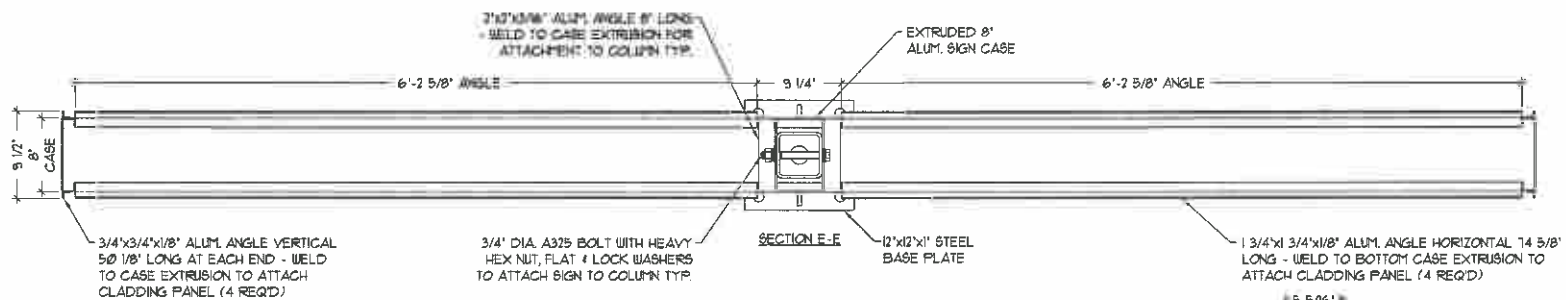




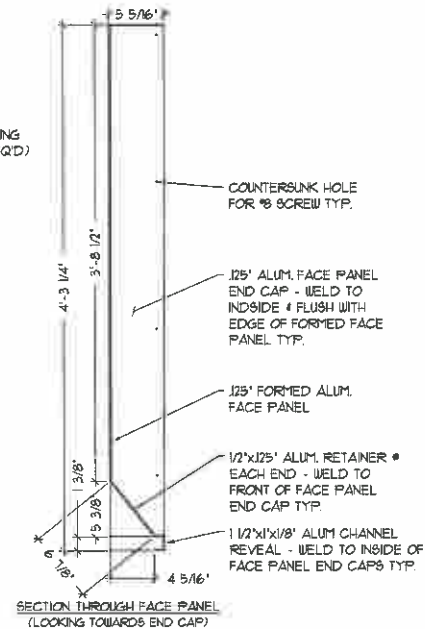
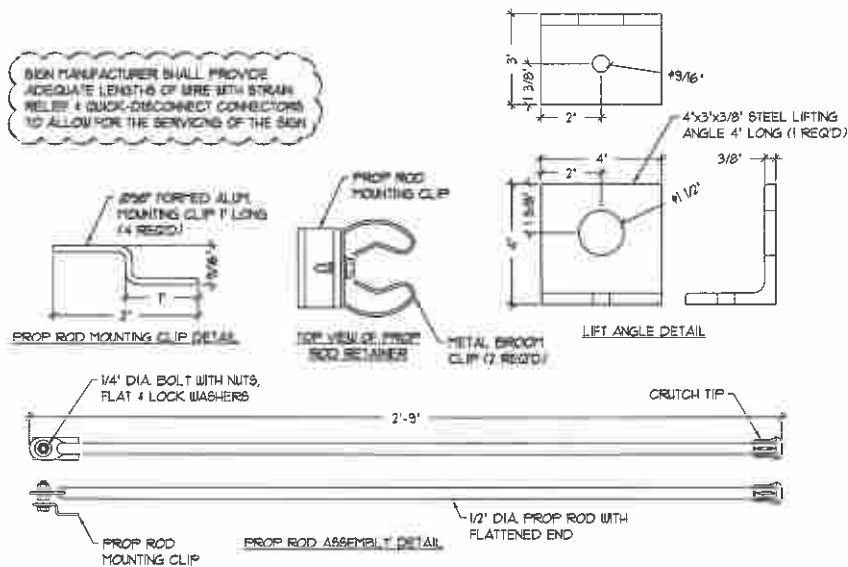








SIGN MANUFACTURER SHALL PROVIDE ADEQUATE LENGTHS OF WIRE WITH STRAIN RELIEF & QUICK-DISCONNECT CONNECTORS TO ALLOW FOR THE SERVICING OF THE SIGN



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### JP Morgan Chase Bank #730

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Puvion  
Coordinator: Tracy Puvion  
Designer: James  
Scale: As noted

### CUSTOMER APPROVAL

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
COPY, COLORS & SIZES

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Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

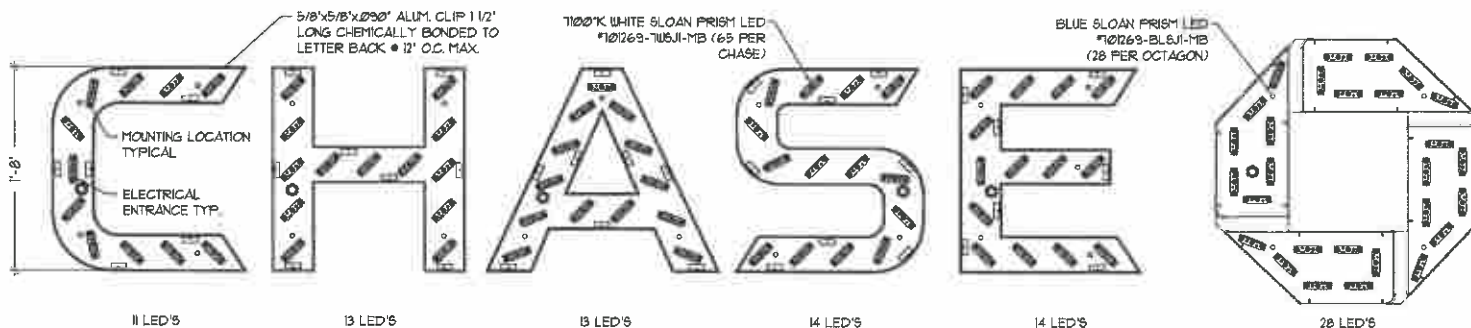
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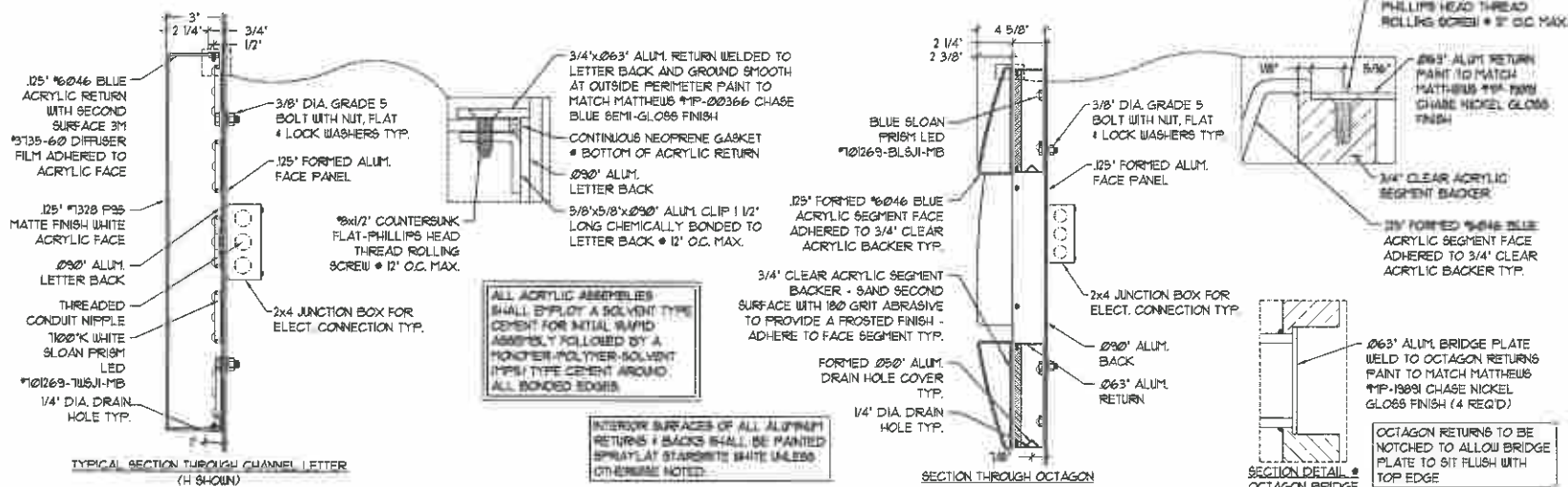
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LETTERSET LED LAYOUT



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### JP Morgan Chase Bank #730

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Timothy Plummer  
Designer: DJJ  
Scale: As noted

### CUSTOMER APPROVAL

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

**COPY, COLORS & SIZES**

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Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

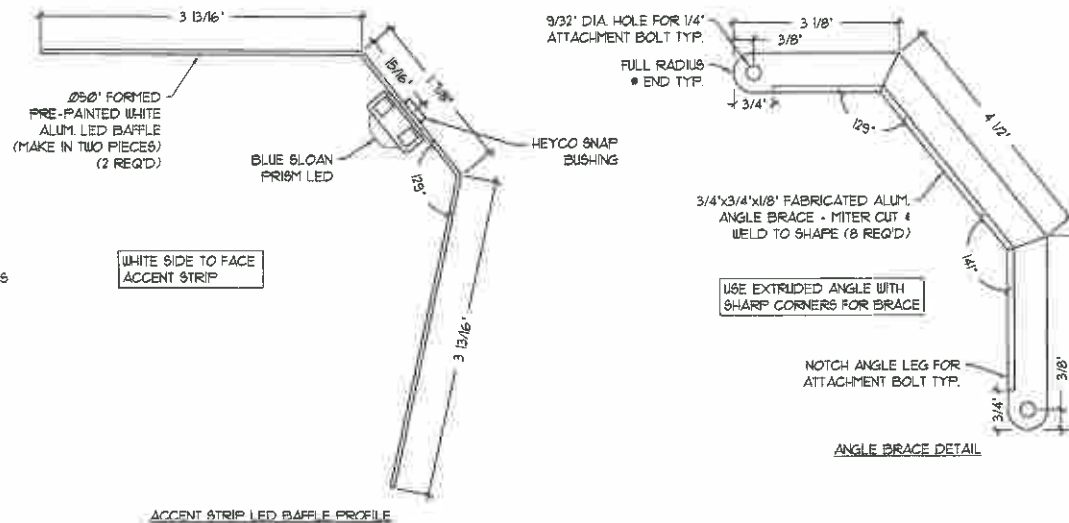
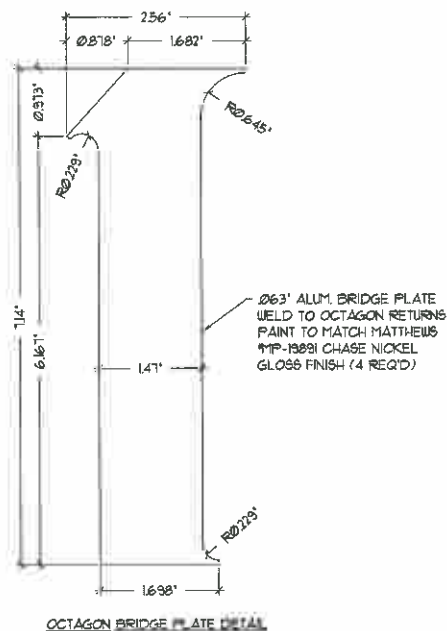
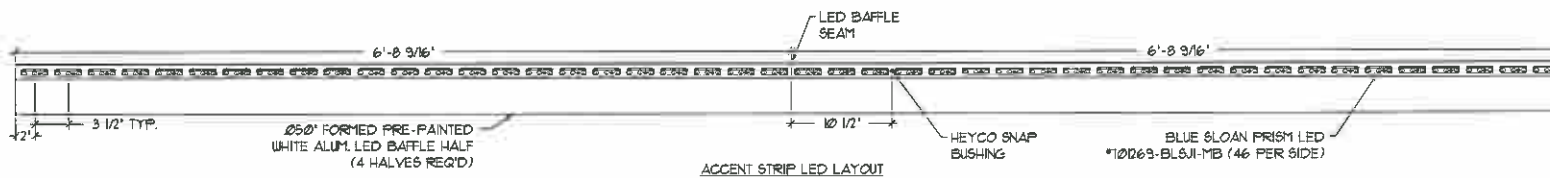
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### JP Morgan Chase Bank #730

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tracey Pichierni  
Designer: Signtech  
Scale: As noted

### CUSTOMER APPROVAL

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
COPY COLORS & SIZES

Signtech does NOT provide primary  
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**RESPONSIBILITY OF OTHERS!**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

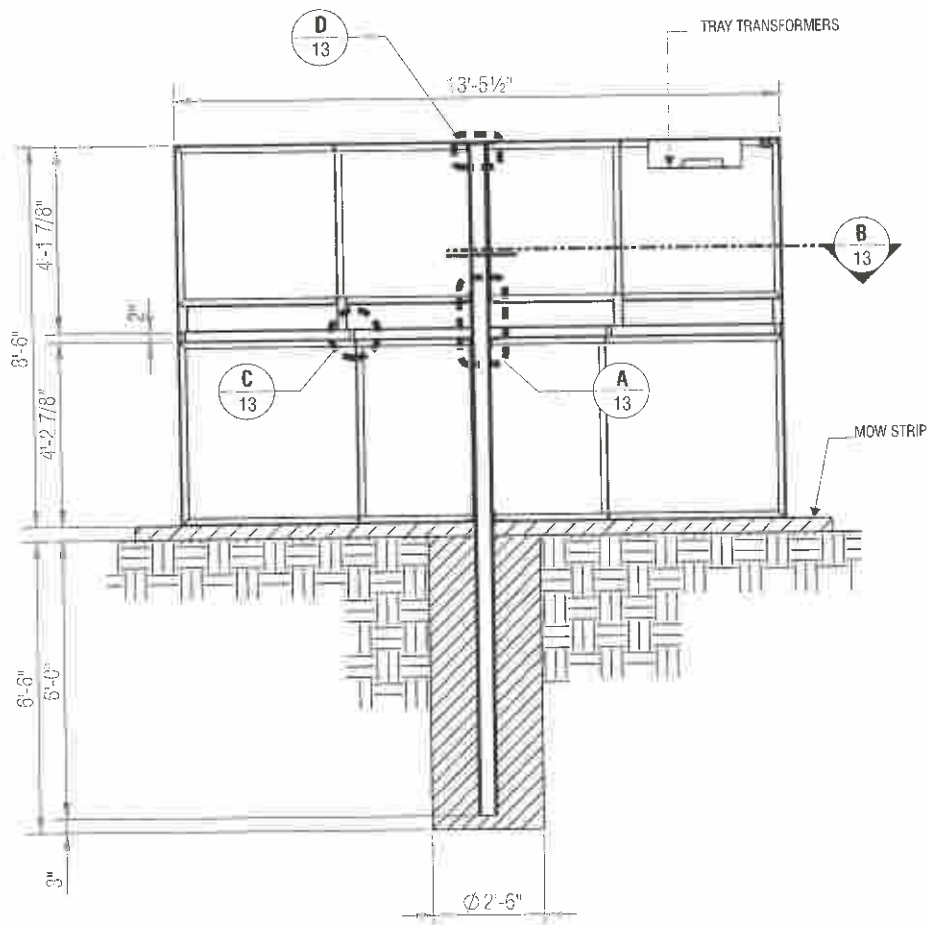
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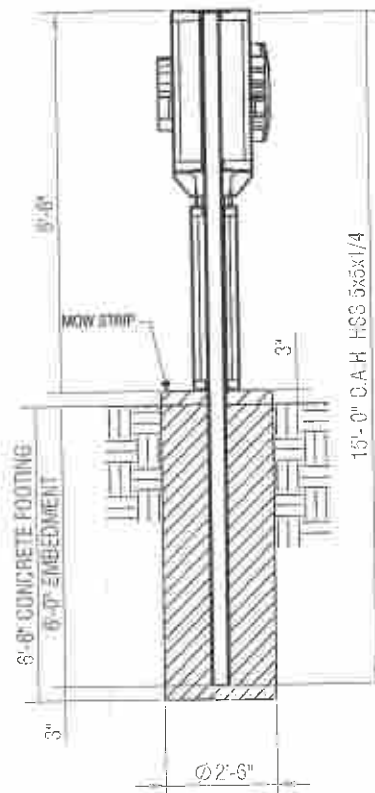
Revision: R13 - 01 19 21





**A-A** INSTALLATION LONGITUDINAL SECTION

Scale: 3/8" = 1'-0"



**B-B** VERTICAL SECTION

Scale: 3/8" = 1'-0"



**Signtech**

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**JP Morgan Chase Bank  
#730**

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tracey Pichieri  
Designer: None  
Scale: As noted

**CUSTOMER APPROVAL**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
COPY, COLORS & SIZES

Signtech does NOT provide primary  
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**RESPONSIBILITY OF OTHERS!**

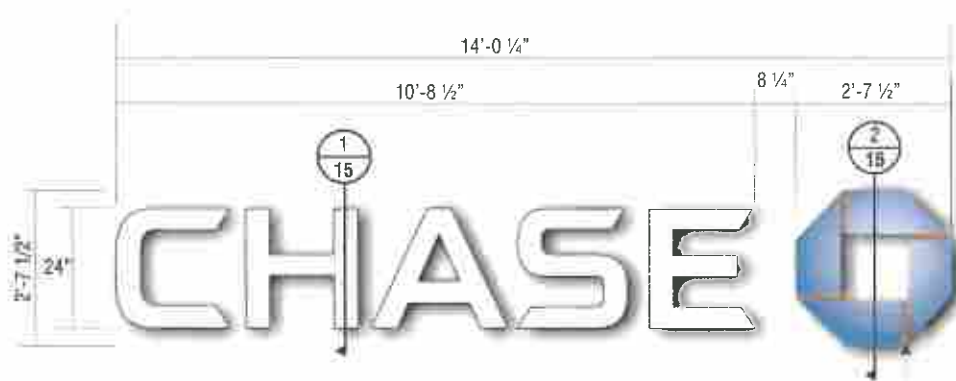
Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
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manner without prior written approval.

Drawing Number: 18-01613

Project ID: CHASE\_730\_1

Revision: R13 - 01.19.21





FRONT VIEW

SCALE : 1/2" = 1'-0"

PAINT OCTAGON BRIDGE  
TO MATCH WALL COLOR  
COLOR TBV



END VIEW  
OF LETTER  
SCALE : 1/2" = 1'-0"



END VIEW  
OF LOGO  
SCALE : 1/2" = 1'-0"



3D VIEW OF OCTAGON

ALL EXPOSED FASTENER HEADS  
SHALL BE PAINTED TO MATCH  
THE EXTERIOR SURFACE FINISH

**ELECTRICAL REQUIREMENTS FOR LED ILLUMINATION**  
As required by standard #12101, Signtech Electrical Manufacturing, Inc., exclusively uses secondary Ground-fault-Interrupted (GFI) transformers.  
Each Sign MUST have:  
- A dedicated branch circuit  
- Three wires: Line, Ground, and Neutral.  
- Wire Size: Min 12 GA THHN Copper Wire.  
NOTE: The gauge of the wire is dictated by the height of the run & ampacity as per NEC Article 444.  
- The ground wire must be continuous & go from the sign to the panelboard ground bus.

E.2 E.3 E.4

SIGN TYPE LIF-WBO-24

**MANUFACTURE AND INSTALL THREE (3) SET OF ILLUMINATED LETTERS & LOGO w/PLEX FACES & RETURNS**

**LETTERS:**

**FACE:** .177" ARISTECH #7328 P-95 WHITE PLEX (MATTE SIDE TO BE 1ST SURFACE) CHEM WELDED TO RETURNS  
**RETURNS:** .0118" x 4" DEEP ARISTECH #6046 BLUE WITH 3635-70 DIFFUSER VINYL ON 2ND (INTERIOR) SURFACE.  
**BACKS:** .080" ALUM BACK WITH 3/4" x .080" OUTSIDE ALUM RETURN AND 1/2" x 1/8" ALUM ANGLE CLIPS  
ON INTERIOR OF LETTER FOR FACE ATTACHMENT

**PAINT:** EXTERIOR PAINTED MATTHEWS MP-00366 CHASE BLUE SEMI-GLOSS FINISH (60-70 UNITS) AND INTERIOR  
PAINTED SPRAY-LAT STAR BRITE WHITE LIGHT ENHANCEMENT PAINT.

**ILLUMINATION:** SLOAN PRISM 7100K #701269-7WSJ1-MB WHITE LEDs AND SLOAN POWER SUPPLY  
REQUIRED FOR PROPER EVEN ILLUMINATION.

**LOGO:**

**BACKS:** .080" ALUM BACK WITH STACK WELDED 4" x .080" ALUM RETURNS.

**LOGO CAN FACE:** .080" ALUM BRIDGE ELEMENTS WELDED TO INSIDE & OUTSIDE RETURNS

**PLEX FACE:** .118" THERMO FORMED ARISTECH #6046 BLUE LOGO ELEMENTS CHEM WELDED TO  
3/4" CLEAR PLEX 2ND SURFACE FACES.

**PAINT:** EXTERIOR PAINT MATTHEWS "CHASE NICKEL" #MP-19891 GLOSS FINISH AND INTERIOR PAINTED  
SPRAY-LAT STAR BRITE WHITE LIGHT ENHANCEMENT PAINT.

**ILLUMINATION:** SLOAN PRISM #701269-BLSJ1-MB BLUE LEDs AND SLOAN POWER SUPPLY AS  
REQUIRED FOR PROPER EVEN ILLUMINATION



**Signtech**

4444 Federal Blvd, San Diego CA 92102  
Phone: (619) 527-6100 / Fax: (619) 527-6111  
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**JP Morgan Chase Bank  
#730**

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tracey Pichierri  
Designer: J. J. J.  
Scale: As noted

**CUSTOMER APPROVAL**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
COPY, COLORS & SIZES

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Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
This sign is to be installed in accordance with the sign specifications and standards of the International Sign Association (ISA) and the International Sign Association (ISA) and the International Sign Association (ISA) and the International Sign Association (ISA).

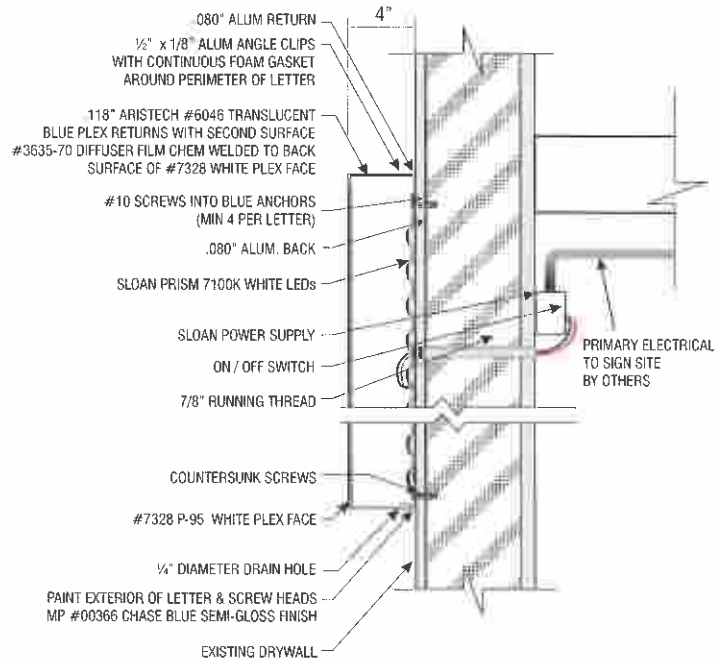
Drawing Number: 18-01613

Project ID: CHASE\_730\_1

Revision: R13 - 01.19.21



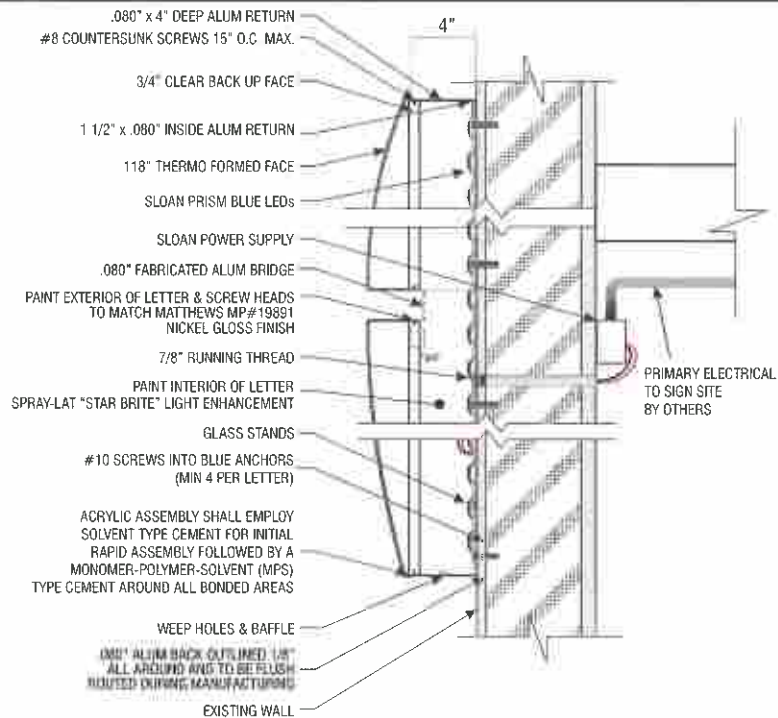
**ATTACHMENT METHOD TO BE VERIFIED BY INSTALLER  
AT THE FIELD DEPENDING ON WALL TYPE AND ACCESS.**



1  
15

**PLEX LETTER SECTION DETAIL**

**SCALE: 1 1/2" = 1'-0"**



2  
15

**LOGO SECTION DETAIL**

**SCALE: 1 1/2" = 1'-0"**



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Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tracey Pichierri  
Designer: [Signature]  
Scale: As noted

**CUSTOMER APPROVAL**

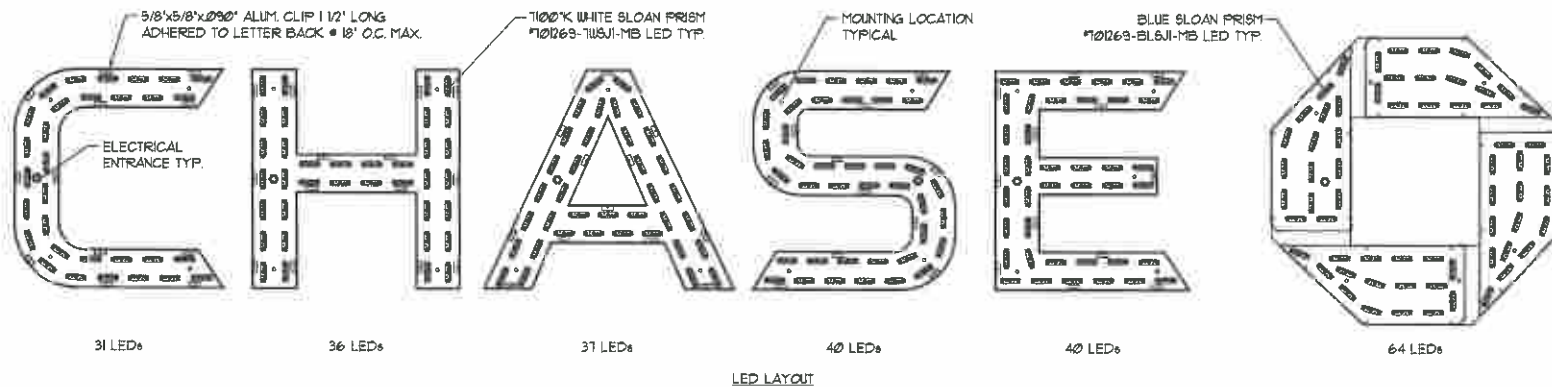
Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
**COPY, COLORS & SIZES**

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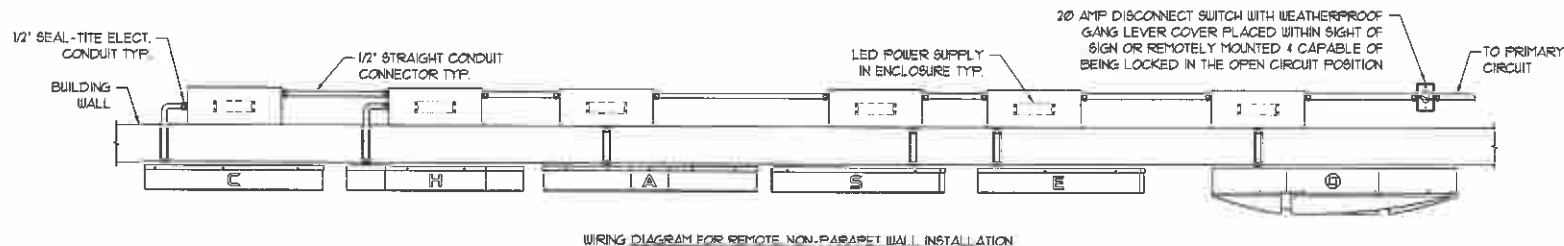


**ELECTRICAL REQUIREMENTS:**

LED's: (184) 1100°K WHITE SLOAN PRISM #101269-T1W5J1-MB  
 (64) BLUE SLOAN PRISM #101269-B1B5J1-MB  
 POWER SUPPLY: (6) SLOAN 6001 60W #101501-60C1 • 0.7A  
 TOTAL LOAD: 420A • 120VAC  
 CIRCUITS: (1) 20 AMP REQUIRED

ALL PRIMARY & SECONDARY  
 WIRING MUST BE GROUNDED  
 PER ALL APPLICABLE UL AND  
 NEC GUIDELINES

SEE SHEET 1 OF 1 FOR PREFERRED DISCONNECT  
 SWITCH LOCATION FOR REMOTE, NON-PARAPET  
 WALL INSTALLATION



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 Coordinator: Tracy Pichini  
 Designer: James  
 Scale: As noted

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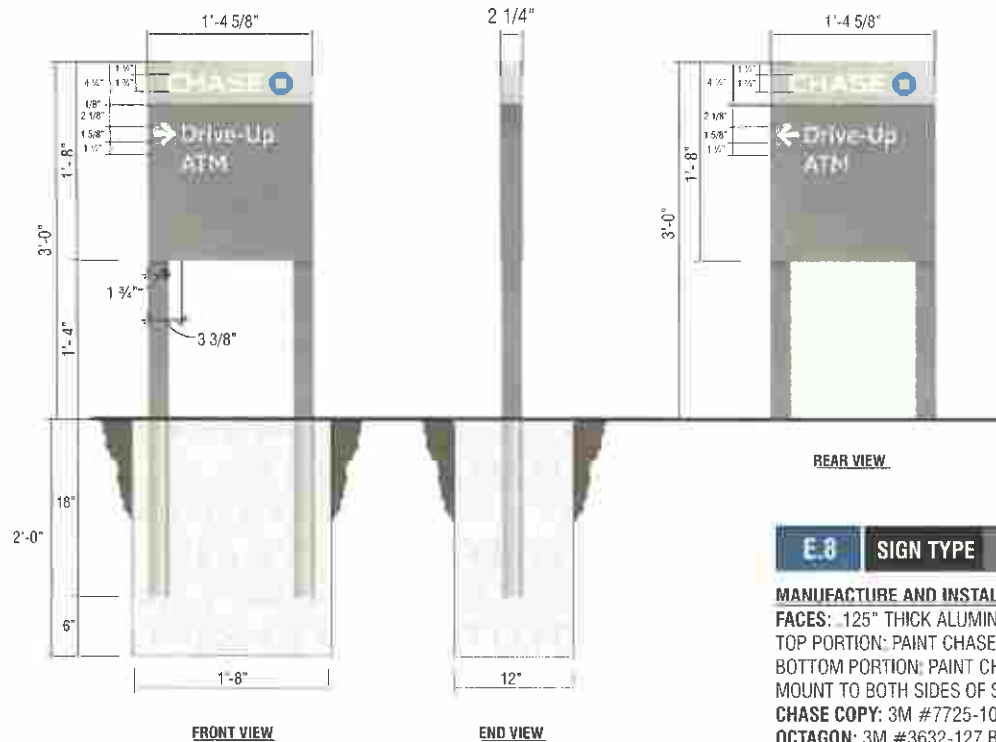
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 Revision: R13 - 01.19.21





TOP VIEW SCALE: 1"=1'-0"



**MANUFACTURE AND INSTALL ONE (1) NON-ILLUMINATED DIRECTIONAL SIGN**

**FACES:** .125" THICK ALUMINUM FACES, QUARTER ROUND EDGES.  
**TOP PORTION:** PAINT CHASE NICKEL #19891, GLOSS FINISH.  
**BOTTOM PORTION:** PAINT CHASE DARK NICKEL #18248, GLOSS FINISH.  
**MOUNT TO BOTH SIDES OF SQUARE TUBE POSTS.**  
**CHASE COPY:** 3M #7725-10 WHITE VINYL.  
**OCTAGON:** 3M #3632-127 BLUE OVER #7725-10 WHITE.  
**SECONDARY COPY:** TO BE 3M SCOTCHLITE #680-10 REFLECTIVE WHITE VINYL.  
**POSTS:** 2" X 2" X .125" ALUMINUM RECTANGLE TUBE POSTS,  
**TOP PORTION:** CONTINUOUS BAND TO BE PAINTED NICKEL #19891, GLOSS FINISH.  
**BOTTOM PORTION:** PAINT CHASE DARK NICKEL #18248, GLOSS FINISH.  
**TOP CAP:** .125" ALUMINUM PAINTED CHASE NICKEL #19891, GLOSS FINISH.  
**BASE:** .125" SHEARED 1/4" ROUNDED ALL EDGES WELDED TO POST, PAINTED CHASE DARK NICKEL #18248 GLOSS FINISH.



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 730 Plainfield Rd.  
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Initial Date: 09/14/18  
 Designer: Arthur Navarro  
 Coordinator: Tracey Pichieri  
 Designer: DODDS  
 Scale: As noted

**CUSTOMER APPROVAL**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
 COPY, COLORS & SIZES

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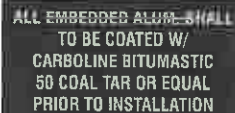
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Project ID: CHASE\_730\_1

Revision: R13 - 01.19.21

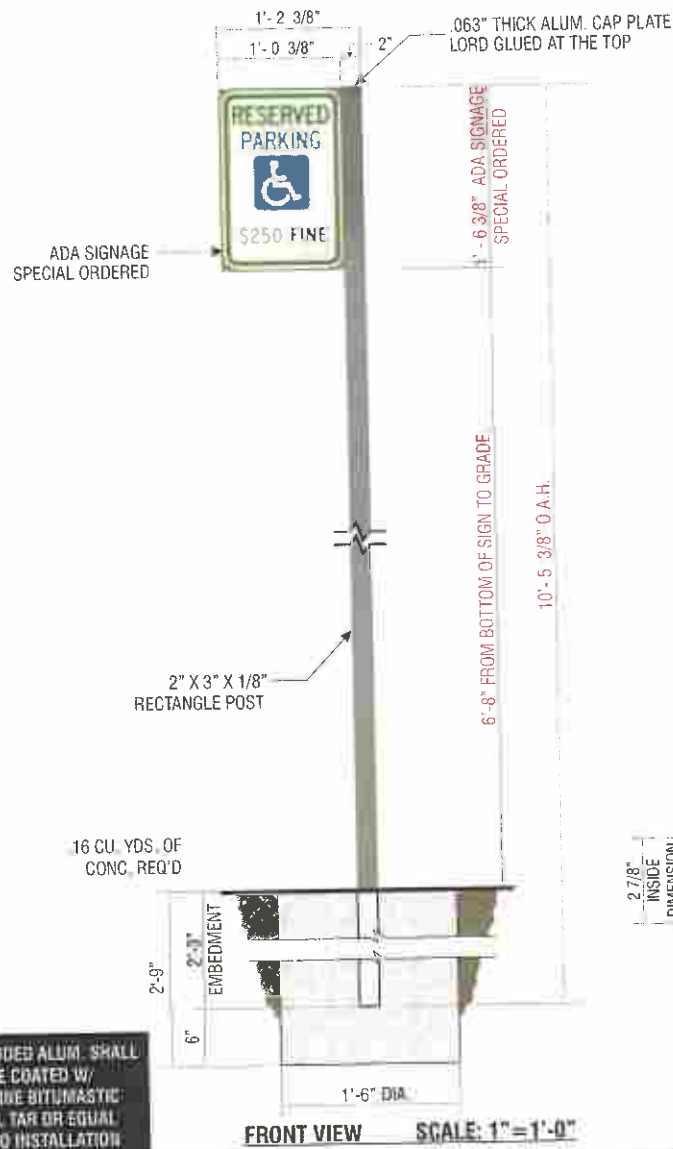




**A HORIZONTAL SECTION**  
**SCALE: 3" = 1'-0"**







**E.10**

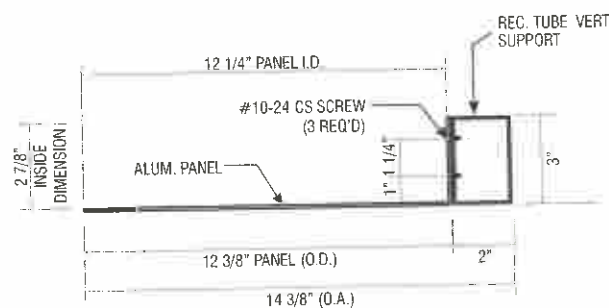
**SIGN TYPE** TC-P-ADA-IL-RE

**MANUFACTURE AND INSTALL ONE (1) N-1 POLE MOUNTED PARKING SIGN**

1. 125" BRAKE FORMED ALUMINUM SIGN FACE.  
PAINT FINISH ALL EXPOSED SURFACES DARK NICKEL  
MATTHEWS MP #18248, GLOSS FINISH (85-90 UNITS)  
FORM AS SHOWN.
2. SIGN FACE TO BE ORDERED BY "COMPLIANCE SIGNS" 800.578.1245  
FACE TO BE LORDS GLUE TO 125" BRAKE FORM ALUM. SIGN FACE
3. 2" X 3" X 1/8" X 10'-5 3/8" LONG EXTRUDED ALUM. TRIANGULAR POST  
W/ .063" THICK ALUM. CAP PLATE @ TOP  
PAINT FINISH ALL EXPOSED SURFACES TO MATCH  
DARK NICKEL MATTHEWS MP #18248, GLOSS FINISH (60-70 UNITS).  
SET SUPPORT INTO CONCRETE FOUNDATION.  
NO DIRECT EARTH BURIAL ALLOWED.



ALL FASTENERS SHALL BE  
STAINLESS STEEL OR OTHERWISE  
COATED TO PREVENT CORROSION.



**A** HORIZONTAL SECTION  
SCALE: 3"=1'-0"



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**JP Morgan Chase Bank  
#730**

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Novato  
Coordinator: Tracey Piccini  
Designer: biondes  
Scale: As noted

**CUSTOMER APPROVAL**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
**COPY, COLORS & SIZES**

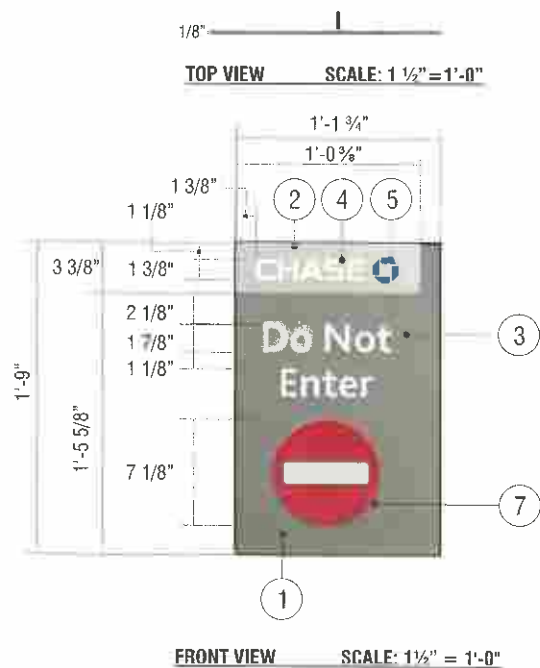
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Page: 19 **DRC Page 19**





**E.11 E.12**

**SIGN TYPE TC-W-H-RE**

**MANUFACTURE AND INSTALL TWO (2) NON-ILLUMINATED PARKING SIGNS**

1. 125" ALUMINUM SIGN FACE. PAINT FINISH ALL EXPOSED SURFACES DK NICKEL. MATTHEWS MP#18248, GLOSS FINISH. FORM AS SHOWN.
2. TOP BAND TO BE FIRST SURFACE APPLIED 3M #180C-220 LIGHT SILVER METALLIC VINYL.
3. SUPPORT COPY TO BE 3M SCOTCHLITE #680-10 WHITE.
4. "CHASE" TO BE 3M FILM #7725-10 WHITE.
5. OCTAGON TO BE 3M FILM #3632-127 BLUE OVER 3M FILM #7725-10 WHITE.
6. PROVIDE NELSON STUDS OFF BACK SIDE OF ALUM. TO ANCHOR TO WALL SURFACE
7. FIRST SURFACE DECORATE "DO NOT ENTER" SYMBOL WITH 3M SCOTCHLITE REFLECTIVE FILM #680-72 RED. HORIZONTAL BAR TO BE 3M SCOTCHLITE FILM #680-10 WHITE.



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**JP Morgan Chase Bank #730**

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tracey Pluhart  
Designer: bjones  
Scale: As noted

**CUSTOMER APPROVAL**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
COPY, COLORS & SIZES

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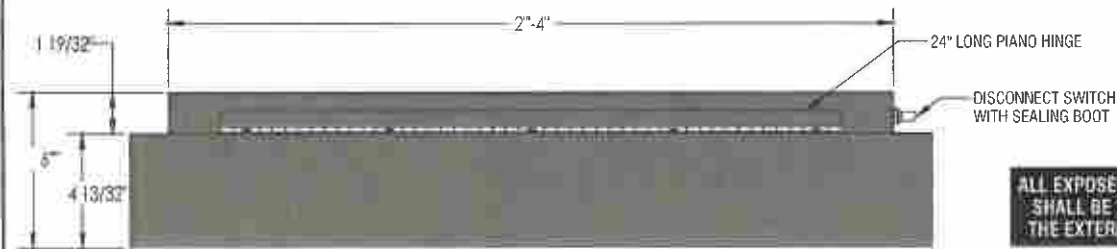
Drawing Number: 18-01613

Project ID: CHASE\_730\_1

Revision: R13 - 01 19.21



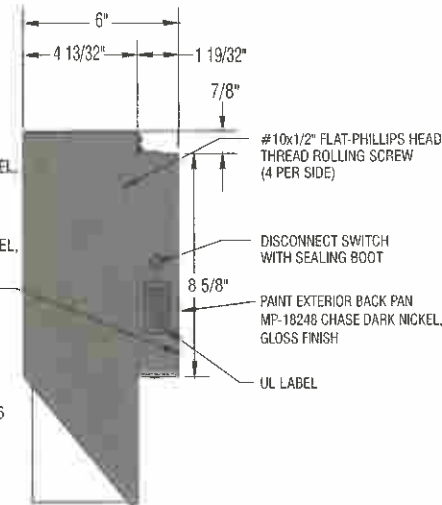
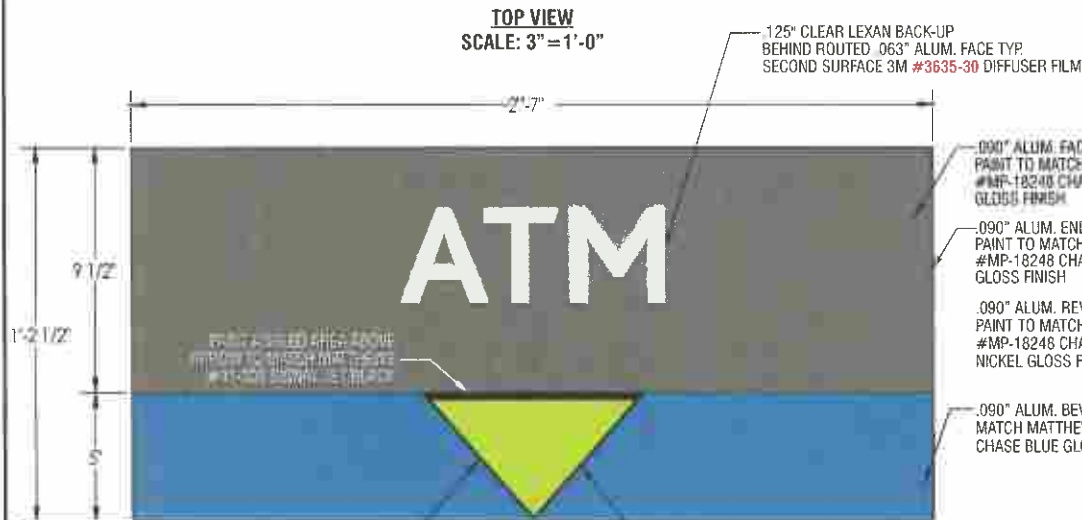
# ADD WEEP HOLES TO THE SIGN



**LANE INDICATOR ILLUMINATION GUIDELINES**  
 WHITE COPY (ATM)  
 ILLUMINATES 24 HOURS A DAY 7 DAYS A WEEK  
 ARROW  
 ILLUMINATES 24 HOURS A DAY, 5 DAYS A WEEK,  
 BRANCH TO SWITCH BETWEEN RED & GREEN  
 AS NECESSARY

**ALL EXPOSED FASTENERS HEADS  
 SHALL BE PAINTED TO MATCH  
 THE EXTERIOR CABINET FINISH**

**INTERIOR SURFACES SHALL BE  
 PAINTED SPRAYLAC STARBRITE  
 WHITE UNLESS OTHERWISE NOTED**



**FRONT VIEW  
 SCALE: 3"=1'-0"**

**END VIEW  
 SCALE: 3"=1'-0"**

**125" #2046 OPTIX LD WHITE PIGMENTED ACRYLIC LENS  
 w/ FIRST SURFACE 3M #1835-222 BLACK PERFORMED VINYL**

**ALL EXPOSED PAINTED SURFACES SHALL BE COATED  
 WITH MATTHEWS #282-208SP VOC GLOSS CLEAR, WITH  
 MINIMUM 2 MILS DRY FILM THICKNESS (DFT) PER  
 MATTHEWS APPLICATION SPECIFICATIONS.**

.090" ALUM. ARROW HOUSING  
 PAINTED TO MATCH MATTHEWS  
 #MP-18248 CHASE DARK NICKEL  
 GLOSS FINISH ON THE OUTSIDE  
 AND MATTHEWS #MP-411-306  
 SIGNAL JET BLACK INSIDE  
 OR DECORATED WITH 3M 3635-22  
 BLACK VINYL WITH A TOP COAT OF  
 MATTHEWS #282-208SP VOC GLOSS CLEAR

## NOTES:

- Design is based on 90 mph 3 sec. gust design wind speed per IBC 2006. Exposure C. Refer to structural drawings.
- Aluminum shapes be extruded from 6061-T6 alloy. Aluminum sheet shall be 3003-H14 alloy. Aluminum plate shall be 5052-H34 alloy.
- Structural bolts shall be zinc coated A325 unless otherwise noted. All other fasteners shall be stainless steel or otherwise coated to prevent corrosion.
- Welds shall be made a 4000 series for aluminum by persons qualified in accordance with AWS standards within the past two years.
- This design is prototypical and should not be used for site specific applications unless deemed suitable by a competent Professional Engineer.

**E.14 E.15 SIGN TYPE DU-C**

**MANUFACTURE & INSTALL TWO (2) INTERNALLY-ILLUMINATED WALL SIGNS w/ ROUT-OUT FACES**

**10/2/18** **TRR K**



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 Designer: TSCM  
 Scale: As noted

## CUSTOMER APPROVAL

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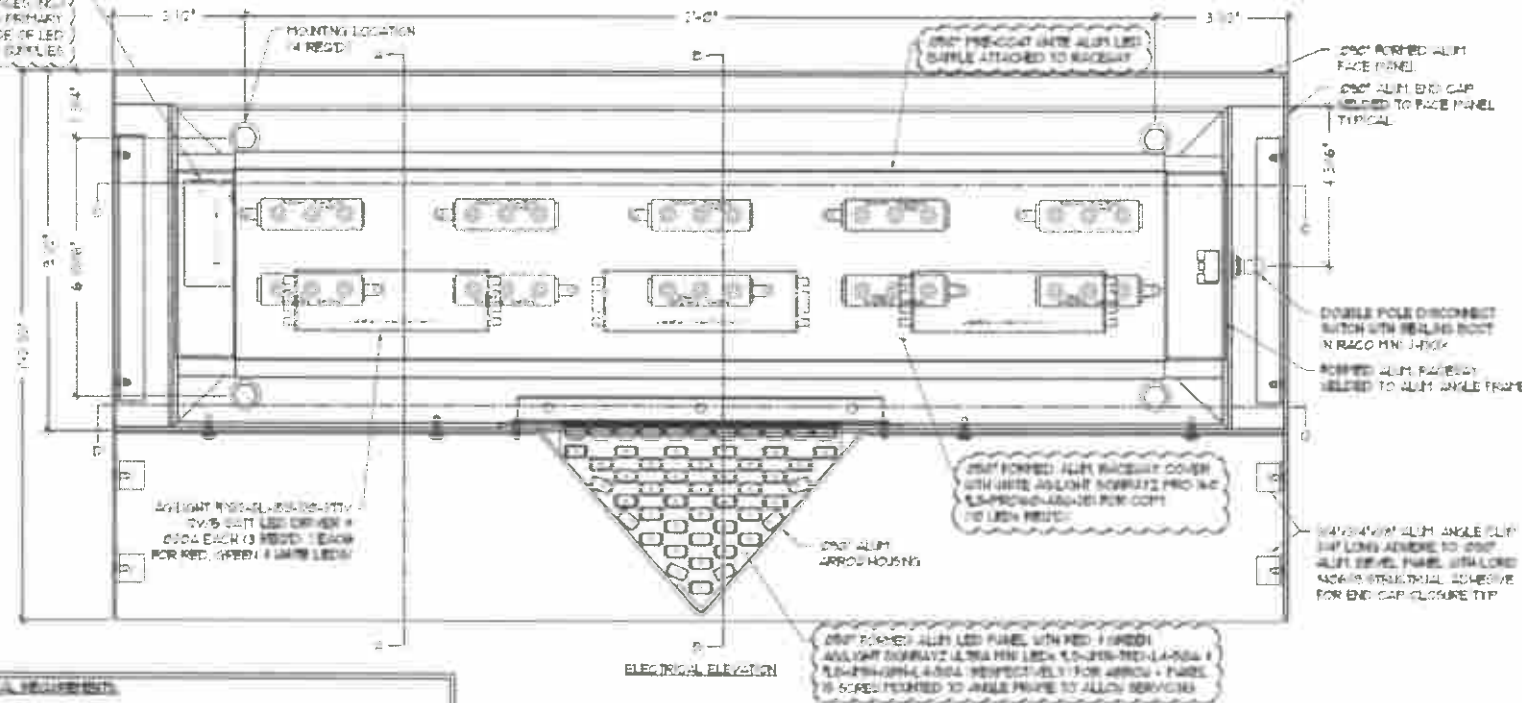
Revision: R13-01.19.21







REPLACE 4-NA RELAY  
BY FUNCTIONAL  
DEVICE IN  
USE TO PRIMARY  
SIDE OF LED  
POWER SUPPLY



**ELECTRICAL REQUIREMENTS:**  
LED4 COPY: (1) WHITE ASLIGHT NOVAFLITE PRO HD 15-PRO-440-01A FOR 15-PRO-440-01A NOVAFLITE PRO HD 15-PRO-440-01A  
LED4 ARROW: (1) WHITE ASLIGHT NOVAFLITE PRO HD 15-PRO-440-01A FOR 15-PRO-440-01A NOVAFLITE PRO HD 15-PRO-440-01A  
POWER SUPPLY: (1) ASLIGHT PRO HD 15-PRO-440-01A & 15-PRO-440-01A  
TOTAL LOAD: 600 WATT  
CIRCUIT: 120V 60 HZ

ALL PRIMARY & SECONDARY WIRING & ELECTRICAL CONNECTIONS SHALL BE IN ACCORDANCE WITH ALL NEC & FEDERAL, STATE & LOCAL CODES

INTERIOR SURFACES SHALL BE PAINTED SPRAY-ON STAINLESS STEEL UNLESS OTHERWISE NOTED



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MEMBER  
NAB CESA  
PROMOTING INNOVATION

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Willowbrook  
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Designer: James  
Scale: AS NOTED

**CUSTOMER APPROVAL**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
COPY, COLORS & SIZES

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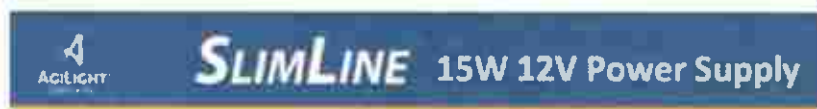
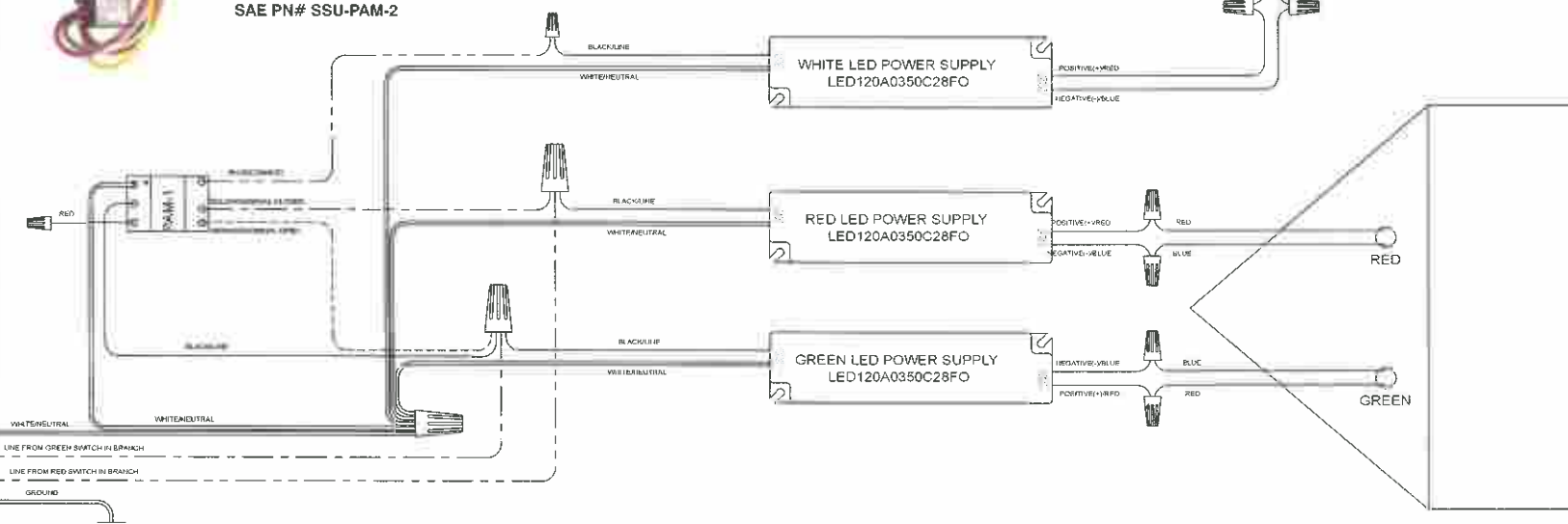


## LANE DESIGNATOR WIRING DIAGRAM

Q= 21505  
L= 249



SSU-PAM-2 RELAY PROVIDES 7.0 AMP FORM C CONTACTS. THE RELAY MAY BE ENERGIZED BY ONE OF TWO INPUT VOLTAGES: 12VDC OR 24VDC. THE INPUT VOLTAGES ARE POLARITY SENSITIVE AND DIODE PROTECTED. A RED LED IS PROVIDED WHICH, WHEN ILLUMINATED, INDICATES THE RELAY COIL IS ENERGIZED.  
SAE PN# SSU-PAM-2



Product information

Part Number PS12-SL-15W-12V-277V

## ELECTRICAL REQUIREMENTS

**LEDS COPY:** (1) AGILIGHT LED PANEL PRE-POPULATED WITH WHITE LEDS FOR "ATM", "BUSINESS", & "DRIVE-UP"

**LED ARROW:** (1) AGILIGHT LED PANEL PRE-POPULATED WITH RED & GREEN LEDS

**POWER SUPPLY:** (3) AGILIGHT SLIM-LINE PS12-SL-15W-120-277V @ 0.20A

**RELAY:** (1) PAM-1 RELAY @ 0.015 AMPS PER POSITION

**SWITCH:** (1) TOGGLE SWITCH

**TOTAL LOAD:** 2.1 AMPS

**CIRCUITS:** (1) 20 AMP REQ'D.



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### CUSTOMER APPROVAL

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
COPY COLORS & SIZES

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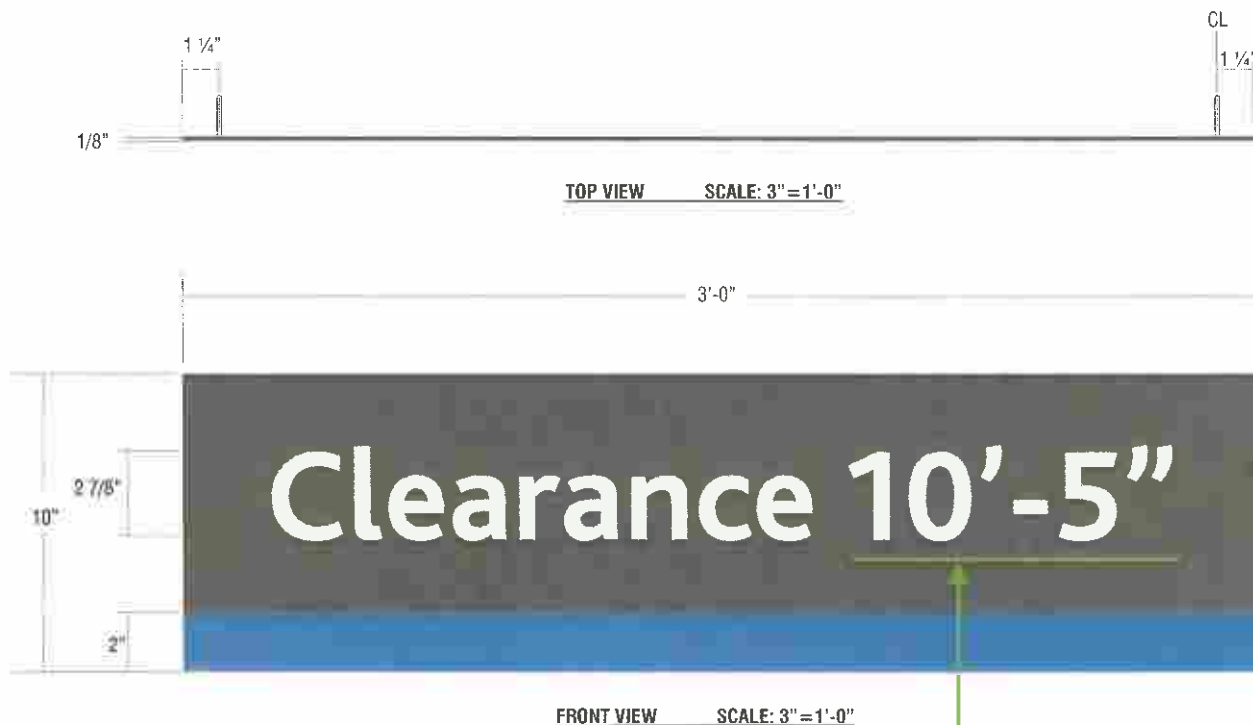
Drawing Number: 18-01613

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Revision: R13 - 01.19.21

Page: 24 **DRC Page 24**





**VERIFY HEIGHT DIMENSION  
PER SITE SURVEY**

**E.16**

**SIGN TYPE TC-CL-W**

**MANUFACTURE AND INSTALL ONE (1) NON-ILLUMINATED S/E CLEARANCE SIGN**

**FACE:** 125" ALUMINUM PAINTED CHASE DARK NICKEL MATTHEWS MP#18248, GLOSS FINISH.  
PAINT ACCENT BAND ACROSS BOTTOM CHASE BLUE MP#00366 BLUE (OR PMS #300 BLUE).

**"CLEARANCE":** COPY TO BE 3M SCOTCHLITE #680-10 WHITE.

**INSTALL:** PROVIDE NELSON STUDS OFF BACK SIDE OF ALUM. TO ANCHOR TO WALL SURFACE



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Designer: [Signature]  
Scale: As noted

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Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

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Drawing Number: 18-01613

Project ID: CHASE\_730\_1

Revision: R13 - 01.19.21





PAINT POLES CHASE DARK NICKEL #MP18248

SAND BAG

2X4 WOOD BASE

2X4 WOOD BRACES

2X4 WOOD BRACES

2X4 WOOD POST

E.17

SIGN TYPE CHA-TEMP-CS-48X96-SKID

#### MANUFACTURE ONE (1) D/F CONSTRUCTION / TEMPORARY SITE SIGN

**FACE:** 4' x 8' x 1/2" MDO PLYWOOD, PRIMED AND PAINTED CHASE DARK NICKEL #MP18248  
OVERLAY WITH DIGITALLY PRINTED GRAPHIC.

**FRAME:** 2" x 4" D.F. WOOD FRAMEWORK WITH 3/8" DEEP 9/16" DADO GROOVES FOR  
FOR FACE TO LOCK INTO. SEE DRAWING ON FOLLOWING PAGES  
FOR WHICH PARTS. PAINT CHASE DARK NICKEL #MP18248

**FRAME ATTACHMENT:** USE 3/8" DIA. S.S. MACHINE BOLT FOR BASE ATTACHMENT  
USE LAG SCREWS TO ATTACH FRAMING TOGETHER.

**SAND BAGS:** (4) ± 40LB SAND BAGS AT EACH BASE CORNER AS SHOWN  
SIGN INSTALLER TO SUPPLY SAND BAGS.

ARTWORK PATH: P:\Design\2K20\Chase Bank\Typicals\COMING SOON - WE'RE OPEN Construction Sign\Final CSS 8x4.cdr



**Signtech**

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#### JP Morgan Chase Bank #730

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tracey Pichler  
Designer: JIMMIE  
Scale: As noted

#### CUSTOMER APPROVAL

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
COPY, COLORS & SIZES

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electrical to sign location -  
**RESPONSIBILITY OF OTHERS!**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

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Drawing Number: 18-01613

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Revision: R13 - 01.19.21

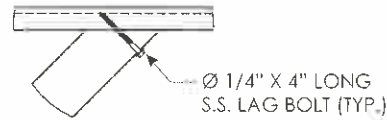


| ITEM NO. | QTY. | DESCRIPTION | LENGTH |
|----------|------|-------------|--------|
| 1        | 4    | 2x4         | 84     |
| 2        | 2    | 2x4         | 84     |
| 3        | 2    | 2x4         | 95.25  |
| 4        | 2    | 2x4 R       | 42.78  |
| 5        | 4    | 2x4 R       | 47.73  |

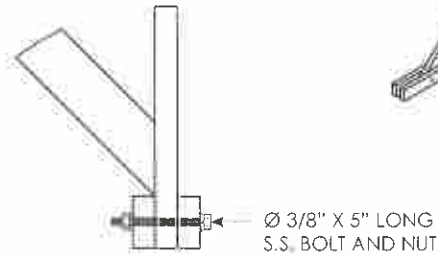
**SIGN TYPE** COMING SOON TEMP SIGN



SECTION A-A

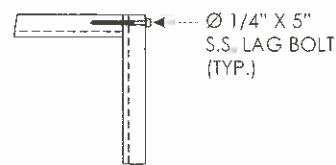
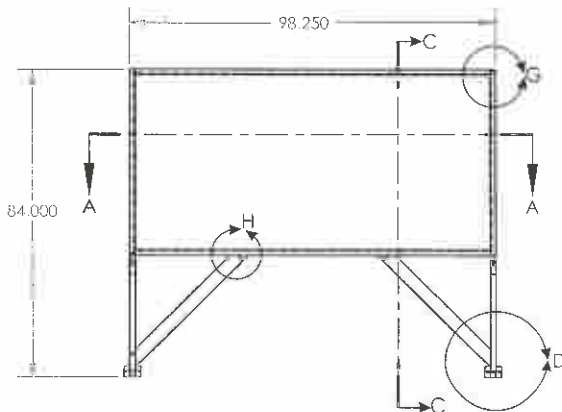
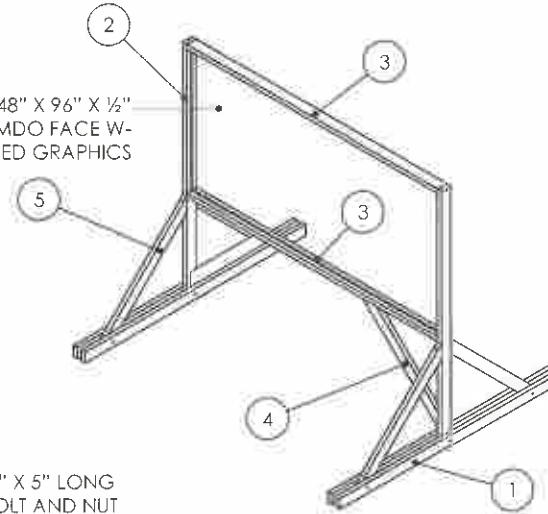


DETAIL H  
SCALE 1 : 8



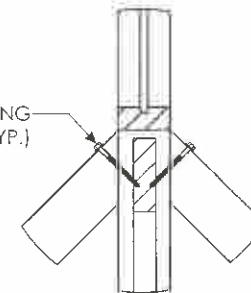
DETAIL D  
SCALE 1 : 8

48" X 96" X 1/2"  
MDO FACE W-  
PRINTED GRAPHICS

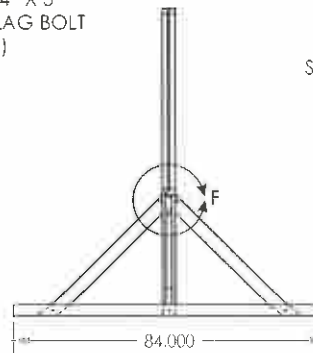


DETAIL G  
SCALE 1 : 8

Ø 1/4" X 4" LONG  
S.S. LAG BOLT (TYP.)



DETAIL F  
SCALE 1 : 8



SECTION C-C



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**JP Morgan Chase Bank  
#730**

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tracie Pichem  
Designer: [Signature]  
Scale: As noted

**CUSTOMER APPROVAL**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
**COPY, COLORS & SIZES**

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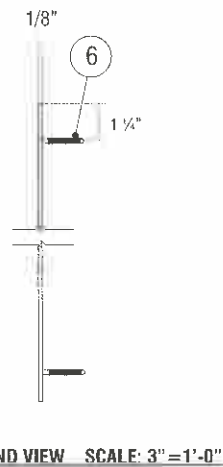
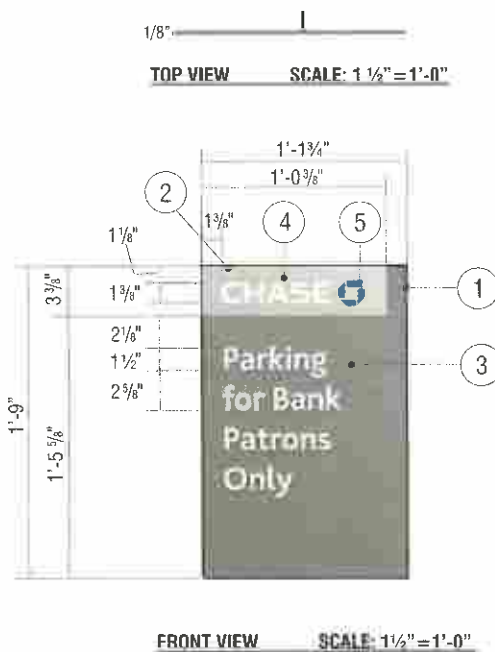
Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
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Drawing Number: 18-01613

Project ID: CHASE\_730\_1

Revision: R13 - 01.19.21





E.18 E.19 E.20

**SIGN TYPE** TC-W-A-RE **PARKING FOR BANK PATRONS ONLY**

**MANUFACTURE AND INSTALL THREE (3) NON-ILLUMINATED CUSTOM MESSAGE SIGNS**

1. 125" ALUMINUM SIGN FACE.  
PAINT FINISH ALL EXPOSED SURFACES MATTHEWS MP#18248, DARK NICKEL GLOSS FINISH.
2. TOP BAND TO BE FIRST SURFACE APPLIED  
3M #180C-220 LIGHT SILVER METALLIC VINYL.
3. SUPPORT COPY TO BE 3M SCOTCHLITE #680-10 WHITE.
4. "CHASE" TO BE 3M FILM #7725-10 WHITE.
5. OCTAGON TO BE 3M FILM #3632-127 BLUE OVER  
3M FILM #7725-10 WHITE.
6. PROVIDE NELSON STUDS OFF BACK SIDE OF ALUM. TO  
ANCHOR TO COLUMN.



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**JP Morgan Chase Bank  
#730**

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tracy Schmitt  
Designer: Thomas  
Scale: As noted

**CUSTOMER APPROVAL**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

**COPY COLORS & SIZES**

Signtech does NOT provide primary  
electrical to sign location.  
**RESPONSIBILITY OF OTHERS!**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

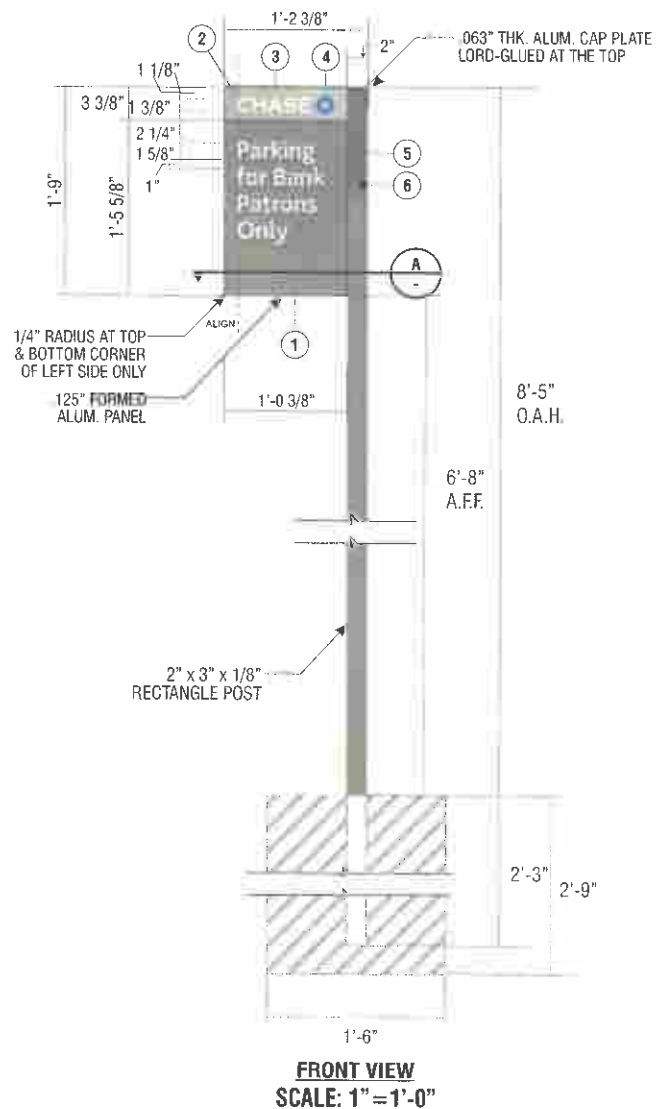
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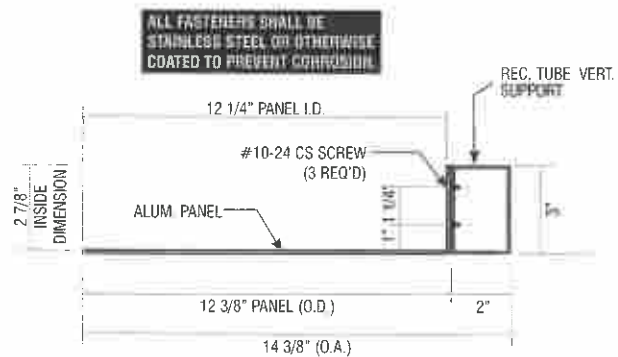


|      |      |      |      |
|------|------|------|------|
| E.21 | E.22 | E.23 | E.24 |
| E.25 | E.26 | E.27 | E.28 |

**SIGN TYPE TC-P-A-RE**

**MANUFACTURE AND INSTALL EIGHT (8) NON-ILLUMINATED PARKING SIGNS**

1. 125" BREAK-FORMED ALUMINUM SIGN FACE.  
PAINT FINISH ALL EXPOSED SURFACES DARK NICKEL MATTHEWS MP #18248, GLOSS FINISH FORM AS SHOWN.
2. TOP BAND - FIRST SURFACE APPLIED - 3M #180C-220 LIGHT SILVER METALLIC VINYL.
3. "CHASE" - 3M FILM #7725-10 WHITE VINYL.
4. OCTAGON - 3M FILM #3632-127 CHASE BLUE VINYL.
5. SUPPORT COPY TO BE 3M SCOTCHLITE #680-10 REFLECTIVE WHITE.
6. 2" X 3" X 1/8" X 128" LONG RECTANGLE ALUM. POST  
W/ .063" THICK ALUM. CAP PLATE @ TOP.



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**JP Morgan Chase Bank #730**

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tracey Pichieri  
Designer: JHones  
Scale: As noted

**CUSTOMER APPROVAL**

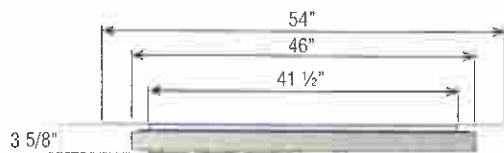
Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
COPY, COLORS & SIZES

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Project ID: CHASE\_730\_1  
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TOP VIEW SUR-TTW-U-4  
SCALE: 3/4" = 1'-0"

STIFFLE FINISH PAINT PROCESS FOR MATTHEWS JPM-TECH CHASE MODEL:  
BASE COAT: 3-4 MEDIUM COATS OF MAP (SHEEN?) FOR BASE COAT.  
ALLOW TO AIR DRY 12-24 HRS. ON FORCE DRY AT 200 DEG. F FOR 1 HR.  
STIFFLE FINISH COAT: 3-4 MEDIUM COATS OF MAP (SHEEN?) WITH CATALYST BUT, TWO" REDUCED.  
SET PRESSURE POT AT 14-20 PSI (30-40 PSI) PRESSURE AND  
20-25 PSI ATOMIZING AIR AT THE PAINT GUN.  
USE 1.0 TO 1.2 FLUID NOZZLE.  
APPLY 5-6 PACES OF A FINE TIGHT STIFFLE.

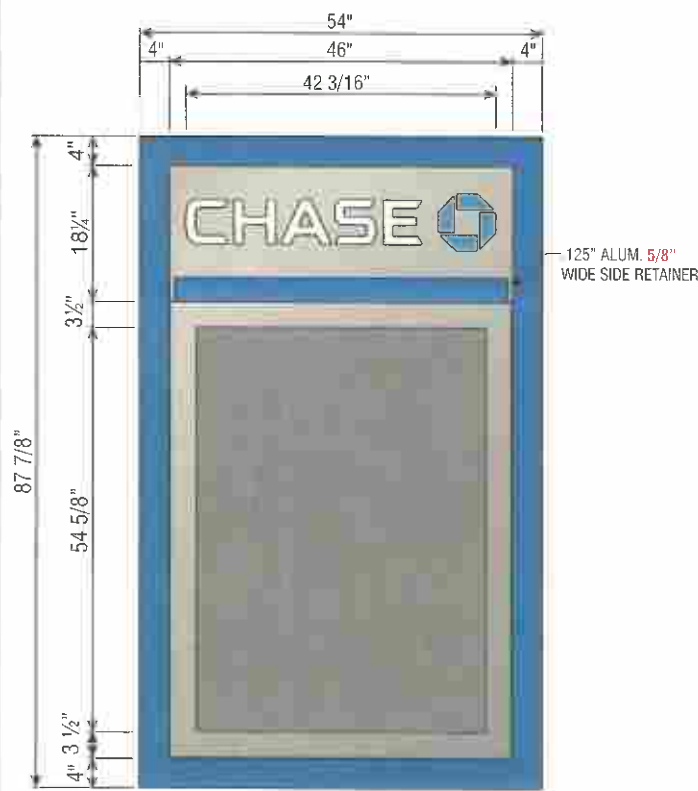
STIFFLE FINISH PAINT PROCESS FOR MATTHEWS JPM-TECH CHASE BANK MATHALD:  
BASE COAT: 3-4 MEDIUM COATS OF MAP (SHEEN?) FOR BASE COAT.  
ALLOW TO AIR DRY 12-24 HRS. ON FORCE DRY AT 200 DEG. F FOR 1 HR.  
STIFFLE FINISH COAT: 3-4 MEDIUM COATS OF MAP (SHEEN?) WITH CATALYST BUT, TWO" REDUCED.  
SET PRESSURE POT AT 14-20 PSI (30-40 PSI) PRESSURE AND  
20-25 PSI ATOMIZING AIR AT THE PAINT GUN.  
USE 1.0 TO 1.2 FLUID NOZZLE.  
APPLY 5-6 PACES OF A FINE TIGHT STIFFLE.

NOTE: PUSH-THRU ACRYLIC SHALL HAVE  
A MILL FINISH ON ALL ROUNDED EDGES

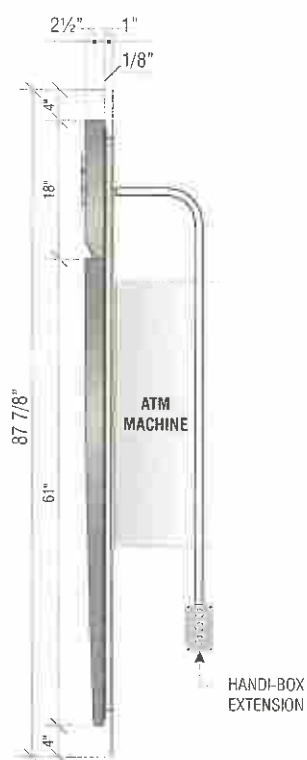
ALL EXPOSED FASTENER HEADS  
SHALL BE PAINTED TO MATCH THE  
EXTERIOR CABINET FINISH.

SLOAN BLUE LED STRIPE AROUND  
PERIMETER TO PROVIDE HALO-LIT  
EFFECT AGAINST BACK PANEL.

GRAPHICS DEPICTED ARE FOR  
ILLUSTRATIVE PURPOSES ONLY.  
USE ONLY APPROVED ARTWORK  
FOR PRODUCTION.



FRONT VIEW SCALE: 3/4" = 1'-0"



SIDE VIEW



PERSPECTIVE VIEW

E.29 L.30 SIGN TYPE SUR-TTW-U-4-TP

MANUFACTURE AND INSTALL TWO (2) INTERNALLY ILLUMINATED THIN PROFILE ATM SURROUNDS



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**JP Morgan Chase Bank  
#730**

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tracy McHenry  
Designer: Mena  
Scale: As noted

**CUSTOMER APPROVAL**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
COPY, COLORS & SIZES

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Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

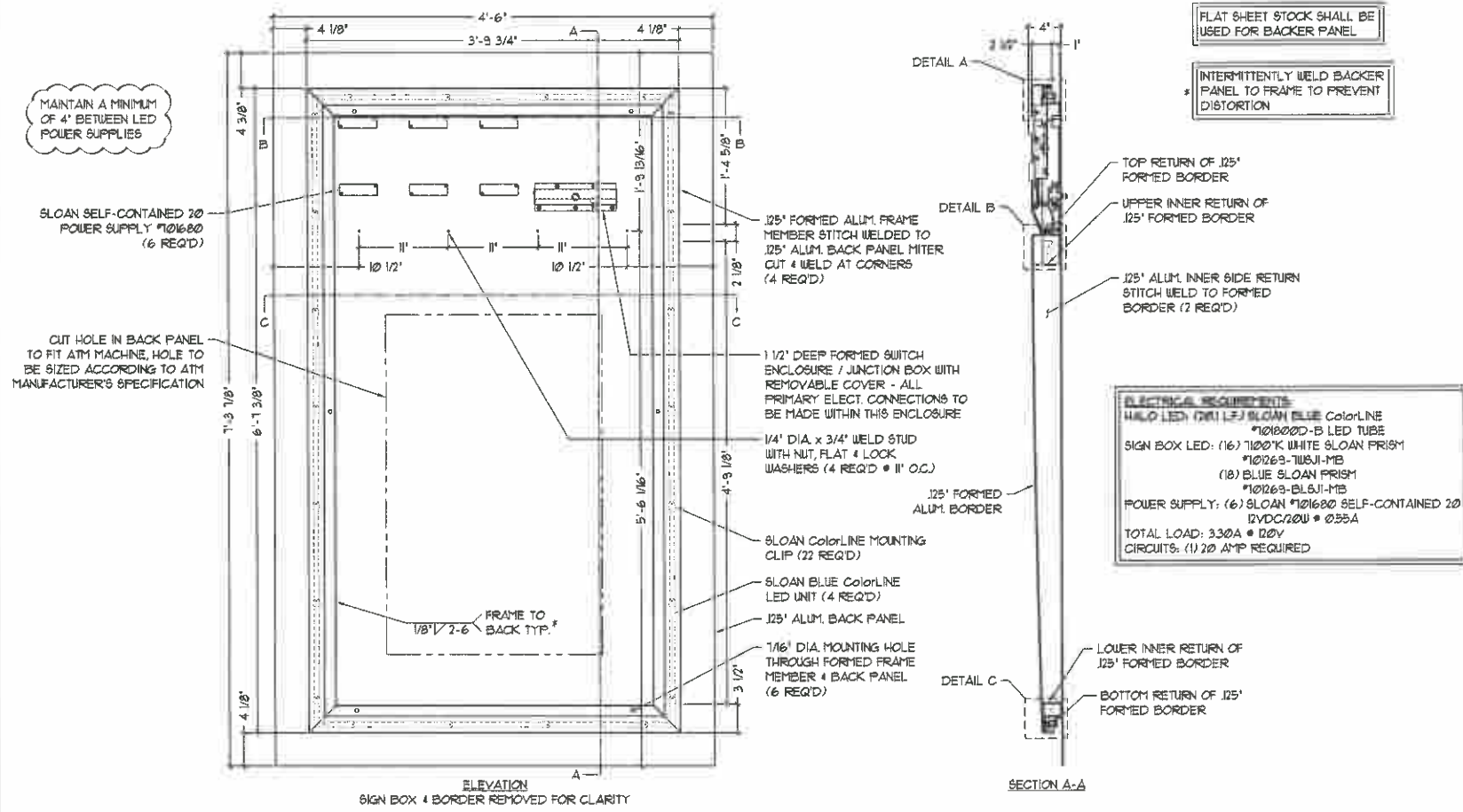
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Drawing Number: 18-01613

Project ID: CHASE\_730\_1

Revision: R13 01.19.21





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**JP Morgan Chase Bank #730**

Willowbrook  
 730 Plainfield Rd.  
 Willowbrook, IL 60527

Initial Date: 09/14/18  
 Salesperson: Arthur Navarro  
 Coordinator: Tracey Pichierri  
 Designer: J. Jones  
 Scale: As noted

**CUSTOMER APPROVAL**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
**COPY COLORS & SIZES**

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Drawing Number: 18-01613  
 Project ID: CHASE\_730\_i  
 Revision: R13 - 01.19.21



# ColorLINE

The Ultimate LED Border Tube



- Super Bright
- Profile Similar to Neon
- Lit, Mitered Corners
- Fully Compatible with All SloanLED 12 VDC Products

## COLORLINE SIZES AND CUT LENGTHS:

- (3) EA. #701800-COLOR-8 (96 1/2" LONG)
- MAKE VERTICAL TUBES FROM (2) 96 1/2" PARTS.
- MAKE HORIZONTAL TUBES FROM (1) 96 1/2" PART.
- EACH PART MUST HAVE A FACTORY RUBBER END.
- ONCE PLASTIC CAP IS ATTACHED TO THE CUT SIDE, THE TUBE WILL FINISH AT **77 5/32"** FOR VERTICAL IN LENGTH INCLUDING CAP
- THE TUBE WILL FINISH AT **45 3/16"** FOR HORIZONTAL IN LENGTH INCLUDING CAP

| Item Description | Part # (Tube)  | Actual Tube Length | Cut Increments | Power per Foot (Meter) | LEDs per Foot (Meter) | Limited Warranty    |
|------------------|----------------|--------------------|----------------|------------------------|-----------------------|---------------------|
| Z Tube           | 701800-Color-2 | 24 1/2" (61cm)     | 2 1/4" (5cm)   | 2.20W (7.5V)           | 20 (66)               | Warm Colors: 5 Year |
| M Tube           | 701800-Color-3 | 48 1/2" (123cm)    | 2 1/4" (5cm)   | 2.20W (7.5V)           | 20 (66)               |                     |
| V Tube           | 701800-Color-4 | 72 1/2" (184cm)    | 2 1/4" (5cm)   | 2.20W (7.5V)           | 20 (66)               | Cool Colors: 3 Year |
| S Tube           | 701800-Color-6 | 96 1/2" (245cm)    | 2 1/4" (5cm)   | 2.20W (7.5V)           | 20 (66)               |                     |

Warm Colors: Red, Orange, Yellow      Cool Colors: Blue, Green, White

Feet (Meters) per 12VDC Power Supply

| SloanLED Power Supply     | Self Contained 20  | Modular 60 | Quad 240           |
|---------------------------|--------------------|------------|--------------------|
| ColorLINE 2 Feet (Meters) | <del>9 (2.7)</del> | 24 (7.5)   | <del>96 (30)</del> |
|                           |                    |            | 24 (7.5) per leg   |



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**JP Morgan Chase Bank #730**

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tracey Pichierri  
Designer: JH/MS  
Scale: As noted

## CUSTOMER APPROVAL

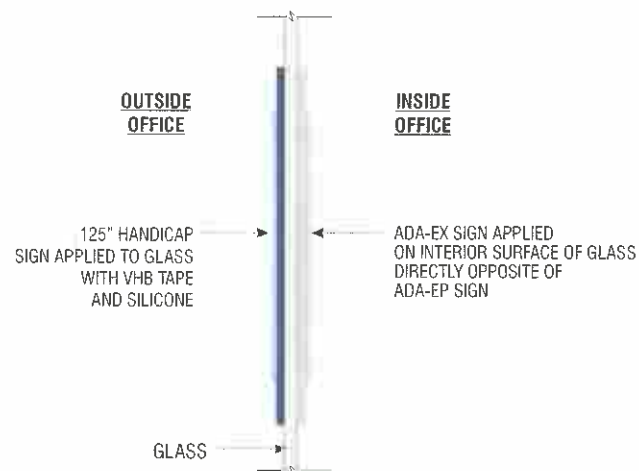
Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
**COPY, COLORS & SIZES**

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**RESPONSIBILITY OF OTHERS!**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
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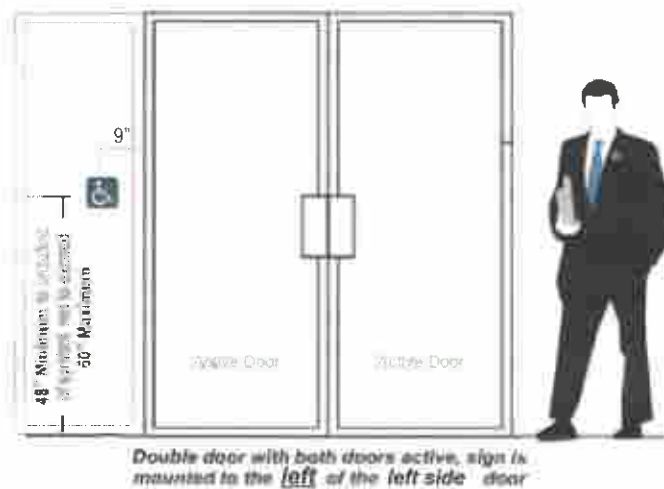
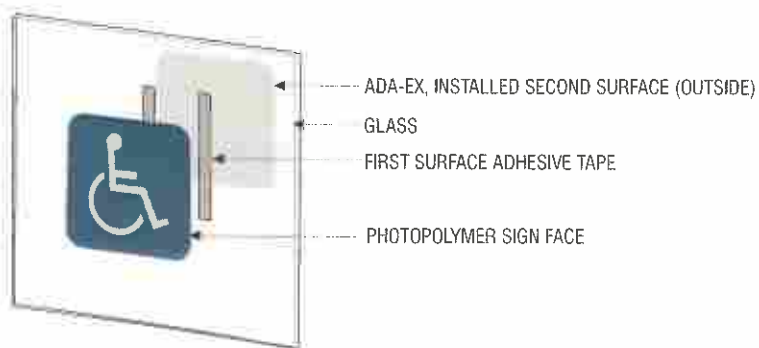
Drawing Number: 18-01613  
Project ID: CHASE\_730\_1  
Revision: R13 - 01.19.21





**SIGN TYPE** **ADA-EP** *ACCESSIBLE BUILDING ENTRANCE - A02*

**MANUFACTURE AND INSTALL TWO (2) ADA STANDARD ENTRANCE PLAQUES**  
**FACE:** 1/8" BLUE GRAVOTAC #341-501V, SATIN NON-GLARE FINISH.  
**ISA SYMBOL:** 1/32" RAISED, BRIGHT WHITE GRAVOTAC  
**BRILLE:** 1/32" RAISED CLEAR BEAD, MAXIMUM 1/16" DIA.  
**INSTALL:** MOUNT TO LATCH SIDE OF DOOR WITH VHB TAPE AND SILICONE.



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**JP Morgan Chase Bank**  
#730

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tracy Pickett  
Designer: bones  
Scale: As noted

**CUSTOMER APPROVAL**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

COPY, COLORS & SIZES

Signtech does NOT provide primary  
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Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

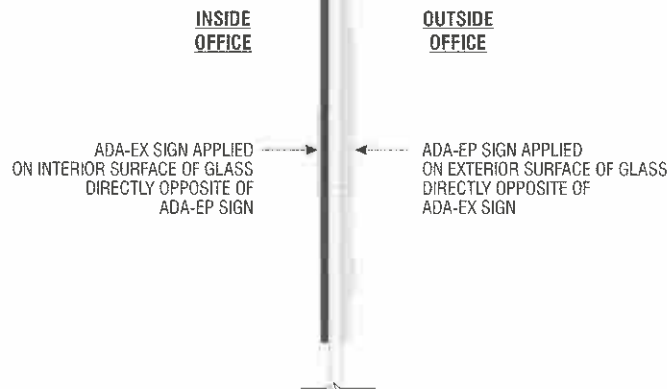
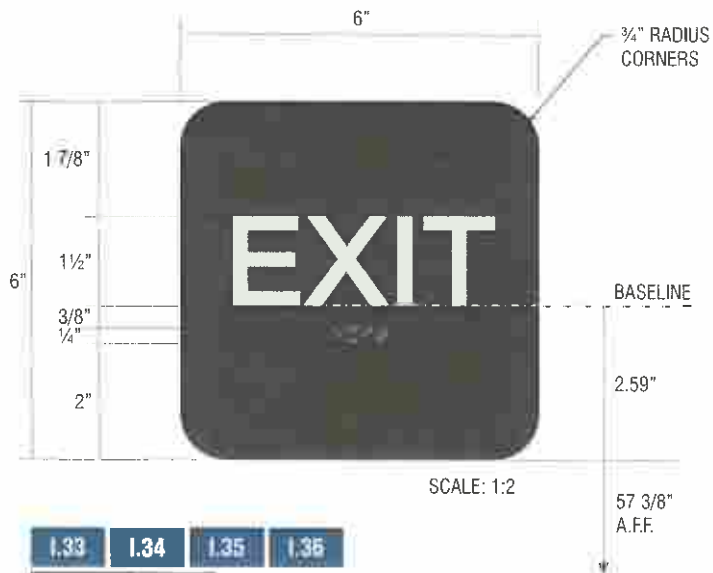
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Drawing Number: 18-01613

Project ID: CHASE 730 1

Revision: 813 - 01 19.21





| SIGN TYPE | ADA-EX | ADA-EP | ADA-EX | ADA-EP |
|-----------|--------|--------|--------|--------|
| 1.33      | 1.34   | 1.35   | 1.36   | 1.37   |

#### EXIT DOOR SIGNAGE - A13

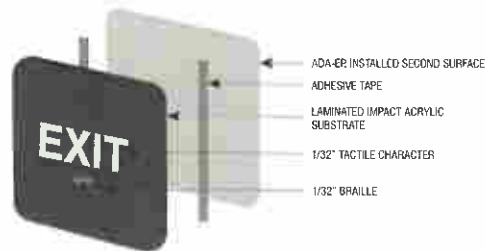
#### MANUFACTURE AND INSTALL FOUR (4) ADA STANDARD EXIT PLACQUES

**FACE:** 1/8" GRAVOTAC CHARCOAL GREY, SATIN NON-GLARE FINISH.

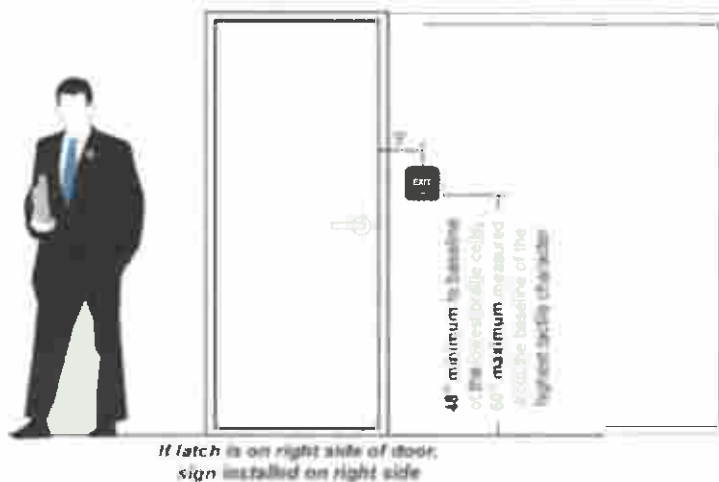
**COPY:** 1/32" RAISED, BRIGHT WHITE GRAVOTAC HELVETICA REGULAR TT FONT.

**BRILLE:** 1/32" RAISED CLEAR BEAD, MAXIMUM 1/16" DIA.

**INSTALL:** MOUNT TO LATCH SIDE OF DOOR WITH VHB TAPE AND SILICONE.



TYPICAL FABRICATION DETAIL NTS



#### JP Morgan Chase Bank #730

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tracey Pichierri  
Designer: J. Jones  
Scale: As noted

#### CUSTOMER APPROVAL

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
COPY, COLORS & SIZES

Signtech does NOT provide primary electrical to sign location -  
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Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

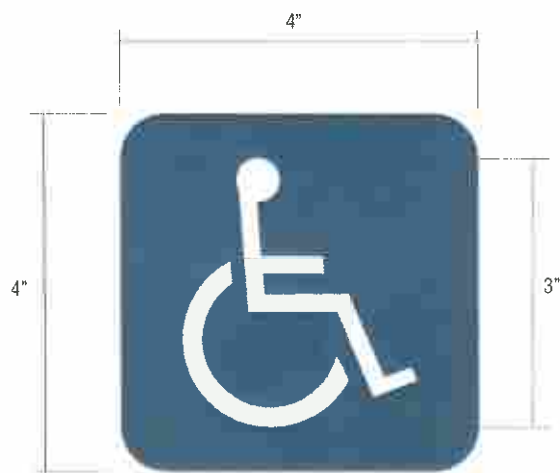
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Drawing Number: 18-01613

Project ID: CHASE\_730\_1

Revision: R13 - 01.19.21





FRONT VIEW

SCALE: 3/8" = 1"



TYPICAL INSTALLATION - NO BULLET-RESISTANT GLASS SCALE: 1/2" = 1'-0"

1.37

SIGN TYPE **ADA-TW** ACCESSIBLE TELLER WINDOW - A14

**MANUFACTURE AND INSTALL ONE (1) ADA ACCESSIBILITY PLAQUE**

**FACE:** 1/8" BLUE GRAVOTAC #341-501V, SATIN NON-GLARE FINISH

**ISA SYMBOL:** 1/32" RAISED, BRIGHT WHITE GRAVOTAC

**INSTALL:** MOUNT AT ACCESSIBLE TELLER LINE, AS SHOWN, WITH VHB TAPE AND SILCONE.



**Signtech**

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**JP Morgan Chase Bank  
#730**

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tracey Pichierri  
Designer: [Signature]  
Scale: As noted

**CUSTOMER APPROVAL**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

**COPY, COLORS & SIZES**

Signtech does NOT provide primary  
electrical to sign location -  
**RESPONSIBILITY OF OTHERS!**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

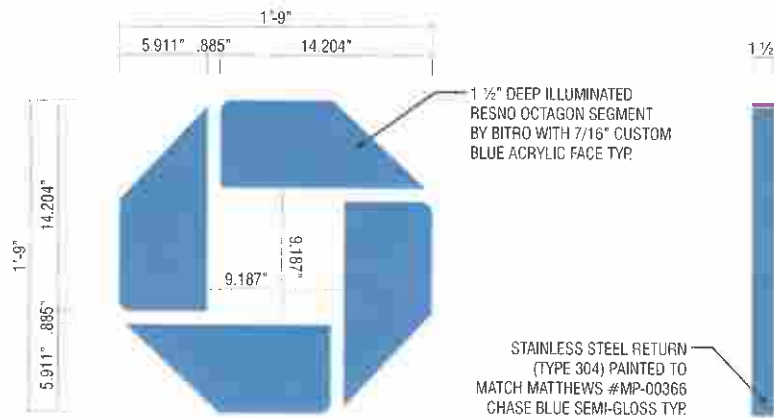
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Drawing Number: 18-01613

Project ID: CHASE\_730\_1

Revision: R13 - 01.19.21





FRONT VIEW

SCALE: 1 1/2" = 1'-0"

SIDE VIEW  
SCALE: 1 1/2" = 1'-0"

**I.38**

**SIGN TYPE OCT-21-INT BLUE BITRO**

**MANUFACTURE AND INSTALL ONE (1) INTERNALLY-ILLUMINATED OCTAGON**

#### GENERAL NOTES:

1. DESIGN IS BASED ON A 5 PSF LATERAL LOAD FOR INTERIOR APPLICATIONS PER IBC 2015 SEISMIC DESIGN CATEGORY D.
2. THE CUSTOMER'S BUILDING ENGINEER IS TO DETERMINE THE ADEQUACY OF THE SUPPORTING STRUCTURE.
3. ALL FASTENERS SHALL BE STAINLESS STEEL OR ZINC COATED TO PREVENT CORROSION.
4. J.P. MORGAN CHASE WILL NOT BE RESPONSIBLE FOR THE SAFETY ON THIS JOB SITE BEFORE, DURING OR AFTER INSTALLATION OF THIS STRUCTURE. IT IS THE RESPONSIBILITY OF THE CONTRACTORS AND INSTALLERS TO ENSURE THAT THE INSTALLATION AND ERECTION OF THIS STRUCTURE IS PERFORMED USING METHODS THAT ARE IN FULL COMPLIANCE WITH OSHA REGULATIONS.
5. ANY DEVIATION FROM THIS DESIGN OR FROM ANY PART OF THIS DRAWING, INCLUDING THE GENERAL NOTES, WITHOUT PRIOR WRITTEN CONSENT FROM J.P. MORGAN CHASE VOIDS THIS DRAWING IN ITS ENTIRETY.
6. THE STRUCTURE DESIGNED ON THIS DRAWING IS PROTOTYPICAL AND SHOULD NOT BE USED FOR SITE SPECIFIC APPLICATIONS UNLESS DEEMED SUITABLE BY A COMPETENT PROFESSIONAL ENGINEER.



**TYPICAL INTERIOR ELEVATION NOT TO SCALE**



**Signtech**  
4444 Federal Blvd, San Diego CA 92102  
Phone: (619) 527-6100 / Fax: (619) 527-6111  
[signtech.com](http://signtech.com)



#### JP Morgan Chase Bank #730

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tracey Pichierni  
Designer: JUPERS  
Scale: As noted

#### CUSTOMER APPROVAL

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
COPY, COLORS & SIZES

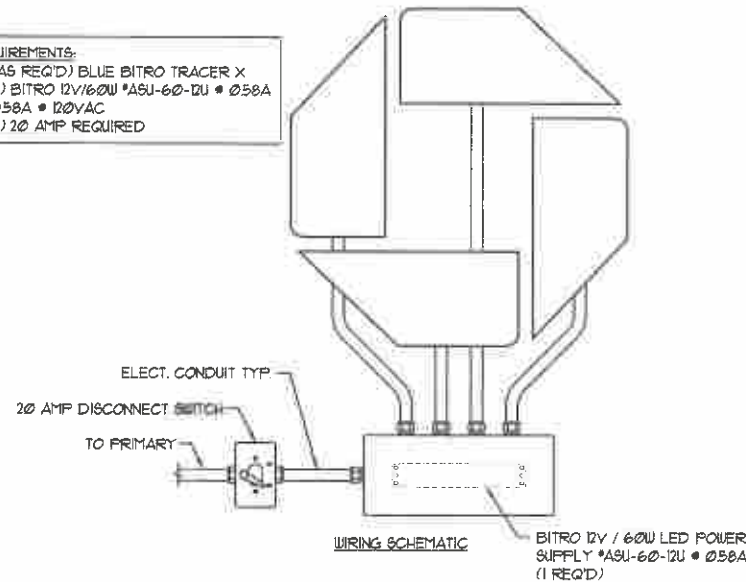
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Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
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Drawing Number: 18-01613  
Project ID: CHASE\_730\_1  
Revision: R13 - 01.19.21



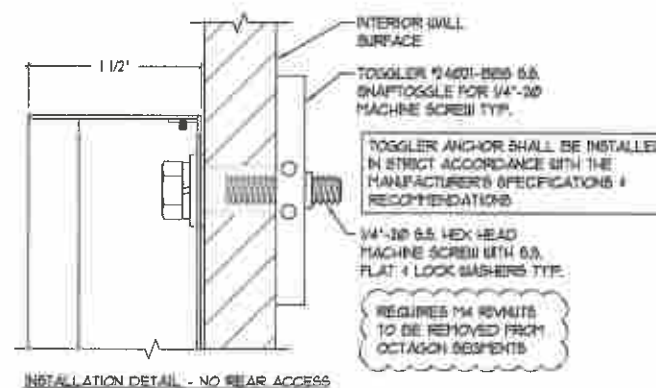
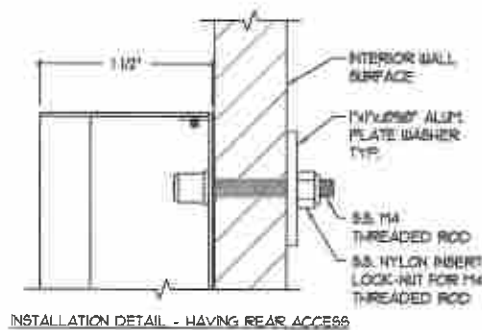
**ELECTRICAL REQUIREMENTS:**  
 LED: (AS REQ'D) BLUE BITRO TRACER X  
 POWER SUPPLY: (1) BITRO 12V/60W \*ASU-60-12U • 0.58A  
 TOTAL LOAD: 0.58A • 120VAC  
 CIRCUITS: (1) 20 AMP REQUIRED



INTERIOR APPLICATION ONLY

ALL EXPOSED FASTENER HEADS SHALL BE PAINTED TO MATCH THE EXTERIOR CABINET FINISH

OCTAGON SHALL BE WIRED TO A REMOTELY MOUNTED POWER SUPPLY



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**JP Morgan Chase Bank #730**

Willowbrook  
 730 Plainfield Rd.  
 Willowbrook, IL 60527

Initial Date: 09/14/18  
 Salesperson: Arthur Navarro  
 Coordinator: Tracey Piclierri  
 Designer: bitro  
 Scale: As noted

#### CUSTOMER APPROVAL

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
 COPY, COLORS & SIZES

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Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
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Drawing Number: 18-01613

Project ID: CHASE\_730\_1

Revision: R13 - 01.19.21





FRONT VIEW SCALE: 3/8" = 1"

1.42

**SIGN TYPE** ADA-RRW-A-G **FOR RESTROOMS THAT ARE ACCESSIBLE - A09**

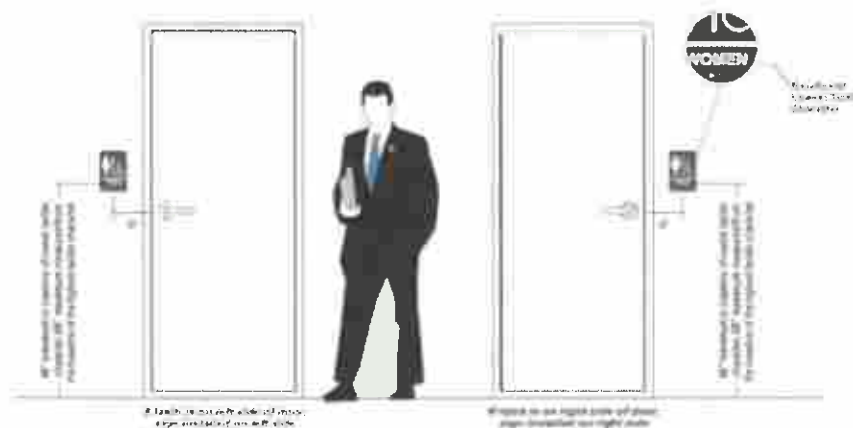
**MANUFACTURE AND INSTALL ONE (1) ADA WOMEN RESTROOM SIGN**

**FACE:** 1/8" GRAVOTAC CHARCOAL GREY, SATIN NON-GLARE FINISH.

**COPY:** 1/32" RAISED, BRIGHT WHITE GRAVOTAC HELVETICA REGULAR TT FONT.

**BRAILLE:** 1/32" RAISED CLEAR BEAD, MAXIMUM 1/16" DIA.

**INSTALL:** MOUNT TO LATCH SIDE OF DOOR WITH VHB TAPE AND SILICONE.



**Signtech**

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**JP Morgan Chase Bank  
#730**

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tracy Padgett  
Designer: bioness  
Scale: As noted

**CUSTOMER APPROVAL**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
**COPY, COLORS & SIZES**

Signtech does NOT provide primary  
electrical to sign location  
**RESPONSIBILITY OF OTHERS!**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

This drawing is the exclusive property of Signtech.  
It shall remain the responsibility of the client to ensure  
that all signs are properly installed and maintained.

Drawing Number: 18-01613

Project ID: CHASE\_730\_1

Revision: R13 - 01.19.21





FRONT VIEW SCALE: 3/8" = 1"

143

**SIGN TYPE** **ADA-ARM-A-G** **FOR RESTROOMS THAT ARE ACCESSIBLE - A09**

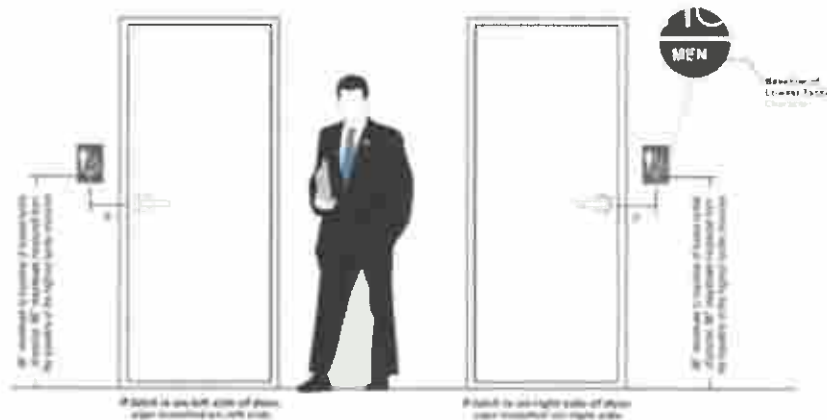
**MANUFACTURE AND INSTALL ONE (1) ADA MEN RESTROOM SIGN**

**FACE:** 1/8" GRAVOTAC CHARCOAL GREY, SATIN NON-GLARE FINISH.

**COPY:** 1/32" RAISED, BRIGHT WHITE GRAVOTAC, HELVETICA REGULAR TT FONT.

**BRAILLE:** 1/32" RAISED CLEAR BEAD, MAXIMUM 1/16" DIA.

**INSTALL:** MOUNT TO LATCH SIDE OF DOOR WITH VHB TAPE AND SILICONE.



**Signtech**

4444 Federal Blvd. San Diego CA 92102  
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signtech.com



**JP Morgan Chase Bank  
#730**

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tracey Pichern  
Designer: J. Vines  
Scale: As noted

**CUSTOMER APPROVAL**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

**COPY, COLORS & SIZES**

Signtech does NOT provide primary  
electrical to sign location -  
**RESPONSIBILITY OF OTHERS!**

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

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Drawing Number: 18-01613

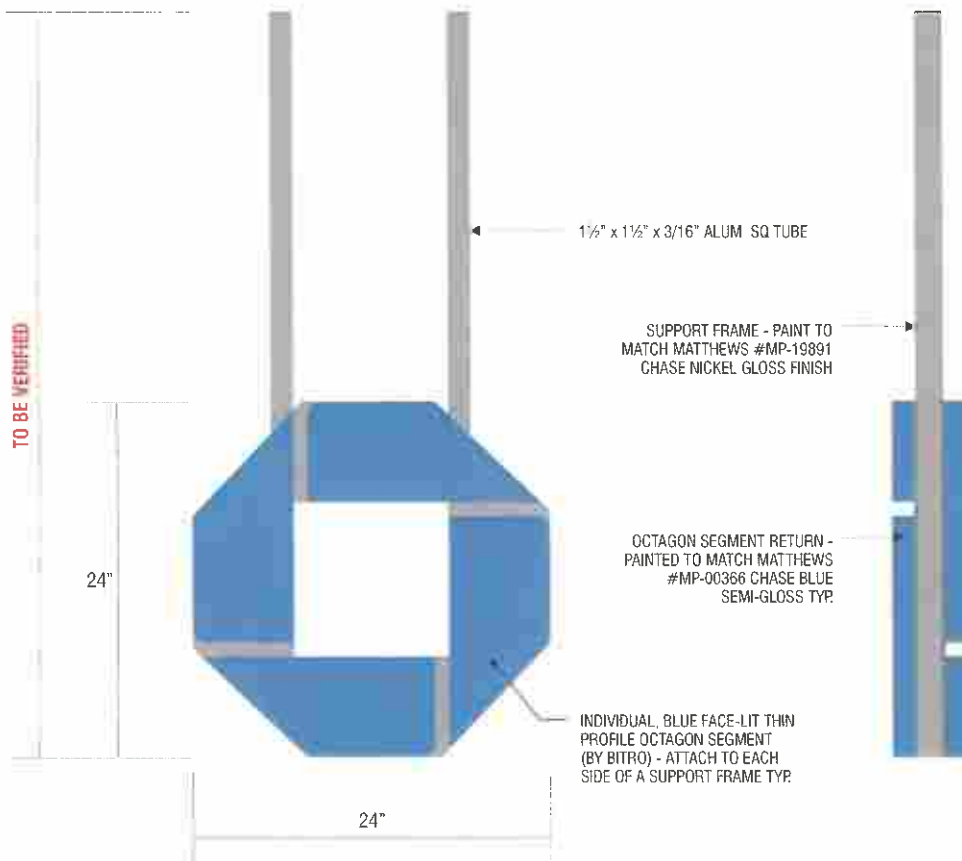
Project ID: CHASE\_730\_1

Revision: R13 - 01.19.21









ELEVATION SCALE: 1 1/2" = 1'-0"

1.45

SIGN TYPE

OCT-TP-H-24

ATTACHMENTS TO BE VERIFIED

MANUFACTURE AND INSTALL ONE (1) INTERNALLY-ILLUMINATED CEILING-HUNG BLUE BITRO OCTAGON

#### General Notes:

1. Design is based on a 5 psf lateral load for interior applications per IBC 1605, Seismic Design Category D. The customer's building engineer is to determine the adequacy of the supporting structure.
2. Extruded aluminum shapes shall be 6061-T6 alloy. Aluminum sheet shall be 3003-H14 alloy. Aluminum plate shall be 5052-H34 alloy.
3. All welds shall be made using 5554 filler for aluminum by persons qualified in accordance with AWS standards within the past two years.
4. All fasteners shall be stainless steel or zinc coated to prevent corrosion.
5. JP Morgan Chase will not be responsible for the safety of the job site before, during or after installation of this structure. It is the responsibility of the contractors and installers to ensure that the installation and erection of this structure is performed using methods that are in full compliance with OSHA regulations.
6. Any deviation from the design or from any part of this drawing, including the General Notes, without prior written consent from JP Morgan Chase voids this drawing in its entirety.
7. The structure designed on this drawing is proprietary and should not be used for site specific applications unless deemed suitable by a Competent Professional Engineer.

INTERIOR APPLICATION ONLY

ALL EXPOSED FASTENER HEADS SHALL BE PAINTED TO MATCH THE EXTERIOR CABINET FINISH

- ILLUMINATED OCTAGON SEGMENTS SHALL BE WIRED TO A REMOTELY MOUNTED 12V / 60W POWER SUPPLY
- 12V / 60W POWER SUPPLY & QUANTITY SHALL BE AS SPECIFIED BY BITRO GROUP



**Signitech**

4444 Federal Blvd. San Diego CA 92102  
Phone: (619) 527-6100 / Fax: (619) 527-6111  
signitech.com



#### JP Morgan Chase Bank #730

Willowbrook  
730 Plainfield Rd.  
Willowbrook, IL 60527

Initial Date: 09/14/18  
Salesperson: Arthur Navarro  
Coordinator: Tracey Pichlerri  
Designer: James  
Scale: As noted

#### CUSTOMER APPROVAL

Customer Signature \_\_\_\_\_ Date \_\_\_\_\_  
COPY, COLORS & SIZES

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Customer Signature \_\_\_\_\_ Date \_\_\_\_\_

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Drawing Number: 18-01613

Project ID: CHASE\_730\_1

Revision: R13 - 01.19.21



PUD PLANS  
GW PROPERTIES  
735 PLAINFIELD ROAD  
WILLOWBROOK, IL 60527



UTILITY AND GOVERNING AGENCY CONTACTS

ENGINEERING DEPARTMENT  
VILLAGE OF WILLOWBROOK  
PUBLIC SERVICES DIVISION  
835 MIDWAY DRIVE  
WILLOWBROOK, IL 60527  
TEL: (847) 823-0500  
CONTACT: DAN LYNCH, P.E.

PLANNING DEPARTMENT  
VILLAGE OF WILLOWBROOK  
PLANNING & DEVELOPMENT  
835 MIDWAY DRIVE  
WILLOWBROOK, IL 60527  
TEL: (630) 920-2233  
CONTACT: ANN CHOI

SANITARY SEWER SERVICE  
VILLAGE OF WILLOWBROOK  
835 MIDWAY DRIVE  
WILLOWBROOK, IL 60527

POWER COMPANY  
COMED

STORM SEWER SERVICE  
VILLAGE OF WILLOWBROOK  
835 MIDWAY DRIVE  
WILLOWBROOK, IL 60527

NATURAL GAS COMPANY  
NICOR GAS

WATER SERVICE  
VILLAGE OF WILLOWBROOK  
835 MIDWAY DRIVE  
WILLOWBROOK, IL 60527  
TEL: (630) 920-2238  
CONTACT: VIRGINIA STOLTZ

TELEPHONE  
AT&T

CABLE  
COMCAST CABLE

PROJECT TEAM

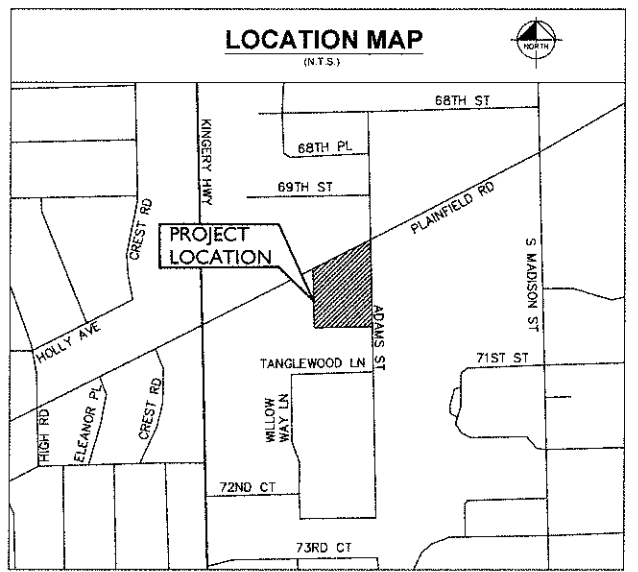
DEVELOPER  
GW PROPERTIES  
2211 N ELSTON AVE, SUITE 304  
CHICAGO, IL 60614  
TEL: (773) 382-0590  
CONTACT: MITCH GOLTZ

CIVIL ENGINEER  
KIMLEY-HORN AND ASSOCIATES, INC.  
4201 WINFIELD RD, SUITE 600  
WARRENVILLE, IL 60555  
TEL: (630) 487-5550  
EMAIL: ERIC.TRACY@KIMLEY-HORN.COM  
CONTACT: ERIC TRACY, P.E.

ARCHITECT  
DESIGN STUDIO 24 LLC  
2211 N ELSTON AVENUE  
CHICAGO, IL 60614  
TEL: (847) 885-8300  
CONTACT: MARK DIGIANCI

LANDSCAPE ARCHITECT  
KIMLEY-HORN AND ASSOCIATES, INC.  
4201 WINFIELD RD, SUITE 600  
WARRENVILLE, IL 60555  
TEL: (630) 487-5550  
EMAIL: DANIEL.GROVE@KIMLEY-HORN.COM  
CONTACT: DANIEL GROVE, P.L.A.

SURVEYOR  
COMPASS SURVEYING LTD.  
2631 GINGER WOODS PARKWAY, SUITE 100  
AURORA, IL 60502  
TEL: (630) 820-9100  
CONTACT: SCOTT KREBS, P.L.S.



| Sheet List Table |                             |
|------------------|-----------------------------|
| Sheet Number     | Sheet Title                 |
| C0.0             | TITLE SHEET                 |
| V0.0             | ALTA & TOPOGRAPHIC SURVEY   |
| C1.0             | SITE PLAN                   |
| C2.0             | GRADING PLAN                |
| C3.0             | UTILITY PLAN                |
| C4.0             | FIRE TRUCK TURN EXHIBIT     |
| C4.1             | DELIVERY TRUCK TURN EXHIBIT |
| L1.0             | TREE PRESERVATION PLAN      |
| L2.0             | LANDSCAPE PLAN              |
| L3.0             | LANDSCAPE NOTES & DETAILS   |
| E1.0             | PHOTOMETRICS PLAN           |

BENCHMARKS

REFERENCE BENCHMARK:  
DUPAGE COUNTY GEODETIC SURVEY MONUMENT #DK3269

BENCHMARK DISK LOCATED 42 FEET WEST OF THE CENTERLINE OF MADISON STREET AND 37.5 FEET NORTH OF THE CENTERLINE OF 83RD STREET

DATUM: NAVD 88  
ELEVATION = 732.93

SITE BENCHMARKS:  
(LOCATIONS SHOWN ON SURVEY)

SBM #1 NORTHEAST BONNET BOLT ON FIRE HYDRANT LOCATED AT THE NORTHEAST CORNER OF SITE ON SOUTH SIDE OF PLAINFIELD ROAD  
ELEVATION=732.28

SBM #2 SOUTHEAST BONNET BOLT ON FIRE HYDRANT NEAR NORTHEAST CORNER OF BUFFALO WILD WINGS ON WEST SIDE OF SITE  
ELEVATION=730.46

LEGAL DESCRIPTION

LOT 1 OF WILLOWBROOK CENTER UNIT NO. 1, OF PART OF LOT 7 IN OWNER'S SUBDIVISION IN SECTIONS 23 & 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED OCTOBER 16, 1963, AS DOCUMENT R63-37895, IN DUPAGE COUNTY, ILLINOIS.

DRAINAGE CERTIFICATION

I, ERIC TRACY, P.E., HEREBY CERTIFY ADEQUATE STORM WATER STORAGE AND DRAINAGE CAPACITY HAS BEEN PROVIDED FOR THIS DEVELOPMENT, SUCH THAT SURFACE WATER FROM THE DEVELOPMENT WILL NOT BE DIVERTED ONTO AND CAUSE DAMAGE TO ADJACENT PROPERTY FOR STORMS UP TO AND INCLUDING THE ONE HUNDRED (100) YEAR EVENT, AND THAT THE DESIGN PLANS ARE IN COMPLIANCE WITH APPLICABLE STATE, COUNTY, AND VILLAGE ORDINANCES.

DATED THIS 21ST DAY OF JANUARY, A.D., 2021.

ILLINOIS LICENSED PROFESSIONAL ENGINEER 062-067482  
MY LICENSE EXPIRES ON NOVEMBER 30, 2021

PROFESSIONAL ENGINEER'S CERTIFICATION

I, ERIC TRACY, A LICENSED PROFESSIONAL ENGINEER OF ILLINOIS, HEREBY CERTIFY THAT THIS SUBMISSION, PERTAINING ONLY TO THE "C" SERIES CIVIL SHEETS LISTED ABOVE, WAS PREPARED ON BEHALF OF CORESTATES, INC. BY KIMLEY-HORN AND ASSOCIATES, INC. UNDER MY PERSONAL DIRECTION. THIS TECHNICAL SUBMISSION IS INTENDED TO BE USED AS AN INTEGRAL PART OF AND IN CONJUNCTION WITH THE PROJECT SPECIFICATIONS AND CONTRACT DOCUMENTS.

DATED THIS 21ST DAY OF JANUARY, A.D., 2021.

ILL. LICENSED PROFESSIONAL ENGINEER 062-067482  
MY LICENSE EXPIRES ON NOVEMBER 30, 2021

Kimley-Horn  
4201 WINFIELD ROAD, SUITE 600  
WARRENVILLE, IL 60555  
PHONE: (630) 487-5550  
WWW.KIMLEY-HORN.COM

SCALE: AS NOTED  
DESIGNED BY: HES  
DRAWN BY: HES  
CHECKED BY: LJT

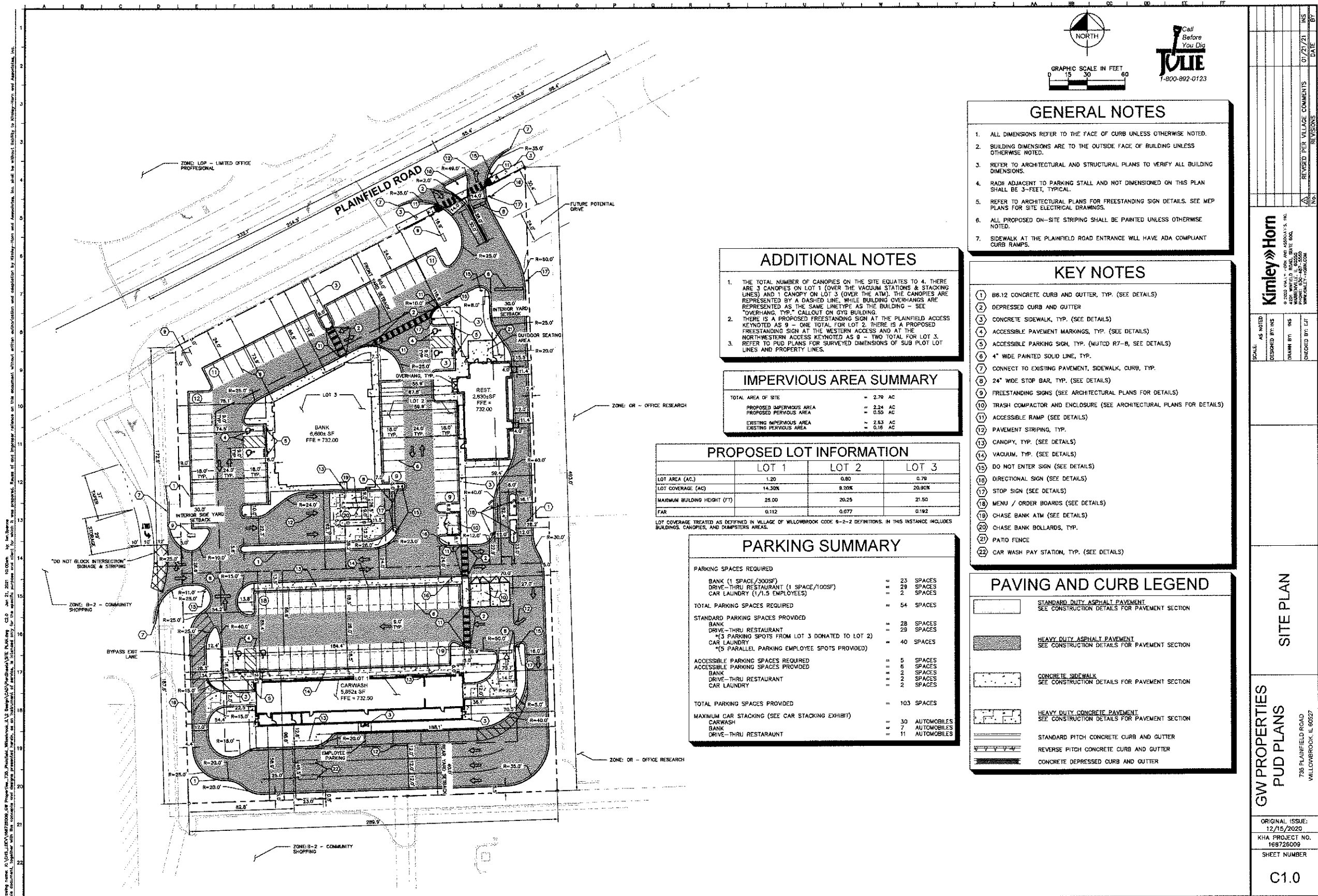
TITLE SHEET

GW PROPERTIES  
PUD PLANS  
735 PLAINFIELD ROAD  
WILLOWBROOK, IL 60527

ORIGINAL ISSUE:  
12/15/2020  
KHA PROJECT NO.  
166726009  
SHEET NUMBER  
C0.0

| NO. | REVISIONS                    | DATE     |
|-----|------------------------------|----------|
| 1   | REVISED PER VILLAGE COMMENTS | 01/21/21 |
| 2   |                              |          |
| 3   |                              |          |
| 4   |                              |          |
| 5   |                              |          |
| 6   |                              |          |
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| 21  |                              |          |
| 22  |                              |          |





GENERAL NOTES

1. ALL DIMENSIONS REFER TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
2. BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
3. REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY ALL BUILDING DIMENSIONS.
4. RADI ADJACENT TO PARKING STALL AND NOT DIMENSIONED ON THIS PLAN SHALL BE 3- FEET, TYPICAL.
5. REFER TO ARCHITECTURAL PLANS FOR FREESTANDING SIGN DETAILS. SEE MEP PLANS FOR SITE ELECTRICAL DRAWINGS.
6. ALL PROPOSED ON-SITE STRIPING SHALL BE PAINTED UNLESS OTHERWISE NOTED.
7. SIDEWALK AT THE PLAINFIELD ROAD ENTRANCE WILL HAVE ADA COMPLIANT CURB RAMPS.

ADDITIONAL NOTES

1. THE TOTAL NUMBER OF CANOPIES ON THE SITE EQUATES TO 4. THERE ARE 3 CANOPIES ON LOT 1 (OVER THE VACUUM STATIONS & STACKING LINES) AND 1 CANOPY ON LOT 3 (OVER THE ATM). THE CANOPIES ARE REPRESENTED BY A DASHED LINE, WHILE BUILDING OVERHANGS ARE REPRESENTED AS THE SAME LINETYPE AS THE BUILDING - SEE "OVERHANG, TYP." CALLOUT ON GYG BUILDING.
2. THERE IS A PROPOSED FREESTANDING SIGN AT THE PLAINFIELD ACCESS KEYNOTED AS 9 - ONE TOTAL FOR LOT 2. THERE IS A PROPOSED FREESTANDING SIGN AT THE WESTERN ACCESS AND AT THE NORTHWESTERN ACCESS KEYNOTED AS 9 - TWO TOTAL FOR LOT 3. REFER TO PUD PLANS FOR SURVEYED DIMENSIONS OF SUB PLOT LOT LINES AND PROPERTY LINES.

IMPERVIOUS AREA SUMMARY

|                          |   |         |
|--------------------------|---|---------|
| TOTAL AREA OF SITE       | = | 2.79 AC |
| PROPOSED IMPERVIOUS AREA | = | 2.24 AC |
| PROPOSED PERVIOUS AREA   | = | 0.55 AC |
| EXISTING IMPERVIOUS AREA | = | 2.63 AC |
| EXISTING PERVIOUS AREA   | = | 0.16 AC |

PROPOSED LOT INFORMATION

|                              | LOT 1  | LOT 2 | LOT 3  |
|------------------------------|--------|-------|--------|
| LOT AREA (AC.)               | 1.20   | 0.80  | 0.79   |
| LOT COVERAGE (AC)            | 14.30% | 8.80% | 20.80% |
| MAXIMUM BUILDING HEIGHT (FT) | 25.00  | 20.25 | 21.50  |
| FAR                          | 0.112  | 0.077 | 0.192  |

LOT COVERAGE TREATED AS DEFINED IN VILLAGE OF WILLOWBROOK CODE 6-2-2 DEFINITIONS. IN THIS INSTANCE INCLUDES BUILDINGS, CANOPIES, AND DUMPSTERS AREAS.

PARKING SUMMARY

|                                                 |                  |
|-------------------------------------------------|------------------|
| PARKING SPACES REQUIRED                         |                  |
| BANK (1 SPACE/300SF)                            | = 23 SPACES      |
| DRIVE-THRU RESTAURANT (1 SPACE/100SF)           | = 29 SPACES      |
| CAR LAUNDRY (1/1.5 EMPLOYEES)                   | = 2 SPACES       |
| TOTAL PARKING SPACES REQUIRED = 54 SPACES       |                  |
| STANDARD PARKING SPACES PROVIDED                |                  |
| BANK                                            | = 28 SPACES      |
| DRIVE-THRU RESTAURANT                           | = 29 SPACES      |
| *(3 PARKING SPOTS FROM LOT 3 DONATED TO LOT 2)  |                  |
| CAR LAUNDRY                                     | = 40 SPACES      |
| *(5 PARALLEL PARKING EMPLOYEE SPOTS PROVIDED)   |                  |
| ACCESSIBLE PARKING SPACES REQUIRED              |                  |
| BANK                                            | = 5 SPACES       |
| DRIVE-THRU RESTAURANT                           | = 6 SPACES       |
| CAR LAUNDRY                                     | = 2 SPACES       |
| TOTAL PARKING SPACES PROVIDED = 103 SPACES      |                  |
| MAXIMUM CAR STACKING (SEE CAR STACKING EXHIBIT) |                  |
| CARWASH                                         | = 30 AUTOMOBILES |
| BANK                                            | = 7 AUTOMOBILES  |
| DRIVE-THRU RESTAURANT                           | = 11 AUTOMOBILES |

KEY NOTES

1. 8x12 CONCRETE CURB AND GUTTER, TYP. (SEE DETAILS)
2. DEPRESSED CURB AND GUTTER
3. CONCRETE SIDEWALK, TYP. (SEE DETAILS)
4. ACCESSIBLE PAVEMENT MARKINGS, TYP. (SEE DETAILS)
5. ACCESSIBLE PARKING SIGN, TYP. (MUTCD R7-8, SEE DETAILS)
6. 4" WIDE PAINTED SOLID LINE, TYP.
7. CONNECT TO EXISTING PAVEMENT, SIDEWALK, CURB, TYP.
8. 24" WIDE STOP BAR, TYP. (SEE DETAILS)
9. FREESTANDING SIGNS (SEE ARCHITECTURAL PLANS FOR DETAILS)
10. TRASH COMPACTOR AND ENCLOSURE (SEE ARCHITECTURAL PLANS FOR DETAILS)
11. ACCESSIBLE RAMP (SEE DETAILS)
12. PAVEMENT STRIPING, TYP.
13. CANOPY, TYP. (SEE DETAILS)
14. VACUUM, TYP. (SEE DETAILS)
15. DO NOT ENTER SIGN (SEE DETAILS)
16. DIRECTIONAL SIGN (SEE DETAILS)
17. STOP SIGN (SEE DETAILS)
18. MENU / ORDER BOARDS (SEE DETAILS)
19. CHASE BANK ATM (SEE DETAILS)
20. CHASE BANK BOLLARDS, TYP.
21. PATIO FENCE
22. CAR WASH PAY STATION, TYP. (SEE DETAILS)

PAVING AND CURB LEGEND

- STANDARD DUTY ASPHALT PAVEMENT  
SEE CONSTRUCTION DETAILS FOR PAVEMENT SECTION
- HEAVY DUTY ASPHALT PAVEMENT  
SEE CONSTRUCTION DETAILS FOR PAVEMENT SECTION
- CONCRETE SIDEWALK  
SEE CONSTRUCTION DETAILS FOR PAVEMENT SECTION
- HEAVY DUTY CONCRETE PAVEMENT  
SEE CONSTRUCTION DETAILS FOR PAVEMENT SECTION
- STANDARD PITCH CONCRETE CURB AND GUTTER
- REVERSE PITCH CONCRETE CURB AND GUTTER
- CONCRETE DEPRESSED CURB AND GUTTER

Kimley-Horn  
© 2020 KIMLEY-HORN AND ASSOCIATES, INC.  
4301 WILLOW ROAD, SUITE 100  
WILLOWBROOK, IL 60527  
PHONE: 630-427-5000  
WWW.KIMLEY-HORN.COM

|              |          |
|--------------|----------|
| SCALE:       | AS NOTED |
| DESIGNED BY: | INS      |
| DRAWN BY:    | INS      |
| CHECKED BY:  | UT       |

SITE PLAN

GW PROPERTIES  
PUD PLANS  
735 PLAINFIELD ROAD  
WILLOWBROOK, IL 60527

ORIGINAL ISSUE:  
12/15/2020  
KHA PROJECT NO.  
169726009  
SHEET NUMBER

C1.0













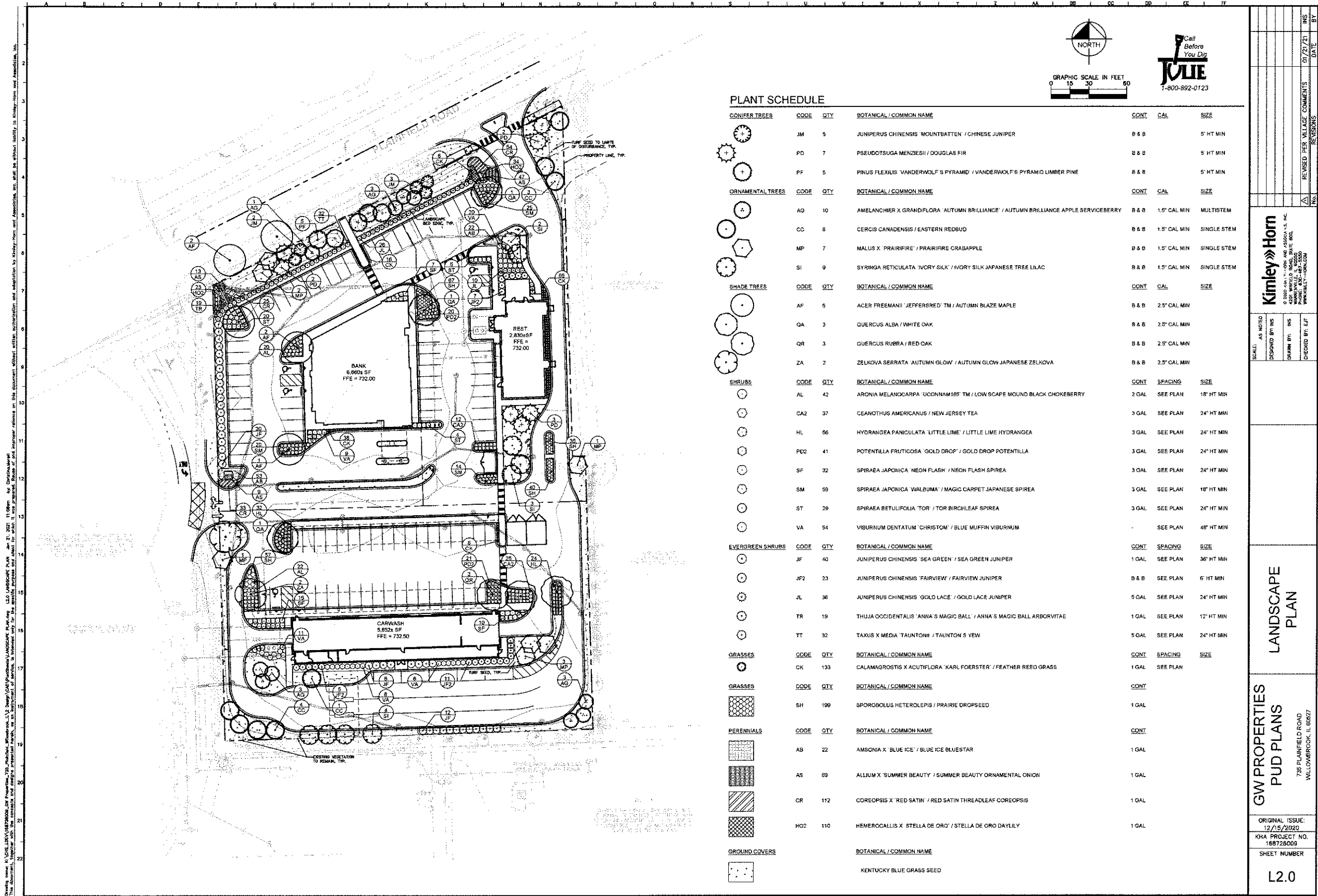












Drawn by: K. NICHOLSON, 18728009, 207 Properties, 730, 2nd Floor, Willowbrook, IL 60527. Date: 12/15/2020. 11:00am. All Construction. This document, together with the contract and design, is intended only for the specific purpose and site for which it was prepared. Reuse of any portion of this document without written authorization and adaptation by KHA is prohibited. KHA and its affiliates shall be held harmless for any and all claims, damages, losses, and expenses, including reasonable attorneys' fees, arising out of or in connection with the use of this document for any purpose other than that for which it was prepared.

Kimley-Horn

© 2020 KHA. KHA, KHA AND ASSOCIATES, INC. 1030 WILLOW ROAD, SUITE 800, WILLOWBROOK, IL 60527. PHONE: 630-487-5500. WWW.KIMLEY-HORN.COM

SCALE: AS NOTED

DESIGNED BY: INS

DRAWN BY: INS

CHECKED BY: KJT

LANDSCAPE PLAN

GW PROPERTIES PUD PLANS

738 PLAINFIELD ROAD WILLOWBROOK, IL 60527

ORIGINAL ISSUE: 12/15/2020

KHA PROJECT NO. 188728009

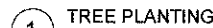
SHEET NUMBER

L2.0

REVISIONS

| NO. | DATE     | BY  | REVISIONS                    |
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| 1   | 07/21/21 | INS | REMOVED PER VILLAGE COMMENTS |





- ## NOTES

1. INSPECT TREE FOR DAMAGED BRANCHES. APPLY CORRECTIVE PRUNING.
2. SET ROOT BALL ON UNEXCAVATED OR TAMPED SOIL. TOP OF ROOTBALL SHALL BE TWO INCHES ABOVE SURROUNDING GRADE WITH BURLAP AND WIRE BASKET INTACT.
3. REMOVE WIRE BASKET AND BURLAP DOWN FOUR TO SIX INCHES BELOW TOP OF ROOT BALL. REMOVE ALL TWIGS AND IF USED, SYNTHETIC MATERIAL. REMOVE EXISTING ROOT GUARD AND ROOTS.
4. TAMP EXCAVATED SOIL AROUND BASE OF ROOTBALL. BACKFILL REMAINDER EXCAVATED SOIL, TAMPED LIGHTLY. HIGH CLAY OR POOR SOIL SHALL RECEIVE SOIL ENHANCEMENT PER LANDSCAPE NOTES. WATER THOROUGHLY WITH TWO HOUSING UNITS TO 10 GAL/ONS OF WATER.
5. APPLY MULCH IN EVEN LAYER, KEEPING AVOIDANCE OF ROOT FLARE.
6. FINAL LOCATION OF TREE TO BE APPROVED BY OWNER.



- ## NOTES

1. APPLY CORRECTIVE PRUNING.
2. SET ROOT BALL OR CONTAINER ON UNEXCAVATED OR TAMPED SOIL TOP OF ROOTBALL (CONTAINER) SHALL BE ONE INCH ABOVE SURROUNDING GRADE. FOR LARGER SHRUBS WITHIN PLANTING BEDS OR DEEPER PIT ONLY FOR THOSE SHRUBS.
3. REMOVE BURLAP OR WRAP TO THE LENGTH OF ROOTBALL. TRIM AND IF USED SYNTHETIC MATERIAL, SHALL BE REMOVED FROM ROOTBALL PRIOR TO CONTAINER GROWN SHRUBS. REMOVE CONTAINER AND LOOSEN ROOTS PRIOR TO INSTALLATION.
4. REMOVE OR CORRECT GIRDLING ROOTS.
5. RUMB AND BACKFILL WITH AMENDED SOIL PER LANDSCAPE NOTES. WATER THOROUGHLY WITHIN TWO HOURS.
6. APPLY MULCH IN EVEN LAYER, KEEPING AWAY FROM ROOT FLATS. MULCH LIMITS FOR SHRUBS EXTEND TO ALL LIMITS OF PLANTING BED. SEE PLANS FOR BED LAYOUTS.



- ## NOTES

1. EXCAVATE PLANTING BED
2. BED HEIGHT IS TO BE 2" ABOVE FINISH GRADE AND WELL DRAINED
3. REMOVE CONTAINER, SCORE SOIL MASS TO REDIRECT AND PREVENT CIRCLING ROOTS, CORRECT GIRDLING ROOTS
4. PLANT MATERIALS TO BE LAYED OUT BY FOLLOWING THE BED EDGE, WORKING TOWARDS THE CENTER OF THE BED USING TRIANGULAR (STAGGERED) SPACING AS PLAUSIBLE
5. PLUMB AND BACKFILL WITH PLANTING MIX AS SPECIFIED IN LANDSCAPE NOTES
6. APPLY MULCH IN EVEN LAYER, KEEPING AWAY FROM ROOT PLACES. MUCH MULCH FOR PERENNIALS/CIRCUMCOVER EXTEND TO ALL UNLIMITED PLANTING BEDS. SEE PLANS FOR BED LAYOUTS
7. SPACING TO BE AS SPECIFIED IN THE PLANT LIST. PERENNIALS SHALL BE PLACED WITH THEIR CENTER 24" FROM EDGE OF BED.

O.C. SPACING, SEE PLANS FOR DETAILS

SECTION

PLAN VIEW

## LANDSCAPE NOTES

1. THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING MATERIALS AND PLANTS SHOWN ON THE LANDSCAPE PLAN. THE CONTRACTOR IS RESPONSIBLE FOR THE COST TO REPAIR UTILITIES, ADJACENT LANDSCAPE, PUBLIC AND PRIVATE PROPERTY THAT IS DAMAGED BY THE CONTRACTOR OR THEIR SUBCONTRACTOR'S OPERATIONS DURING INSTALLATION OR DURING THE SPECIFIED MAINTENANCE PERIOD. CALL FOR UTILITY LOCATIONS PRIOR TO ANY EXCAVATION.
2. THE CONTRACTOR SHALL REPORT ANY DISCREPANCY IN PLAN VS. FIELD CONDITIONS IMMEDIATELY TO THE LANDSCAPE ARCHITECT, PRIOR TO CONTINUING WITH THAT PORTION OF WORK.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPAIR OF ANY OF THEIR TRENCHES OR EXCAVATIONS THAT SETTLE.
4. ALL NURSERY STOCK SHALL BE WELL BRANCHED, HEALTHY, FULL, PRE-INOCULATED AND FERTILIZED. DECIDUOUS TREES SHALL BE FREE OF FRESH CANKS, TRUNKS WILL BE WRAPPED IF NECESSARY TO PREVENT SUN SCALD AND INSECT DAMAGE. THE LANDSCAPE CONTRACTOR SHALL REMOVE THE WRAP AT THE PROPER TIME AS A PART OF THIS CONTRACT.
5. ALL NURSERY STOCK SHALL BE GUARANTEED, BY THE CONTRACTOR, FOR ONE YEAR FROM DATE OF FINAL INSPECTION.
6. AMENDED SOIL SHALL BE PROVIDED AND GRADED BY THE GENERAL CONTRACTOR UP TO 6 INCHES BELOW FINISHED GRADE IN TURF AREAS AND 18 INCHES IN PLANTING AREAS.
7. PLANTING AREA SOIL SHALL BE AMENDED WITH 25% SPHAGNUM PEATMOS, 5% HUMUS AND 65% PULVERIZED SOIL. FOR ALL SHRUB, ORNAMENTAL GRASS, PERENNIAL AND ANNUAL BEDS. AMENDED TURF AREA SOIL SHALL BE STANDARD TOPSOIL.
8. SEED/SOD LIMIT LINES ARE APPROXIMATE. CONTRACTOR SHALL SEED/SOD ALL AREAS WHICH ARE DISTURBED BY GRADING WITH THE SPECIFIED SEED/SOD MIXES.
9. CONTRACTOR SHALL INSTALL 3" SHREDED HARDWOOD MULCH AT A 3" DEPTH TO ALL TREES, SHRUB, PERENNIAL, AND GROUND COVER AREAS. TREES PLANTED IN AREA COVERED BY TURF SHALL RECEIVE A 4 FT WIDE MAXIMUM TREE RING WITH 3" DEPTH SHREDED HARDWOOD MULCH. A SPADED BED EDGE SHALL SEPARATE MULCH BEDS FROM TURF OR SEEDED AREAS. A SPADED EDGE IS NOT REQUIRED ALONG CURBED EDGES.
10. INSTALLATION OF TREES WITHIN PARKWAYS SHALL BE COORDINATED IN THE FIELD WITH LOCATIONS OF UNDERGROUND UTILITIES. TREES SHALL NOT BE LOCATED CLOSER THAN 5' FROM UNDERGROUND UTILITY LINES AND NO CLOSER THAN 10' FROM UTILITY STRUCTURES. DO NOT DISTURB THE EXISTING PARKING, LIGHTING, OR LANDSCAPING THAT EXISTS ADJACENT TO THE SITE LINE UNLESS OTHERWISE NOTED.
11. PLANT QUANTITIES SHOWN ARE FOR THE CONVENIENCE OF THE OWNER AND JURISDICTIONAL REVIEW AGENCIES. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL PLANT QUANTITIES AS DRAWN.
12. THE OWNER'S REPRESENTATIVE MAY REJECT ANY PLANT MATERIALS THAT ARE DISEASED, DEFORMED, OR OTHERWISE NOT EXHIBITING SUPERIOR QUALITY.
13. THE CONTINUED MAINTENANCE OF ALL REQUIRED LANDSCAPING SHALL BE THE RESPONSIBILITY OF THE OWNER OF THE PROPERTY ON WHICH SAID MATERIALS ARE REQUIRED. ALL PLANT MATERIALS NEEDING REPLACEMENT SHALL BE REPLACED BY THE CONTRACTOR AS LIVING VEGETATION AND SHALL BE PROMPTLY REPLACED IF THE PLANT MATERIAL HAS DIED PRIOR TO FINAL ACCEPTANCE. PLANTING AREAS SHALL BE KEPT FREE OF TRASH, LITTER, AND WEEDS AT ALL TIMES.

| WILLOWBORO, IL LANDSCAPE CODE                                                 |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                 |
|-------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| CODE                                                                          | REQUIRED                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       | PROVIDED                                                                                                                                                                                                                                                                                                                                                                                                                                                                                        |
| 9-6-10:<br>FRONT AND EXTERIOR SIDE<br>YARD LANDSCAPE PLANTING<br>REQUIREMENTS | <p>QUANTITY OF PLANTINGS REQUIRED WITHIN ALL YARDS ADJACENT TO A STREET USING THE POINT SYSTEM IN ADDITION TO THE REQUIRED PARKWAY TREES SHALL BE 5 TIMES THE LINEAR LENGTH OF THE SPECIFIED YARD FRONTAGE MEASURED AT PROPERTY LINE.</p> <p>LENGTH OF PLAINFIELD ROAD = 282.77 LF<br/> <math>282.77 \text{ LF} \times 5 = 1,413.85</math><br/> <b>1,414 PLANT UNITS REQUIRED</b></p>                                                                                                                                                                                                                                                                                                                                                                                          | <p>SHADE TREE = <math>0 \times 100 \text{ pts} = 40 \text{ pts}</math><br/>           CONIFER TREE = <math>14 \times 60 \text{ pts} = 840 \text{ pts}</math><br/>           ORNAMENTAL TREE = <math>7 \times 50 \text{ pts} = 350 \text{ pts}</math><br/>           EVERGREEN SHRUB = <math>19 \times 15 \text{ pts} = 285 \text{ pts}</math><br/>           DECIDUOUS SHRUB = <math>0 \times 10 \text{ pts} = 0 \text{ pts}</math></p> <p>TOTAL ALONG PLAINFIELD ROAD = 1,475 PLANT UNITS.</p> |
| 9-10-5:<br>OFF STREET PARKING                                                 | <p>FRONT YARDS AND SIDE YARDS:<br/> <b>SCREENING ADJACENT TO PARKING AREAS REQUIRED</b><br/>           (EARTH BERM, DENSELY PLANTED EVERGREEN SHRUBS OR COMBINATION)</p>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       | DENSELY PLANTED EVERGREEN SHRUBS ADJACENT TO ALL PARKING AREAS                                                                                                                                                                                                                                                                                                                                                                                                                                  |
|                                                                               | <p>INTERIOR SIDE AND REAR YARDS:<br/> <b>SCREENING ADJACENT TO PARKING AREAS REQUIRED</b><br/>           (EARTH BERM, DENSELY PLANTED EVERGREEN SHRUBS OR COMBINATION)</p>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                     | DENSELY PLANTED EVERGREEN SHRUBS ADJACENT TO ALL PARKING AREAS                                                                                                                                                                                                                                                                                                                                                                                                                                  |
|                                                                               | <p>INTERIOR PARKING LOT AREA SHALL BE NO LESS THAN 15% OF SITE AREA (EXCLUDING BUILDING, FRONT YARD AND ALL ROW AREA). 1 SHADE TREE AND 2 DWARF VARIETY SHRUBS REQUIRED PER ISLAND (EVERY 20 PARKING SPACES) OR 50 LF OF PARKING AREA.</p> <p>INTERIOR VEHICULAR USE AREA = <math>71,391,5789 \text{ SQ FT}</math><br/> <math>71,391,5789 \text{ SQ FT} \times 0.10 = 7,139.16</math><br/> <b>7,139 SQ FT OF LANDSCAPING REQUIRED</b></p> <p><b>ALL PARKING LOT ISLANDS REQUIRED TO HAVE ONE SHADE TREE AND 20 DWARF VARIETY SHRUBS</b></p>                                                                                                                                                                                                                                    | <p>9,792.08 SQ FT OF INTERIOR LANDSCAPE AREA PROVIDED</p> <p>ALL PARKING LOT ISLANDS CONTAIN ONE SHADE TREE AND 2 DWARF VARIETY SHRUBS</p>                                                                                                                                                                                                                                                                                                                                                      |
| 9-11-14:<br>SIGN LANDSCAPE                                                    | <p>EVERY 1 SF OF SIGN FACE SURFACE AREA THERE WILL BE 1.5 SF OF LANDSCAPE AREA PROVIDED</p> <p><b>24 SF OF TOTAL SIGN AREA = 1.5 = 36 SF OF LANDSCAPE AREA REQUIRED</b></p>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                    | 400 TOTAL SQUARE FEET OF LANDSCAPE PROVIDED AROUND THE BASE OF BOTH SIGNS                                                                                                                                                                                                                                                                                                                                                                                                                       |
| 9-14-2:<br>MINIMUM PLANTINGS                                                  | <p>TOTAL GROSS LOT AREA = 121,552.44 SQ. FT<br/> <math>121,552.44 \text{ SQ. FT.} / 725 \text{ SQ. FT.} = 167.65</math><br/> <b>168 PLANTING UNITS REQUIRED</b></p> <p>SHADE TREES = 5% OF REQUIRED PLANTING UNITS<br/> <math>167.65 \times 0.05 = 8.38</math><br/> <b>8 SHADE TREES REQUIRED</b></p> <p>CONIFER TREES = 10% OF REQUIRED PLANTING UNITS<br/> <math>167.65 \times 0.10 = 16.76</math><br/> <b>17 CONIFER TREES REQUIRED</b></p> <p>ORNAMENTAL TREES = 20% OF REQUIRED PLANTING UNITS<br/> <math>167.65 \times 0.20 = 33.53</math><br/> <b>34 ORNAMENTAL TREES REQUIRED</b></p> <p>SHRUBS / GRASSES / PERENNIALS = OTHER 65% OF REQUIRED PLANTING UNITS<br/> <math>167.65 \times 0.65 = 108.97</math><br/> <b>109 SHRUBS / GRASSES / PERENNIALS REQUIRED</b></p> | <p>13 SHADE TREES PROVIDED</p> <p>17 CONIFER TREES PROVIDED</p> <p>34 ORNAMENTAL TREES PROVIDED</p> <p>500 DECIDUOUS AND EVERGREEN SHRUBS PROVIDED</p> <p>332 GRASSES PROVIDED</p> <p>252 PERENNIALS PROVIDED</p> <p>* NOTE - SHRUBS, GRASSES AND PERENNIALS PLANTED THROUGHOUT THE SITE ALONG THE PARKING PERIMETER, IF PARKING LOT ISLANDS AND ALONG BUILDING FOUNDATION</p>                                                                                                                  |

Call  
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**Kimley»»Horn**

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|--------------|----------|
| SCALE:       | AS NOTED |
| DESIGNED BY: | INS      |
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| CHECKED BY:  | EJT      |

LANDSCAPE  
NOTES &  
DETAILSGW PROPERTIES  
PUD PLANS

|                 |            |
|-----------------|------------|
| ORIGINAL ISSUE: | 12/15/2020 |
| KHA PROJECT NO. | 168726009  |
| SHEET NUMBER    |            |

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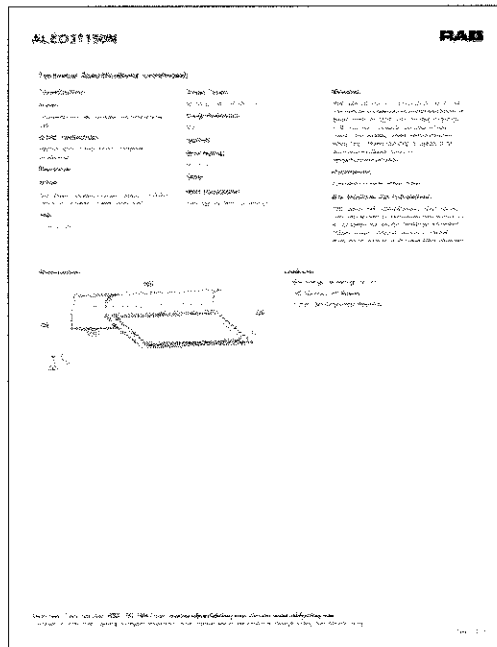
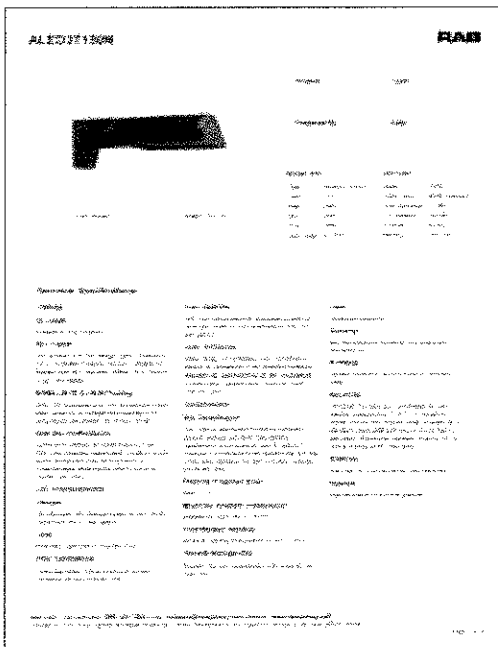
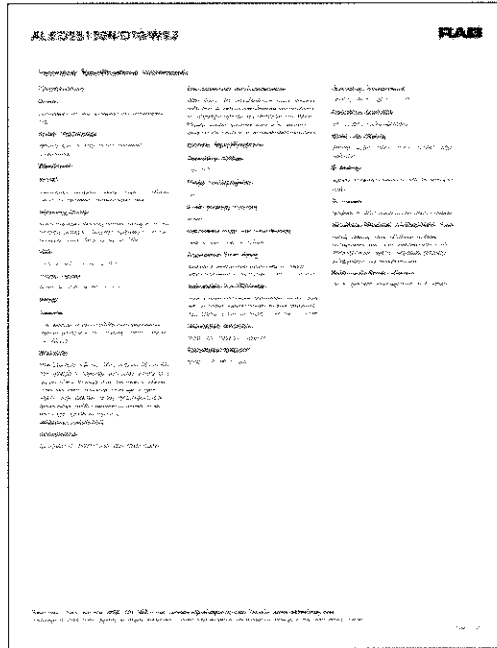
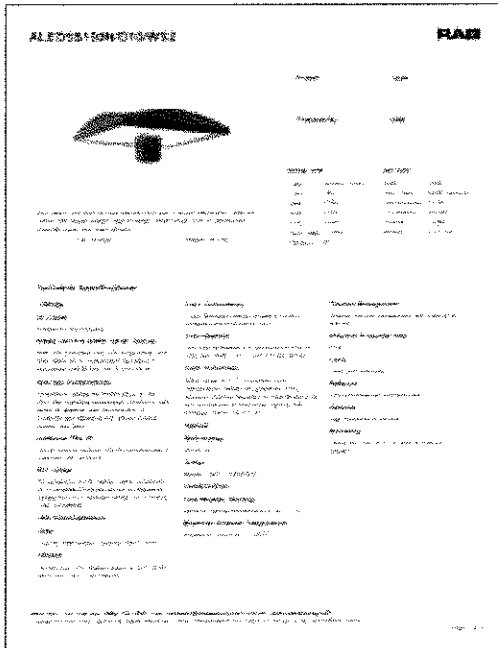
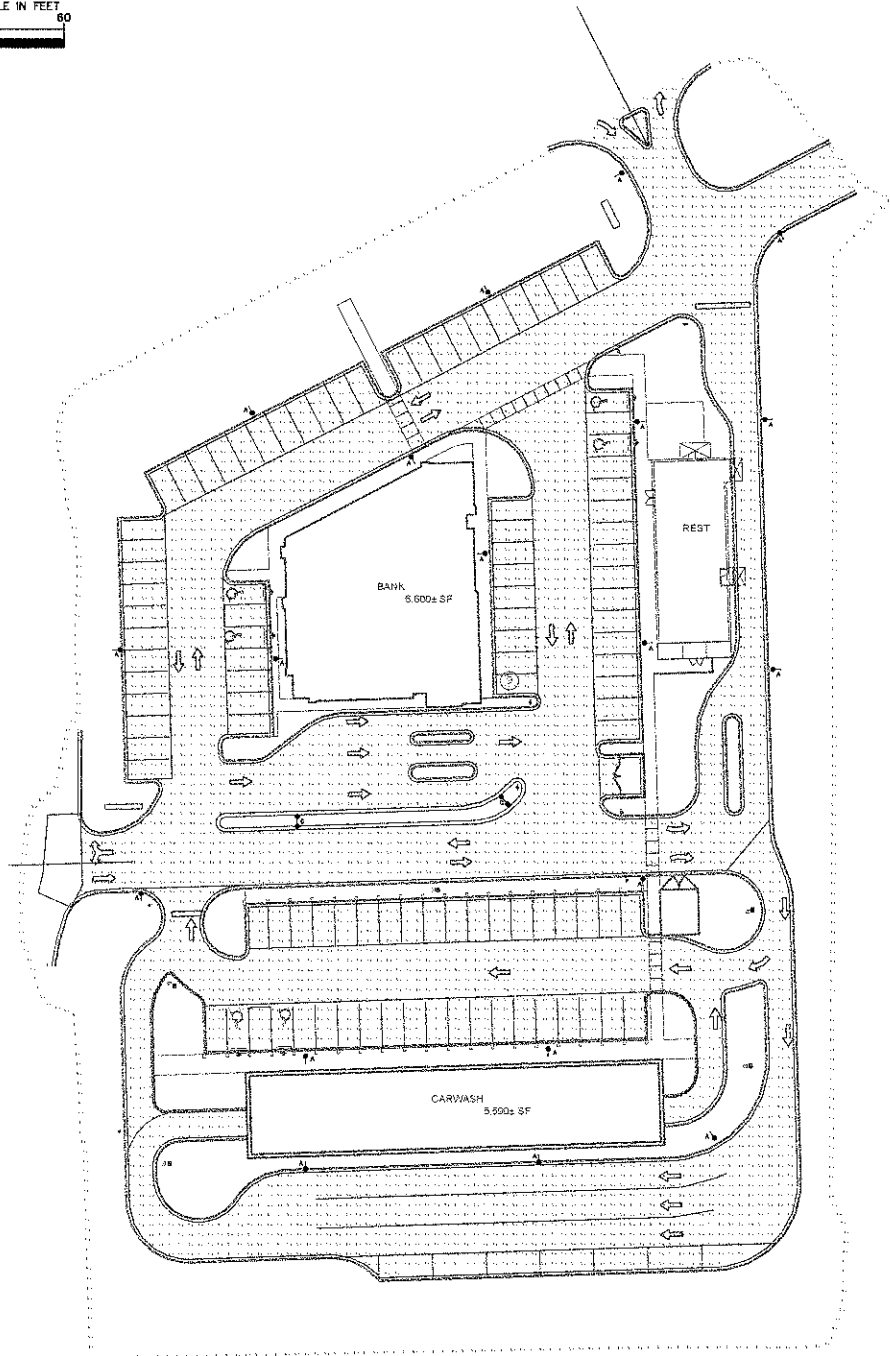




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NOTE: LIGHT POLES SHALL BE 20 FEET MOUNTED ON 3 FOOT EXPOSED FOUNDATION FOR A LIGHT FIXTURE MOUNTING HEIGHT OF 23 FEET.

|                      |         |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |
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| CRANFIELD, M. SOWERY | 1954-75 | 1954 | 1955 | 1956 | 1957 | 1958 | 1959 | 1960 | 1961 | 1962 | 1963 | 1964 | 1965 | 1966 | 1967 | 1968 | 1969 | 1970 | 1971 | 1972 | 1973 | 1974 | 1975 | 1976 | 1977 | 1978 | 1979 | 1980 | 1981 | 1982 | 1983 | 1984 | 1985 | 1986 | 1987 | 1988 | 1989 | 1990 | 1991 | 1992 | 1993 | 1994 | 1995 | 1996 | 1997 | 1998 | 1999 | 2000 | 2001 | 2002 | 2003 | 2004 | 2005 | 2006 | 2007 | 2008 | 2009 | 2010 | 2011 | 2012 | 2013 | 2014 | 2015 | 2016 | 2017 | 2018 | 2019 | 2020 | 2021 | 2022 | 2023 | 2024 | 2025 | 2026 | 2027 | 2028 | 2029 | 2030 | 2031 | 2032 | 2033 | 2034 | 2035 | 2036 | 2037 | 2038 | 2039 | 2040 | 2041 | 2042 | 2043 | 2044 | 2045 | 2046 | 2047 | 2048 | 2049 | 2050 | 2051 | 2052 | 2053 | 2054 | 2055 | 2056 | 2057 | 2058 | 2059 | 2060 | 2061 | 2062 | 2063 | 2064 | 2065 | 2066 | 2067 | 2068 | 2069 | 2070 | 2071 | 2072 | 2073 | 2074 | 2075 | 2076 | 2077 | 2078 | 2079 | 2080 | 2081 | 2082 | 2083 | 2084 | 2085 | 2086 | 2087 | 2088 | 2089 | 2090 | 2091 | 2092 | 2093 | 2094 | 2095 | 2096 | 2097 | 2098 | 2099 | 2100 | 2101 | 2102 | 2103 | 2104 | 2105 | 2106 | 2107 | 2108 | 2109 | 2110 | 2111 | 2112 | 2113 | 2114 | 2115 | 2116 | 2117 | 2118 | 2119 | 2120 | 2121 | 2122 | 2123 | 2124 | 2125 | 2126 | 2127 | 2128 | 2129 | 2130 | 2131 | 2132 | 2133 | 2134 | 2135 | 2136 | 2137 | 2138 | 2139 | 2140 | 2141 | 2142 | 2143 | 2144 | 2145 | 2146 | 2147 | 2148 | 2149 | 2150 | 2151 | 2152 | 2153 | 2154 | 2155 | 2156 | 2157 | 2158 | 2159 | 2160 | 2161 | 2162 | 2163 | 2164 | 2165 | 2166 | 2167 | 2168 | 2169 | 2170 | 2171 | 2172 | 2173 | 2174 | 2175 | 2176 | 2177 | 2178 | 2179 | 2180 | 2181 | 2182 | 2183 | 2184 | 2185 | 2186 | 2187 | 2188 | 2189 | 2190 | 2191 | 2192 | 2193 | 2194 | 2195 | 2196 | 2197 | 2198 | 2199 | 2200 | 2201 | 2202 | 2203 | 2204 | 2205 | 2206 | 2207 | 2208 | 2209 | 2210 | 2211 | 2212 | 2213 | 2214 | 2215 | 2216 | 2217 | 2218 | 2219 | 2220 | 2221 | 2222 | 2223 | 2224 | 2225 | 2226 | 2227 | 2228 | 2229 | 2230 | 2231 | 2232 | 2233 | 2234 | 2235 | 2236 | 2237 | 2238 | 2239 | 2240 | 2241 | 2242 | 2243 | 2244 | 2245 | 2246 | 2247 | 2248 | 2249 | 2250 | 2251 | 2252 | 2253 | 2254 | 2255 | 2256 | 2257 | 2258 | 2259 | 2260 | 2261 | 2262 | 2263 | 2264 | 2265 | 2266 | 2267 | 2268 | 2269 | 2270 | 2271 | 2272 | 2273 | 2274 | 2275 | 2276 | 2277 | 2278 | 2279 | 2280 | 2281 | 2282 | 2283 | 2284 | 2285 | 2286 | 2287 | 2288 | 2289 | 2290 | 2291 | 2292 | 2293 | 2294 | 2295 | 2296 | 2297 | 2298 | 2299 | 2300 | 2301 | 2302 | 2303 | 2304 | 2305 | 2306 | 2307 | 2308 | 2309 | 2310 | 2311 | 2312 | 2313 | 2314 | 2315 | 2316 | 2317 | 2318 | 2319 | 2320 | 2321 | 2322 | 2323 | 2324 | 2325 | 2326 | 2327 | 2328 | 2329 | 2330 | 2331 | 2332 | 2333 | 2334 | 2335 | 2336 | 2337 | 2338 | 2339 | 2340 | 2341 | 2342 | 2343 | 2344 | 2345 | 2346 | 2347 | 2348 | 2349 | 2350 | 2351 | 2352 | 2353 | 2354 | 2355 | 2356 | 2357 | 2358 | 2359 |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |   |
| Labels               |         | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1    | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 |



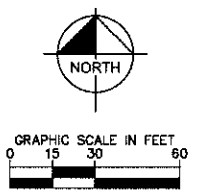
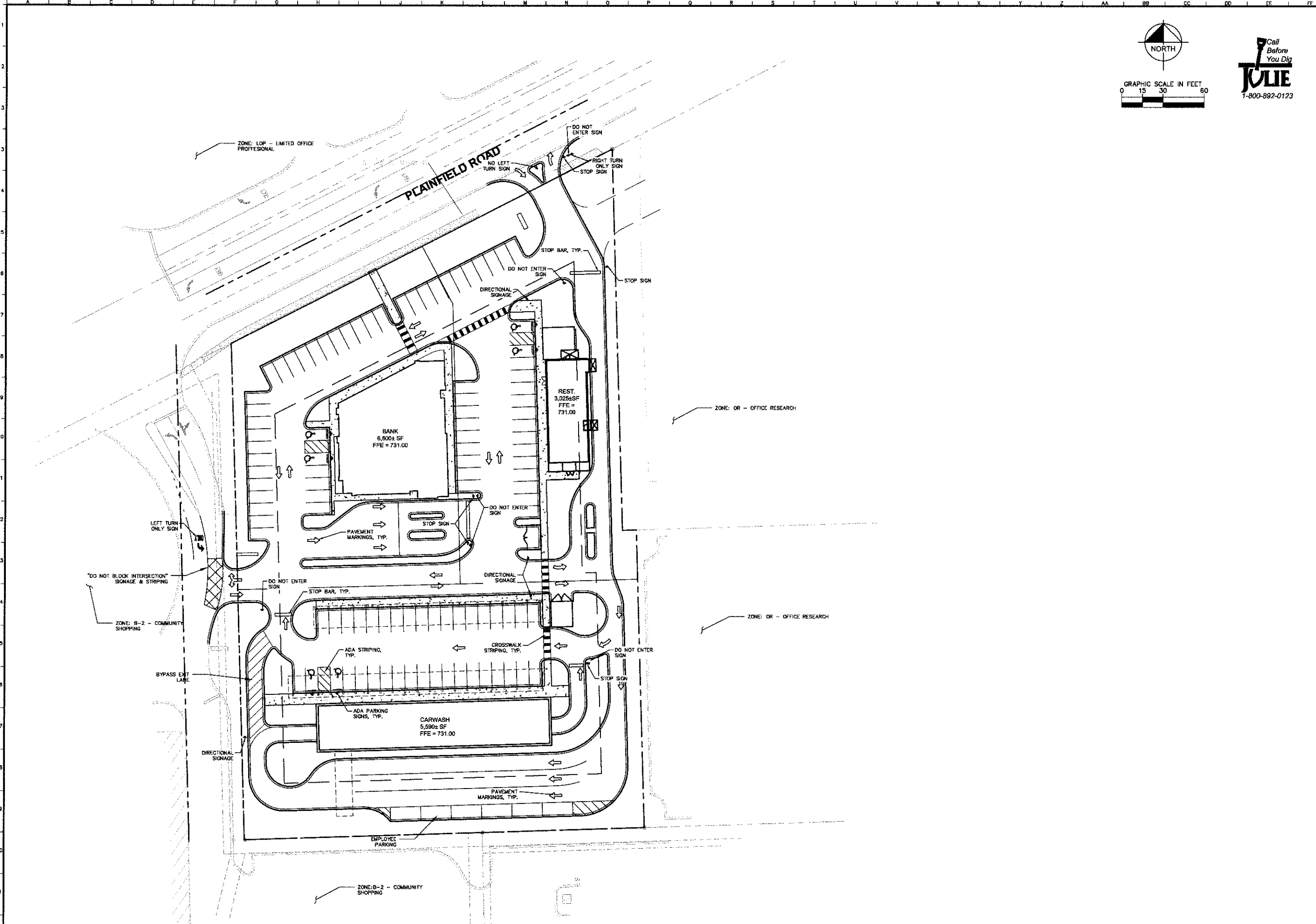
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|--------------------------------------------------------------------------------|----------------------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------|----------------------|---------------|-----------------|-----------------------|----------------------------------------------|----|
| GW PROPERTIES<br>PUD PLANS<br><br>795 PLAINFIELD ROAD<br>YELLOWBROOK, IL 60527 | PHOTOMETRICS<br>PLAN | <br>D 2500 HILLTOP DRIVE AND ASSOCIATES, INC.<br>1000 WEST 10TH AVENUE, SUITE 100<br>WASHINGTON, D.C. 20004<br>PHONE: (202) 638-2200<br>WWW.KIMLEY-HORN.COM | SCALE: AS NOTED                                               | DESIGNED BY: INS     | DRAWN BY: INS | CHECKED BY: EJT | REVISIONS<br>No. DATE | REVISED PER VILLAGE COMMENTS<br>01/21/21 INS | BY |
|                                                                                |                      |                                                                                                                                                                                                                                                | ORIGINAL ISSUE:<br>12/15/2020<br>KHA PROJECT NO.<br>168728009 | SHEET NUMBER<br>E1.0 |               |                 |                       |                                              |    |



File name: K:\CHS\LD\168726009 GW Properties\_735 Plainfield Road\168726009 GW Properties\_735 Plainfield Road\168726009 GW Properties\_735 Plainfield Road.dwg Date: 12/15/2020 10:10:10 AM by: J. S. Spence  
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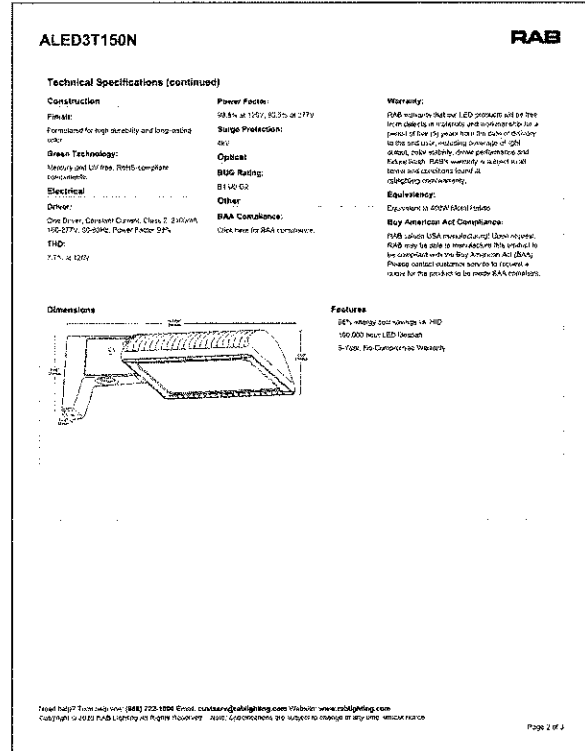
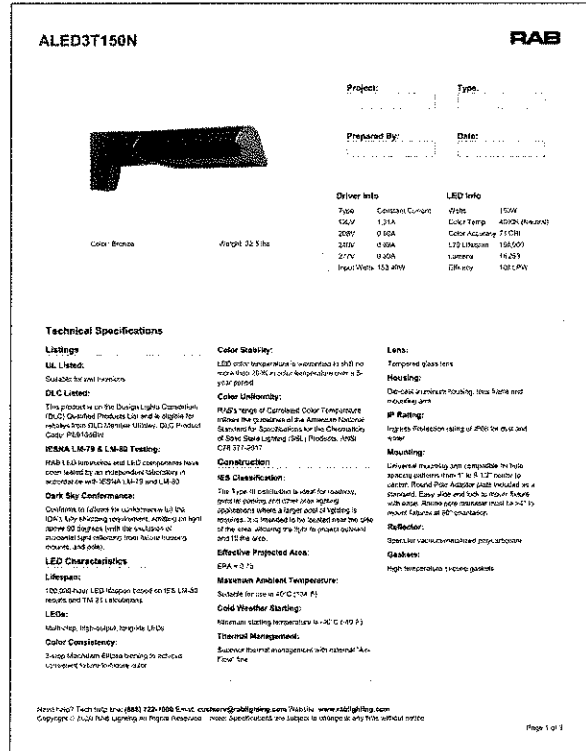
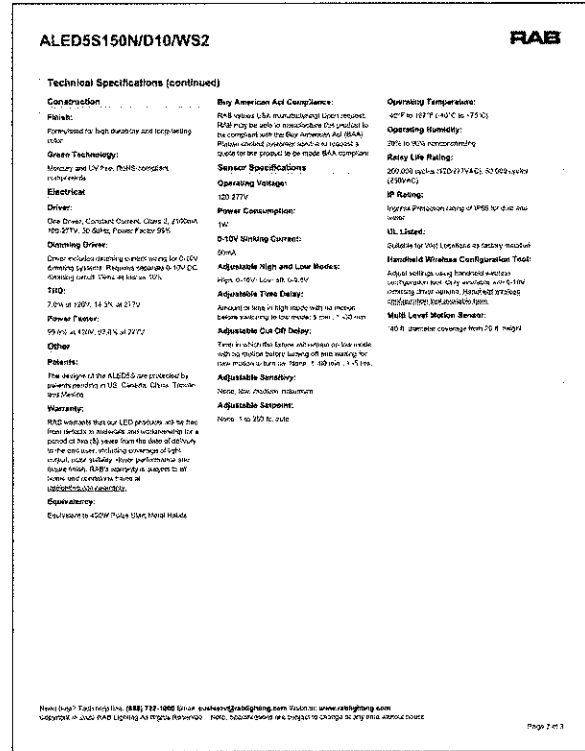
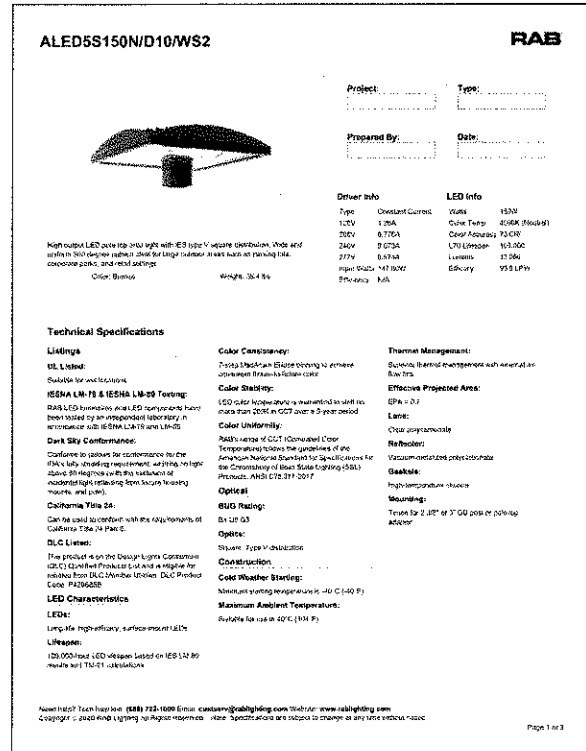


|                                              |  |                               |  |                 |  |                                                                                                                          |  |
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| 735 PLAINFIELD ROAD<br>WILLOWBROOK, IL 60527 |  | ORIGINAL ISSUE:<br>12/15/2020 |  | SCALE: AS NOTED |  | DESIGNED BY: NS                                                                                                          |  |
| SHEET NUMBER                                 |  | KHA PROJECT NO.<br>168726009  |  | DRAWN BY: NS    |  | CHECKED BY: EJT                                                                                                          |  |
| 1 OF 1                                       |  |                               |  |                 |  | REVISE NO. DATE BY                                                                                                       |  |



NOTE: LIGHT POLES SHALL BE 20 FEET MOUNTED ON 3 FOOT EXPOSED FOUNDATION FOR A LIGHT FIXTURE MOUNTING HEIGHT OF 23 FEET.

| Calculation Summary   |             |       |      |      |     |         |         |
|-----------------------|-------------|-------|------|------|-----|---------|---------|
| Label                 | CalcType    | Units | Avg  | Max  | Min | Avg/Min | Max/Min |
| PARKING LOT           | Illuminance | Fc    | 3.00 | 6.4  | 1.0 | 3.33    | 7.11    |
| PERIMETER             | Illuminance | Fc    | 0.37 | 1.8  | 0.0 | N.A.    | N.A.    |
| WILLOWBROOK STANDARDS | Illuminance | Fc    | N.A. | 5    | 1   | 6:1     | 20:1    |
| TESNA STANDARDS       | Illuminance | Fc    | N.A. | N.A. | 1   | N.A.    | 19:1    |

[illegible]

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|              |          |
|--------------|----------|
| SCALE:       | AS NOTED |
| DESIGNED BY: | INS      |
| DRAWN BY:    | INS      |
| CHECKED BY:  | EJT      |

# PHOTOMETRICS PLAN

**GW PROPERTIES  
PUD PLANS**

ORIGINAL ISSUE:  
12/15/2020  
KHA PROJECT NO,  
188726009  
SHEET NUMBER



PRELIMINARY PLAT OF SUBDIVISION  
735 PLAINFIELD ROAD SUBDIVISION  
A PLANNED UNIT DEVELOPMENT

OWNER'S CERTIFICATE

STATE OF \_\_\_\_\_ } SS  
COUNTY OF \_\_\_\_\_ }

THIS IS TO CERTIFY THAT \_\_\_\_\_, LIMITED LIABILITY COMPANY, IS THE OWNER OF THE PROPERTY DESCRIBED AND SHOWN HEREON AND AS SUCH OWNER, HAS CAUSED THE PROPERTY TO BE SURVEYED AND SUBDIVIDED AS SHOWN HEREON, FOR THE USES AND PURPOSES THEREIN SET FORTH AND AS ALLOWED AND PROVIDED BY STATUTES, AND SAID LIMITED LIABILITY COMPANY DOES HEREBY ACKNOWLEDGE AND ADOPT THE SAME UNDER THE STYLE AND TITLE AFORESAID.

ALSO, THIS IS TO CERTIFY THAT THE PROPERTY BEING SUBDIVIDED AFORESAID AND, TO THE BEST OF OWNER'S KNOWLEDGE AND BELIEF, SAID SUBDIVISION LIES ENTIRELY WITHIN THE LIMITS OF SCHOOL DISTRICT \_\_\_\_\_.

DATED AT \_\_\_\_\_, THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, A.D., 20\_\_\_\_.

OWNER NAME: \_\_\_\_\_  
ADDRESS: \_\_\_\_\_

BY: \_\_\_\_\_  
TITLE: \_\_\_\_\_  
PRINT TITLE

NOTARY'S CERTIFICATE

STATE OF \_\_\_\_\_ } SS  
COUNTY OF \_\_\_\_\_ }

I, \_\_\_\_\_, A NOTARY PUBLIC IN AND FOR THE SAID COUNTY IN THE STATE AFORESAID, DO HEREBY CERTIFY THAT \_\_\_\_\_ (PRINT NAME), (TITLE) OF SAID LIMITED LIABILITY COMPANY, WHO ARE PERSONALLY KNOWN TO ME TO BE THE SAME PERSON WHOSE NAME IS SUBSCRIBED TO THE FOREGOING INSTRUMENT AS SUCH APPEARED BEFORE ME THIS DAY IN PERSON AND ACKNOWLEDGED THAT THEY SIGNED AND DELIVERED THE SAID INSTRUMENT AS THEIR OWN FREE AND VOLUNTARY ACT AND AS THE FREE AND VOLUNTARY ACT OF SAID LIMITED LIABILITY COMPANY, FOR THE USES AND PURPOSES THEREIN SET FORTH.

GIVEN UNDER MY HAND AND NOTARIAL SEAL THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, A.D., 20\_\_\_\_.

NOTARY PUBLIC SIGNATURE \_\_\_\_\_

(PRINT NAME)

MORTGAGEE'S CERTIFICATE

STATE OF \_\_\_\_\_ } SS  
COUNTY OF \_\_\_\_\_ }

MORTGAGEE UNDER \_\_\_\_\_, AS PROVISIONS OF A CERTAIN MORTGAGE DATED \_\_\_\_\_ AND RECORDED IN THE RECORDER'S OFFICE OF \_\_\_\_\_ COUNTY, ILLINOIS, AS DOCUMENT NUMBER \_\_\_\_\_, HEREBY CONSENTS TO RECORDING OF THE PLAT HEREIN SHOWN.

DATED AT \_\_\_\_\_, THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, A.D., 20\_\_\_\_.

BY: \_\_\_\_\_ BY: \_\_\_\_\_  
TITLE: \_\_\_\_\_ TITLE: \_\_\_\_\_

NOTARY'S CERTIFICATE

STATE OF \_\_\_\_\_ }  
COUNTY OF \_\_\_\_\_ }

I, \_\_\_\_\_, A NOTARY PUBLIC IN THE COUNTY AND STATE AFORESAID, DO HEREBY CERTIFY THAT \_\_\_\_\_ (TITLE) AND \_\_\_\_\_ (TITLE) ARE PERSONALLY KNOWN TO ME TO BE THE SAME PERSONS WHO ARE SUBSCRIBED TO THE FOREGOING MORTGAGEE'S CERTIFICATE APPEARED BEFORE ME THIS DAY IN PERSON AND ACKNOWLEDGED THE EXECUTION OF THIS INSTRUMENT IN THEIR CAPACITY FOR THE FOR THE USES AND PURPOSES THEREIN SET FORTH AS THE FREE AND VOLUNTARY ACT AN DEED OF SAID CORPORATION.

GIVEN UNDER MY HAND AND NOTARIAL SEAL THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, A.D., 20\_\_\_\_.

BY: \_\_\_\_\_  
NOTARY PUBLIC

PLANNING COMMISSION CERTIFICATE

STATE OF ILLINOIS }  
COUNTY OF DUPAGE }

I, \_\_\_\_\_, THE UNDERSIGNED, AS CHAIRMAN OF THE PLANNING COMMISSION OF THE VILLAGE OF WILLOWBROOK, DUPAGE COUNTY, ILLINOIS, DO HEREBY CERTIFY THAT THIS DOCUMENT HAS BEEN APPROVED BY SAID PLANNING COMMISSION.

THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, A.D., 2020.

PLANNING COMMISSION, VILLAGE OF WILLOWBROOK

CHAIRMAN \_\_\_\_\_

VILLAGE ENGINEER'S CERTIFICATE

STATE OF ILLINOIS }  
COUNTY OF DUPAGE }

THIS IS TO CERTIFY THAT I, \_\_\_\_\_, THE VILLAGE ENGINEER FOR THE VILLAGE OF WILLOWBROOK, ILLINOIS, HAVE REVIEWED AND APPROVED THIS PLAT.

DATED THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 2020 A.D.

VILLAGE ENGINEER \_\_\_\_\_

HEALTH OFFICER CERTIFICATE

STATE OF ILLINOIS }  
COUNTY OF DUPAGE }

ACCEPTED AND APPROVED BY THE COUNTY HEALTH OFFICER, OF DUPAGE COUNTY, ILLINOIS THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, A.D. 2017

COUNTY HEALTH OFFICER \_\_\_\_\_

VILLAGE BOARD CERTIFICATE

STATE OF ILLINOIS }  
COUNTY OF DUPAGE }

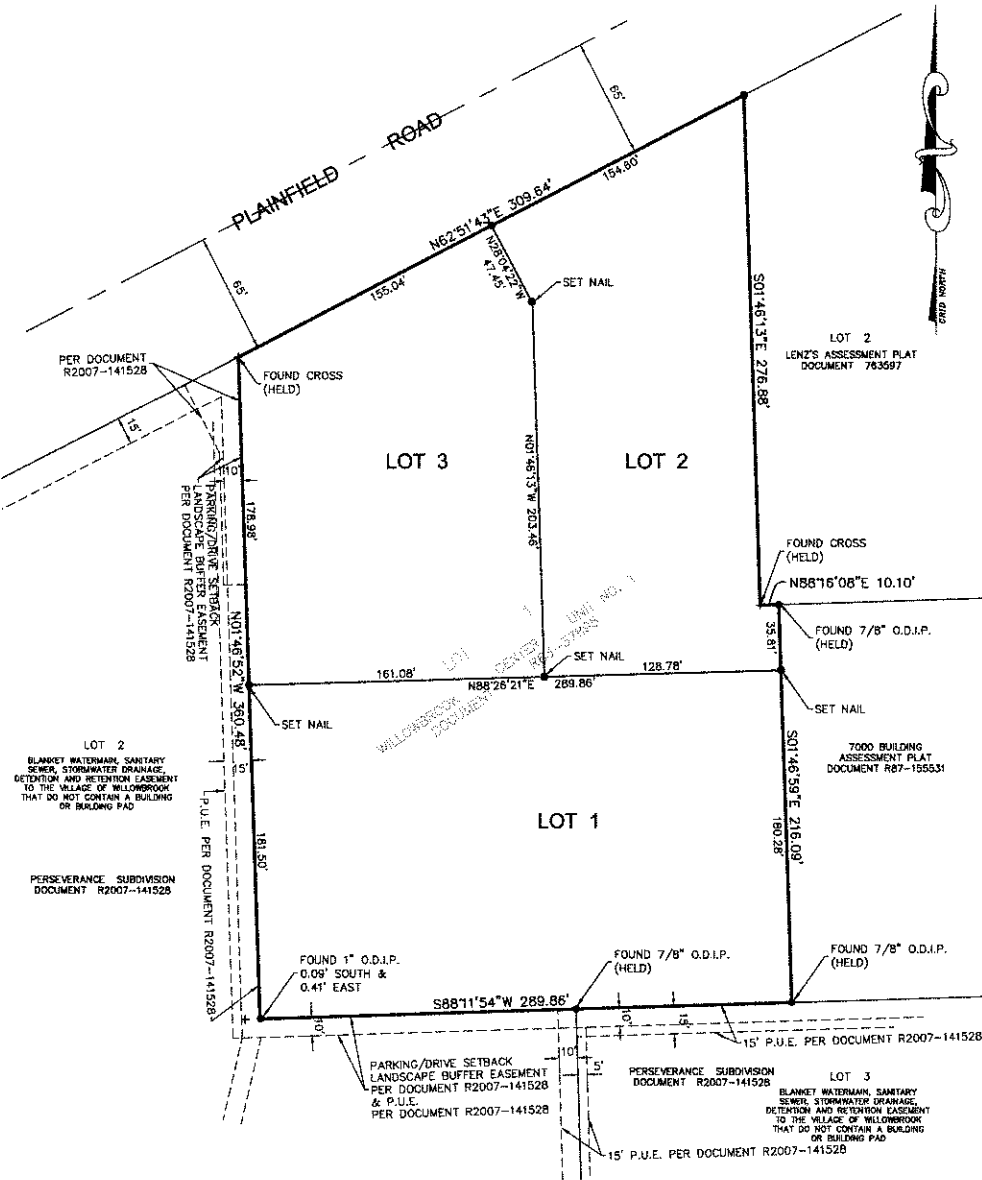
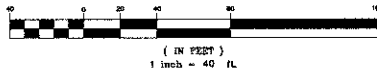
APPROVED BY THE VILLAGE PRESIDENT AND THE VILLAGE BOARD OF TRUSTEES OF THE VILLAGE OF WILLOWBROOK, DUPAGE COUNTY, ILLINOIS, THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 2020 A.D.

PRESIDENT \_\_\_\_\_

ATTEST: \_\_\_\_\_  
VILLAGE CLERK

LOT 1 IN WILLOWBROOK CENTER UNIT NO. 1, OF PART OF LOT 7 IN OWNER'S SUBDIVISION IN SECTIONS 23 AND 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED OCTOBER 16, 1963, AS DOCUMENT R63-37895, IN DUPAGE COUNTY, ILLINOIS.

GRAPHIC SCALE



LEGEND

- SET 7/8" O.D.I.P. UNLESS OTHERWISE NOTED
- SET CONCRETE MONUMENT UNLESS OTHERWISE NOTED
- SET CROSS IN CONCRETE UNLESS OTHERWISE NOTED

LINE LEGEND

- SUBDIVISION BOUNDARY LINE
- ADJACENT LAND PARCEL LINE
- LOT LINE
- EASEMENT LINE
- CENTERLINE
- BUILDING SETBACK LINE
- SECTION LINE

ABBREVIATIONS

- O.D.I.P. = OUTSIDE DIAMETER IRON PIPE
- N = NORTH
- S = SOUTH
- E = EAST
- W = WEST
- (R) = RECORD BEARING OR DISTANCE
- (M) = MEASURED BEARING OR DISTANCE
- (C) = CALCULATED BEARING OR DISTANCE
- (D) = DEED BEARING OR DISTANCE
- B.S.L. = BUILDING SETBACK LINE
- U.E. = UTILITY EASEMENT
- D.E. = DRAINAGE EASEMENT
- P.U.E. = PUBLIC UTILITY EASEMENT
- P.O.B. = POINT OF BEGINNING
- P.U. & D.E. = PUBLIC UTILITY AND DRAINAGE EASEMENT

AREA SUMMARY

|                            |                     |    |             |
|----------------------------|---------------------|----|-------------|
| LOT 1                      | 52,432 SQUARE FEET  | OR | 1.204 ACRES |
| LOT 2                      | 34,652 SQUARE FEET  | OR | 0.795 ACRES |
| LOT 3                      | 34,483 SQUARE FEET  | OR | 0.792 ACRES |
| R.O.W. DEDICATION          | 0 SQUARE FEET       | OR | 0 ACRES     |
| NET AREA                   | 121,567 SQUARE FEET | OR | 2.791 ACRES |
| (TO HEAVY LINES)           |                     |    |             |
| (BASED ON MEASURED VALUES) |                     |    |             |

SURVEYOR'S AUTHORIZATION TO RECORD

STATE OF ILLINOIS }  
COUNTY OF KANE }

I HEREBY DESIGNATE \_\_\_\_\_ AND/OR REPRESENTATIVES THEREOF, TO RECORD THIS PLAT, A TRUE COPY OF WHICH HAS BEEN RETAINED BY ME TO ASSURE NO CHANGES HAVE BEEN MADE TO SAID PLAT.

DATED THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_\_\_, AT AURORA, KANE COUNTY, ILLINOIS.

COMPASS SURVEYING LTD.  
PROFESSIONAL DESIGN FIRM  
LAND SURVEYOR CORPORATION NO. 184-002778  
LICENSE EXPIRES 4/30/2023

BY: \_\_\_\_\_  
SCOTT C. KREBS  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3509  
EXPIRES 11/30/2022

SURVEYOR'S CERTIFICATE

STATE OF ILLINOIS }  
COUNTY OF KANE }

I, SCOTT C. KREBS, ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3509, HAVE SURVEYED AND SUBDIVIDED THE FOLLOWING PROPERTY:

LOT 1 IN WILLOWBROOK CENTER UNIT NO. 1 IN SECTION 23, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT RECORDED OCTOBER 16, 1963 AS DOCUMENT R63-37895, IN DUPAGE COUNTY, ILLINOIS.

AS SHOWN BY THE ANNEXED PLAT WHICH IS A CORRECT REPRESENTATION OF SAID SURVEY AND SUBDIVISION. ALL DISTANCES ARE SHOWN IN FEET AND DECIMALS THEREOF. THIS SUBDIVISION IS WITHIN THE VILLAGE OF WILLOWBROOK WHICH HAS ADOPTED AN OFFICIAL COMPREHENSIVE PLAN AND IS EXERCISING THE SPECIAL POWERS AUTHORIZED BY THE STATE OF ILLINOIS ACCORDING TO 65 ILCS 5/11-12-6 AS HERETOFORE AND HEREAFTER AMENDED, AND THIS SITE FALLS WITHIN "OTHER AREAS ZONE X" (AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN) AS DEFINED BY THE FLOOD INSURANCE RATE MAP COMMUNITY PANEL NUMBER 170220 0189 J, MAP NUMBER 17043C0189J HAVING A REVISED DATE OF AUGUST 1, 2019.

GIVEN UNDER MY HAND AND SEAL AT AURORA, ILLINOIS THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_\_\_.

COMPASS SURVEYING LTD.  
PROFESSIONAL DESIGN FIRM  
LAND SURVEYOR CORPORATION NO. 184-002778  
LICENSE EXPIRES 4/30/2023

BY: \_\_\_\_\_  
SCOTT C. KREBS  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3509  
LICENSE EXPIRES 11/30/2022

|                                               |  |                            |                                                                                                                                                                                                                                                                            |                                                                                                                                 |                                                                                                                                                                                                                                                  |                                                                                 |  |  |  |  |  |  |  |  |  |                                                              |  |
|-----------------------------------------------|--|----------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------|--|--|--|--|--|--|--|--|--|--------------------------------------------------------------|--|
| <div>2020 NO. 20 0006</div> <div>1 OF 1</div> |  | <div>SCALE: 1" = 40'</div> | <div><div>COMPASS</div><div>SURVEYING LTD</div><div>ALTA SURVEYS • TOPOGRAPHY • CONSTRUCTION STAKING</div><div>2631 GINGER WOODS PARKWAY, STE. 100</div><div>AURORA, IL 60002</div><div>PHONE: (630) 820-9100 FAX: (630) 820-7630 EMAIL: ADMIN@CLSURVEYING.COM</div></div> | <div><div>PROJECT</div><div>735 PLAINFIELD ROAD SUBDIVISION</div><div>735 Plainfield Road</div><div>Willowbrook, IL</div></div> | <div><div>CLIENT</div><div><div>Kimley»Horn</div><div>© 2017 KIMLEY-HORN AND ASSOCIATES, INC.</div><div>1001 WARRENVILLE ROAD, SUITE 350,</div><div>JOLI, IL 60532</div><div>PHONE: 630-497-5500</div><div>WWW.KIMLEY-HORN.COM</div></div></div> | <div>DATE: 1/31/20 PG: N/A DRAWN BY: MRA CHECKED BY: SK BOOK: N/A PG: N/A</div> |  |  |  |  |  |  |  |  |  | <div>J:\Projects\2020 Projects\20 0006\20 0006 PSD.dwg</div> |  |
|                                               |  |                            |                                                                                                                                                                                                                                                                            |                                                                                                                                 |                                                                                                                                                                                                                                                  | <div>REVISIONS</div> <div>DATE BY</div>                                         |  |  |  |  |  |  |  |  |  |                                                              |  |
|                                               |  |                            |                                                                                                                                                                                                                                                                            |                                                                                                                                 |                                                                                                                                                                                                                                                  | <div>1 REVISION LOT LINES 9/2/20 MRA</div>                                      |  |  |  |  |  |  |  |  |  |                                                              |  |
|                                               |  |                            |                                                                                                                                                                                                                                                                            |                                                                                                                                 |                                                                                                                                                                                                                                                  | <div>2 PER CLIENT COMMENTS 11/24/20 MRA</div>                                   |  |  |  |  |  |  |  |  |  |                                                              |  |
|                                               |  |                            |                                                                                                                                                                                                                                                                            |                                                                                                                                 |                                                                                                                                                                                                                                                  | <div>3 PER IN HOUSE REVIEW 12/16/20 RHW</div>                                   |  |  |  |  |  |  |  |  |  |                                                              |  |
|                                               |  |                            |                                                                                                                                                                                                                                                                            |                                                                                                                                 |                                                                                                                                                                                                                                                  | <div>4 ADD CERTS. 01/19/21 MP</div>                                             |  |  |  |  |  |  |  |  |  |                                                              |  |



# Traffic Impact Study Proposed Retail Development Willowbrook, Illinois



Prepared For:



January 20, 2021



# 1. Introduction

This report summarizes the methodologies, results, and findings of a traffic impact study conducted by Kenig, Lindgren, O'Hara, Aboona, Inc. (KLOA, Inc.) for a proposed retail development to be located in Willowbrook, Illinois. The site, which is currently mostly vacant, is located in the southeast quadrant of the signalized intersection of Plainfield Road with the access drive serving Willowbrook Town Center. As proposed, the site will be developed with an approximate 2,800 square-foot quick service restaurant with a drive-through window, an approximate 6,600 square-foot drive-in bank, an automatic car wash tunnel, and approximately 107 parking spaces. Access to the proposed development will be provided via a right-in/right-out access drive on Plainfield Road and via an internal access drive off the Willowbrook Town Center access drive.

The purpose of this study was to examine background traffic conditions, assess the impact that the proposed development will have on traffic conditions in the area, and determine if any roadway or access improvements are necessary to accommodate the traffic generated by the proposed development.

**Figure 1** shows the location of the site in relation to the area roadway system. **Figure 2** shows an aerial view of the site. The sections of this report present the following:

- Existing roadway conditions
- A description of the proposed development
- Directional distribution of the development traffic
- Vehicle trip generation for the development
- Future traffic conditions including access to the development
- Traffic analyses for the weekday morning, weekday evening, and Saturday midday peak hours
- Recommendations with respect to adequacy of the site access and adjacent roadway system

Traffic capacity analyses were conducted for the weekday morning, weekday evening, and Saturday midday peak hours for the following conditions:

1. Existing Conditions – Analyzes the capacity of the existing roadway system using existing peak hour traffic volumes in the surrounding area.
2. No-Build Conditions – Analyzes the capacity of the existing roadway system using existing peak hour traffic volumes including ambient traffic growth.
3. Projected Conditions – Analyzes the capacity of the future roadway system using the projected traffic volumes that include the existing traffic volumes, ambient traffic growth, and the traffic estimated to be generated by the full buildout of the proposed development.





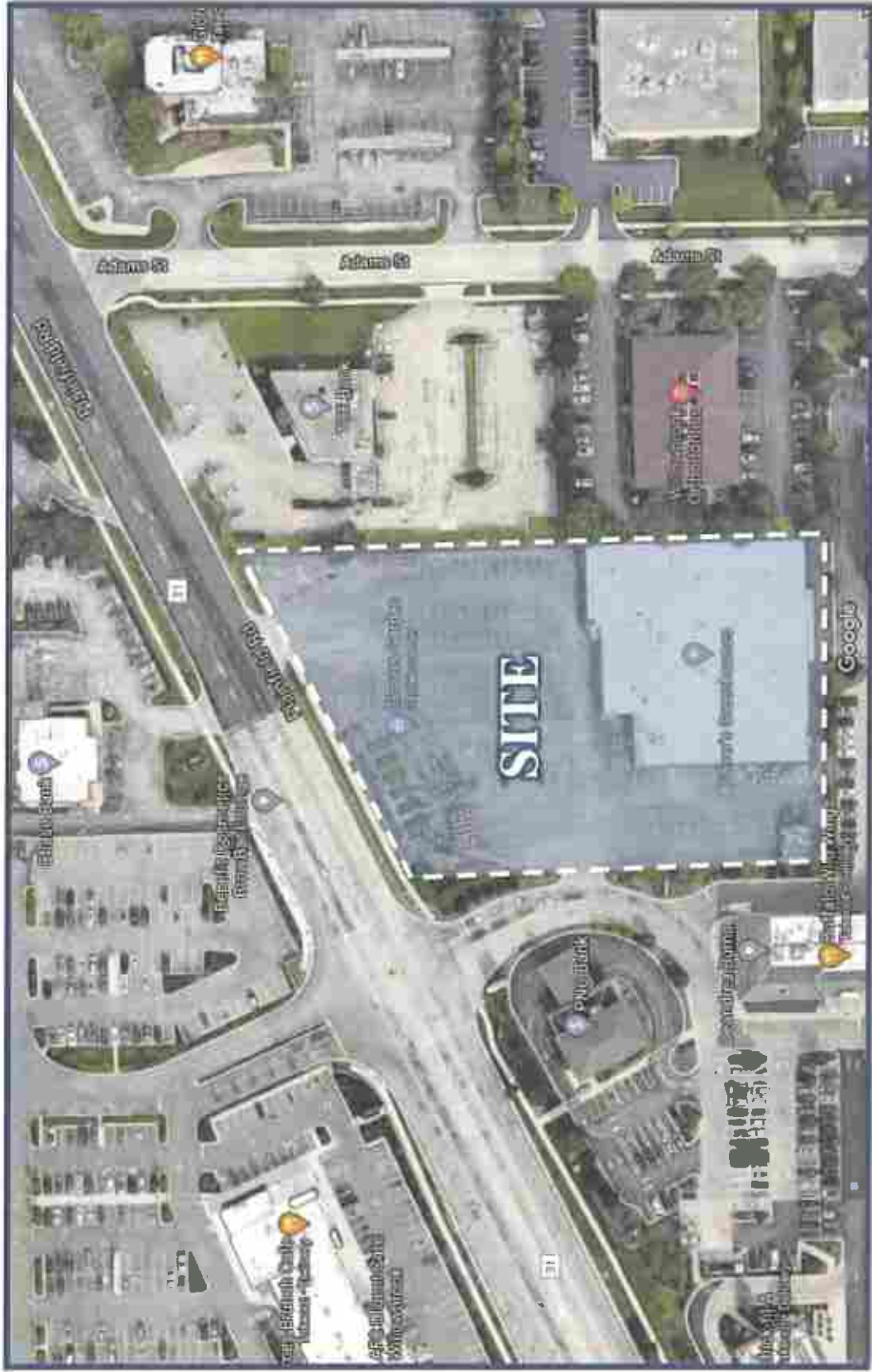
**Site Location**

**Figure 1**

*Proposed Retail Development  
Willowbrook, Illinois*







Aerial View of Site

Figure 2



## 2. Existing Conditions

The following provides a detailed description of the physical characteristics of the roadways including geometry and traffic control, adjacent land uses, and peak hour traffic flows along area roadways.

### Site Location

The site, which is currently mostly vacant, is bounded by Plainfield Road to the north, TCF bank and Willowbrook Orthodontics to the east, Marshall's department store and The Lock Up Self Storage to the south, and the Willowbrook Town Center access drive to the west. Land uses in the vicinity of the site are primarily commercial.

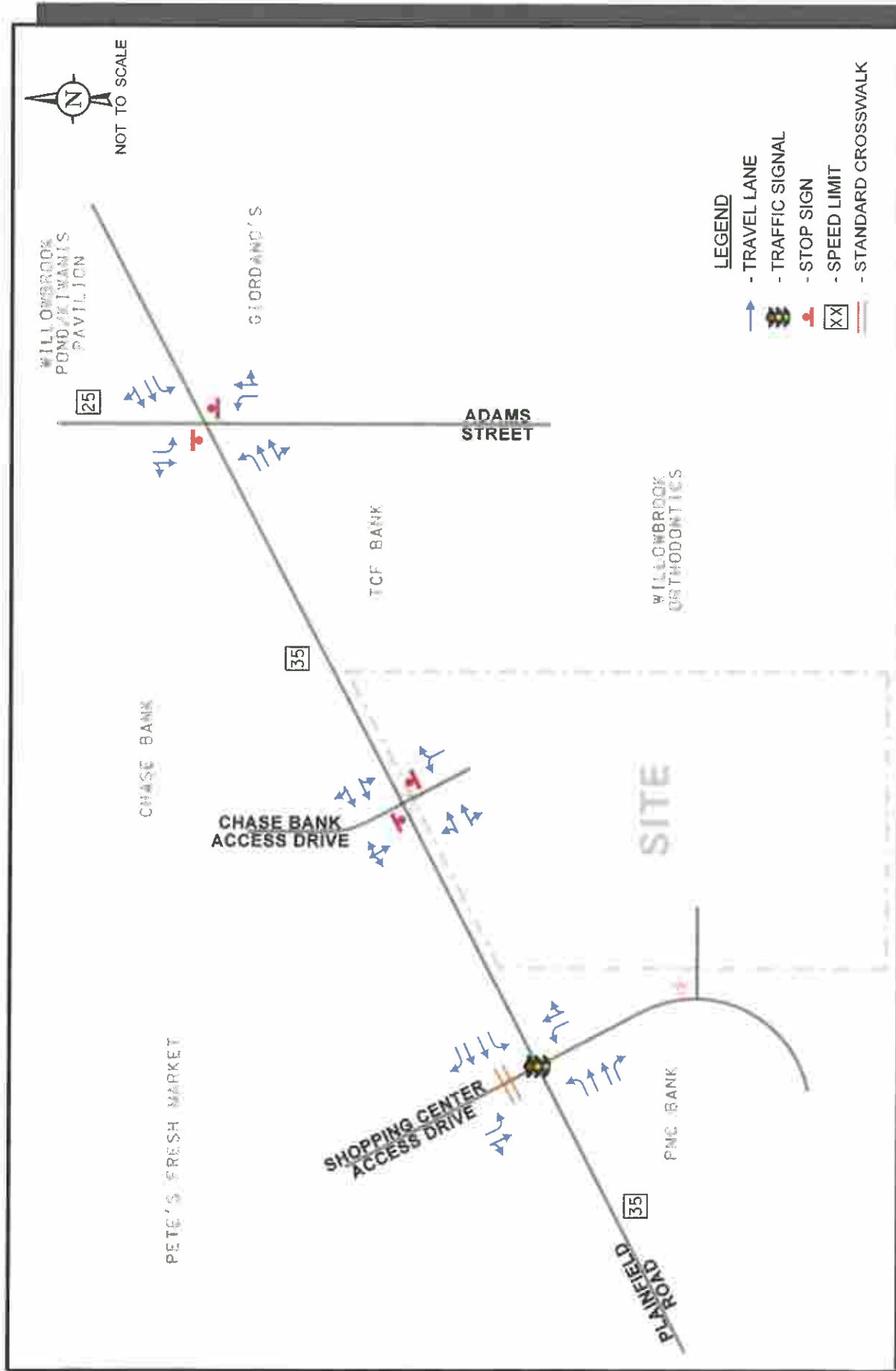
### Existing Roadway System Characteristics

The characteristics of the existing roadways that surround the proposed development are illustrated in **Figure 3** and described below.

*Plainfield Road* is an east-west minor arterial roadway that generally provides two lanes in each direction in the vicinity of the site. At its signalized intersection with the access drive serving the Willowbrook Town Center, Plainfield Road provides an exclusive left-turn lane, two through lanes and an exclusive right-turn lane on both approaches. At its unsignalized intersection with Adams Street, Plainfield Road provides an exclusive left-turn lane, a through lane and a combined through/right-turn lane on both approaches. At its unsignalized intersection with the access serving Chase Bank, Plainfield Road provides a combined through/left-turn lane and a combined through/right-turn lane on both approaches. Plainfield Road is under the jurisdiction of DuPage County Division of Transportation (DuDOT), is not designated as a Strategic Regional Arterial (SRA) and carries an Annual Average Daily Traffic (AADT) volume of 21,700 vehicles (IDOT 2016). In addition, Plainfield Road has a posted speed limit of 35 miles per hour.

*Adams Street* is a north-south local roadway that generally provides one lane in each direction in the vicinity of the site. At its unsignalized intersection with Plainfield Road, Adams Street provides an exclusive left-turn lane and a combined through/right-turn lane on both approaches under stop sign control. Adams Street is under the jurisdiction of the Village of Willowbrook and has a posted speed limit of 25 miles per hour.







## Existing Traffic Volumes

In order to determine current traffic conditions in the vicinity of the site, KLOA, Inc. conducted peak period vehicle, pedestrian, and bicycle movement traffic counts on Thursday, March 5, 2020 during the weekday morning (6:00 to 9:00 A.M.) and evening (3:00 to 6:00 P.M.) peak periods and on Saturday, February 29, 2020 during the midday (11:00 A.M. to 2:00 P.M.) peak period at the following intersections:

- Plainfield Road with Willowbrook Town Center Access Drive
- Plainfield Road with Chase Bank Access Drive
- Plainfield Road with Adams Street
- Willowbrook Town Center Access Drive with Internal Driveway

It should be noted that the traffic counts were conducted before the COVID-19 lockdown and, as such, the counts represent normal traffic conditions. The results of the traffic counts showed that the weekday morning peak hour of traffic occurs from 7:45 A.M. to 8:45 A.M., the weekday evening peak hour of traffic occurs from 4:45 P.M. to 5:45 P.M., and the Saturday midday peak hour occurs from 1:00 P.M. to 2:00 P.M.

**Figure 4** illustrates the existing traffic volumes. Copies of the traffic count summary sheets are included in the Appendix.

## Crash Data Analysis

KLOA, Inc. obtained crash data<sup>1</sup> for the past five years (2014 to 2018) for the intersections of Plainfield Road with Willowbrook Town Center access drive, the Chase Bank access drive, and Adams Street. **Tables 1** through **3** summarize the crash data for these intersections. A review of the crash data indicated that no fatalities were reported at any of these intersections between 2014 and 2018.

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<sup>1</sup> IDOT DISCLAIMER: The motor vehicle crash data referenced herein was provided by the Illinois Department of Transportation. Any conclusions drawn from analysis of the aforementioned data are the sole responsibility of the data recipient(s). Additionally, for coding years 2015 to present, the Bureau of Data Collection uses the exact latitude/longitude supplied by the investigating law enforcement agency to locate crashes. Therefore, location data may vary in previous years since data prior to 2015 was physically located by bureau personnel.



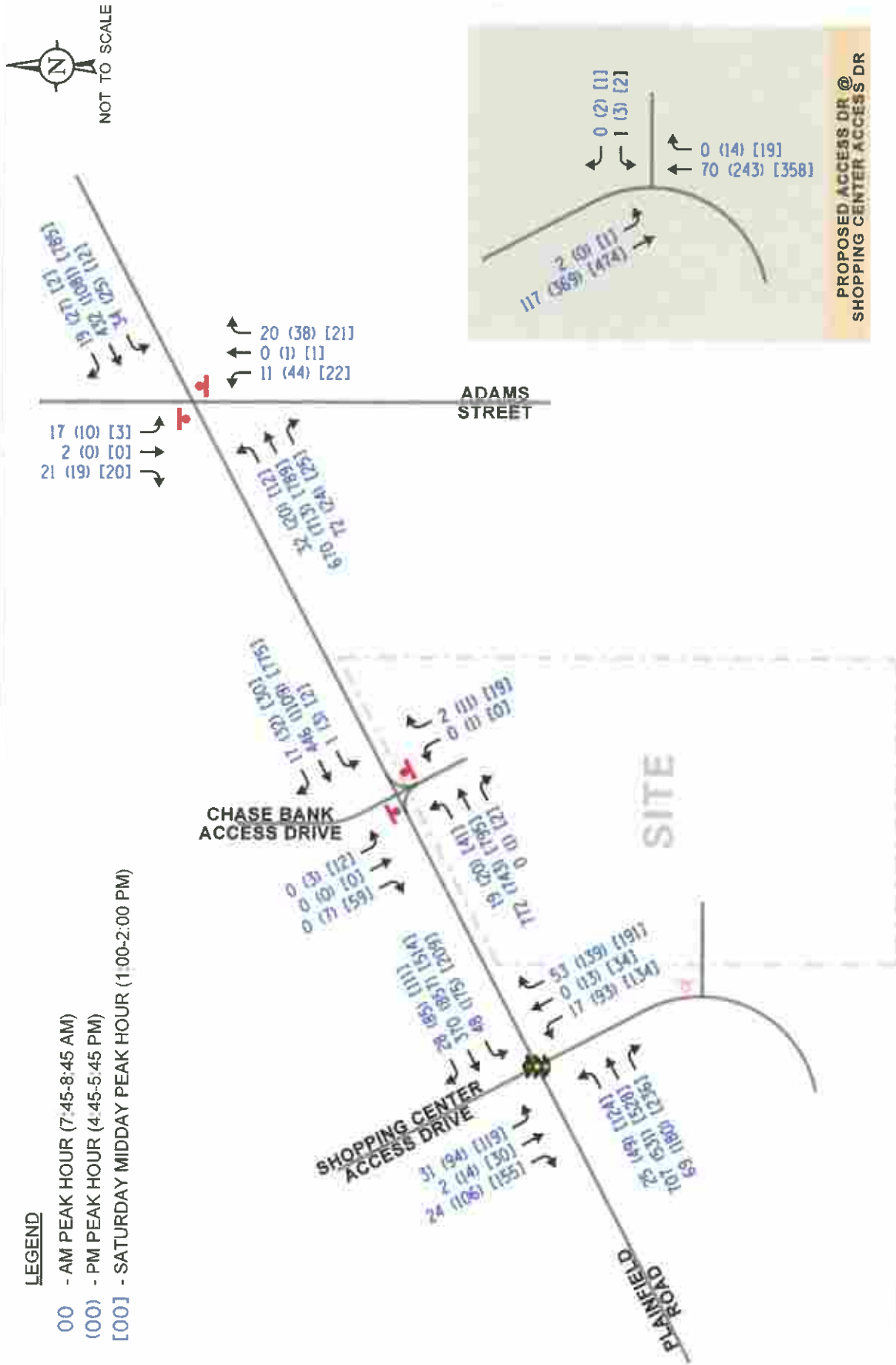




Table 1

## PLAINFIELD ROAD WITH WILLOWBROOK TOWN CENTER ACCESS DRIVE – CRASH SUMMARY

| Type of Crash Frequency |                |          |          |                |           |            |          |            |
|-------------------------|----------------|----------|----------|----------------|-----------|------------|----------|------------|
| Year                    | Angle          | Head On  | Object   | Rear End       | Sideswipe | Turning    | Other    | Total      |
| 2014                    | 0              | 0        | 0        | 0              | 0         | 0          | 0        | 0          |
| 2015                    | 0              | 0        | 0        | 1              | 0         | 5          | 0        | 6          |
| 2016                    | 0              | 0        | 0        | 0              | 0         | 0          | 0        | 0          |
| 2017                    | 0              | 0        | 0        | 0              | 0         | 0          | 0        | 0          |
| 2018                    | <u>1</u>       | <u>0</u> | <u>0</u> | <u>0</u>       | <u>0</u>  | <u>3</u>   | <u>0</u> | <u>4</u>   |
| <b>Total</b>            | <b>1</b>       | <b>0</b> | <b>0</b> | <b>1</b>       | <b>0</b>  | <b>8</b>   | <b>0</b> | <b>10</b>  |
| <b>Average</b>          | <b>&lt;1.0</b> | <b>0</b> | <b>0</b> | <b>&lt;1.0</b> | <b>0</b>  | <b>1.6</b> | <b>0</b> | <b>2.0</b> |

Table 2

## PLAINFIELD ROAD WITH CHASE BANK ACCESS DRIVE – CRASH SUMMARY

| Type of Crash Frequency |                |          |          |                |           |            |          |            |
|-------------------------|----------------|----------|----------|----------------|-----------|------------|----------|------------|
| Year                    | Angle          | Head On  | Object   | Rear End       | Sideswipe | Turning    | Other    | Total      |
| 2014                    | 0              | 0        | 0        | 0              | 0         | 0          | 0        | 0          |
| 2015                    | 1              | 0        | 0        | 0              | 0         | 0          | 0        | 1          |
| 2016                    | 0              | 0        | 0        | 1              | 0         | 2          | 0        | 3          |
| 2017                    | 0              | 0        | 0        | 0              | 0         | 1          | 0        | 1          |
| 2018                    | <u>0</u>       | <u>0</u> | <u>0</u> | <u>0</u>       | <u>0</u>  | <u>5</u>   | <u>0</u> | <u>5</u>   |
| <b>Total</b>            | <b>1</b>       | <b>0</b> | <b>0</b> | <b>1</b>       | <b>0</b>  | <b>8</b>   | <b>0</b> | <b>10</b>  |
| <b>Average</b>          | <b>&lt;1.0</b> | <b>0</b> | <b>0</b> | <b>&lt;1.0</b> | <b>0</b>  | <b>1.6</b> | <b>0</b> | <b>2.0</b> |



Table 3

## PLAINFIELD ROAD WITH ADAMS STREET – CRASH SUMMARY

| Year           | Type of Crash Frequency |          |          |                |                |            |                | Total      |
|----------------|-------------------------|----------|----------|----------------|----------------|------------|----------------|------------|
|                | Angle                   | Head On  | Object   | Rear End       | Sideswipe      | Turning    | Other          |            |
| 2014           | 0                       | 0        | 0        | 1              | 0              | 2          | 0              | 3          |
| 2015           | 0                       | 0        | 0        | 0              | 0              | 3          | 0              | 3          |
| 2016           | 0                       | 0        | 0        | 0              | 0              | 0          | 0              | 0          |
| 2017           | 0                       | 0        | 0        | 0              | 0              | 2          | 1              | 3          |
| 2018           | <u>0</u>                | <u>0</u> | <u>0</u> | <u>0</u>       | <u>1</u>       | <u>0</u>   | <u>0</u>       | <u>1</u>   |
| <b>Total</b>   | <b>0</b>                | <b>0</b> | <b>0</b> | <b>1</b>       | <b>1</b>       | <b>7</b>   | <b>1</b>       | <b>10</b>  |
| <b>Average</b> | <b>0</b>                | <b>0</b> | <b>0</b> | <b>&lt;1.0</b> | <b>&lt;1.0</b> | <b>1.4</b> | <b>&lt;1.0</b> | <b>2.0</b> |



### 3. Traffic Characteristics of the Proposed Development

To evaluate the impact of the subject development on the area roadway system, it was necessary to quantify the number of vehicle trips the site will generate during the respective three peak hours and then determine the directions from which the proposed traffic will approach and depart the site.

#### Proposed Site and Development Plan

As proposed, the site will be developed with an approximate 2,800 square-foot quick serve restaurant with a drive-through window, an approximate 6,600 square-foot drive-in bank, an automatic car wash tunnel, and approximately 107 parking spaces. Access to the proposed development will be provided via the following:

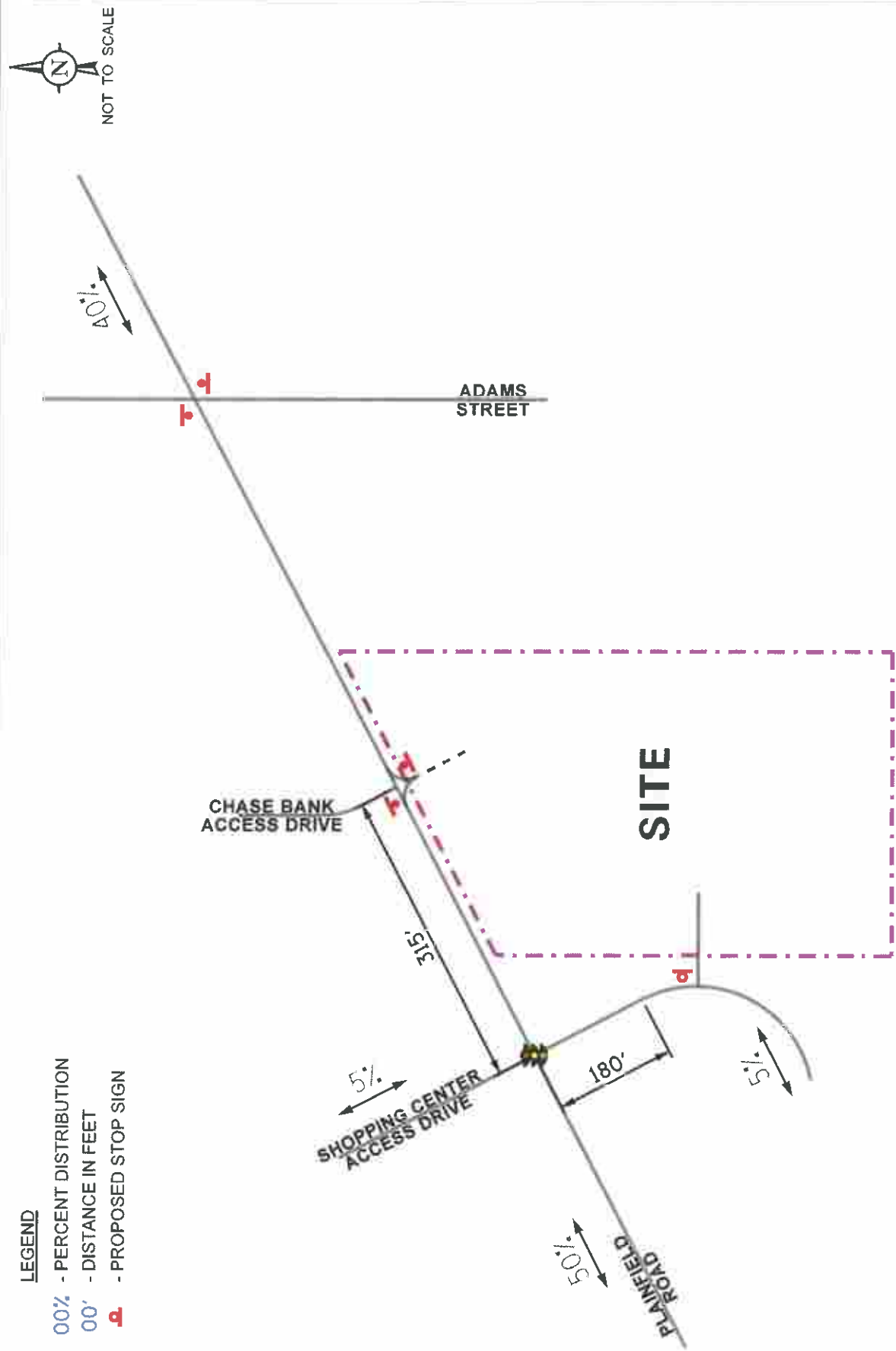
- A proposed right-in/right-out access drive off Plainfield Road (which is currently a full movement access drive that serves Clovers Garden) located approximately 315 feet east of the main access drive serving the Willowbrook Town Center. This access drive will provide one inbound lane and one outbound lane with turning movements physically restricted to right turns only via a mountable triangular median. Outbound movements should be under stop sign control.
- An existing full movement access drive (which currently serves Clovers Garden) off the main access drive serving the Willowbrook Town Center located approximately 180 feet south of Plainfield Road. This access drive provides one inbound lane and one outbound lane with outbound movements under stop sign control. It should be noted that this intersection will be analyzed as follows:
  - With a southbound combined through/left-turn lane
  - With a southbound through lane and a proposed southbound exclusive left-turn lane serving the site of approximately 29 feet of storage and 37 feet of taper

A copy of the site plan is included in the Appendix.

#### Directional Distribution of Development Traffic

The directional distribution of how traffic will approach and depart the site was estimated based on the general travel patterns through the study area derived from the peak hour traffic volumes. **Figure 5** shows the established directional distribution for this development.







## Development Traffic Generation

The estimate of vehicle traffic to be generated by the proposed development is based upon the proposed land use types and sizes. The vehicle trip generation for the overall development was calculated using data published in the Institute of Transportation Engineers (ITE) *Trip Generation Manual*, 10<sup>th</sup> Edition.

It is important to note that surveys conducted by ITE have shown that approximately 50 and 30 percent of trips made to fast-food restaurant uses, and drive-in bank uses, respectively, are diverted from the existing traffic on the roadway system. This is particularly true during the weekday morning and evening peak hours when traffic is diverted from the home-to-work and work-to-home trips. Such diverted trips are referred to as pass-by traffic. As such, these pass-by percentages were applied to the trips estimated to be generated by these uses. It should be noted that while it is likely internal interaction will occur between the proposed land uses, which will further reduce the estimated trips, it was not applied in order to present a conservative analysis.

**Table 4** shows the estimated vehicle trip generation for the weekday morning, weekday evening, and Saturday midday peak hours and daily trips. It should be noted that ITE does not have daily trip data for car washes. The ITE trip generation summary sheets are included in the Appendix.



Table 4

## ESTIMATED PEAK HOUR VEHICLE TRIP GENERATION FOR PROPOSED RETAIL DEVELOPMENT

| ITE<br>Land-<br>Use<br>Code | Type/Size                                                               | Weekday<br>Morning<br>Peak Hour |           |            | Weekday<br>Evening<br>Peak Hour |            |            | Saturday<br>Midday Peak<br>Hour |            |            | Daily      |
|-----------------------------|-------------------------------------------------------------------------|---------------------------------|-----------|------------|---------------------------------|------------|------------|---------------------------------|------------|------------|------------|
|                             |                                                                         | In                              | Out       | Total      | In                              | Out        | Total      | In                              | Out        | Total      |            |
| 934                         | Quick Serve Restaurant with Drive<br>Through Window (2,800 square feet) | 58                              | 55        | 113        | 47                              | 44         | 91         | 79                              | 75         | 154        | 1,319      |
|                             | <i>50 Percent Pass-By Reduction</i>                                     | -28                             | -28       | -56        | -23                             | -23        | -46        | -38                             | -38        | -76        | -660       |
| 932                         | Drive-In Bank (6,600 square feet)                                       | 37                              | 26        | 63         | 68                              | 67         | 135        | 89                              | 85         | 174        | 660        |
|                             | <i>30 Percent Pass-By Reduction</i>                                     | -9                              | -9        | -18        | -20                             | -20        | -40        | -26                             | -26        | -52        | -198       |
| 948                         | Automatic Car Wash (one tunnel)                                         | 10                              | 10        | 20         | 39                              | 39         | 78         | 19                              | 22         | 41         | NA         |
|                             | <b>Total Development Trips</b>                                          | <b>105</b>                      | <b>91</b> | <b>196</b> | <b>154</b>                      | <b>150</b> | <b>304</b> | <b>187</b>                      | <b>182</b> | <b>369</b> | <b>NA</b>  |
|                             | <b>Total Pass-By Trips</b>                                              | <b>37</b>                       | <b>37</b> | <b>74</b>  | <b>43</b>                       | <b>43</b>  | <b>86</b>  | <b>64</b>                       | <b>64</b>  | <b>128</b> | <b>858</b> |
|                             | <b>Total New Trips</b>                                                  | <b>68</b>                       | <b>54</b> | <b>122</b> | <b>111</b>                      | <b>107</b> | <b>218</b> | <b>123</b>                      | <b>118</b> | <b>241</b> | <b>NA</b>  |



## 4. Projected Traffic Conditions

The total projected traffic volumes take into consideration the existing traffic volumes, increase in background traffic due to growth, and the traffic estimated to be generated by the proposed subject development.

### Development Traffic Assignment

The estimated weekday morning, weekday evening, and Saturday midday peak hour traffic volumes that will be generated by the proposed development were assigned to the roadway system in accordance with the previously described directional distribution (Figure 5). **Figure 7** illustrates the traffic assignment of the new passenger vehicle trips and **Figure 8** illustrates the traffic assignment of the pass-by passenger vehicle trips.

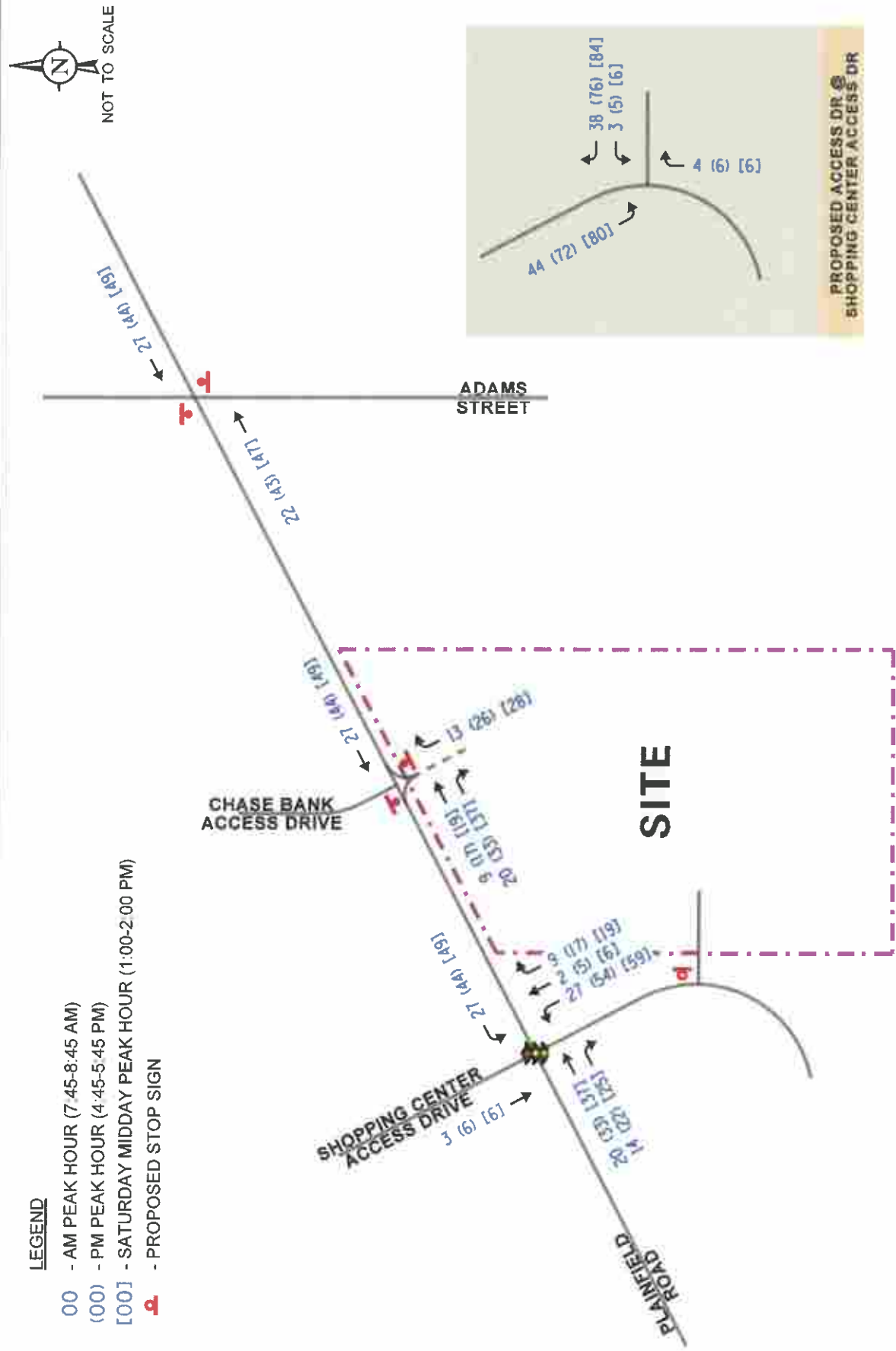
### Background Traffic Conditions

The existing traffic volumes (Figure 4) were increased by a regional growth factor to account for the increase in existing traffic related to regional growth in the area (i.e., not attributable to any particular planned development). Based on 2050 Average Daily Traffic (ADT) projections provided by the Chicago Metropolitan Agency for Planning (CMAP) in a letter dated December 2, 2020, the existing traffic volumes were increased by an annually compounded growth rate for six years (one-year buildout plus five years) totaling 2.4 percent to represent Year 2026 no-build conditions. **Figure 9** shows the Year 2026 no-build traffic conditions. A copy of the CMAP 2050 projections letter is included in the Appendix.

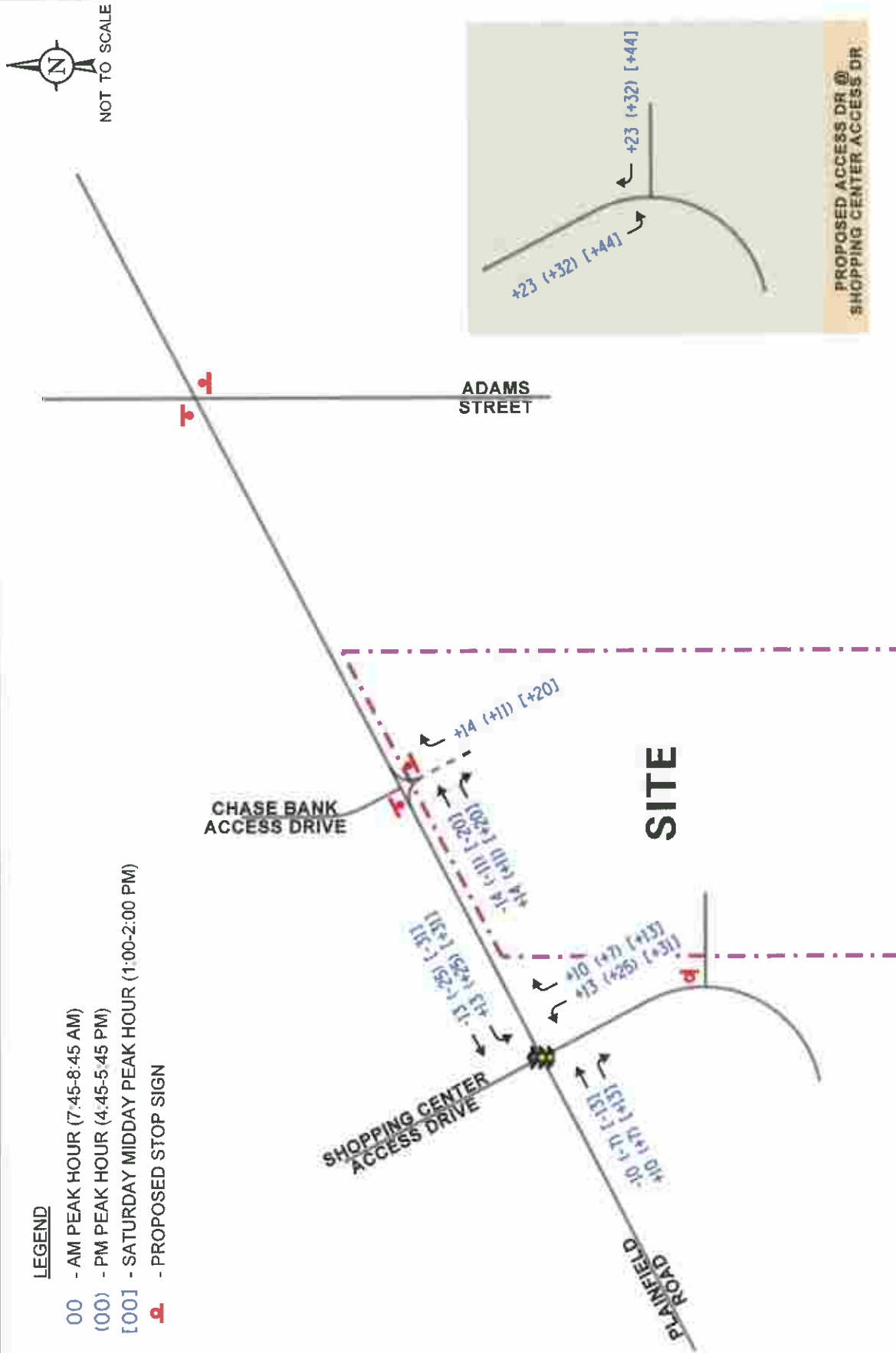
### Year 2026 Total Projected Traffic Conditions

The new and pass-by development-generated traffic (Figures 7 and 8) was added to the no-build traffic volumes (Figure 9) to determine the Year 2026 total projected traffic volumes, which are illustrated in **Figure 10**.





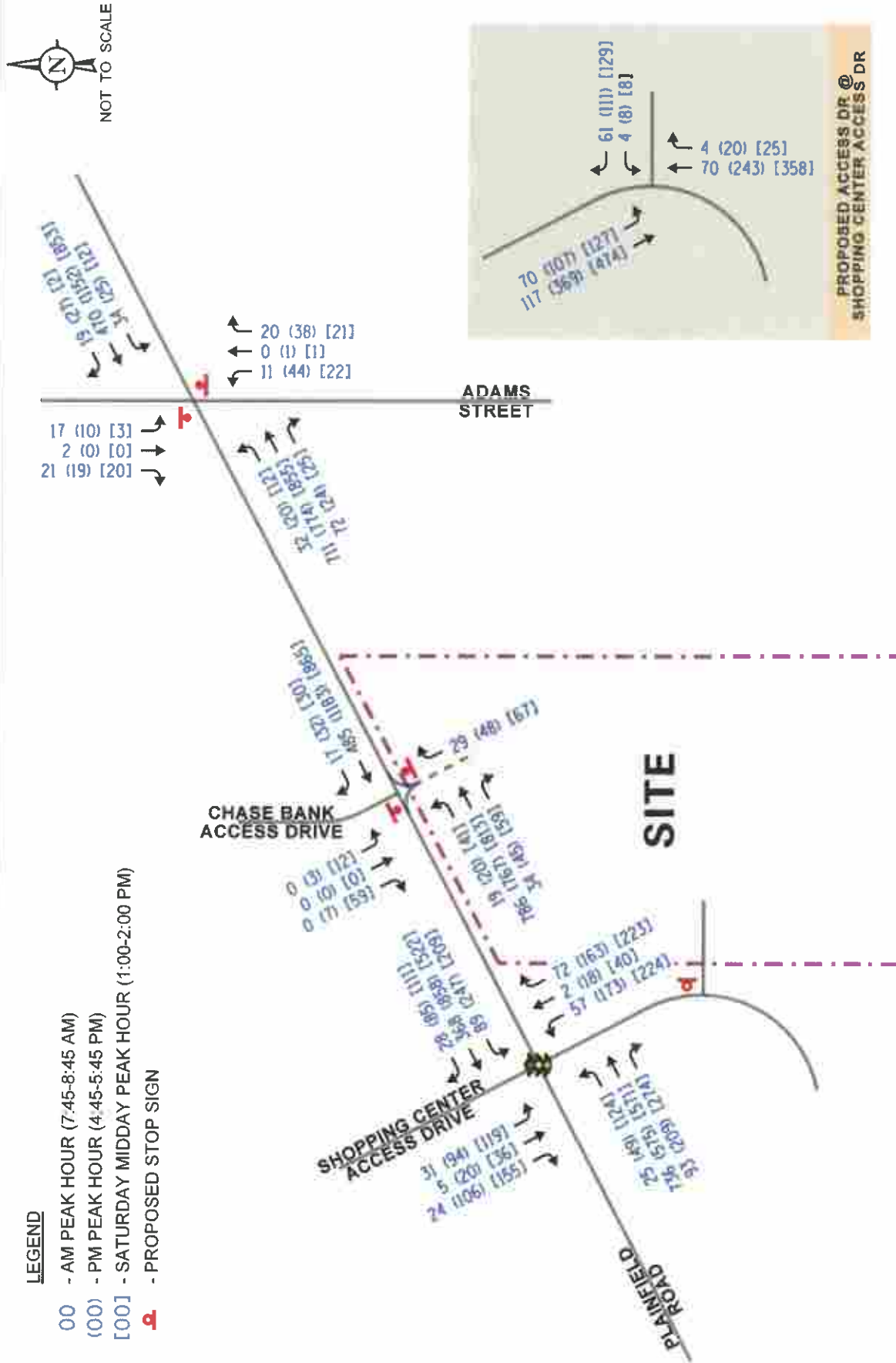














## 5. Traffic Analysis and Recommendations

The following provides an evaluation conducted for the weekday morning, weekday evening, and Saturday midday peak hours. The analysis includes conducting capacity analyses to determine how well the roadway system and access drives are projected to operate and whether any road improvements or modifications are required.

### Traffic Analyses

Roadway and adjacent or nearby intersection analyses were performed for the weekday morning, weekday evening, and Saturday midday peak hours for the existing (Year 2020), Year 2026 no-build, and Year 2026 total projected traffic volumes.

The traffic analyses were performed using the methodologies outlined in the Transportation Research Board's *Highway Capacity Manual (HCM)*, 6<sup>th</sup> Edition and analyzed using Synchro/SimTraffic 10 software. The analysis for the traffic-signal controlled intersections were accomplished using actual cycle lengths, phasings and offsets to determine the average overall vehicle delay and levels of service.

The analyses for the unsignalized intersections determine the average control delay to vehicles at an intersection. Control delay is the elapsed time from a vehicle joining the queue at a stop sign (includes the time required to decelerate to a stop) until its departure from the stop sign and resumption of free flow speed. The methodology analyzes each intersection approach controlled by a stop sign and considers traffic volumes on all approaches and lane characteristics.

The ability of an intersection to accommodate traffic flow is expressed in terms of level of service, which is assigned a letter from A to F based on the average control delay experienced by vehicles passing through the intersection. The *Highway Capacity Manual* definitions for levels of service and the corresponding control delay for signalized intersections and unsignalized intersections are included in the Appendix of this report.

Summaries of the traffic analysis results showing the level of service and overall intersection delay (measured in seconds) for the existing and total projected conditions are presented in **Tables 5 through 8**. A discussion of each intersection follows. Summary sheets for the capacity analyses are included in the Appendix.



Table 5

**CAPACITY ANALYSIS RESULTS  
PLAINFIELD ROAD WITH WILLOWBROOK TOWN CENTER ACCESS DRIVE-- SIGNALIZED**

|                                         | Peak Hour                 | Eastbound |        |       | Westbound |        |       | Northbound |        |        | Southbound |        |        | Overall |        |
|-----------------------------------------|---------------------------|-----------|--------|-------|-----------|--------|-------|------------|--------|--------|------------|--------|--------|---------|--------|
|                                         |                           | L         | T      | R     | L         | T      | R     | L          | T/R    | L      | T/R        | L      | T/R    |         |        |
| Year 2020<br>Conditions                 | Weekday Morning Peak Hour | A 5.4     | A 9.6  | A 4.6 | A 5.5     | A 8.0  | A 4.4 | D 46.1     | E 71.8 | D 49.3 | E 62.4     | D 49.3 | E 62.4 | B 13.4  |        |
|                                         |                           | A - 9.1   |        |       | A - 7.5   |        |       | E - 65.4   |        |        | D - 55.3   |        |        |         |        |
|                                         | Weekday Evening Peak Hour | A 8.6     | B 15.7 | A 7.9 | A 9.5     | B 15.5 | A 6.0 | D 44.8     | F 87.2 | D 45.8 | E 72.0     | D 45.8 | E 72.0 |         | C 23.9 |
|                                         |                           | B - 13.4  |        |       | B - 13.9  |        |       | E - 69.9   |        |        | E - 60.5   |        |        |         |        |
| Year 2026 No-Build<br>Conditions        | Saturday Midday Peak Hour | B 10.7    | B 19.1 | A 9.7 | B 12.4    | B 18.5 | A 8.3 | D 36.5     | F 87.2 | D 37.1 | E 70.9     | D 37.1 | E 70.9 | C 28.8  |        |
|                                         |                           | B - 15.4  |        |       | B - 15.6  |        |       | E - 68.3   |        |        | E - 57.7   |        |        |         |        |
|                                         | Weekday Morning Peak Hour | A 5.4     | A 9.7  | A 4.6 | A 5.5     | A 8.0  | A 4.4 | D 46.1     | E 71.8 | D 49.3 | E 62.4     | D 49.3 | E 62.4 | B 13.3  |        |
|                                         |                           | A - 9.1   |        |       | A - 7.5   |        |       | E - 65.4   |        |        | D - 55.3   |        |        |         |        |
| Year 2026 Total<br>Projected Conditions | Weekday Evening Peak Hour | A 8.6     | B 15.8 | A 7.9 | A 9.6     | B 15.7 | A 6.0 | D 44.8     | F 85.2 | D 45.8 | E 72.0     | D 45.8 | E 72.0 | C 23.8  |        |
|                                         |                           | B - 13.5  |        |       | B - 14.1  |        |       | E - 69.9   |        |        | E - 60.5   |        |        |         |        |
|                                         | Saturday Midday Peak Hour | B 10.8    | B 19.3 | A 9.7 | B 12.5    | B 18.7 | A 8.3 | D 36.5     | F 87.2 | D 37.1 | E 70.9     | D 37.1 | E 70.9 | C 28.7  |        |
|                                         |                           | B - 15.6  |        |       | B - 15.9  |        |       | E - 68.3   |        |        | E - 57.7   |        |        |         |        |
| Year 2026 Total<br>Projected Conditions | Weekday Morning Peak Hour | A 5.7     | A 11.4 | A 4.6 | A 6.1     | A 8.6  | A 4.6 | D 49.2     | E 75.7 | D 48.8 | E 65.1     | D 48.8 | E 65.1 | B 16.1  |        |
|                                         |                           | B - 10.5  |        |       | A - 7.9   |        |       | E - 64.1   |        |        | E - 56.7   |        |        |         |        |
|                                         | Weekday Evening Peak Hour | A 8.7     | B 16.8 | A 7.6 | B 11.8    | B 16.0 | A 6.1 | D 52.6     | F 91.8 | D 46.0 | F 80.3     | D 46.0 | F 80.3 | C 26.6  |        |
|                                         |                           | B - 14.0  |        |       | B - 14.5  |        |       | E - 72.7   |        |        | E - 65.7   |        |        |         |        |
| Year 2026 Total<br>Projected Conditions | Saturday Midday Peak Hour | B 10.8    | B 19.6 | A 9.2 | B 16.2    | B 18.5 | A 8.3 | D 49.0     | F 99+  | D 39.5 | F 96.9     | D 39.5 | F 96.9 | D 35.8  |        |
|                                         |                           | B - 15.6  |        |       | B - 16.6  |        |       | F - 88.0   |        |        | E - 74.9   |        |        |         |        |
|                                         |                           |           |        |       |           |        |       |            |        |        |            |        |        |         |        |
|                                         |                           |           |        |       |           |        |       |            |        |        |            |        |        |         |        |

Letter denotes Level of Service; Delay is measured in seconds.  
L - Left Turns  
T - Through  
R - Right Turns

Letter denotes Level of Service; Delay is measured in seconds.

L - Left Turns

T - Through

R - Right Turns



Table 6  
CAPACITY ANALYSIS RESULTS  
UNSIGNALIZED INTERSECTIONS – EXISTING CONDITIONS

| Intersection                                              | Weekday<br>Morning<br>Peak Hour |       | Weekday<br>Evening<br>Peak Hour |       | Saturday<br>Midday<br>Peak Hour |       |
|-----------------------------------------------------------|---------------------------------|-------|---------------------------------|-------|---------------------------------|-------|
|                                                           | LOS                             | Delay | LOS                             | Delay | LOS                             | Delay |
| <b>Plainfield Road with Chase Bank Access Drive</b>       |                                 |       |                                 |       |                                 |       |
| • Northbound Approach                                     | B                               | 11.1  | B                               | 14.2  | B                               | 11.4  |
| • Eastbound Left Turn                                     | A                               | 8.4   | B                               | 11.3  | A                               | 9.8   |
| • Westbound Left Turn                                     | A                               | 9.5   | B                               | 10.5  | A                               | 9.6   |
| • Southbound Approach                                     | A                               | 0.1   | D                               | 28.7  | C                               | 20.4  |
| <b>Plainfield Road with Adams Street</b>                  |                                 |       |                                 |       |                                 |       |
| • Northbound Approach                                     | B                               | 14.8  | C                               | 21.5  | C                               | 17.9  |
| • Eastbound Left Turn                                     | A                               | 8.6   | B                               | 11.2  | A                               | 9.5   |
| • Westbound Left Turn                                     | A                               | 9.7   | A                               | 9.7   | A                               | 9.7   |
| • Southbound Approach                                     | B                               | 13.8  | C                               | 19.7  | B                               | 12.8  |
| <b>Willowbrook Town Center with Internal Access Drive</b> |                                 |       |                                 |       |                                 |       |
| • Westbound Approach                                      | A                               | 9.6   | B                               | 12.0  | B                               | 14.8  |
| • Southbound Left Turn                                    | A                               | 7.3   | A                               | 0.1   | A                               | 8.1   |
| LOS = Level of Service<br>Delay is measured in seconds.   |                                 |       |                                 |       |                                 |       |



Table 7  
CAPACITY ANALYSIS RESULTS  
UNSIGNALIZED INTERSECTIONS – YEAR 2026 NO-BUILD CONDITIONS

| Intersection                                              | Weekday<br>Morning<br>Peak Hour |       | Weekday<br>Evening<br>Peak Hour |       | Saturday<br>Midday<br>Peak Hour |       |
|-----------------------------------------------------------|---------------------------------|-------|---------------------------------|-------|---------------------------------|-------|
|                                                           | LOS                             | Delay | LOS                             | Delay | LOS                             | Delay |
| <b>Plainfield Road with Chase Bank Access Drive</b>       |                                 |       |                                 |       |                                 |       |
| • Northbound Approach                                     | B                               | 11.2  | B                               | 14.5  | B                               | 11.5  |
| • Eastbound Left Turn                                     | A                               | 8.5   | B                               | 11.4  | B                               | 10.0  |
| • Westbound Left Turn                                     | A                               | 9.6   | B                               | 10.6  | A                               | 9.6   |
| • Southbound Approach                                     | A                               | 0.1   | D                               | 29.7  | C                               | 21.8  |
| <b>Plainfield Road with Adams Street</b>                  |                                 |       |                                 |       |                                 |       |
| • Northbound Approach                                     | C                               | 15.0  | C                               | 22.1  | C                               | 18.3  |
| • Eastbound Left Turn                                     | A                               | 8.6   | B                               | 11.4  | A                               | 9.7   |
| • Westbound Left Turn                                     | A                               | 9.8   | A                               | 9.8   | A                               | 9.8   |
| • Southbound Approach                                     | B                               | 14.0  | C                               | 20.2  | B                               | 13.1  |
| <b>Willowbrook Town Center with Internal Access Drive</b> |                                 |       |                                 |       |                                 |       |
| • Westbound Approach                                      | A                               | 9.6   | B                               | 12.0  | B                               | 14.8  |
| • Southbound Left Turn                                    | A                               | 7.3   | A                               | 0.1   | A                               | 8.1   |
| LOS = Level of Service<br>Delay is measured in seconds.   |                                 |       |                                 |       |                                 |       |



Table 8

## CAPACITY ANALYSIS RESULTS

## UNSIGNALIZED INTERSECTIONS – YEAR 2026 TOTAL PROJECTED CONDITIONS

| Intersection                                                                                                                                                                                     | Weekday Morning Peak Hour |              | Weekday Evening Peak Hour |                | Saturday Midday Peak Hour |                |
|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------|--------------|---------------------------|----------------|---------------------------|----------------|
|                                                                                                                                                                                                  | LOS                       | Delay        | LOS                       | Delay          | LOS                       | Delay          |
| <b>Plainfield Road with Chase Bank Access Drive/Proposed Right-In/Right-Out Access Drive</b>                                                                                                     |                           |              |                           |                |                           |                |
| • Northbound Approach                                                                                                                                                                            | B                         | 11.5         | B                         | 11.8           | B                         | 12.2           |
| • Eastbound Left Turns                                                                                                                                                                           | A                         | 8.6          | B                         | 11.7           | B                         | 10.3           |
| • Southbound Approach                                                                                                                                                                            | A                         | 0.1          | D                         | 33.0           | C                         | 24.8           |
| <b>Plainfield Road with Adams Street</b>                                                                                                                                                         |                           |              |                           |                |                           |                |
| Northbound Approach                                                                                                                                                                              | C                         | 15.3         | C                         | 23.5           | C                         | 19.4           |
| • Eastbound Left Turn                                                                                                                                                                            | A                         | 8.7          | B                         | 11.7           | A                         | 9.9            |
| • Westbound Left Turn                                                                                                                                                                            | A                         | 9.9          | B                         | 10.0           | B                         | 10.0           |
| • Southbound Approach                                                                                                                                                                            | B                         | 14.4         | C                         | 21.1           | B                         | 13.5           |
| <b>Willowbrook Town Center with Internal Access Drive</b>                                                                                                                                        |                           |              |                           |                |                           |                |
| • Westbound Approach                                                                                                                                                                             | A<br>(A)                  | 9.1<br>(9.1) | B<br>(B)                  | 11.4<br>(11.4) | B<br>(B)                  | 13.7<br>(13.6) |
| • Southbound Left Turn                                                                                                                                                                           | A<br>(A)                  | 7.5<br>(7.5) | A<br>(A)                  | 8.1<br>(8.1)   | A<br>(A)                  | 8.6 (8.6)      |
| LOS = Level of Service<br>Delay is measured in seconds.<br>XX(XX) – With a southbound combined through/left-turn lane (With a southbound exclusive left turn lane and a southbound through lane) |                           |              |                           |                |                           |                |



## Discussion and Recommendations

The following is an evaluation of the analyzed intersections based on the projected traffic volumes and the capacity analyses performed.

### *Plainfield Road with Willowbrook Town Center Access Drive*

The results of the capacity analysis indicate that overall this intersection currently operates at Level of Service (LOS) B during the weekday morning peak hour and LOS C during the weekday evening and Saturday midday peak hours. The southbound approach currently operates at LOS D during the weekday morning peak hour and LOS E during the weekday evening and Saturday midday peak hours, and the northbound approach is operating at LOS E during the three peak hours. In addition, the eastbound and westbound approaches currently operate at LOS B or better during all three peak hours.

Under Year 2026 no-build conditions, overall this intersection will continue to operate at the same existing levels of service during the weekday morning, weekday evening, and Saturday midday peak hours with increases in delay of less than one second. All approaches will continue operating at the same existing levels of service during all three peak hours with increases in delay of approximately one second.

Under Year 2026 total projected conditions, overall this intersection will continue to operate at LOS C or better during all three peak hours with increases in delay of approximately three seconds during the weekday morning and evening peak hours and seven seconds during the Saturday midday peak hour, over no-build conditions. The eastbound and westbound approaches will continue to operate at LOS B or better during the weekday morning, weekday evening, and Saturday midday peak hours with increases in delay of approximately one second. In addition, the northbound approach will operate at LOS E during the weekday morning and evening peak hours and LOS F during the Saturday midday peak hour, and the southbound approach will operate at LOS E during all three peak hours. It should be noted that these analyses are conservative, since they do not take into consideration right-turn on red movements. Based on the results of the simulation, the northbound queues will experience 95<sup>th</sup> percentile queues of approximately 90 feet during the weekday morning peak hour, 167 feet during the weekday evening peak hour, and 164 feet during the Saturday midday peak hour and, as such, will extend to and beyond the proposed full movement access drive by one to two vehicle lengths during the weekday evening and Saturday midday peak hours. However, it is important to note that the queues will generally clear with every green phase during all three peak hours. As such, this intersection has sufficient reserve capacity to accommodate the traffic estimated to be generated by the proposed development and no roadway or traffic control improvements will be required.



*Plainfield Road with Chase Bank Access Drive and the Proposed Right-In/Right-Out Access Drive*

The results of the capacity analysis indicate that the southbound approach is operating at LOS A during the weekday morning peak hour, LOS D during the weekday evening peak hour, and LOS C during the Saturday midday peak hour. All other movements will operate at LOS B or better during all three peak hours.

Under Year 2026 no-build conditions, all movements will operate at the same existing levels of service with increases in delay of approximately one second during all three peak hours.

As previously indicated, the turning movements at the access drive serving the site will be physically restricted to right turns only via a mountable triangular median. Under Year 2026 total projected conditions, the northbound right-turn movement will operate at LOS B during the weekday morning, weekday evening, and Saturday midday peak hours with 95<sup>th</sup> percentile queues of one to two vehicles. In addition, the eastbound left-turn movement and the southbound approach will continue to operate at an acceptable LOS D or better during all three peak hours. It should also be noted that the restriction of left-turn inbound and outbound movements at this access drive will reduce the impact of the site-generated traffic on the westbound through movements. As such, this access drive will be adequate in accommodating the traffic estimated to be generated by the proposed development, will ensure efficient and flexible access is provided, and will reduce the traffic load experienced at the signalized intersection of Plainfield Road with the access drive serving Willowbrook Town Center.

*Plainfield Road with Adams Street*

The results of the capacity analysis indicate that the northbound and southbound approaches are operating at LOS C or better during the weekday morning, weekday evening, and Saturday midday peak hours. In addition, the eastbound and westbound left-turn movements currently operate at LOS B or better during all three peak hours.

Under Year 2026 no-build conditions, all movements will continue to operate at LOS C or better during the weekday morning, weekday evening, and Saturday midday peak hours with increases in delay of less than one second.

Under Year 2026 total projected conditions, all movements will continue to operate at LOS C or better during all three peak hours with increases in delay of approximately one second over no-build conditions. As such, this intersection has sufficient reserve capacity to accommodate the traffic estimated to be generated by the proposed development and no roadway or traffic control improvements will be required.



### *Willowbrook Town Center with Internal Access Drive*

The results of the capacity analysis indicate that the outbound movements will operate at LOS A during the weekday morning peak hour and LOS B during the weekday evening and Saturday midday peak hours with 95<sup>th</sup> percentile queues of one to two vehicles, with or without the provision of an exclusive southbound left-turn lane. In addition, the southbound left-turn movement will operate at LOS A during the weekday morning, weekday evening, and Saturday midday peak hours, with or without the provision of an exclusive southbound left-turn lane.

When a southbound exclusive left-turn lane is not provided at the access drive, the results of the simulation indicate that vehicles are queueing back toward Plainfield Road, especially during the weekday evening and Saturday midday peak hours.

Therefore, the provision of an exclusive southbound left-turn lane is needed in order to keep the southbound through lane clear and minimize the queueing back toward Plainfield Road. In addition, “Do Not Block Intersection” signage should be provided to prevent vehicles from blocking the access drive. It is also important to note that the provision of a right-in/right-out access drive on Plainfield Road will reduce the impacts of the proposed development-generated traffic at this intersection. As such, this access drive will be adequate in accommodating the traffic estimated to be generated by the proposed development and will ensure efficient and flexible access is provided.

### **Parking Evaluation**

As previously stated, the proposed development calls for a retail development with an approximate 2,800 square-foot quick service restaurant with a drive-through window, an approximate 6,600 square-foot drive-in bank, an automatic car wash tunnel, and approximately 107 parking spaces. From our understanding, the site will contain approximately 43 parking spaces for the use of vacuums and for employee parking of the car wash, with the remaining 64 spaces for the use of the drive-in bank and the quick-service restaurant with drive-through window. In order to determine the parking of the bank and the restaurant, the parking demand was estimated based on the Village of Willowbrook Zoning Code and the rates published in the Institute of Transportation Engineers’ (ITE) Parking Generation Manual, 5<sup>th</sup> Edition. Based on the two methodologies, the parking demand for the bank and the restaurant uses is as follows:

#### *Village of Willowbrook Zoning Ordinance*

- Bank: 22 parking spaces (ratio of one space per 300 square feet)
- Restaurant: 28 parking spaces (ratio of one space per 100 square feet)

#### *ITE Parking Generation Manual*

- Drive-In Bank: 25 parking spaces (ratio of 3.72 spaces per 1,000 square feet)
- Fast-Food Restaurant with Drive-Through Window: 24 parking spaces (ratio of 8.66 spaces per 1,000 square feet)



Based on the two above methodologies, the total parking demand for the proposed bank and restaurant will be 50 spaces and 49 spaces, respectively. Therefore, the proposed parking supply of 64 parking spaces will be adequate in accommodating the parking needs of the proposed drive-in bank and the quick-serve restaurant with drive-through window.

## On-Site Circulation and Drive-Through Stacking

### *Car Wash Stacking and Circulation*

The single-lane automatic car wash tunnel will have a clockwise circulation. The entrance to the car wash circulation drive will be located in the southeast corner of the site off the two-way drive aisle that connects the site access drive. The circulation drive will provide three approach lanes and each lane will be individually gated with a separate automatic pay station. The gates will meter the traffic flow proceeding to the car wash tunnel entrance and will open in sequence based on the order of vehicle arrival. Once the gate is lifted for the respective lane, the individual vehicle will proceed west and north to the entrance to the tunnel. At the entrance to the car wash tunnel, the driver will remain in the vehicle and the car wash will automatically pull the vehicle through the tunnel. After exiting the tunnel, the vehicles will proceed to turn left to continue to the two-day drive aisle. According to the site plan, there is stacking for approximately 30 vehicles before extending onto the internal circulation drives. Based on KLOA, Inc.'s experience with similar car washes, the site plan provides for efficient circulation and adequate stacking for the proposed car wash. In addition, based on surveys conducted by Spack Consulting at six car washes located in Minnesota, the average maximum queue at a car wash was five vehicles. Wayfinding signage should be posted to guide vehicles to the circulation drive to minimize vehicle turning movements within the internal site circulation area. In addition, "Do Not Enter" signs should be posted at the exit of the car wash tunnel to deter opposing traffic from entering the car wash tunnel or circulation drive from the one-way exit direction.

### *Quick-Service Restaurant Drive-Through Stacking and Circulation*

The drive-through facility for the proposed quick-serve restaurant will extend along the west side of the building. As proposed, vehicles will access the drive-through lane at the southeast corner of the building and exit at the northeast corner of the building. A review of the site plan indicated that approximately eleven vehicles will be able to be accommodated within the drive-through lane without blocking the access drives or internal circulations. This stacking area will be sufficient to accommodate the peak demand of the proposed QSR. Wayfinding signage should be provided directing vehicles to the entrance of the drive-through. Additionally, exiting movements from the drive-through should be under stop sign control.



### *Bank Drive-Through Stacking and Circulation*

The three drive-through lanes for the proposed bank will extend along the south side of the building. As proposed, vehicles will access the drive-through lane at the southwest corner of the building and exit at the southeast corner of the building. A review of the site plan indicated that approximately three to four vehicles will be able to be accommodated within each of the drive-through lanes without blocking the access drives or internal circulation. This should be sufficient to accommodate the peak demand of the proposed bank. Wayfinding signage should be provided directing vehicles to the entrance of the drive-through. Additionally, exiting movements from the drive-through should be under stop sign control.

Based on the above, the following is recommended:

- The site plan provides for efficient circulation and adequate stacking for the proposed uses.
- Appropriate wayfinding signs and striping should be provided within the site directing customers to and from the entrance of the drive-throughs.
- “Do Not Enter” signs should be placed at the exit of each drive-through lane to deter opposing traffic from entering the drive-throughs from the one-way exit direction.
- Exiting movements from the drive-throughs should be under stop sign control.



## 6. Conclusion

Based on existing conditions and the traffic capacity analyses for the full buildout of the development, the findings and recommendations of this study are outlined below:

- The volume of traffic estimated to be generated by the proposed retail development will be reduced due to pass-by trips and internal capture.
- The results of the capacity analysis indicate that the proposed development traffic will not have a significant impact on the area roadways.
- The proposed access drives will be adequate in accommodating the traffic projected to be generated by the proposed development and will ensure that a flexible access system is provided.
- The provision of a right-in/right out access drive on Plainfield Road will reduce the volume of site-generated traffic traversing the signalized intersection of Plainfield Road with the Willowbrook Town Center access drive.
- The restriction of left-turn inbound and outbound movements at the Plainfield Road access drive will reduce the impact of the site-generated traffic on the westbound through movements.
- Based on a review of the simulation, an exclusive southbound left-turn lane should be provided at the proposed full movement access drive in order to keep the southbound through lane clear and minimize the queueing back toward Plainfield Road.
- The proposed exclusive southbound left-turn lane serving the site will provide approximately 29 feet of storage and 37 feet of taper.
- The site plan provides for efficient circulation and adequate stacking for the proposed uses.
- Appropriate wayfinding signs and striping should be provided within the site directing customers to and from the entrance of the drive-throughs.
- “Do Not Enter” signs should be placed at the exit of each drive-through lane to deter opposing traffic from entering the drive-throughs from the one-way exit direction.
- Exiting movements from the drive-throughs should be under stop sign control.
- Based on a review of the Village of Willowbrook Zoning Code and the rates published in the Institute of Transportation Engineers’ (ITE) *Parking Generation Manual*, 5<sup>th</sup> Edition, the proposed parking supply of 64 parking spaces will be adequate in accommodating the parking needs of the proposed drive-in bank and the quick-service restaurant with drive-through window.



Return Recorded Document  
to:

Brian Pabst  
Village Administrator  
Village of Willowbrook  
835 Midway Drive  
Willowbrook, IL 60527

### **TRAFFIC REGULATION AGREEMENT**

THIS TRAFFIC REGULATION AND ENFORCEMENT AGREEMENT (the "Agreement") is made as of this 16<sup>th</sup> day of December, 2020, by and between GW Property Group LLC (or its designee), a limited liability company authorized to conduct business in the State of Illinois (the "Owner"), and the Village of Willowbrook, an Illinois home-rule municipal corporation (the "Village"), within which the "Complex" (as defined in Section 1.1 below) is located, for the regulation of traffic and other matters within the Complex and for the enforcement of said regulations by the assigned traffic law enforcement personnel of the Village.

WITNESSETH:

ARTICLE 1. DEFINITIONS: As used in this Agreement, the following definitions apply:

- 1.1 Complex: The land, buildings and other improvements commonly known as 735 Plainfield Road, situated in the Village of Willowbrook, DuPage County, Illinois, and legally described in the attached Exhibit "A".
- 1.2 Permanent Index Numbers (PINs): 09-23-406-003



- 1.3 Manager: Those persons or entity employed or retained by Owner from time to time with authority to administer, manage and operate the Complex for the purposes of this Agreement.
- 1.4 Owner: GW Property Group, LLC (or its designee).
- 1.5 Village: Village of Willowbrook, DuPage County, Illinois.

ARTICLE 2. RECITAL OF FACTS: The following recitals of fact are an integral part of this Agreement.

- 2.1 Owner holds record title to the Complex.
- 2.2 The Complex is located within the corporate jurisdiction of the Village.
- 2.3 It is the mutual desire of the parties hereto that the Village shall have the authority, but not the obligation, to regulate the parking of vehicles, traffic, roller skating, bicycle riding, skateboarding and/or other recreational activities within the Complex, and to enforce said regulations by the assigned traffic law enforcement personnel of the Village.
- 2.4 The Illinois Vehicle Code (625 ILCS 5/11-209), the Illinois Municipal Code (65 ILCS 5/1-1-7) and the Village's home rule authority provide authority for such agreement between the Village and the Owner and said statutory authorization enumerated on those matters which may be included in such agreement. Further, additional matters may be included in such agreements pursuant to Article VII, Section 6 and Section 10 of the Illinois Constitution.
- 2.5 Manager, in its capacity with Owner, is empowered to enter into this Agreement.

ARTICLE 3. COVENANTS: In consideration of the foregoing premises and of the covenants and conditions hereinafter contained, the adequacy and sufficiency of which the parties hereto hereby stipulate, each of the respective parties hereto covenants and agrees as follows:

- 3.1 The Owner will cause the Manager or its designated representative to cooperate with the Chief of Police of the Village or his designated representative in inspecting the parking area of the Complex to determine what, if any, stop signs, yield signs, person with disabilities parking area signs, or any other traffic markers are to be erected, and to determine what, if any, areas are to be marked as stop intersections, yield intersections, person with disabilities parking areas or pedestrian crossings, in order to provide for the safe and efficient trafficking of the parking area of the Complex. Further, such determination may include the regulation and/or prohibition of roller skating, the riding of bicycles, the riding or



operation of skateboards and/or other recreational activities in and upon the Complex, and the posting of the signs with the respect thereto.

- 3.2 If it be determined, pursuant to Section 3.1 of this Agreement, that stop signs, yield signs, person with disabilities parking area signs, or any other markers are to be erected or that specified intersections are to be marked as stop intersections, yield intersections, or pedestrian crossings, the Owner agrees to erect such signs and markers at the indicated places on the Complex, in accordance with all applicable regulations and specifications promulgated by the State of Illinois. The Owner shall bear the costs and expenses of obtaining, erecting and maintaining any and all such signs and markers necessary for enforcement of the regulations agreed to by the parties herein.
- 3.3 The Owner shall cause the Manager to mark such fire lanes as the local Fire Protection District Chief or his designated representatives shall recommend as necessary for effective movement of Fire Department and other emergency vehicles.
- 3.4 Signs or other devices providing for the regulation of traffic and parking, or the regulation or prohibition of roller skating, bicycle riding, skateboarding and/or other recreational activities, within the Complex, as well as a designation of the exact regulations to be imposed thereon, shall be installed as shown in the Traffic Control Plan for GW Properties Willowbrook as prepared by Kimley-Horn, 4201 Winfield Road, Warrenville IL, 60555, referenced as Project No. 168726009, consisting of one sheet, dated December 15<sup>th</sup>, a copy of which is attached hereto as Exhibit "B" and by this reference incorporated herein.
- 3.5 The Village has the authority, but not the obligation, to enforce all regulations in the parking areas of the Complex by use of assigned traffic enforcement personnel of the Village; to issue citations to any and all violators of such regulations; and to adopt and enforce any additional reasonable rules and regulations with respect to traffic and parking in the parking area as local conditions may require or the safety and convenience of the public or the users of the parking area.
- 3.6 The Owner hereby agrees to provide for the removal of vehicles that are abandoned or parked in areas where stopping, standing or parking is prohibited. Such removal shall be done pursuant to a towing agreement and in accordance with the requirements of 625 ILCS 5/4-203, including, but not limited to, posting of the notice required therein.
- 3.7 Neither the Owner nor the Manager shall permit any person to park a vehicle at any location in the Complex for the purpose of displaying such vehicle for sale, lease or ride-share. The owner of any such vehicle may be ticketed by the Village.



- 3.8 The Owner has named the Village as an additional insured on a primary non-contributory basis on its Comprehensive General Liability Insurance Policy and on their Excess Liability Insurance Policy and a copy of said Certificate of Insurance is attached hereto as Exhibit "C" and incorporated herein. Such liability insurance shall provide that the Village, its officers, agents, agencies, employees, and departments shall be additional insured under such insurance. Said insurance shall be in the minimum amount of one million dollars (\$1,000,000) combined single limit or in such amounts acceptable to the Village and shall be in such form and with such Company as shall be approved by the corporate authorities.

The Owner hereby agrees to keep said policies in full force and effect throughout the terms of this Agreement. A mandatory written notice must be provided upon the cancellation of any policy as outlined under the Certificate of Insurance evidencing the coverage provided for herein.

- 3.9 In the event the Owner changes Managers, the Owner shall notify in writing the Village within ten (10) days of such change.

#### ARTICLE 4. TERM:

- 4.1 This Agreement shall be in full force and effect from and after the date of its execution for a period of twenty (20) years of the date thereof, and may, by further agreement of the parties, be continued for additional periods of like duration.

Notwithstanding any provision contained herein to the contrary, after this Agreement has been in effect for a term of one (1) year, this Agreement may be canceled upon the giving of thirty (30) days prior written notice by either party hereto, except to the extent that the Owner may be required to maintain this Agreement pursuant to any zoning relief granted by the Village.

- 4.2 The sole remedy available to the Owner, upon any breach of this Agreement by the Village, shall be the cancellation of the Agreement under its terms. It is of the essence of this Agreement that the Village shall not be liable in money damages for any breach of this Agreement.

ARTICLE 5. SUCCESSORS: This Agreement shall be binding upon and inure to benefit the respective assigns, successors and personal representatives of each of the parties hereto, and this Agreement shall run with title to the Complex and the obligations of the Owner shall be and are binding on future owners of any portion of the Complex.



ARTICLE 6. GOVERNING LAW: This Agreement shall be governed by the laws of the State of Illinois.

ARTICLE 7. EXECUTION AND RECORDING: This Agreement shall be executed by the parties' prior to the commencement of operations of the Complex. A fully executed copy of the Agreement shall be recorded in the Office of the Recorder of Deeds of the County of DuPage of the State of Illinois against title to the Complex, and it is agreed, pursuant to the statutes set forth above, that no regulation made pursuant to this Agreement shall be effective or enforceable until three (3) days after this Agreement is recorded.

ARTICLE 8. NOTICES: All notices hereunder shall be in writing and sent by Certified Mail, addressed to the Manager at 2211 N Elston, Suite 304, Chicago, IL 60614 and, if to the Village, at the Office of the Village Administrator of Willowbrook, 7760 Quincy Street, Willowbrook, Illinois 60527.



IN WITNESS WHEREOF, the parties hereto have caused this Agreement to be duly executed, as of the date and year first above mentioned.

OWNER

BY:

  
(Managing Agent or Owner)

VILLAGE OF WILLOWBROOK

BY.

\_\_\_\_\_  
President

ATTEST:

\_\_\_\_\_  
Village Clerk



EXHIBIT "A"

LEGAL DESCRIPTION OF COMPLEX



EXHIBIT "B"

TRAFFIC REGULATION PLAN

Pursuant to the attached agreement, the Willowbrook Police Department has the authority, but not the obligation, to enforce the following areas as designated on the traffic control plan attached hereto.

1. Traffic control signs including posted speed limit signs, stop signs, yield signs, and one-way signs.
2. No parking within 15 feet of hydrant locations as marked.
3. Handicap parking areas as marked with an official sign.
4. Prohibition of parking along all streets between 2:00 a.m. and 6:00 a.m.
5. Enforcement of yellow curb markings with adjacent "No Parking" signs indicating no parking areas.
6. Enforcement of posted, "No Trespassing" signs.
7. Posted "No Parking" zones.
8. Parking of vehicles for the purpose of being displayed for sale.
9. Enforcement of Village ordinance violations.
10. Prohibition of roller skating, bicycle riding, skateboarding and/or other recreational activities within the complex as posted.

Any future signs, crosswalks, and so forth may be agreed upon at a later date.

\_\_\_\_\_  
Owners' Representative

\_\_\_\_\_  
Chief of Police

[ATTACH PLAN]



EXHIBIT "C"

CERTIFICATE OF INSURANCE



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SPACE ABOVE RESERVED FOR RECORDER' S USE

After recording, return to:  
Northstone Law LLC  
1016 W. Jackson Blvd., Suite 509  
Chicago, IL 60607  
Attn: Animesh K. Ravani, Esq.

**DECLARATION OF OPERATING AND RECIPROCAL EASEMENT AGREEMENT**

THIS DECLARATION OF OPERATING AND RECIPROCAL EASEMENT AGREEMENT ("Agreement"), entered into as of the \_\_\_\_ day of \_\_\_\_\_, 2020 (the "Effective Date"), by **735 PLAINFIELD PROPERTY LLC**, an Illinois limited liability company, **JPMORGAN CHASE BANK, NATIONAL ASSOCIATION**, a national banking association, and **ENCORE CW, LLC**, a Delaware limited liability company.

**RECITALS**

A. Encore CW, LLC is the fee simple owner of land located in the Village of Willowbrook, DuPage County, Illinois, which parcel of land is hereinafter referred to as the "Lot 1" (the "Lot 1 Owner"). Lot 1 is legally described on **Exhibit A-1** attached hereto and is depicted on **Exhibit B** (the "Site Plan").

B. 735 Plainfield Property LLC is the fee simple owner of land located in the Village of Willowbrook, DuPage County, Illinois, which parcel of land is hereinafter referred to as the "Lot 2" (the "Lot 2 Owner"). Lot 2 is legally described on **Exhibit A-2** attached hereto and is depicted on the Site Plan.

C. JPMorgan Chase Bank is the fee simple owner of land located in the Village of Willowbrook, DuPage County, Illinois, which parcel of land is hereinafter referred to as the "Lot 3" (the "Lot 3 Owner"). Lot 3 is legally described on **Exhibit A-3** attached hereto and is depicted on the Site Plan.



D. Lot 1, Lot 2, and Lot 3 are sometimes together referred to as the "Overall Development" and individually as a "Lot" or "Parcel" and collectively, as the "Lots" or the "Parcels." The fee simple owner(s) of Lots are sometimes together referred to as the "Lot Owners" or "Parties" or singularly as a "Lot Owner" or "Party." The Overall Development is located south of that public right of way currently known as 735 Plainfield Road, Willowbrook, Illinois (collectively, the "Adjacent Public Rights of Way").

E. Lot 2 Owner intends to develop the Overall Development in accordance with the Site Plan, with common roads, common entry drives and certain common utilities although nothing in this Agreement shall constitute an express or implied obligation on the part of either Owner to make any improvements to their respective Lot, except as expressly provided in this Agreement. The Overall Development shall contain (i) driveways to allow for the ingress and egress of persons and vehicles to and from the Overall Development and \_\_\_\_\_ and to and from the Overall Development and \_\_\_\_\_ (collectively, the "Interior Drives"), (ii) designated paved areas for the parking of vehicles (the "Interior Parking Areas"). Together the Interior Drives and the Interior Parking Areas are referred to herein as the "Common Areas." The Common Areas are depicted on the Site Plan.

**NOW, THEREFORE,** in consideration of the foregoing, subject to the conditions, covenants and agreements set forth herein, each Lot Owner hereby declares, grants and imposes the following easements, rights, obligations and conditions as to the Overall Development:

## **ARTICLE I EASEMENTS**

### **1.1 Project Signs.**

a. Each Lot Owner shall have the right to signage on the common monument signs ("Project Signs") located on the Overall Development, and easement rights associated therewith, including, without limitation, access and maintenance right ("Project Sign Easement Areas"). Each Lot Owner hereby grants to the other Lot Owners, perpetual, nonexclusive easements within the Project Sign Easement Areas for the installation, operation, maintenance, repair and replacement of the Project Signs.

b. The Project Signs shall be operated, controlled and maintained in good condition and repair by the Lot 2 Owner, at the Lot 2 Owner's sole cost and expense, in conformity with all governmental regulations. Each Lot Owner granted an easement to a Project Sign shall pay to the Lot 2 Owner, on an annual basis, its allocable share of the Lot 2 Owner's reasonable and documented maintenance costs for the respective Project Signs. Each Lot Owner's allocable share of such maintenance costs shall be based on the square footage of the Lot Owner's sign panels on a Project Sign, in relation to the total square footage of all the sign panels on the applicable Project Sign (but excluding such portions of the sign used for the identification of the Overall Development). Each request for payment shall be in writing, shall be accompanied by paid receipts and bills and a calculation showing how the respective Lot Owner's allocable share was determined. Each such reimbursement shall be due and payable within twenty (20) days after receipt of such items.



c. Notwithstanding the foregoing, each Lot Owner having panel space on a Project Sign shall, at its sole cost and expense, (i) obtain all permits and approvals required for the installation of its individual sign panels, (ii) fabricate its identification panels, install the panels, and (iii) maintain and/or replace its individual sign panels pursuant to all governmental regulations, and in a safe condition and good state of repair.

d. If a Project Sign is no longer available for freestanding sign purposes because of a condemnation or any governmental requirements, the respective owners of the "Project Sign Easement Areas", shall designate a replacement Project Sign area with comparable visibility as close to the original location as is reasonably possible and all signage rights granted herein shall transfer to such replacement Project Sign.

#### **1.2 Interior Drives; Interior Parking Areas (Common Areas)**

a. The Lot 1 Owner hereby grants for the benefit of each Lot, each Lot Owner, and each Lot Owner's respective successors, assigns, tenants, licensees, invitees, customers and employees, a perpetual, reciprocal and non-exclusive easements over and across the (i) Interior Drives, as they exist from time to time, for the ingress and egress of persons and vehicles between the Overall Development and the Adjacent Public Rights of Way; and (ii) the Interior Parking Areas, to the extent located on any Lot as they exist from time to time, for the parking of vehicles in connection with business operations conducted on the Lots.

b. The Lot 2 Owner hereby grants for the benefit of each Lot, each Lot Owner, and each Lot Owner's respective successors, assigns, tenants, licensees, invitees, customers and employees, a perpetual, reciprocal and non-exclusive easements over and across the (i) Interior Drives, as they exist from time to time, for the ingress and egress of persons and vehicles between the Overall Development and the Adjacent Public Rights of Way; and (ii) the Interior Parking Areas, to the extent located on any Lot as they exist from time to time, for the parking of vehicles in connection with business operations conducted on the Lots.

c. The Lot 3 Owner hereby grants for the benefit of each Lot, each Lot Owner, and each Lot Owner's respective successors, assigns, tenants, licensees, invitees, customers and employees, a perpetual, reciprocal and non-exclusive easements over and across the (i) Interior Drives, as they exist from time to time, for the ingress and egress of persons and vehicles between the Overall Development and the Adjacent Public Rights of Way; and (ii) the Interior Parking Areas, to the extent located on any Lot as they exist from time to time, for the parking of vehicles in connection with business operations conducted on the Lots.

#### **1.3 Utilities.**

a. The term "Utility Lines" shall mean those facilities and systems providing for the transmission of water, sanitary sewer, storm sewer, gas, electric and telecommunications, excluding public utility lines, existing as of the date of this Agreement. The term "Common Utility Lines" shall mean those Utility Lines, which are installed to provide the applicable service to more than one parcel, excluding public utility lines. The term "Separate Utility Lines" shall mean those Utility Lines, which are installed to provide the applicable service to one



particular Parcel. For the purpose of this Agreement, the portion of a Utility Line extending between a Common Utility Line and a building or the termination of the line shall be considered a Separate Utility Line.

b. Each Lot Owner hereby grants for the benefit of each Lot, each Lot Owner, and each Lot Owner's respective successors and assigns, a perpetual, nonexclusive easement under, through and across such portions of the Overall Development for the operation, flow, passage, use, maintenance, connection, repair, removal and replacement the Utility Lines.

c. The Lot Owners shall maintain, repair, and replace the Separate Utility Lines serving their respective Parcel, at such Lot Owner's sole cost and expense, in good order, appearance and repair in full compliance with the applicable laws and regulations and in such a manner to minimize any interference with utility services. The Lot Owners shall use commercially reasonable efforts to complete any maintenance, repair, or replacement to any Separate Utility Lines that are located on another Party's Parcel, expeditiously, in order to minimize any interference with business operations at the repair site and shall promptly restore such Party's Parcel to the condition immediately prior to completing said maintenance, repair, or replacement.

d. The Utility Lines shall not be removed, reconstructed or reconfigured without first obtaining the prior written approval of the Lot 2 Owner, which the Lot 2 Owner may withhold in its sole discretion.

#### **1.4 Common Areas.**

a. The Lot 2 Owner shall maintain the Common Areas, at its initial cost, in good order, appearance and repair in full compliance with the applicable laws at a standard that is at least comparable to the standard of other first-class shopping centers in the metropolitan area where the Overall Development is located. The Lot 2 Owner shall use commercially reasonable efforts to complete any maintenance, repair, or replacement to any Common Areas that are located on another Party's Parcel, expeditiously, in order to minimize any interference with business operations at the repair site and shall promptly restore such Party's Parcel to the condition immediately prior to completing said maintenance, repair, or replacement.

b. The Lot Owners shall pay the Lot 2 Owner, on an annual basis, its allocable share of the Lot 2 Owner's out-of-pocket maintenance costs for the Common Areas. The Lot Owner's allocable share of such maintenance costs shall be based on the acreage of the respective Parcel in relation to the acreage of the Overall Development utilizing the respective Common Areas. Each request for payment shall be in writing, shall be accompanied by paid receipts and bills and a calculation showing how each Party's allocable share was determined. Each such reimbursement shall be due and payable within thirty (30) days after receipt of such items. In the event a Lot Owner fails to pay the Lot 2 Owner within the specified time period, (a) the Lot Owner shall be responsible for interest on such amount computed at the rate of the smaller of (i) ten (10%) percent per annum and (ii) the highest interest rate allowed by law, calculated monthly, from the date of any such claim by the Lot 2 Owner to the date of payment; (b) the Lot 2 Owner shall be entitled to pursue whatever remedies it may have in law or equity; and (c) the Lot 2 Owner shall have the right to file a lien against such Lot in the amount of the claim. Any



such lien shall be considered to be akin to a mortgage lien and the Lot 2 Owner may foreclose upon it in the same manner as a mortgage lien. In no event shall any lien filed under this paragraph be superior to any lien of any construction mortgage utilized to facilitate the initial development of a Lot from its current status as a vacant parcel and recorded in the real property records prior to the date such Lot Owner has completed such initial development. The provisions and requirements of the Illinois Construction Lien Act shall not apply to this lien.

## **ARTICLE II**

### **PARCEL RESTRICTIONS & STANDARDS**

#### **2.1 Restricted Uses.**

a. During the Term, no part of Lot 1 or Lot 2 shall be used, directly or indirectly, for the operation a full service financial institution, including, without limitation, the provision of banking, mortgage lending, insurance and securities services, as well as to operate drive-through banking facilities or exterior automatic teller machines (“ATM”). In addition, there shall be no ATMs from operating within the interior of any improvements located on the Lot 1 or Lot 2 that is visible from the exterior or for which there is signage that is visible from the exterior of the any structure located within the Overall Development which advertises or otherwise indicates the presence of an ATM.

2.2 **Applicable Definitions.** As used in this Article II, the following terms have the meaning provided herein:

a. The term “operation” shall include not only the store building but also, the parking areas, drives, entries, truck docks, or any other improvements used in connection with or to support such a store or to support a shopping center of which such a store is a part.

b. The term “gross building floor area” shall mean the gross interior dimensions of the main floor of the building, excluding shelving, racks, decks, mezzanines, basements, and second story areas.

#### **2.3 Tenant-Specific Restricted Uses.**

a. Pursuant to the terms and conditioned contained within that certain lease by and between the Lot 2 Owner, as landlord, and Guzman Y Gomez, as tenant (“Guzman”), dated \_\_\_\_\_, 2020 (the “Guzman Lease”), Lot 1, Lot 2, and Lot 3 shall be subject to the restrictions as described on **Exhibit D** attached hereto (the “Guzman Restrictions”). The Guzman Restrictions shall continue during the term of the Guzman Lease, which shall be confirmed by the Lot 2 Owner pursuant to Section 6.12.

2.4 **Maintenance Standard.** Each Lot Owner shall maintain their respective Parcel, including all landscaping and improvements thereon, in a sightly, safe condition and in a good state of repair at a standard that is at least comparable to the standard of other first-class shopping centers in the metropolitan area where the Overall Development is located.



### ARTICLE III INSURANCE & INDEMNITY

3.1 **Property Insurance.** During the Term of this Agreement and subject to the right to self-insure set forth in Section 3.1(g), each Lot Owner shall, at its sole cost and expense, obtain and keep in force:

a. Commercial general liability insurance, including premises/operations, products/completed operations, blanket contractual liability, broad form property damage, independent contractors and personal/advertising injury coverage, fire and explosion legal liability, explosion/collapse/and underground hazard coverage in an amount not less than Three Million Dollars (\$3,000,000) per occurrence; such policy shall be an occurrence policy and not a claims-made policy.

b. Each Party may satisfy its obligation under this Section 3.1(a) by appropriate endorsement of its blanket insurance policies. Such insurance shall name the other Parties (and their parent and affiliated companies) as additional insureds on an endorsement acceptable to the other Parties. The additional insured endorsement shall extend coverage to the contractual liability and completed operations coverage. All liability policies maintained by a Party shall be written as primary policies, not contributing with and not in excess of coverage that any other Party may carry.

c. Automobile liability insurance including coverage for owned, hired, and non-owned automobiles. The limits of liability shall not be less than \$1,000,000 combined single limit each accident for bodily injury and property damage combined.

d. Workers' Compensation coverage for its employees or contractors with statutory limits.

e. All insurance required in this Agreement and all renewals of it shall be issued by companies authorized to transact business within the state in which the Overall Development is located.

f. Subject to the right to self-insure set forth in Section 3.1(g), each Party shall, upon the written request of another Party, promptly furnish the other Party with one (1) or more certificates of insurance evidencing the existence of the insurance required to be carried pursuant to this Agreement. No Party shall be required during any given 180-day period or term of the policy, whichever is shorter, to honor more than one (1) such request from another Party.

g. The Owners each hereby waive any rights one may have against the other on account of any loss or damage occurring to an individual Owner, or its respective property, either real or personal, arising from any risk generally covered by the Owner's property insurance, from any risk covered by standard forms of special form property insurance policies then in effect and from any risk covered by any other property insurance which is actually carried by said Owner. Said Owners shall use reasonable efforts to obtain, if needed, appropriate endorsements to the Owner's property insurance with respect to the foregoing waiver; provided, however, that failure to obtain such endorsements shall not affect the waiver hereinabove given. In addition, said Owners shall cause the insurance companies issuing the Owner's property



insurance to waive any right of subrogation that said insurance companies may have against the Owners. It is the intent of the Parties that with respect to any loss from a peril required to be covered under a policy of Owner's property insurance, the Parties shall look solely to their respective insurance company for recovery.

**h.** If a Party elects to self-insure all or a part of the insurance required in Section 3.1(a), then Section 3.1(b), 3.1(e) and 3.1(f) hereof shall not apply to any Party or such Party's successors or assigns so long as such Party, or its parent company, maintains a net worth in excess of One Hundred Million Dollars (\$100,000,000) as certified by an officer of such Party.

**3.2 Indemnification.** Each Lot Owner, for itself and its successors and assigns (each an "Indemnifying Party"), agrees to indemnify and hold the other Lot Owners (each an "Indemnified Party") harmless from and against any loss or damage of any type or nature to the extent caused by the act or omission of an employee, agent or contractor of an Indemnifying Party while performing any construction, maintenance or repair activities on the Indemnified Party's Parcel for the benefit of the Indemnifying Party's Parcel or the Overall Development, including, without limitation, attorneys' fees and costs; provided, however that such indemnification shall not extend to an Owner's negligence or the negligence of such Owner's tenants or invitees. Subject to the provisions of Section 3.1(g) hereof regarding waiver of subrogation with respect to damage to property, each Owner shall defend, indemnify, protect and hold the other Owners harmless for, from and against any and all claims in connection with the loss of life, personal injury and/or damage to property (i) arising from or out of any occurrence in or upon the indemnifying Owner's Lot, including an Owner's own negligence; (ii) occasioned wholly by any negligent or willful act or omission of the indemnifying Owner, its tenants, its invitees or their respective its agents, contractors, servants or employees; or (iii) in connection with the failure to comply with the provisions of this Agreement. An Owner shall have the right to engage its own attorneys in connection with any of the provisions of this Section 3.2 or any of the provisions of this Agreement, including, but not limited to, any defense of an Owner, notwithstanding any contrary provisions of the laws or court decisions of the state in which the Overall Development is located.

#### **ARTICLE IV TERM AND TERMINATION**

**4.1 Term and Termination.** This Agreement shall be effective as of the date first above written and shall continue in full force and effect until 11:59 p.m. on the date that is forty (40) years after the date that this Agreement is recorded in the county and state where the Overall Development is located (the "Term"); provided, however, that (i) the easements referred to in Article I hereof which are specified as being perpetual or as continuing beyond the Term of this Agreement shall continue in full force and effect as provided herein, (ii) the Guzman Restrictions set forth in Section 2.4 of Article II shall continue for the term described in the Guzman Lease and (iii) other time limits shall be as specifically provided in this Agreement. Except as provided in the preceding sentence, upon the termination of this Agreement, all rights and privileges derived from and all duties and obligations created and imposed by the provisions of this Agreement shall terminate and have no further force or effect; provided, however, that the termination of this Agreement shall not limit or affect any remedy at law or in equity that a Party



may have against any other Party with respect to any liability or obligation arising or to be performed under this Agreement prior to the date of such termination.

## **ARTICLE V**

### **ENFORCEMENT**

5.2 **Defaults; Self-Help.** If a Lot Owner shall default in the performance of an obligation of such Lot Owner under this Agreement (a "Defaulting Party"), the other Lot Owners (the "Nondefaulting Party"), in addition to all other remedies provided at law or equity, after thirty (30) days' prior written notice to the Defaulting Party, shall have the right of "self-help" to perform such obligation on behalf of the Defaulting Party; provided, however, that in the event of an emergency, the Nondefaulting Party may exercise the right of self-help if such failure is not cured within a period reasonable for the nature and circumstances of such emergency. In such event, the Defaulting Party shall promptly reimburse the Nondefaulting Party the reasonable, actual out-of-pocket cost thereof, together with interest thereon from the date of outlay at a rate equal to ten percent (10%) in excess of the prime interest rate published in the money rates section of the Wall Street Journal (the "Interest Rate"). The Parties agree that to the extent that the Nondefaulting Party must enter upon the property of the Defaulting Party in order to exercise such right of "self-help", the Defaulting Party hereby grants the Nondefaulting Party a license to enter and perform such obligation.

## **ARTICLE VI**

### **TAXES AND ASSESSMENTS**

#### **6.1 Taxes and Assessments.**

a. Lot Owners 1 and 3 shall have the right to pay any taxes due with regard to the Overall Development that includes its Lot if the Lot 2 Owner shall fail to pay such taxes on or prior to the due date, which date may be extended to the extent permitted by law if Lot 2 Owner is contesting such taxes. In the event Lot Owner 1 or Lot Owner 3 pays the taxes for the Overall Development within the specified time period, (a) Lot 2 Owner shall reimburse the Lot Owner 1 or Lot Owner 3 for the amount paid on its behalf, less the amount of such taxes applicable to its Lot within fifteen (15) days after Lot 2 Owner's receipt of written demand therefor from either Lot Owner 1 or Lot Owner 3; (b) Lot Owner 1 or Lot Owner 3 shall be entitled to pursue whatever remedies it may have in law or equity; and (c) Lot Owner 1 or Lot Owner 3 shall have the right to file a lien against such Lot 2 in the amount of the claim. Any such lien shall be considered to be akin to a mortgage lien and the Lot Owner 1 or Lot Owner 3 may foreclose upon it in the same manner as a mortgage lien. In no event shall any lien filed under this paragraph be superior to any lien of any construction mortgage utilized to facilitate the initial development of Lot 2 from its current status as a vacant parcel and recorded in the real property records prior to the date Lot 2 Owner has completed such initial development. The provisions and requirements of the Illinois Construction Lien Act shall not apply to this lien.

b. Each Owner shall on or before the due date therefor all taxes, assessments, or charges of any type levied or made by any governmental body or agency with respect to its Lot.



## ARTICLE VII GENERAL

7.1 **No Covenant to Build.** This Agreement shall not create or impose any obligation on any Lot Owner to build, open, operate, or continuously operate, a business or any particular business on its respective Parcel(s).

7.2 **Easements and Covenants Running With the Land.**

a. Each of the easements granted by this Agreement shall constitute a servitude on the property of the grantor and an appurtenance to the property of the grantee for the Term of this Agreement (unless otherwise stated to be perpetual), shall survive the total or partial destruction of the subject matter of the easement and/or the servient tenement of such grant, and shall run with the land. The grantee of any such easement may extend the benefits thereof to each of its tenants and/or occupants of the benefitted property. An estate for whose benefit an easement is established hereunder shall for the purposes of this Section 6.2 be deemed to be the grantee of such easement.

b. It is intended that during the Term each and all of the covenants and agreements in this Agreement to be performed by or on the part of a Party, whether affirmative or negative in nature, shall be construed as covenants and not as conditions. To the fullest extent legally possible, all such covenants by a Party shall run with the land to the end that (i) the covenants of a Party undertaking a burden on its Parcel shall be appurtenant to the benefitted Parcels, except as otherwise provided, and shall constitute covenants running with the land as between the respective Parcels, with the benefitted Parcel as the dominant tenement, and the burdened Parcel as the servient tenement.

7.3 **Exhibits.** All exhibits and schedules to this Agreement are by this reference made a part of this Agreement to the same extent as if the same had been written directly within the body of this Agreement.

7.4 **Rights Cumulative.** All rights, powers and privileges conferred hereunder shall be cumulative and not restricted to those given by law.

7.5 **Partial Invalidity.** If any covenant, term or condition of this Agreement or any application thereof shall be invalid or unenforceable, the remainder of this Agreement and any other application of such covenant, term or condition shall not be affected thereby.

7.6 **Governing Law.** This Agreement shall be construed according to, and be governed by, the laws of the state in which the Overall Development is located.

7.7 **Waiver of Performance by Any Party.** One or more waivers of any covenant, term or condition of this Agreement by any Party shall not be construed as a waiver of a subsequent breach of the same or any other covenant, term or condition; nor shall any delay or omission by any Party to seek a remedy for any breach of this Agreement or to exercise a right accruing to such Party by reason of such breach be deemed a waiver by such Party of its remedies or rights with respect to such breach. The consent or approval by any Party to or of any act by another



Party requiring such consent or approval shall not be deemed to waive or render unnecessary consent to or approval of any similar act.

7.8 **Counterparts.** This Agreement may be executed in any number of counterparts, each of which shall be an original, but all of which shall together constitute one and the same instrument.

7.9 **Modification.** There shall be one approving Party representing each Parcel (individually an "Approving Party" and together the "Approving Parties"). Each Approving Party shall have absolute authority to make the decisions and/or give the approvals expressly designated to be made and/or given on behalf of the real estate represented by such position regardless of whether the Approving Party then owns all or less than all of the particular Parcel. As to any Parcel, the holder of the Approving Party position shall have the right to assign such position to any other entity having a fee interest in land within that Parcel. If an Approving Party transfers all of its fee interest in a Parcel, and does not assign its Approving Party position, then such Approving Party position shall automatically be deemed assigned to the entity acquiring fee title to the land owned by the transferring Approving Party. The signatories hereto shall be the initial Approving Party for their respective Parcels as herein identified.

7.10 **Notice.** Any notice, demand, consent, approval, request, statement, document or other communication required or permitted to be given to or served upon either Party hereto pursuant to this Agreement or applicable law shall be in writing and shall be sent by certified mail, or by a recognized national courier service, such as, but not limited to, Federal Express or United Parcel Service, postage prepaid, addressed and shall be deemed to have been given on the day after the date on which the notice was delivered to the overnight courier for delivery, or two days after the date the notice was postmarked if the notice was sent by certified mail. The Parties may, by ten (10) days prior notice to the other Parties as aforesaid, designate a different address or different addresses to which communications intended for it are to be sent. Each Party's initial address for notices pursuant to this Section 6.10 are:

If to The Lot 1 Owner:       Encore CW, LLC  
                                      12137 W. 159<sup>th</sup> Street  
                                      Homer Glen, IL 60491

With a copy to:            Sosin, Arnold & Schoenbeck, Ltd.  
                                      9501 W. 144<sup>th</sup> Place, Suite 205  
                                      Orland Park, IL 60462  
                                      Attn: George J. Arnold

If to The Lot 2 Owner:     735 Plainfield Property LLC  
                                      2211 N. Elston Avenue, Suite 304  
                                      Chicago, IL 60614  
                                      Attn: Mitchell Goltz

With a copy to:            Northstone Law LLC  
                                      1016 W. Jackson Blvd., Suite 509  
                                      Chicago, IL 60607



Attn: Animesh K. Ravani, Esq.

If to The Lot 3 Owner: JPMorgan Chase Bank, National Association  
1111 Polaris Parkway, Suite 2A  
Mail Code OH1-0274  
Columbus, OH 43240

With copies to: JPMorgan Chase Bank, National Association  
Legal Department  
1111 Polaris Parkway, Suite 4P  
Mail Code OH1-0152  
Columbus, OH 43240  
Attn: Real Estate Counsel

Freeborn & Peters, LLP  
311 South Wacker Drive, Suite 3000  
Chicago, IL 60606  
Attn: Michael A. Moynihan

7.11 **Time of Essence.** Time is of the essence in the performance of the terms and conditions of this Agreement.

7.12 **Estoppel Certificate.** Each Party severally covenants that within 15 business days after written request of the other Party, it will from time to time, but not more than twice in any twelve (12) month period, issue to another Party, to a prospective purchaser or lender or to any mortgagee of such Party, an estoppel certificate stating: (i) whether to the best knowledge of the Party to whom the request has been directed, any default of the requesting Party exists under this Agreement and, if there are known defaults, specifying the nature thereof; (ii) whether to its knowledge this Agreement has been modified or amended in any way (or if it has, then stating the nature thereof); (iii) that to the Party's knowledge this Agreement is, as of that date, in full force and effect; and (iv) to the extent a parcel is subject to any of the use restrictions set forth in Section 2.4, whether or not the lease creating such use restrictions remain in full force and effect.

7.13 **Rule of Construction.** The Parties and their counsel have reviewed and negotiated this Agreement and that the normal rule of construction to the effect that any ambiguities are to be resolved against the drafting Party shall not be employed in the interpretation of this Agreement, or any portion hereof, or any exhibits or amendments or agreements supplementary hereto.

7.14 **No Joint Venture.** Nothing contained in this Agreement shall be construed to make the Parties hereto partners or joint venturers or to render any Party liable for the debts or obligations of any other Party, except as this Agreement may expressly provide.

7.15 **Agreement for Benefit of Parties Only.** This Agreement is not intended to and shall not be construed to dedicate any rights or easements to the general public, nor to benefit any third parties which are not either Parties hereto nor their successors or assigns.



7.16 **Right to Enjoin.** In the event of any violation or threatened violation by any Party or occupant of any term, restriction, condition or covenant of the terms of this Agreement, the Party not in violation shall have the right, to seek actual damages or to enjoin such violation or threatened violation in a court of competent jurisdiction.

7.17 **Attorneys Fees.** In the event a Party commences or engages in any legal action or proceeding against the other (including, without limitation, litigation or arbitration, whether in contract, tort or both) arising out of or in connection with this Agreement or the Overall Development or any other aspect of the Overall Development or the development on the Overall Development (including, without limitation, (a) the enforcement or interpretation of either Party's rights or obligations under this Agreement or (b) the declaration of any rights or obligations under this Agreement), the prevailing Party shall be entitled to recover from the non-prevailing Party, the prevailing Party's reasonable attorney's fees, together with any costs and expenses, incurred in any such action or proceeding, including any reasonable attorney's fees, costs, and expenses incurred on collection and on appeal.

7.18 **Arbitration.** Except where another dispute resolution procedure has been specifically provided for in this Agreement (unless such dispute is not resolved using such procedure), any and all disputes, claims, or other controversy arising out of or relating to this Agreement, or the breach thereof, shall be settled by arbitration administered by the American Arbitration Association, and judgment on the award rendered by the arbitrator(s) may be entered in any court of competent jurisdiction. Within fifteen (15) days after any of the Parties hereto shall have notified another that it desires arbitration, the Parties hereto shall attempt to agree upon one arbitrator to conduct the arbitration; if the Parties cannot agree upon one arbitrator within said fifteen-day period, each Party shall select one arbitrator, and the two arbitrators so selected shall select a third arbitrator; and if they cannot agree, the procedures set forth in this Section 6.18 with respect to completing the panel of arbitrators shall apply.

a. If any of the Parties shall fail to make a choice within the time herein provided, then the Party not in default in selecting an arbitrator, may upon five days' notice to the other Party request the presiding judge of the District Court of the United States for the district in which the Overall Development is located acting in his private and non-judicial capacity to choose an arbitrator or arbitrators to fill the vacancy or vacancies. Such judge may thereupon appoint an arbitrator or arbitrators, as the case may be, to complete the panel of three arbitrators. If such judge shall fail or refuse to make such appointment, the arbitrator or arbitrators needed to complete the panel shall be named or appointed in accordance with the prevailing laws applicable to this Agreement.

b. Either the one arbitrator, or the three arbitrators, as the case may be, upon being duly appointed shall investigate the facts and hold hearings at which the Parties hereto may present their evidence and arguments. A decision by the one, or a majority vote of the three arbitrators shall be rendered within sixty (60) days after the date upon which the last arbitrator is appointed. Such decision shall be final and binding on the Parties. If the arbitrator(s) shall fail to render a decision within said period of sixty (60) days, then either Party shall have the right to institute such action or proceeding in such court as such Party shall deem appropriate in the circumstances. Judgment upon the award rendered in such arbitration may be entered by any court having jurisdiction thereof. In determining any question, matter or dispute before them, the



arbitrators shall apply the provisions of this Agreement, without varying therefrom in any respect and shall be without power to add to, modify or change any of said provisions.

c. No Party shall be considered in default hereunder during the pending of arbitration proceedings relating to such default.

d. The arbitrator(s) shall determine in what proportion the Parties hereto shall bear the cost of such arbitration, except that each Party thereto shall pay the fees and expenses of the arbitrator appointed by or on behalf of such Party and one-half of the fees and expenses of the third arbitrator, if any.

7.19 **Waiver of Jury Trial.** Each Party hereby waives trial by jury, to the extent permitted by law, in any action, proceeding or counterclaim brought by a Party against any other Party hereto or any matter whatsoever arising out of or in any way connected with this Agreement, the relationship of the Parties, the use or occupancy of the Parties' respective parcels or any person claiming through or under a Party, any claim of injury or damage, and any emergency or other statutory remedy; provided, however, the foregoing waiver shall not apply to any action for personal injury or property damage.

**IN WITNESS WHEREOF**, the Parties have caused this Agreement to be executed and delivered in multiple counterparts, each counterpart being deemed an original, to be effective on the date signed by the last Party to sign hereunder.

**[Signature Pages Follow]**



**ENCORE CW, LLC,**  
a Delaware limited liability company

Title:

STATE OF \_\_\_\_\_ )  
 )SS.:  
COUNTY OF \_\_\_\_\_ )

My Commission Expires:

1



**735 PLAINFIELD PROPERTY LLC,**  
an Illinois limited liability company

Title: \_\_\_\_\_

STATE OF \_\_\_\_\_ )  
 )SS.:  
COUNTY OF \_\_\_\_\_ )

**IN WITNESS WHEREOF**, I hereunto set my hand and official seal.

Notary Public



**JPMORGAN CHASE BANK, NATIONAL  
ASSOCIATION,**  
a national banking association

Title: \_\_\_\_\_

STATE OF \_\_\_\_\_ )  
 )SS.:  
COUNTY OF \_\_\_\_\_ )

My Commission Expires:

A-2



**EXHIBIT A-1**  
**To Declaration of Operating and Reciprocal Easement Agreement**  
(Legal Description of Lot 1)

**EXHIBIT A-2**  
**To Declaration of Operating and Reciprocal Easement Agreement**  
(Legal Description of Lot 2)

**EXHIBIT A-3**  
**To Declaration of Operating and Reciprocal Easement Agreement**  
(Legal Description of Lot 3)



**EXHIBIT B**

**To Declaration of Operating and Reciprocal Easement Agreement**

(Site Plan depicting: (i) Lots, (v) Common Areas [including Interior Drives and Interior Parking Areas], and (vi) Project Sign Easement Areas)

[Site Plan Attached]



**EXHIBIT C**  
**To Declaration of Operating and Reciprocal Easement Agreement**  
**(Sign Exhibit)**



**EXHIBIT D**  
**To Declaration of Operating and Reciprocal Easement Agreement**  
**(Guzman Use Restrictions)**

(a) Exclusive Use. Guzman shall have the exclusive right to sell Mexican and Latin cuisine (the "Exclusive Use").

(b) Other Prohibited Uses. theatre, bowling alley, billiard parlor, funeral parlor, night club, or other place of recreation or amusement (such as laser tag or paint ball facilities; miniature or putt-putt golf facilities; go-cart tracks; rock climbing facilities; party, dance, cheer, tumbling, or gymnastics facilities; flea market, industrial manufacturing facility, skating rink, bar, exercise facility; massage parlor (but therapeutic massage is permitted), modeling studio, adult bookstore or other establishment primarily engaged in the business of selling, exhibiting or distributing pornographic or obscene materials or live models or dancers, an amusement arcade or facility providing coin-operated amusement devices, rides, pinball machines, mechanical or electronic games, and/or similar types of equipment or devices, car repair facility, central laundry or dry cleaning plant (other than a dry cleaning drop-off facility which does not use dry cleaning fluids or similar chemicals or substances on site in connection with the dry cleaning of clothes) or any business which creates unreasonably or unusually strong or offensive odors, fumes, emissions or sounds (the "Prohibited Uses")





**Attachment 3**  
Standards for Special Use Permits



### Standards for Special Use Permit

Requested Special Use Permit for the Planned Unit Development at 735 Plainfield Road for the financial institution with a drive-thru component, automobile washing and cleaning facility, and drive-thru-fast food establishment.

#### 9-14-5.2: Standards:

- A. That the establishment, maintenance, or operation of the special use will not be detrimental to or endanger the public health, safety, morals, comfort or general welfare. ***The development will not be detrimental to or endanger the public health, safety, morals, comfort or general welfare of the public.***
- B. That the special use will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish and impair property values within the neighborhood. ***The special use will not be injurious to the use and enjoyment of the other properties in the immediate vicinity for the purposes already permitted, nor substantially diminish and impair property values within the neighborhood.***
- C. That the establishment of the special use will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district. ***The special use will not impede on the normal and orderly development and improvement of surrounding property for uses permitted in the district.***
- D. That adequate utilities, access roads, drainage, and/or other necessary facilities have been or are being provided. ***Adequate utilities, access roads, and drainage are being provided.***
- E. That adequate measures have been or will be taken to provide ingress and egress so designed as to minimize traffic congestion in the public streets. ***Adequate measures have been taking to provide ingress and egress so designed to minimized traffic congestion in the public streets.***
- F. That the special use shall in all other respects conform to the applicable regulations of the district in which it is located except as such regulations may in each instance be modified by the Village Board pursuant to the recommendation of the Plan Commission. ***The special use will conform to the applicable regulations of the district as deemed appropriate by the Plan Commission and Village Board.***
- G. Conditions in the area have substantially changed, and at least one year has elapsed since any denial by the Village Board of any prior application for a special use permit that would have authorized substantially the same use of all or part of the site. ***There has not been an application for this property within the last year.***





**Attachment 4**  
Standards for Variations



### Standards for Variations

Requested variation from 9-3-3 (B) to subdivide into three individual lots

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Given the three individual uses including a financial institution, fast food establishment, and automobile washing and cleaning facility, the site will not yield a reasonable return by providing only two subdivided lots as stated per 9-3-3 (B).***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Providing three individual uses is appropriate for the proposed improvements. Allowing only two lots is deemed a hardship given the three separate uses.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation for 9-3-3 (C) – Minimum Lot Areas for Lots 1, 2, and 3 from five acres (5 ac) to two and 79/100ths acres (2.79 ac)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***The site is capable of providing multiple uses otherwise, however, due to the existing overall site area, meeting the five acre minimum requirement is not possible***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the existing site area, relief from this requirement is necessary to provide any development greater than a singular use.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-3-3 (C) to reduce the minimum lot area for Lot 3 (financial institution with drive-thru) from two acres (2 ac) to seventy-nine hundredths of an acre (0.79 ac)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***The site is capable of providing multiple uses otherwise, however, due to the existing overall site area, meeting the five acre minimum requirement is not possible***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the existing site area, relief from this requirement is necessary to provide any development greater than a singular use.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Noted that under Section 9-6B-2, the drive-thru fast food establishment is identified as a special use. Requested variation from 9-6B-3 to reduce the minimum lot area for the drive-thru fast food establishment (Lot 2) from one acre (1 ac) to 0.795 acres

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the size of the existing parcel, meeting the minimum lot area of one-acre cannot be achieved for the fast food establishment use.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the size of the existing parcel and configuration of the site layout, the lot area identified in the Plat of Subdivision meets the minimum area necessary for the proposed use and thus is within the intent of the regulation.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Noted that under Section 9-6B-2, the drive-thru financial institution is identified as a special use.

Requested variation from 9-6B-3 to reduce the minimum lot area for the drive-thru financial institution (Lot 3) from one acre (1 ac) to 0.792 acres

9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the size of the existing parcel, meeting the minimum lot area of one-acre cannot be achieved for financial institution use.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the size of the existing parcel and configuration of the site layout, the lot area identified in the Plat of Subdivision meets the minimum area necessary for the proposed use and thus is within the intent of the regulation.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-10-5 (L) 1 to allow for parallel parking for spaces to serve employee parking for Lot 1 (automobile washing and cleaning facility)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Given the site constraints, utilizing parallel parking along the south of the building for employee use is the most necessary to meet parking count requirements and best utilize site space.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the site layout and parking required for the site, allowing parallel parking for employee use is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-10-5 (K) to reduce the vehicle staking requirement from 50 vehicles to 27 vehicles for the approach to the car wash on the south side of Lot 1 (automobile washing and cleaning facility)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Given the site constraints, reducing the required stacking element***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the site layout and parking required for the site, allowing parallel parking for employee use is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-6B-3 (E) 2 to reduce the side yard setback from 30' to 20' for Lot 2 (fast food establishment with drive-thru)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the side yard setback of 30' cannot be achieved for the fast food establishment use.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the parking required for the site, relief from the side yard setback requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-10-5 (G) to reduce the minimum required front yard pavement setback from 15' to 0' for Lot 1 (automobile washing and cleaning facility)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the pavement setback of 15' cannot be achieved for the bypass exit lane from the car wash***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent right-of-way and need for internal vehicular movements, relief from the pavement setback requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-10-5 (G) to reduce the minimum required parking areas, access drives, or drive aisles setback from 10' to 4.4' (west side), 3.9' (south side), and 5.0' (east side) for Lot 1 (automobile washing and cleaning facility)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the interior side yard paving setback of 10' cannot be achieved and allow vehicular access around the perimeter of the car wash building and meet the minimum parking requirements for the site.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the parking required for the site, relief from the interior side yard paving setback requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-10-4 (H) to reduce the minimum required loading berths for Lot 1 (automobile washing and cleaning facility) from one (1) to zero (0)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to proposed architecture and building use, a loading berth is not necessary.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to proposed architecture and building use, a loading berth is not necessary.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-10-4 (H) to reduce the minimum required loading berths for Lot 3 (financial institution with drive-thru) from one (1) to zero (0)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to proposed architecture and building use, a loading berth is not necessary.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to proposed architecture and building use, a loading berth is not necessary.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-6-1 (E) to allow for aluminum composite (ACM) panels on the building within Lot 3 (financial institution with drive-thru)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***As shown in the proposed architecture, the ACM panels are a contemporary cladding material that is long lasting, has high corrosion and water resistance and will blend well with the architecture of the overall development and adjacent commercial architecture within the surrounding area.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to proposed architecture and building use, allowing ACM panels is requested given the functionality of the building.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-6B-3 (G) to increase the allowable building height from thirty-feet (30') to forty-feet (40') for Lot 1 (automobile washing and cleaning facility)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***A request to a variation of this requirement is due to the proposed architecture with the water tower included. The main structure is at the required maximum height per code and only the accessory water tower feature exceeds the maximum allowable.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to proposed architecture and building use, allowing a greater building height is requested given the functionality of the building.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-6B-3 (A) to reduce the minimum lot area from one acre (1 ac) to 0.79 acres or Lot 2 (fast food establishment with drive-thru)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the minimum lot area reduction is necessary for proposed site use while still meeting requirements for minimum parking.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the site orientation, relief from the minimum lot area requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-6B-3 (C) 1 to reduce the minimum required lot width from 200' to 118.68' for Lot 2 (fast food establishment with drive-thru)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the minimum lot width reduction is necessary for proposed site use while still meeting requirements for minimum parking.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the site orientation, relief from the minimum lot width requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-10-5 (G) to reduce the minimum required front yard pavement setback from 25' to 24' for Lot 2 (fast food establishment with drive-thru)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the pavement setback of 25' cannot be achieved while still meeting the parking requirements for the site.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent right-of-way and need for internal vehicular movements and minimum parking requirements, relief from the pavement setback requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-10-5 (G) to reduce the minimum required interior side yard parking area, access drives, and drive aisle setback from 10' to 2.5' on the east side and 0' on the south and west sides for Lot 2 (fast food establishment with drive-thru)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the interior side yard paving area setback of 10' cannot be achieved and allow vehicular access between the fast food establishment and financial institution, while still meeting minimum parking requirements.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the parking required for the site, relief from the interior side yard paving area setback requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-10-5 (G) 3 to reduce the minimum required end parking island width from 9' to 5.0' for Lot 2 (fast food establishment with drive-thru)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the end parking island requirement of 9' is too wide to allow for vehicular movement throughout the site and meet the minimum parking requirements.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the site, the reduced parking island width is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-3-7 (A) 4 to reduce the minimum required specific setback from Plainfield Road from 100' to 71.8' for Lot 3 (financial institution with drive-thru)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the specific Plainfield Road setback cannot be achieved while meeting minimum parking requirements and providing ATM drive-up access entirely within the Lot 3 limits. Note this setback is in general conformance to the existing western adjacent shopping center property.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and proximity of the Plainfield Road right-of-way, relief from the front yard setback requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-6-1 (A) to reduce the minimum lot area with two uses from two acre (2 acres) to 0.79 acres or Lot 2 (fast food establishment with drive-thru)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the minimum lot area reduction is necessary for proposed site use while still meeting requirements for minimum parking.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the site orientation, relief from the minimum lot area requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-6B-3 (A) to reduce the minimum lot area from one acre (1 ac) to 0.79 acres or Lot 3 (financial institution with drive-thru)

#### 9-14-4.5: Standards for Variation:

- H. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the minimum lot area reduction is necessary for proposed site use while still meeting requirements for minimum parking.***
- I. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the site orientation, relief from the minimum lot area requirement is necessary for the site to function as needed.***
- J. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- K. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- L. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- M. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- N. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-6-1 (A) to reduce the minimum lot area with two uses from two acre (2 acres) to 0.79 acres or Lot 3 (financial institution with drive-thru)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the minimum lot area reduction is necessary for proposed site use while still meeting requirements for minimum parking.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the site orientation, relief from the minimum lot area requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-6B-3 (E) 2 to reduce the side yard setback from 30' to 5' for Lot 3 (financial institution with drive-thru)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the side yard setback of 30' cannot be achieved for the financial institution use while still providing vehicular access around the building and meeting minimum parking requirements.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the parking required for the site, relief from the side yard setback requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-10-5 (G) to reduce the minimum required interior side yard parking area, access drive, and drive aisle setback from 10' to 9' on the west lot line and 0' for the south and east lot lines for Lot 3 (financial institution with drive-thru)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the interior side yard parking area setback of 10' cannot be achieved and allow vehicular access around the perimeter of the financial institution building and meet the minimum parking requirements for the site.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the parking required for the site, relief from the interior side yard paving setback requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-10-5 (G) to reduce the minimum required front yard pavement setback from 25' to 24' for Lot 3 (financial institution with drive-thru)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the pavement setback of 25' cannot be achieved while still meeting the parking requirements for the site.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent right-of-way and need for internal vehicular movements and minimum parking requirements, relief from the pavement setback requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-10-5 (L) 2(g) to reduce the site egress driveway storage area providing access to Plainfield Road from 100' to 38' as measured from the property line to the end of the driveway median

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***The proposed driveway location matches the site access in the current condition. Due to the existing site constraints, increasing the driveway storage will be detrimental to providing a usable site while still providing vehicular circulation.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the parcel dimensions, relief from the driveway storage requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-10-5 (L) 2 (d) to reduce the minimum spacing between the center of the proposed driveway entrance and the adjoining property line from 70' to 55.4'

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***The proposed driveway location matches the site access in the current condition and cannot shift due to proximity to the existing intersection to the west. Without this site entrance, the site does not meet the minimum requirement for access.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***The proposed driveway location is located at the same location as the existing access and shifting the driveway will cause potential concerns with the intersection to the west. The driveway location shall remain as-is.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-10-5 (L) 2 (e) to reduce the minimum spacing between separate driveway entrances from 400' to 153.8' for site access the Planned Unit Development

#### 9-14-4.5: Standards for Variation:

- H. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Meeting the requirement per the ordinance is impossible as the limits of the existing intersection to the west and existing driveway access for the neighboring adjoiner to the east is less than 500-feet.***
- I. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***The proposed driveway location is located at the same location as the existing access and shifting the driveway will cause potential concerns with the intersection to the west. The driveway location shall remain as-is.***
- J. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- K. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- L. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- M. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- N. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-12-2 to allow for building canopy encroachment for Lot 1 (automobile washing and cleaning facility) to encroach beyond 1/3 of the required yard

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints including the minimal rear yard dimension and the proposed use, restricting the canopy to encroach less than 1/3 into the required yard would not allow for coverage across all drives.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the parcel dimensions, relief from the allowable distance for the canopy to encroach into the required yard is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-12-2 to allow for building canopy encroachment for Lot 3 (financial institution) to encroach beyond 1/3 of the required yard

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints including the minimal rear yard dimension and the proposed use, restricting the canopy to encroach less than 1/3 into the required yard would not allow for coverage for the bank drive-thru.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the parcel dimensions, relief from the allowable distance for the canopy to encroach into the required yard is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-13-6 (F) to reduce the minimum required yards or setbacks along the periphery of the planned unit development ("PUD") be at least equal in width or depth to the of the applicable side yard within the adjacent zoning district

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the minimum required yard widths and depths cannot meet or exceed those of the surrounding adjacent parcels while still providing vehicular access around the perimeter of all buildings and meeting the minimum parking requirements for the site.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the parking required for the site, relief from the required yard setback requirements is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-13-6 (B) to waive the requirement that the subject realty be under single ownership and/or unified control

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the nature of the subdivision, the individual lots should be available for separate ownership due to separate uses.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the site and nature of the subdivision, multiple ownership is required for multiple uses.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 10-4-3 (B) to waive the requirement that Lot 1 (automobile washing and cleaning facility) front on a public street

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, providing Lot 1 frontage to a public street would cause adverse effects to traffic in the public right-of-way. Lot 1 is provided an accessible ingress/egress at two locations.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent public roadways, providing site access to Lot 1 via a public street is not a feasible option.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 10-4-3 (A) to waive the requirements to meet minimum lot areas and widths for Lots 2 and 3

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the minimum lot area and widths reduction is necessary for proposed site use while still meeting requirements for minimum parking.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the site orientation, relief from the minimum lot area and widths requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 10-4-3 (A) to waive the requirements to meet minimum building setback requirements for Lots 2 and 3

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the existing site constraints, the minimum building setbacks cannot be achieved for the financial institution and fast food establishment use while still providing vehicular access around the building and meeting minimum parking requirements.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the configuration of the adjacent uses and the parking required for the Lots 2 and 3, relief from the minimum building setback requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-11-5 (E) 2 to increase the allowable maximum illuminated window sign requirement from 4 SF to 5.35 SF for Lot 3 (financial institution with drive-thru)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***ATM signage is crucial for financial institution purposes.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***ATM signage is crucial for financial institution purposes.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-11-12 (8) 1(a) and 9-11-12 (2) 2 to increase the allowable maximum SSA from 160.70 SF to 315.20 SF for Lot 2 (fast food establishment with a drive-thru)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the narrow site frontage for Lot 2, the allowable SSA is reduced from the typical 350 SF maximum to 160.70 SF (calculated as 1.25 SF per LF of site frontage). Given this constraint, exceeding the requirement is necessary to provide proper advertising and wayfinding.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the narrow site width, providing an increase in the allowable SSA is necessary for the site to function properly regarding informing the public of the site use.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-11-11 (B) 2 to allow two wall signs to exceed the maximum allowed height (grade at top of sign) of 20-feet for Lot 2 (fast food establishment)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to how this restaurant must be located and positioned on this property the building is set 160ft from Plainfield Rd. This very real hardship directly impacts the commercial viability of this site. A Sign Visibility Study was conducted as this site was being evaluated and the result was two signs being proposed at height of 26ft-6in.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***In the case of this restaurant strict adherence to the sign code essentially rules out this location as a potential site. The setback from Plainfield Rd. and the angle at which Plainfield Rd. runs would render the GyG branding almost invisible in this highly competitive QSR market.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***This hardship was existing by virtue of how the sign code relates to this property.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***This variation only affects the placement of two wall signs and this will not have a negative effect on the public welfare or adjacent properties.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***This variation relates only to aesthetic sign issues and not any construction that impacts the neighboring properties' supply of light, street congestion, danger of fire, or be a public safety issue.***
- F. The proposed variation will not alter the essential character of the locality. ***The proposed relief from the sign code does not change the character of this corridor as it pertains only to sign height on two wall signs.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***This variation is aligned with the goals of Title 9 of the Willowbrook sign code.***



### Standards for Variations

Requested variation from 9-11-4 (C) and 9-11-4 (D) to waive requirement and allow roof signage and projecting signage for Lot 1 (automobile washing and cleaning facility)

#### 9-14-4.5: Standards for Variation:

- H. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the proposed roofline architecture, providing main signage by projecting from the bottom of the roof is necessary to be at an appropriate height for safety and advertising purposes.***
- I. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***The roof gradually slopes upward, starting at about 1½ stories. Due to the architecture, the proposed signage must project from the roof for height and visibility purposes.***
- J. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- K. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- L. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- M. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- N. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-11-11 (B) 2 to allow for building signage to extend higher than one foot (1') below the top line of the face of the building and exceed a height of twenty feet (20') for Lot 1 (automobile washing and cleaning facility).

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the proposed roofline architecture, providing main signage by projecting from the bottom of the roof is necessary. Providing signage one-foot below the face of the building would conflict with the buildings windows and doors at the main entrance.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***The roof gradually slopes upward, starting at about 1½ stories. Due to the architecture, the proposed signage must project from the roof for height and visibility purposes.***
- A. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- B. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- C. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- D. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- E. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-11-12 (B) 1(a) to increase the maximum allowable sign surface area from 193.80 SF (calculated as 1.25 SF times the length of the site frontage) to 339.50 SF for Lot 3 (financial institution with drive-thru)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Given the site layout and the effective site frontages along the west and north side, multiple signs are necessary for advertising and wayfinding purposes. The proposed signage is in-line with other financial institutions in the vicinity.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Providing appropriate signage is necessary for site functionality.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-12-4 (C) 3 to decrease the minimum clearance height for the canopy within Lot 1 (automobile washing and cleaning facility) from fourteen-feet (14.0-feet) to twelve-feet (12.0-feet)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Due to the proposed use and vehicles allowed to enter the automobile washing area, providing a clearance canopy of 12.0-feet maximum is necessary.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Reducing the minimum clearance height is necessary for the car wash facility to function based on proposed use.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-14-2(D)2(c)4 to decrease the minimum allowable foundation landscaping from seven feet (7-feet) to zero feet (0-feet) for all building facades within Lot 1, Lot 2, and Lot 3

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Given the site layout and building entrances, meeting the landscaping requirement and allowing pedestrian maneuverability and access to each building is not feasible.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Due to the site configuration and need for pedestrian access to each building while also meeting drive width and parking requirements, relief from the foundation landscaping requirement is necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***



### Standards for Variations

Requested variation from 9-10-5 (G) to eliminate requirement for one tree and three small shrubs required within the parking islands within Lot 2 (fast food establishment with drive-thru)

#### 9-14-4.5: Standards for Variation:

- A. The property in question cannot yield a reasonable return if permitted to be used only under the conditions allowed by the regulations of the district in which it is located. ***Given the site dimensions and need to meet parking and drive access requirements, the parking islands do not have the capacity to provide one tree and three small shrubs.***
- B. The proposed variation will not merely serve as a convenience to the applicant, but will alleviate some demonstrable and unusual hardship which will result if the strict letter of the regulations were carried out and which is not generally applicable to property within the same district. ***Increasing the parking islands enough to provide the landscaping identified will restrict parking and paved areas necessary for the site to function as needed.***
- C. The alleged hardship has not been created by any person presently having a proprietary interest in the premises. ***The hardship has not been created by any person presently having a proprietary interest in the premises.***
- D. The proposed variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood. ***The variation will not be materially detrimental to the public welfare or injurious to other property or improvements in the neighborhood.***
- E. The proposed variation will not impair an adequate supply of light and air to adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety. ***The variation will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- F. The proposed variation will not alter the essential character of the locality. ***The character of the locality will not be altered by the variation.***
- G. The proposed variation is in harmony with the spirit and intent of Title 9 of this Code. ***The variation is in harmony with the spirit and intent of Title 9 of the Code.***





**Attachment 5**  
Standards for Planned Unit Development  
Findings of Fact for Planned Unit Developments



## Standards for Planned Unit Development

### 9-13-6: Standards for Planned Unit Development

- A. Comprehensive Plan: A planned unit development must conform with the intent and spirit of the planning goals and objectives of the Village Comprehensive Plan. ***No variance required – the proposed PUD will meet the goals and intents of the Village Comprehensive Plan, as the uses proposed to be appropriate for the area and beneficial the community overall.***
- B. Size And Ownership: The site of the planned unit development must be under single ownership and/or unified control and be not less than two (2) acres in area. ***Variance is requested, due to the nature of the planned unit development, the individual lots should be available for separate ownership due to separate uses. No variance is required regarding minimum size as the lot area of the total PUD exceeds two (2) acres in area.***
- C. Compatibility: The uses permitted in the planned unit development must be of a type and so located as to exercise no undue detrimental influence upon surrounding properties. ***No variance required – the proposed PUD will align with current adjacent zoning and will not have detrimental influence upon the surrounding properties.***
- D. Stormwater Management: Adequate facilities, both on site and off site, shall be provided to ensure that all drainage related issues are addressed. ***No variance required – the engineering plans and drainage reports will identify that the all stormwater management proposed on-site will conform to current municipal stormwater ordinances.***
- A. Traffic: That adequate provision be made to provide ingress and egress so designed as to minimize traffic congestion in the public streets. ***No variance required – ingress and egress are designed to minimize traffic congestion in the public streets to the extents possible.***
- B. Preservation Standards: Due regard shall be demonstrated for any existing site conditions worthy of consideration including: desirable trees of a minimum six inch (6") caliper or greater, natural bodies of water, designated wetlands and/or flood hazard areas, etc. All desirable vegetation shall be identified by location, caliper size, type, condition and a method by which preservation efforts will be conducted. ***A tree preservation plan and landscape plans are provided as part of our submittal package.***
- K. Uses For PUDs Greater Than Five Acres: Any permitted or special use available under any of the various zoning districts classifications, whether singly or in combination, and any other use permitted by law, may be allowed as either a permitted or special use within a planned unit development consisting of more than five (5) acres, but only to the extent specifically approved by the Village Board, after a finding that such use is consistent with the objectives of this chapter. ***Variance requested as existing site area is below five (5) acres, however, plan meets intent of a planned unit development given combination of uses proposed.***



## Findings of Fact

### 9-13-7: Findings of Fact for Planned Unit Developments

- A. In what respects the proposed plan is consistent with the stated purpose of the planned unit development regulations. ***The proposed plan is consistent with the stated purposes identified specifically including providing diversification in uses (9-13-1 (A) 2) and rational and economic development (9-13-1 (A) 6).***
- B. The extent to which the proposed plan meets the requirements and standards of the planned unit development regulations. (Ord. 97-O-05, 1-27-1997). ***The proposed plan meets the requirements and standards of the planned unit development regulations aside from the items presented within the standards for special use & variations document.***
- C. The extent to which the proposed plan departs from the zoning and subdivision regulations otherwise applicable to the subject property, including, but not limited to, the density, dimension, area, bulk and use, and the reasons why such departures are deemed to be in the public interest. ***The proposed plan departs from the zoning and subdivision regulations as noted in the standards for special use & variations document.***
- D. The method by which the proposed plan makes adequate provision for public services, provides adequate control over vehicular traffic, provides for and protects designated common open space, and furthers the amenities of light and air, recreation and visual enjoyment. ***The proposed plan will not impair an adequate supply of light and air to the adjacent property, substantially increase congestion in the public streets, substantially increase the danger of fire, or endanger the public safety.***
- E. The relationship and compatibility of the proposed plan to the adjacent properties and neighborhood. (Ord. 75-O-3, 3-10-1975). ***The proposed plan fronting a major route provides similar use as in the current condition and is not a departure from current uses of the adjacent properties along the route.***
- F. The desirability of the proposed plan with respect to the physical development, tax base and economic well being of the Village. ***The proposed plan will provide physical and economic benefits to the community in comparison to the existing use.***
- G. The conformity with the intent and spirit of the current planning objectives of the Village. (Ord. 97-O-05, 1-27-1997). ***The proposed PUD will meet the goals and intents of the Village Comprehensive Plan, as the uses proposed to be appropriate for the area and beneficial the community overall.***





**Attachment 6**  
Public Hearing Notice



**NOTICE OF PUBLIC HEARING**  
**ZONING HEARING CASE NO. 21-03**

NOTICE IS HEREBY GIVEN that the Plan Commission of the Village of Willowbrook, DuPage County, Illinois, will conduct a public hearing at a regular meeting of the Plan Commission on the 3rd of February, 2021 at the hour of 7:00 P.M. This meeting would typically take place in the Willowbrook Police Department Training Room, 7760 S. Quincy St, Willowbrook, IL 60527. However, due to the current circumstances concerning Covid-19, this meeting will be held virtually. Internet address and access instructions will be provided on the Village of Willowbrook's Plan Commission website once available:

<https://www.willowbrookil.org/Archive.aspx?AMID=44>

The purpose of this public hearing shall be to consider a petition requesting approval of a special use permit for a planned unit development, including a financial institution with drive through, fast-food establishment with drive through, an automobile washing and cleaning facility, including certain relief, exceptions and variations from Title 9 and Title 10 of the Village Code; approval of a Preliminary Plat of Subdivision; and approval of a Preliminary Plat of PUD on property legally described as follows on the property legally described as follows:

LOT 1 IN WILLOWBROOK CENTER UNIT NO. 1, OF PART OF LOT 7 IN OWNER'S SUBDIVISION IN SECTIONS 23 AND 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED OCTOBER 16, 1963, AS DOCUMENT R63-37895, IN DUPAGE COUNTY, ILLINOIS.

PIN: 09-23-406-003

ADDRESS: 735 Plainfield Road, Willowbrook IL 60527.

The applicant for this petition is Alex Katz of G.W. Property Group LLC, 2211 N. Elston Avenue, Suite 304. The property owner is Louis W. Viren Jr. of Viren-Gill Ltd., LLC, 735 Plainfield Road, Willowbrook IL 60527.

A copy of the Village's petition is on file in the Office of the Village Planner, Village of Willowbrook, 835 Midway, Willowbrook, Illinois, and is available for public inspection. Any individual with a disability requiring a reasonable accommodation in order to participate in any public meeting held under the authority of the Village of Willowbrook should contact Ann Choi, Village of Willowbrook, 835 Midway, Willowbrook, IL 60527, or call (630) 920-2233, Monday through Friday, between 8:30 A.M. and 4:30 P.M. All persons desiring to be heard in support or opposition to the application shall be afforded an opportunity and may submit their statements orally, in written form, or both. Written comments may be submitted up to the hour of 6:00pm on February 3, 2021 to [planner@willowbrook.il.us](mailto:planner@willowbrook.il.us). This hearing may be recessed to another date if not concluded on the evening scheduled.

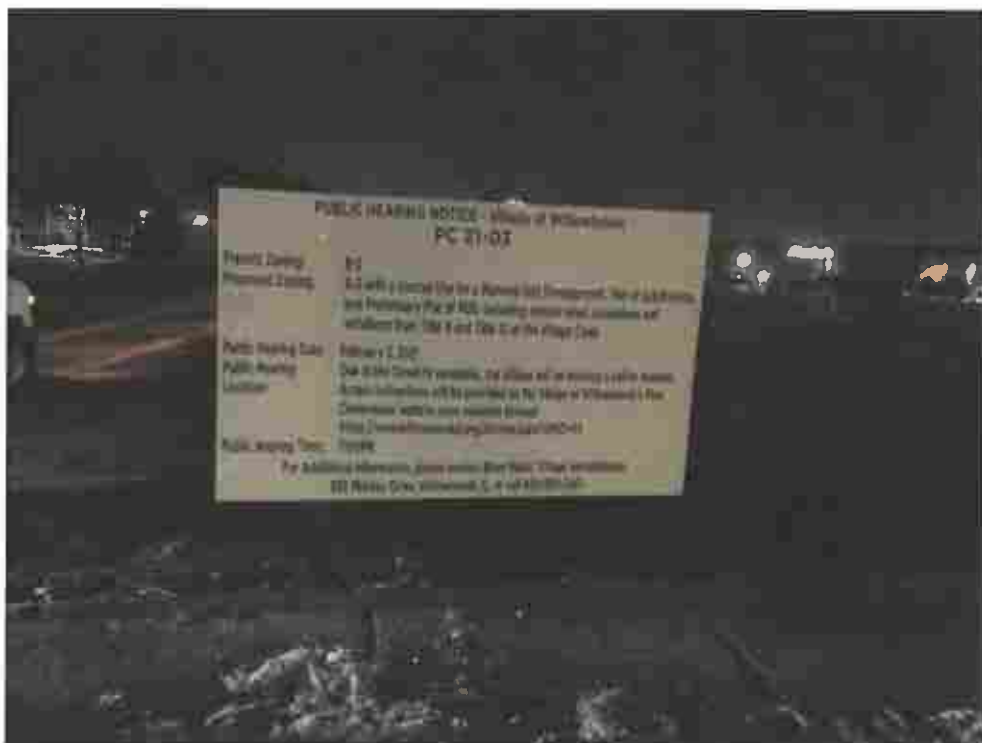
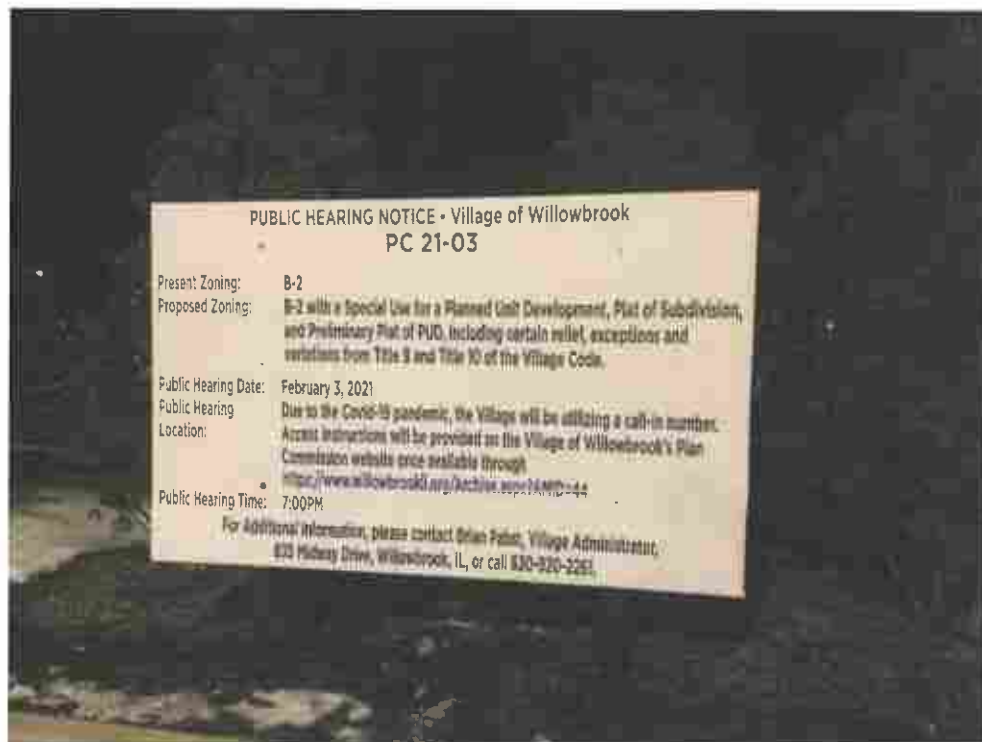
/s/ Brian Pabst  
Village Administrator  
(630) 920-2261

Published in the January 14, 2021 edition of *The Doings* Newspaper.







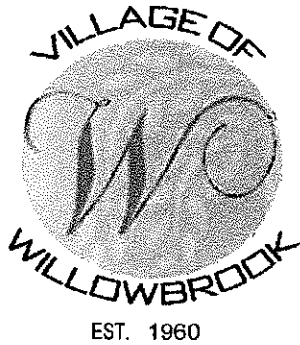






**Attachment 7**  
Village of Willowbrook Review Letter(s)





# Willowbrook

835 Midway Drive  
Willowbrook, IL 60527-5549

Phone: (630) 323-8215 Fax: (630) 323-0787 [www.willowbrookil.org](http://www.willowbrookil.org)

## Mayor

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Gregory Ruffolo

## Village Administrator

Brian Pabst

## Chief of Police

Robert Schaller

## Director of Finance

Carrie Dittman



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January 27, 2021

Atten: Alex Katz  
GW Properties  
2211 N. Elston Avenue, Suite 304  
Chicago, IL 60614

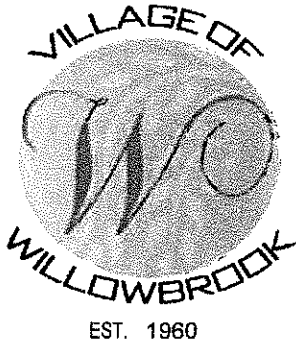
Re: **GW Properties – Willowbrook Planning Review #2**

Mr. Katz,

Planning staff has reviewed the revised submittal received by the Village on January 21, 2021, January 24, 2021, and January 25, 2021, for the GW Properties – Willowbrook planned unit development including an automobile cleaning and washing facility, a fast food establishment with drive-through, and a financial institution with drive-through, on the lot currently occupied by the vacant Willowbrook Bowling Alley located at 735 Plainfield Road in Willowbrook, IL 60527. The following plans have been reviewed per the requirements of the Village of Willowbrook Zoning Ordinance:

1. **"Preliminary Plat of PUD"** as prepared by Kimley-Horn and Associates, Inc., KHA Project No. 168726009, Sheet No. C1.0, bearing an original issue date of 12/15/2020, and bearing the latest revision date of 01/21/21.
2. **"Car Wash First Floor Plan"** as prepared by Mark Shively Architecture, Sheet No. A2.1, and bearing a revision date of 01.14.2021.
3. **"Preliminary Floor Plan" for Fast Food Establishment** as prepared by Interplan LLC, Project No. 2020.0919, 1 page, Sheet PFP, and dated 01.20.21.
4. **"Floor Plan" for Chase Bank** as prepared by The Architects Partnership, TAP Project No. 17073, 1 page, and dated 01.22.2021.
5. **"Master Sign Key Plan" for Fast Food Establishment** – 1 page, undated and provided by GW Properties on January 24, 2021.
6. **"Exterior Perspective" for Fast Food Establishment** as prepared by Interplan LLC, 1 page, and dated 01.20.21.
7. **"Architectural Elevations with Signs" for Fast Food Establishment** as prepared by Interplan LLC, 2 pages, and dated 01.20.21.
8. **"Freestanding Sign" for Fast Food Establishment** – 1 page, Sheet ST1.1, bearing the latest revision date of 09.28.20, and provided by GW Properties on January 21, 2021.
9. **"Architectural Elevations" for Chase Bank** as prepared by The Architects Partnership, TAP Project No. 19108, 2 pages, and dated 1.18.2021.
10. **"Sign Package" for Chase Bank** as prepared by Signtech, Drawing No. 18-01613, Project No. CHASE\_730\_1, 41 pages, bearing an original issue date of 09/14/18, and bearing the latest revision date of 01.19.2021.
11. **"Pay Station & Trash Enclosure Elevations for Car Wash"** as prepared by Mark Shively Architecture, Sheet No. A1.2, and bearing a revision date of 01.14.2021.
12. **"Car Wash Exterior Elevations"** as prepared by Mark Shively Architecture, Sheet No. A4.0, and bearing a revision date of 12.01.2020.
13. **"Vacuum Canopy Plans, Elevations, Sections"** as prepared by Mark Shively Architecture, Sheet No. A1.1, and bearing a plot date of 01.25.2021.





# Willowbrook

835 Midway Drive  
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Phone: (630) 323-8215 Fax: (630) 323-0787 www.willowbrookil.org

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14. "Vacuum Canopy Plan Diagram" as prepared by Mark Shively Architecture, Sheet No. A1.2, and bearing a plot date of 01.25.2021.
15. "PUD Plans" as prepared by Kimley-Horn and Associates, Inc., KHA Project No. 168726009, 9 Sheets, bearing an original issue date of 12/15/2020, and bearing the latest revision date of 01/21/21.
16. "Traffic Impact Study" as prepared by KLOA, 130 pages, and dated January 20, 2021.
17. "Traffic Comment Response Letter – DuDOT" as prepared by KLOA, 2 pages, and dated January 20, 2021.
18. "Traffic Comment Response Letter – Willowbrook" as prepared by KLOA, 6 pages, and dated January 20, 2021.
19. "Landscape Plan" and "Landscape Notes and Details" as prepared by Kimley-Horn and Associates, Inc., KHA Project No. 168726009, 2 Sheets, Sheet Nos. L2.0 and L3.0, bearing an original issue date of 12/15/2020, and bearing the latest revision date of 01/21/21.
20. "Preliminary Plat of Subdivision" as prepared by Compass Surveying Group Ltd., Project No. 20.0006, Sheet 1 of 1, and bearing the latest revision date of 01/19/21.
21. "Standards for Special Use & Variations" – 51 pages and undated.

Staff offers the following comments:

### General

1. The requirements of the Village Code for Preliminary PUD Approval under Section 9-13-5(B)15 and Section 9-13-5(B)17 include submission of a Market Study and a Tax Impact Study. It is the Village's understanding that the developer of 735 Plainfield Road has not submitted either document.
2. The proposed preliminary site plan should include a depiction of all existing and proposed easements. It is the Village's understanding that the applicant has failed to include any proposed easements on the preliminary plat.
3. The absence of the above-referenced information will be noted in the staff report and should be addressed by the applicant at the Plan Commission Hearing.
4. The Findings of Fact under Section 9-13-7(B) and Section 9-13-7(F) of the Zoning Ordinance cannot be made without the review of these two studies.

### Site Layout Plan

1. Include the Lot Areas, Lot Width, Lot Depth, Lot Coverage, Maximum Height and FAR of each lot on the Site Plan according to the definitions provided in Section 9-2-2. Provide a table summarizing this information.

### Traffic Regulation Agreement

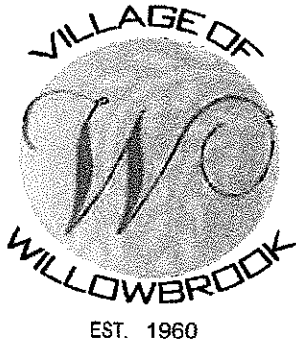
2. A copy of the Certificate of Insurance will be required as an attachment to the Traffic Regulation Agreement.

### Traffic Impact Study and Traffic Control Plan

Per your request, Gewalt Hamilton Associates, Inc. (GHA) has reviewed the following materials for the above captioned project pertaining to traffic, parking and on-site circulation:

- A. KLOA response to GHA's traffic comments dated January 20, 2021.





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- B. KLOA response to DuDOT's comments dated January 20, 2021.
- C. Traffic Impact Study (TIS) prepared by KLOA dated January 20, 2021
- D. Site Plan, Sheet Number C1.0, prepared by Kimley Horn dated January 21, 2021.

Conclusions of this effort and recommendations are presented below:

- 3. GHA has found the documentation and/or responses provided, in general, to adequately address the comments made and concerns raised in our previous Project Traffic Review dated January 15, 2021.
- 4. The applicant should provide documentation of the DuDOT approval of TIS and Plainfield Road access, upon receipt.
- 5. The response letter adequately addressed the comment pertaining to the trip rates assumed in the traffic study for the proposed carwash component of the development during the weekday morning peak hour; however, the concern raised pertaining to the Saturday morning timeframe was not addressed. Please provide response and/or supporting documentation for use of the lower trip generation estimates based on the tunnel independent variable versus the square-footage of the use.
- 6. The updated study appropriately provides a queuing exhibit that illustrates the stacking capability of each of the proposed uses, as well as additional supporting documentation. However, the supporting carwash queue data provided is insufficient to verify the projected queue lengths. It references a study that was performed in Minnesota in 2012, where 5 of the 6 carwash locations surveyed were at gasoline station facilities, as well as the referenced study did not include average daily traffic for a measure of comparison. While we understand it may be difficult to obtain peak observations at similar facilities, as carwash operations tend to peak in the wintertime on a sunny day following a snowfall, please provide supporting operational characteristics (e.g., transaction information), as available, pertaining to peak operations and/or supporting operations / documentation from similar Chicagoland area existing sites during average, typical conditions.

In addition, as we understand, the quick service restaurant use has been identified as Guzman Y Gomez. There is an existing drive-through location in Naperville. Again, please provide either data from observations and/or transaction data to support the capacity provided is adequate to support the peak queue.

- 7. GHA does not believe the comments noted above will materially impact the findings / recommendations of the TIS.

## Police Department Comments

Police Chief Robert Schaller reviewed the proposed Traffic Regulation Agreement and Site Plan and identified the following three main areas of concern that could impact police/fire:

- 8. To the west of the development is the entrance/exit into the shared Town Center drive which is heavily congested at times and will lead to traffic backing up in all directions. Currently at the Chase bank location across the street, the teller lanes often are 5-6 vehicles deep during peak times. Similar queues at the proposed bank location may lead to vehicles stacking up impacting the west entrance/exit and creating an even greater traffic bottleneck.
- 9. To the east at the restaurant drive through, Chief Schaller agrees with the concerns raised at the November 4th planning commission concept review. If the drive-through restaurant





EST. 1960

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is in any way close to the volume of traffic Chick-fil-A produces, this will no doubt impact the ability to access the car wash.

10. Lastly, the northeast entrance/exit onto Plainfield could present an issue. During peak rush hour times there are occurrences where westbound Plainfield traffic is backed up past the northeast entrance/exit. Regardless, if there is right turn only lane, it is inevitable that traffic will turn left from that lane.

### Bulk Regulations and Landscaping

11. Confirm that the requested variation from Section 9-6B-3(E)2 is for 7 feet, not 5 feet as stated in your Standards for Variations.
12. Sheet L2.0 Landscape Plan labels the 53 "AB" perennials (near the ground sign of the shared drive with Town Center) when these should be 53 "HO2" perennials to correspond to the total number of HO2 perennials (110). The total number of "AB" perennials is correct at 22.

### Architectural Elevations

13. The architectural elevations for the car wash indicate that black corrugated metal will be used along the south, west and elevations and are not in compliance with Section 9-6-1(E)2. **A waiver from this code section may be required.**

### Loading & Parking

14. Section 9-10-5(G) In-Yards, also requires that all parking areas located within a required yard adjacent to Plainfield Road shall be set back a minimum of twenty five feet (25') from the property line. A landscaped earth berm of a minimum three foot (3') height shall be required and appropriately landscaped with densely planted evergreen trees or shrubs to an effective minimum height of four feet (4') upon installation. Currently there is only two feet of landscape berm along the Plainfield Road frontage. **Staff recommends that the Landscape Plan and Grading Plan be revised to accommodate the required landscape berm of three feet (3') across the entire length of the frontage. A waiver from this code section will be required.**
15. Your responses to the Standards for Variations state under Comment #43, the minimum stacking requirement is 27 vehicles. The Traffic Site study states that is 30 vehicles. Please confirm which is correct. **A waiver from this code section will be required.**

### Signage for Lot 1

16. The total sign surface area for Lot 1 is calculated as follows:

This is considered a freestanding business and therefore business site frontage shall be based on the lot width. Therefore,  $289.9' \times 1.25 = 362.375$  SF. Total sign surface area permitted is 350 SF. The total sign surface area of 350 SF will also be reduced due to the proposed ground sign that exceeds a height of 8 feet as required by Section (B)1(d)6. **Confirm that the total sign surface area permitted is 335 SF. Adjust the total sign surface area accordingly.**

17. Provide the total sign surface area of each of the painted signs on the east and west elevations to count towards the total sign surface area for the lot.
18. The freestanding sign must contain brick or other similar material in keeping with the design of the principal structure under Section (B)1(d)1.





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19. The freestanding sign must also be located a minimum of 20 feet away from the existing self-storage ground sign under Section (B)1(d)3. **A waiver is required for total sign surface area.**
20. The proposed digital display sign for Encore Car Wash is located on the Lot 3. **Staff recommends relocating this freestanding sign to Lot 1.** There will be a condition that advertising provided by the digital displays shall be restricted to Encore Car Wash advertising only. It cannot contain advertisements not related to the conduct of the proposed car wash. There will also be conditions imposed on the operation limitations, minimal display time, message change sequence, illumination, etc.
21. Please confirm that Lot 1 is proposing the following signs:
  - a. Wall Sign E.1 (not a roof sign).
  - b. Two (2) painted wall signs E.7 (prohibited). **A waiver is required.**
  - c. Three (3) roof signs E.2, E.3 and E.4 (prohibited). **A waiver is required.**
  - d. One (1) Free standing Sign with Digital Display E.5 – digital display signs are prohibited. **A waiver is required.**
  - e. Three (3) Menu Signs E.8 and one (1) Exit Indicator Sign will not count towards the total SSA of 350 SF, but the Exit Indicator Sign will be considered a Private Traffic Direction Sign and shall not exceed four (4) square feet in area and four feet (4') in height for each sign. **A waiver is required for height and SSA.**
22. In your Standard for Variations, you included a waiver for Projecting Signs. **Please confirm where these projecting signs are located.**
23. For signs E1 and E2, provide the dimensions for the entire length of the signs, not just the width of the individual letters. Otherwise, each letter would be considered a wall sign. **Please adjust the total SSA accordingly.**
24. Sign E.5 needs to count both faces which is a total SSA of 95 square feet. **Please adjust the total proposed SSA accordingly.**
25. The three signs E.6 should also count both faces towards the SSA requirement of 4 SF. Each sign is 12 SF. Please adjust on those sheets. **A waiver for SSA is required.**

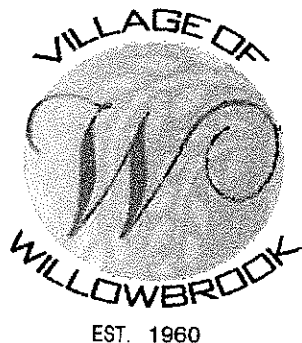
### Signage for Lot 2

26. Section 9-11-12(B)1(a) permits 1.25 square feet of sign surface area for each foot of business site frontage up to a maximum of 350 square feet of sign surface area. **Confirm that the total sign surface area permitted is 138.4 SF (a deduction was taken for the ground sign height above 8').**
27. Staff calculates the following:
  - a. Signs A and D = 46.7 SF each.
  - b. Signs B, C and E = 49 SF each.
  - c. Sign F = 75 SF
  - d. Sign G = 68 SF (double faces)

**Therefore, Staff calculates the total sign surface area proposed to be three hundred eight-three and four tenths square feet (383.4 SF). Please confirm.**

28. Please confirm if Signs H, I and J are double faced and what the copy is on each sign. There are only two copies proposed "No Entry" and "Thank You" when there are three signs proposed. **Indicated which copy goes with the corresponding letter. Please also confirm if both faces of each sign were included in the SSA.**
29. The proposed digital display sign for the fast food establishment is located on the bank lot (Lot 3). **Staff recommends relocating this freestanding sign to Lot 2.** Each ground





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sign should be located on the lot that for which it is intended under Section 9-11-12(B)1(d)4.

30. The sign plan for the fast food establishment indicates a multi-tenant sign. **Staff cannot provide any comments since no drawings were submitted for this. Staff recommends eliminating this from the plans.**
31. Section 9-11-12(B)1(b)2 permits a maximum of three wall signs on any one facade and have a minimum separation distance upon the building facade equal to or greater than twenty percent (20%) of the linear dimension of the business site frontage and further provided that the total sign surface area of all signs shall not exceed the total sign surface area permitted under subsection (B)1(a) of this section. **Include the linear distances between each sign on the fast food establishment to comply with said section. A waiver may be required.**

### Signage for Lot 3

32. Section 9-11-12(B)1(a) permits 1.25 square feet of sign surface area for each foot of business site frontage up to a maximum of 350 square feet of sign surface area. **Confirm that the total sign surface area permitted is 197.9 SF (a deduction was taken for the ground sign height above 8').**
33. Staff calculates the proposed sign surface area for E.1 as 233.3 SF and for E.8 as 4.6 SF, therefore the total proposed SSA is 344 SF (not 339.5 SF). **Please confirm and change the area on the table accordingly.**
34. Staff calculates the sign surface area for Signs E.29 and I.30 to be 8.34 SF each. **Please confirm and change the area on the table accordingly.**

### Fences

35. Staff cannot provide any comments on fence height and % openness as no detailed fence drawings were included as part of the submittal. **Any fences shall comply with Section 9-12-4(D)2 of the Zoning Ordinance.**

### Canopies

36. The plan for the pay canopy, Sheet A1.2, does not correspond to what is shown on the site plan. There are only two columns/vertical support structures shown on the site plan when Sheet A1.2 indicated there are four columns. Dimensions from the west and south lot lines to the column closest to the south lot line must be provided.

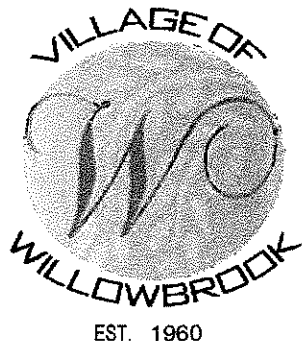
### Engineering

As requested by email on January 21, 2021, we have reviewed the PUD Plans and supporting documents. Based on our discussion, this is intended to be the Preliminary PUD submittal and this review is for compliance with Section 9-13-5.B of the Village Code.

### Preliminary PUD and Subdivision Plat

We have discussed with the design engineer that the Preliminary PUD and Preliminary Plat of Subdivision will be combined into a two page document and that, combined with the Preliminary Engineering Plans will meet the plan content requirements for both the Preliminary PUD and Preliminary Plat of Subdivision. The following comments will be incorporated into the document:





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37. On the Preliminary Plat of Subdivision, all of the certificates required on the Final Plat will be removed.
38. The Preliminary Plat will provide clarification that Public Utility and Drainage Easements will be provided as required. It remains to be determined if that will be specific easements for each utility, or a blanket easement covering all of the property except for building envelopes.
39. Section 9-13-5.B.4 provides that a statement of ownership be included listing the existing ownership and certification that there are no delinquent taxes on the property. We presume that this has been provided separately or will be added to the Preliminary PUD.
40. Section 9-13-5.B.7 provides for covenants providing various information be submitted. We presume that this will be reviewed by others.
41. Section 9-13-5.B.12 provides for preliminary landscape plans. We will defer review of these drawings to the Village Planner.
42. Section 9-13-5.B.13 provides for preliminary engineering information to be shown. These comments are provided in the next section of this letter.
43. Sections 9-13-5.B.14 – 17 provides for various studies to be provided. We presume these are being reviewed by others.

### **Preliminary Engineering Plans**

Cover Sheet – C0.0

44. Under Utility and Governing Agencies, change the Sanitary Sewer Service to Du Page County Public Works.

Site Plan – C1.0

45. No comments.

Grading Plan – C2.0

46. No comments.

Utility Plan – C4.0

47. The alignment for the proposed watermain has been discussed with the design engineer and will be revised. The connection to the existing main in the town center will be made at the driveway connection between the two sites, rather than south near the proposed car wash building. Also, the proposed watermain will be an 8" main. This will result in less new public watermain and is expected to be a lower cost for the applicant. A 20 foot wide utility easement would be required over this main.
48. Each building service shall have an 8" x 6" Tee with valve. This will allow the public main to be constructed, and then each building service can be installed at the time of building construction without interrupting the public main.
49. The last segment of storm sewer west of the car wash, where the south end of the site connects to the off-site sewer, is shown to be 18". We presume this will be revised down to 12" on the final plans.
50. We presume the intent is for the sanitary sewer serving the bank and restaurant to be a public sewer. This will be owned by Du Page County Public Works and will be reviewed by that agency. We presume they will require a 20 foot wide easement over the main.

### **General Comments**

51. A permit will be required from Du Page County Division of Transportation for work within the Plainfield Road right of way.





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52. A permit will be required from Du Page County Public Works for the proposed sanitary sewer and connections.
53. The Construction Schedule and Engineer's Opinion of Cost will be reviewed with the Final PUD Plat.
54. The fire truck turning templates indicate that presumed truck can maneuver the site. We presume that prior to final approval the plans will be provided to the fire district for comment on both the geometrics, but also the location of hydrants and fire department connections on each building.

### Tri-State Fire Protection District

55. Curb radius around the entire site to have a 25' right hand turn radius.
56. Drive width of 20' clear aisles. This includes the drive adjacent to the car wash. The curb radius and drive space is less than 20' and our fire apparatus will have a difficult time accessing the building.
57. Clear height around drive of 13'6".
58. An Auto-Turn exhibit shall be conducted prior to any approval of a site plan.

### Relief/waivers Requested

#### Zoning Ordinance

Approval of a Special Use Permit for a planned unit development consisting of a financial institution with drive through, fast food establishment with drive through, an automobile washing and cleaning facility, and associated site improvements. The approval of the special use permit will include, but is not limited to, the following waivers:

#### Overall Site

1. That Section 9-3-3(B), Division of Lots, be varied to permit the division of the SUBJECT REALTY into more than two (2) lots that do not conform with all the applicable bulk regulations of the B-2 zoning district.
2. That Section 9-3-3(C), Multiple Uses on a Lot, be varied to permit a reduction in the minimum lot area for two (2) or more special uses from five acres (5 AC) to two and 79/100ths acres (2.79 AC).
3. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required front yard parking area setback from twenty-five feet (25') to twenty-four feet (24') along Plainfield Road.
4. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required height of a landscaped earth berm along the Plainfield Road frontage from three feet (3') to two feet (2'). **STAFF RECOMMENDS REVISIONS TO THE LANDSCAPE PLAN TO ELIMINATE THIS WAIVER.**
5. That Section 9-10-5(L)2(d), Driveway Location On Lots For All Uses Other Than Single-Family Residential, be varied to permit a reduction in the minimum separation requirement between an access driveway entrance and an adjoining lot line from seventy feet (70') to fifty-five point four feet (55.4').
6. That Section 9-10-5(L)2(e), Spacing Between Separate Driveway Entrances On All Lots Other Than In Single-Family Attached Districts, be varied to permit a reduction in the minimum access driveway spacing for separate driveways from four hundred feet (400') to one hundred fifty-three and 8/10ths feet (153.8').





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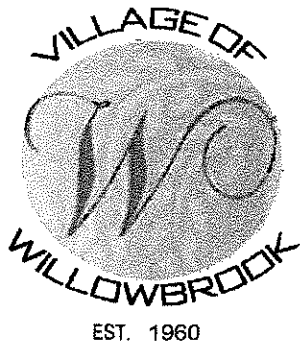
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7. That Section 9-10-5(L)2(g), Access Driveways From Arterial Streets For All Uses Other Than Single-Family Residential, be varied to permit a reduction in the driveway storage area from one hundred feet (100') to thirty-eight feet (38').

### LOT 1 (Automobile Washing and Cleaning Facility)

8. That Section 9-6-1(E)2, General Conditions, Building Façade Materials, be varied to permit metal siding and metal panels as building façade materials, as shown on the architectural elevations.
9. That Section 9-6B-3(D), Minimum Lot Depth, be varied to permit a reduction in the minimum required lot depth from two hundred feet (200') to one hundred eighty and three tenths feet (180.3').
10. That Section 9-6B-3(E)1, Required Setbacks, Front Yard, be varied to permit a reduction in the minimum required front yard setback from sixty feet (60') to thirteen and five tenths feet (13.5') for the detached canopy along the north side of the proposed car wash building.
11. That Section 9-6B-3(E)4, Required Setbacks, Rear Yard, be varied to permit a reduction in the minimum required rear yard setback from forty feet (40') to eight tenths feet (0.8') for the detached canopy along the south side of the proposed car wash building.
12. That Section 9-6B-3(G), Maximum Height, be varied to permit an increase in the maximum building height from thirty feet (30') to forty feet (40') to the top of the proposed water tank.
13. That Section 9-10-4(H) Off-Street Loading, Required Berths, be varied to permit a reduction of the minimum number of required loading berths from one (1) loading berth to zero (0) loading berths.
14. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required interior side and rear yard parking area setbacks from ten feet (10') to four and four tenths feet (4.4') along the west lot line, to five feet (5') along the east and south lot lines.
15. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required front yard parking area setback from fifteen feet (15') to zero feet along the north lot line.
16. That Section 9-10-5(K), Off-Street Parking, Required Spaces, be varied to permit a reduction in the minimum number of stacking spaces for automobile laundries from fifty (50) spaces to thirty (30) spaces for the automobile washing and cleaning facility.
17. That Section 9-10-5(L)1, Off-Street Parking, Minimum Parking Space And Aisle Dimensions, be varied to permit five (5) parallel parking spaces for employee parking along the southern building façade.
18. That Section 9-11-4(C), Prohibited Signs, Roof Signs, be varied to permit the roof signs, as shown in the elevations.
19. That Section 9-11-4(K), Prohibited Signs, Painted Signs, be varied to permit painted signs on the east and west building facades.
20. That Section 9-11-4(R), Prohibited Signs, Multiple Message, Digital, Dynamic and/or Video Display Signs, be varied to permit the digital displays on the ground sign located near the shared access driveway with the Willowbrook Town Center, as shown in the elevations.
21. That Section 9-11-12(B)1(a), Sign Surface Area, Number and Height Limitations, Business Districts, be varied to permit an increase in the maximum total sign surface area from three hundred thirty-five square feet (335 SF) to [??] (?? SF) for the one (1)





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- freestanding sign proposed at the northwest corner of the lot, for each wall sign proposed on the north, east and west building facades for a total of three wall signs, and for the three roof signs proposed on each of the three detached canopies, as shown in the sign package.
22. That Section 9-11-11(B)2 "Sign Location", be varied to allow building mounted sign to extend higher than one foot (1') below the top line of the face of the building and exceed a height of twenty feet (20') for the signage on the north elevation of the automobile washing and cleaning facility.
  23. That Section 9-12-2, Permitted Accessory Buildings, Structures and Uses, Awning/Canopies/Marquees, be varied to permit an increase in the permitted encroachment from 1/3 bulk district standard to allow the detached canopy located to the south of the building to be set back eight tenths feet (0.8') from the south lot line, and to allow the northernmost detached canopy located to the north of the building to be set back thirteen and five tenths feet (13.5') from the north lot line.
  24. That Section 9-12-4(C)3, Bulk Regulations, Detached Accessory Structures, be varied to permit a reduction in the minimum clearance above grade from fourteen feet (14') to twelve feet (12') for the detached pay canopy located to the south of the building and the detached canopies over the vacuum stations.
  25. That Section 9-14-2(D)2(c)4, Border Plantings and Foundation Plantings, be varied to permit a reduction in the minimum foundation landscape area width from seven feet (7') to zero feet along the north, east and west building facades.

### **LOT 2 (Fast Food Establishment and Drive-Through)**

26. That Section 9-3-3(C), Multiple Uses on a Lot, be varied to permit a reduction in the minimum lot area for two (2) or more special uses from two acres (2 AC) to seven hundred ninety-five thousandths acre (0.795 AC).
27. That Section 9-6-1(A), Minimum Lot Area; Two Or More Uses On A Lot, be varied to permit a reduction in the minimum lot area for two (2) or more special uses (fast food establishment and drive-through) from two acres (2 AC) to seven hundred ninety-five thousandths acre (0.795 AC).
28. That Section 9-6B-3(A), Minimum Lot Area, be varied to permit a reduction in the minimum lot area for the fast food establishment and drive-through from two acres (2 AC) to seven hundred ninety-five thousandths acre (0.795 AC).
29. That Section 9-6B-3(C)1, Minimum Lot Width, be varied to permit a reduction in the minimum lot width for restaurants from two hundred feet (200') to one hundred twenty-two and seven tenths feet (122.7').
30. That Section 9-6B-3(E)2, Minimum Interior Side Yard Setback, be varied to permit a reduction in the minimum interior side yard setback along the east lot line from thirty feet (30') to fifteen and five tenths feet (15.5') for the proposed fast food establishment, to eleven and four tenths feet (11.4') for the proposed attached canopies, to sixteen and one tenths feet (16.1') and to twenty-six and two tenths feet (26.2') for the menu/order boards closest to the east lot line.
31. That Section 9-6B-3(E)4, Required Setbacks, Rear Yard, be varied to permit a reduction in the minimum required rear yard setback from forty feet (40') to twenty-two and nine tenths feet (22.9') and twenty-three feet (23') for the menu/order boards located closest to the south lot line.
32. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required interior side and rear yard parking area setbacks from ten feet (10') to





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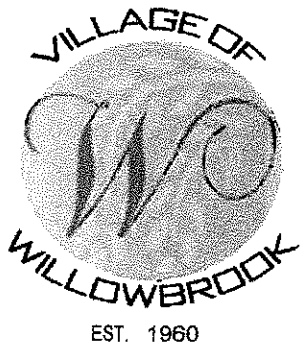
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- two and four tenths feet (2.4') along the east lot line and to zero feet along the west and south lot lines.
33. That Section 9-10-5(G)3 Off-Street Parking, Interior Parking Lot Landscaping, be varied to permit a reduction in the minimum end parking island width from nine feet (9') to five feet (5') and six and eight tenths feet (6.8') for two end parking islands.
34. That Section 9-10-5(G)3 Off-Street Parking, Interior Parking Lot Landscaping, be varied to eliminate the requirement for one (1) shade tree and nine (9) dwarf variety shrubs required within the three parking islands.
35. That Section 9-11-6(L), Signs, Exemptions, be varied to permit an increase in the permitted sign surface area for private traffic direction signs from four (4) square feet to eight and one tenths square feet (8.1 SF) for each of the three private traffic direction signs and fifteen and six tenths square feet (15.6 SF) for the private traffic direction sign located underneath the freestanding sign.
36. That Section 9-11-12(B)1, Sign Surface Area, Number and Height Limitations, Business Districts, be varied to permit an increase in the total sign surface area from one hundred thirty-eight and four tenths square feet (138.4 SF) to three hundred eighty-three and four tenths square feet (383.4 SF), to permit an increase in the maximum number of wall signs from three (3) wall signs to six (6) wall signs, to permit building mounted signage to exceed a height of twenty feet (20') for the wall signs on the north and west building elevations, and to permit a reduction in the minimum separation distance between wall signs on the north and west building facades to less than 20% of the linear dimension of the business site frontage, in accordance with the building elevations.
37. That Section 9-12-2, Permitted Accessory Buildings, Structures and Uses, Awning/Canopies/Marquees, be varied to permit an increase in the permitted encroachment from 1/3 bulk district standard to eleven and four tenths feet (11.4') from the east lot line for the two attached canopies located along the east building façade.
38. That Section 9-14-2(D)2(c)4, Border Plantings and Foundation Plantings, be varied to permit a reduction in the minimum foundation landscape area width from seven feet (7') to zero feet along the north, west and south building facades, and to less than seven feet (7') along the east building façade.

## LOT 3 (Financial Institution and Drive-Through)

39. That Section 9-3-3(C), Multiple Uses on a Lot, be varied to permit a reduction in the minimum lot area for two (2) or more special uses from two acres (2 AC) to seven hundred ninety-two thousandths acre (0.792 AC).
40. That Section 9-3-7(A)4, Specific Setbacks, Plainfield Road, be varied to permit a reduction in the minimum special setback from for one hundred feet (100') to seventy-one and 8/10ths feet (71.8') for the proposed building.
41. That Section 9-6-1(A), Minimum Lot Area; Two Or More Uses On A Lot, be varied to permit reduction in the minimum lot area for two (2) or more special uses (financial institution and drive-through) from two acres (2 AC) to seven hundred ninety-two thousandths acre (0.792 AC).
42. That Section 9-6-1(E)2, General Conditions, Building Façade Materials, be varied to allow aluminum composite panels as building façade materials, as shown on the architectural elevations.





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43. That Section 9-6B-3(A), Minimum Lot Area, be varied to permit a reduction in the minimum lot area for the financial institution and drive-through from two acres (2 AC) to seven hundred ninety-two thousandths acre (0.792 AC).
44. That Section 9-6B-3(E)2, Minimum Interior Side Yard Setback, be varied to permit a reduction in the minimum interior side yard setback along the east lot line from thirty feet (30') to seven feet (7') for the proposed building.
45. That Section 9-6B-3(E)4, Required Setbacks, Rear Yard, be varied to permit a reduction in the minimum required rear yard setback from forty feet (40') to thirty six and two tenths feet (36.2') for the attached canopy along the south side of the building.
46. That Section 9-10-4(H) Off-Street Loading, Required Berths, be varied to permit a reduction of the minimum number of required loading berths from one (1) loading berth to zero (0) loading berths.
47. That Section 9-10-5(G) Off-Street Parking, In Yards, be varied to permit a reduction in the minimum required interior side and rear yard parking area setbacks from ten feet (10') to eight and five tenths feet (8.5') along the west lot line and to zero feet along the east and south lot lines.
48. That Section 9-11-6(L), Signs, Exemptions, be varied to permit an increase in the permitted sign surface area for private traffic direction signs from four (4) square feet to four and six tenths square feet (4.6 SF) for the private traffic direction sign located to the southwest of the building.
49. That Section 9-11-5(E)2, Illuminated Window Signs, be varied to permit an increase in the maximum sign surface area for an illuminated window sign for the Chase ATM located in the vestibule of the west building facade from four square feet (4 SF) to eight and thirty-four hundredths square feet (8.34 SF).
50. That Section 9-11-6(M), Exemptions, ATM Signs, be varied to permit an increase in the maximum sign surface area from four square feet (4 SF) to eight and thirty-four hundredths square feet (8.34 SF).
51. That Section 9-11-12(B)1(a), Sign Surface Area, Number and Height Limitations, Business Districts, be varied to permit an increase in the maximum total sign surface area from one hundred ninety-seven and eighty-five hundredths square feet (197.85 SF) to three hundred forty-four square feet (344 SF) for the one (1) freestanding sign proposed at the northwest corner of the lot and for each wall sign proposed on the north, east and west building facades for a total of three wall signs, as shown in the sign package.
52. That Section 9-11-12(B), Sign Surface Area, Number and Height Limitations, Business Districts, be varied to permit the signs for the financial institution in accordance with the building elevations.
53. That Section 9-12-2, Permitted Accessory Buildings, Structures and Uses, Awning/Canopies/Marquees, be varied to permit an increase in the permitted encroachment from 1/3 bulk district standard to allow the attached canopy located to the south of the building to be set back eleven and five tenths feet (11.5') from the east lot line.
54. That Section 9-14-2(D)2(c)4, Border Plantings and Foundation Plantings, be varied to permit a reduction in the minimum foundation landscape area width from seven feet (7') to zero feet along the north, east, west and south building facades.

## **PUD Standards**

Pursuant to Section 9-13-6 of the Village Code, the following variations from the provisions of the Planned Unit Development Regulations include but are not limited to:





EST. 1960

# Willowbrook

835 Midway Drive  
Willowbrook, IL 60527-5549

Phone: (630) 323-8215 Fax: (630) 323-0787 [www.willowbrookil.org](http://www.willowbrookil.org)

## Mayor

Frank A. Trilla

## Village Clerk

Deborah Hahn

## Village Trustees

Sue Berglund

Umberto Davi

Michael Mistele

Gayle Neal

Paul Oggerino

Gregory Ruffolo

## Village Administrator

Brian Pabst

## Chief of Police

Robert Schaller

## Director of Finance

Carrie Dittman

1. That Section 9-13-6(B) PUD Standards, Size and Ownership, be varied to waive the requirement that the subject realty be under single ownership and/or unified control.
2. That Section 9-13-6(F) PUD Standards, Yards, be varied to waive the requirement that the required yards or setbacks along the periphery of the planned unit development be at least equal in width or depth to that of the applicable required yard within the adjacent zoning district.
3. That Section 9-13-6(L) PUD Standards, Other Standards, be varied to waive the requirement that the planned unit development must comply with the minimum standards set forth in subsections (A), (B), (C), (D), (H), (I) and (K) of this section.

## Subdivision Ordinance

4. That Section 10-4-3(A)2, Lots, Sizes and Shapes, be varied to waive the requirement that the lot areas and lot widths conform to at least the minimum requirements of the zoning ordinance for the district in which the subdivision is proposed for Lots 2 and 3.
5. That Section 10-4-3(A)3, Lots, Sizes and Shapes, be varied to waive the requirement that building setback lines conform to at least the minimum requirements of the zoning ordinance for the district.
6. That Section 10-4-3(B), Lots, Arrangement, be varied to waive the requirement that Lot 1 front on a public street.

If you have any questions, you may contact me at [planner@willowbrook.il.us](mailto:planner@willowbrook.il.us) or 630.920.2233.

Sincerely,

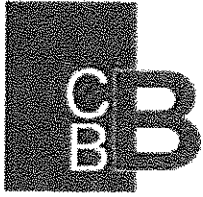
**Ann Choi**  
Village Planner

Cc: Brian Pabst  
Michael S. Mertens  
Dan Lynch  
Roy Giuntoli



Proud Member of the  
Illinois Route 66 Scenic Byway





**CHRISTOPHER B. BURKE ENGINEERING, LTD.**

9575 W Higgins Road, Suite 600 Rosemont, Illinois 60018-4920 Tel (847) 823-0500 Fax (847) 823-0520

January 27, 2021

Village of Willowbrook  
835 Midway Drive  
Willowbrook, IL 60527

Attention: Ann Choi

Subject: 735 Plainfield Road – Final Engineering and PUD  
2nd Submittal  
(CBBEL Project No. 900144.H160)

Dear Ann:

As requested by email on January 21, 2021, we have reviewed the PUD Plans and supporting documents. Based on our discussion, this is intended to be the Preliminary PUD submittal and this review is for compliance with Section 9-13-5.B of the Village Code.

**Preliminary PUD and Subdivision Plat**

We have discussed with the design engineer that the Preliminary PUD and Preliminary Plat of Subdivision will be combined into a two page document and that, combined with the Preliminary Engineering Plans will meet the plan content requirements for both the Preliminary PUD and Preliminary Plat of Subdivision. The following comments will be incorporated into the document:

1. On the Preliminary Plat of Subdivision, all of the certificates required on the Final Plat will be removed.
2. The Preliminary Plat will provide clarification that Public Utility and Drainage Easements will be provided as required. It remains to be determined if that will be specific easements for each utility, or a blanket easement covering all of the property except for building envelopes.
3. Section 9-13-5.B.4 provides that a statement of ownership be included listing the existing ownership and certification that there are no delinquent taxes on the property. We presume that this has been provided separately or will be added to the Preliminary PUD
4. Section 9-13-5.B.7 provides for covenants providing various information be submitted. We presume that this will be reviewed by others.
5. Section 9-13-5.B.12 provides for preliminary landscape plans. We will defer review of these drawings to the Village Planner.
6. Section 9-13-5.B.13 provides for preliminary engineering information to be shown. These comments are provided in the next section of this letter.
7. Sections 9-13-5.B.14 – 17 provides for various studies to be provided. We presume these are being reviewed by others.



### **Preliminary Engineering Plans**

#### Cover Sheet – C0.0

1. Under Utility and Governing Agencies, change the Sanitary Sewer Service to Du Page County Public Works.

#### Site Plan – C1.0

1. No comments.

#### Grading Plan – C2.0

1. No comments.

#### Utility Plan – C4.0

2. The alignment for the proposed watermain has been discussed with the design engineer and will be revised. The connection to the existing main in the town center will be made at the driveway connection between the two sites, rather than south near the proposed car wash building. Also, the proposed watermain will be an 8" main. This will result in less new public watermain and is expected to be a lower cost for the applicant. A 20 foot wide utility easement would be required over this main.
3. Each building service shall have an 8" x 6" Tee with valve. This will allow the public main to be constructed, and then each building service can be installed at the time of building construction without interrupting the public main.
4. The last segment of storm sewer west of the car wash, where the south end of the site connects to the off-site sewer, is shown to be 18". We presume this will be revised down to 12" on the final plans.
5. We presume the intent is for the sanitary sewer serving the bank and restaurant to be a public sewer. This will be owned by Du Page County Public Works and will be reviewed by that agency. We presume they will require a 20 foot wide easement over the main.

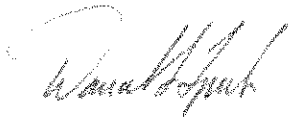
### **General Comments**

1. A permit will be required from Du Page County Division of Transportation for work within the Plainfield Road right of way.
2. A permit will be required from Du Page County Public Works for the proposed sanitary sewer and connections.
3. The Construction Schedule and Engineer's Opinion of Cost will be reviewed with the Final PUD Plat.
4. The fire truck turning templates indicate that presumed truck can maneuver the site. We presume that prior to final approval the plans will be provided to the fire district for comment on both the geometrics, but also the location of hydrants and fire department connections on each building.



If you have any questions, please do not hesitate to contact me.

Sincerely,

A handwritten signature in dark ink, appearing to read "Daniel Lynch". The signature is fluid and cursive, with a large initial "D" and "L".

Daniel L. Lynch, PE, CFM  
Head, Municipal Engineering Department

Cc     Brian Pabst  
         Mike Mertens  
         Roy Guintoli  
         AJ Passero



## Project Traffic Review #2



CONSULTING ENGINEERS

625 Forest Edge Drive, Vernon Hills, IL 60061

Tel 847.478.9700 • Fax 847.478.9701

[www.gha-engineers.com](http://www.gha-engineers.com)

To: Ms. Ann Choi  
Planning Consultant, Village of Willowbrook

From: Lynn M. Means, P.E., PTOE  
Senior Transportation Engineer

Date: January 26, 2021

Subject: Proposed Retail Development  
735 Plainfield Road  
Willowbrook, Illinois

Per your request, Gewalt Hamilton Associates, Inc. (GHA) has reviewed the following materials for the above captioned project pertaining to traffic, parking and on-site circulation:

1. KLOA response to GHA's traffic comments dated January 20, 2021.
2. KLOA response to DuDOT's comments dated January 20, 2021.
3. Traffic Impact Study (TIS) prepared by KLOA dated January 20, 2021.
4. Site Plan, Sheet Number C1.0, prepared by Kimley Horn dated January 21, 2021.

Conclusions of this effort and **recommendations** are presented below:

1. GHA has found the documentation and/or responses provided, in general, to adequately address the comments made and concerns raised in our previous Project Traffic Review dated January 15, 2021.
2. The applicant should provide documentation of the DuDOT approval of TIS and Plainfield Road access, upon receipt.
3. The response letter adequately addressed the comment pertaining to the trip rates assumed in the traffic study for the proposed carwash component of the development during the weekday morning peak hour; however, the concern raised pertaining to the Saturday morning timeframe was not addressed. Please provide response and/or supporting documentation for use of the lower trip generation estimates based on the tunnel independent variable versus the square-footage of the use.
4. The updated study appropriately provides a queuing exhibit that illustrates the stacking capability of each of the proposed uses, as well as additional supporting documentation. However, the supporting carwash queue data provided is insufficient to verify the projected queue lengths. It references a study that was performed in Minnesota in 2012, where 5 of the 6 carwash locations surveyed were at gasoline station facilities, as well as the referenced study did not include average daily traffic for a measure of comparison. While we understand it may be difficult to obtain peak observations at similar facilities, as carwash operations tend to peak in the wintertime on a sunny day following a snowfall, please provide supporting operational characteristics (e.g., transaction information), as available, pertaining to peak operations and/or supporting operations / documentation from similar Chicagoland area existing sites during average, typical conditions.

In addition, as we understand, the quick service restaurant use has been identified as Guzman Y Gomez. There is an existing drive-through location in Naperville. Again, please provide either data from observations and/or transaction data to support the capacity provided is adequate to support the peak queue.

\* \* \* \* \*

We hope you find these comments helpful in your assessment of the proposed development. *GHA does not believe the comments noted above will materially impact the findings / **recommendations** of the TIS.* Should you have any questions please do not hesitate to contact GHA at 847-478-9700.

5670.900\_735 Plainfield\_Traffic Review 2\_01.26.21



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**TRI-STATE**  
**FIRE PROTECTION DISTRICT**

419 PLAINFIELD ROAD • DARIEN, ILLINOIS 60561 • (630) 323-6445

January 28, 2021

Kimley Horn and Associates Inc.  
Attn: Eric Tracy  
4201 Winfield Road Suite 600  
Warrenville, IL 60555  
630-487-5560

[planner@willowbrook.il.us](mailto:planner@willowbrook.il.us)  
[eric.tracy@kimley-horn.com](mailto:eric.tracy@kimley-horn.com)

RE: Site Plan Review  
735 Plainfield Road,  
Willowbrook, IL 60527

To whom it may concern,

We have received a copy of the site plans and turn exhibits for the above listed project. The following comments are from this office:

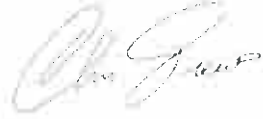
1. Drive width of 20' clear aisles. This includes the drive adjacent to the car wash. The curb radius and drive space is less than 20' and our fire apparatus will have a difficult time accessing the building.
2. Fire truck dimensions are not an inner city bus and shall be for a vehicle that is 47.68' long with a clear height of 13'6" and a width of 10'4".
3. The trash enclosures located in that area make it impossible for a fire vehicle to access the drive that is adjacent to the building. Prove that the turn radius is available.
4. The canopies for the vacuum stations may not be an issue if they don't extend into the drive area. This needs to maintain a 20' clear path in the drive with a 13'6" clear height.
5. The exit from the car wash is less than 20' and the fire vehicles will not be able to exit without hitting the curbs and causing damage.
6. All the other canopies will not be an issue. There needs to be a verification with the car wash.

Necessary inspections are to be performed along with any outstanding fees paid before occupancy is granted.



*The Bureau of Fire Prevention has been asked to review the plans, specifications or other documents submitted to see if compliance has been made with the Fire Prevention Codes and Ordinances of the Tri-State Fire Protection District. Errors or omissions by representatives of the Bureau of Fire Prevention do not constitute permission to cancel, set aside or waive any provision of any applicable Code or Ordinance of the Tri-State Fire Prevention District. Approvals by the Bureau of Fire Prevention will be in writing only.*

Sincerely,



Chris Drews  
Plan Review Specialist  
Bureau of Fire Prevention  
[cdrews@tristatefd.com](mailto:cdrews@tristatefd.com)  
630-654-6284

